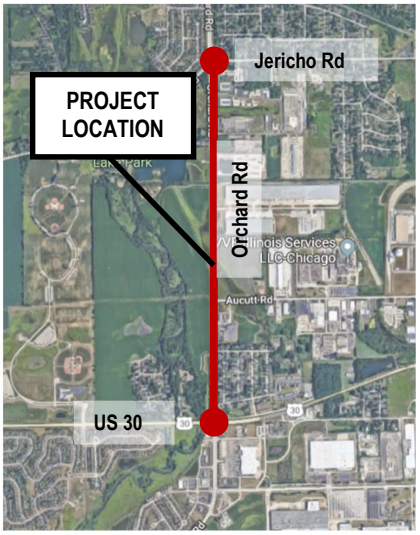
Illinois Department of Transportation

PROJECT REPORT

KDOT 2018 HSIP IMPROVEMENT PROJECT

Fabyan Parkway at IL 31 and
Orchard Road from North of US 30 to
South of Jericho Road



Kane County, Illinois

Section No. 18-00493-00-SP

Region 1 – IDOT – Bureau of Local Roads

MAY 2020

TABLE OF CONTENTS

Illinois Department of Transportation

Categorical Exclusion Group I

County:	Kane
Local Public Agency:	Kane County Division of Transportation
Section Number:	18-00493-00-SP
Route:	FAP 363, FAP 336

Project Number: I1JF(707) Construction

Project Length: 2.0 miles

Type of Funds: Federal (HSIP), Local

Project Termini: Fabyan Parkway at IL 31; and Orchard Rd from North of US 30 to South of Jericho Rd

Local Agency Jurisdiction: Kane County Division of Transportation (KDOT)

Project Description: The purpose of this proposed Kane County 2018 Highway Safety Improvement Program (HSIP) project is to provide safety countermeasures at the intersection of Fabyan Parkway at IL 31 (Batavia Avenue) in Batavia, IL and along the Orchard Road corridor from north of US 30 (Baseline Road) to south of Jericho Road, in Montgomery, IL and Unincorporated Kane County.

Proposed improvements as part of this project include replacement of signal heads and other traffic signal modifications, installation of wet reflective pavement markings, installation of advanced warning flashers and radar detection speed signs, a dilemma zone detection system, and curb ramp (Americans with Disabilities Act / ADA) improvements.

Fabyan Parkway at IL Route 31 (Batavia Avenue)

Proposed improvements at the Fabyan Parkway and IL Route 31 intersection are confined within the existing right-of-way (ROW). Improvements include the installation of retroreflective backplates on the existing signal heads, installation of wet reflective pavement markings, installation of advanced warning flashers, a dilemma zone detection system and curb ramp, sidewalk, and bus boarding area improvements. There are no geometric revisions proposed to the intersection, and the proposed pavement markings will match the existing lane widths and configurations. There are no geometric revisions proposed to the existing bridge over the Fox River, and the proposed pavement markings will end in advance of the bridge at approximately Station 77+51 (Bridge Omission Station 77+51 to Station 85+51).

A dilemma zone detection system is proposed along Fabyan Parkway for the eastbound and westbound movements. The system will include advance detection and advance warning signs with wigwag flashing beacons. The dilemma zone detection system will be interconnected with the traffic signals at the intersection and programmed in the signal control cabinet. Advance detection involves sensors that detect and track vehicles approaching an intersection. The sensors are activated prior to the phase change from green to yellow, and they are programmed to detect the presence of vehicles within the dilemma zone. If a vehicle is detected, the system will extend the signal green time and prevent the onset of a phase change, until the vehicle has passed the dilemma zone. The system will be programmed to extend the green time up to a maximum green interval as long as the sensors continually detect vehicles. In some cases, particularly when there is a high volume of traffic, the system would “max-out” and the green signal would transition to yellow regardless of the presence of vehicles in the dilemma zone. The dilemma zone detection system at this intersection will be programmed in a manner as to not impact the IL Route 31 signal timing. Only the green time for Fabyan Parkway through movement will be affected. This green time will be reduced, and the additional time allocated while the dilemma zone detection system is running will extend to the current maximum green time if the presence of vehicles is continuously detected.

The advance warning signs will consist of a BE PREPARED TO STOP (W3-4) and WHEN FLASHING (W16-13P) sign panels with two yellow, alternately flashing beacons, one on either side. The wigwag flashing beacons will be programmed to become illuminated a set period prior to the termination of the green signal. This period is called the "Leading Flash." The advance warning flashers shall continue to flash through the Leading Flash, yellow clearance and the red time. This warns approaching drivers of the coming change. The advance warning flashers will also flash if the entire signal goes into flash operation. The advance warning sign for the eastbound Fabyan Parkway movement will be located approximately 500 ft. to the west of the stop bar and mounted to a new signpost to the west of an existing cantilever signpost on the south side of the road. One advance warning sign for the westbound movement will be located approximately 750 feet east of the stop bar on the north side of the road, mounted to an existing light pole at the Fabyan Road bridge over the Fox River. No structural work to the bridge is proposed.

Wet reflective pavement markings provide increased visibility to lane line delineations. Wet reflective pavement markings are proposed to replace the existing pavement markings on all approaches without revision to existing geometry. Existing lane widths and configuration will be maintained on all legs of the intersection. The proposed pavement marking meets the required storage lengths and taper lengths except on the east leg where the taper rate was reduced due to space limitations confined within the existing roadway geometry.

The existing traffic signal heads are placed appropriately on the mast arms. Signal head adjustments are not recommended at this intersection. On westbound Fabyan Parkway and northbound and southbound IL Route 31, the traffic signals are positioned in the center of the lanes. For eastbound Fabyan Parkway, the through movement traffic signals are located on the outside edge of the lanes; however, due to the kink in the road and the driver perception at the approach to the intersection, motorists may become confused if the signals are moved further to the left (north) of the direction they are facing. Therefore, it is not recommended to relocate the signal heads to the center of the lane for the eastbound Fabyan Parkway signals. However, retroreflective backplates are proposed on all mast arm mounted signals at this intersection to increase the visibility of the signal, particularly at night.

Pedestrian improvements are proposed as part of this project as well. Curb ramps in the northwest, southwest and southeast corners of the intersection have been redesigned to meet ADA requirements. A new crosswalk is proposed along the west side of IL Route 31 and a new curb ramp is proposed in the southwest quadrant of the intersection connecting the sidewalk from the northwest leg to the intersection to the sidewalk and multi-use path on the southwest leg of the intersection.

No impacts to the Compana Factory property, the Forest Preserve, or the Fox River Trail are anticipated as part of the improvement.

Orchard Road from Jericho Road to US Route 30 (Baseline Road)

Proposed improvements to the Orchard Road corridor are confined within the existing right-of-way. Improvements include the installation of wet reflective pavement markings, new ADA ramps, traffic signal improvements, and the installation of radar speed limit signs.

Existing pavement markings will be replaced with more visible wet reflective thermoplastic pavement markings throughout the whole corridor. Existing lane widths and configuration will be maintained. Wet reflective pavement markings will provide increased visibility to lane line delineations. Permanent radar speed signs will be installed on new signposts throughout the study corridor.

Signals for the southbound Orchard Road at Aucutt Road left turn movement and the southbound and northbound Orchard Road at Rochester Drive left turn movement will be replaced with a 4-section signal head. The existing 5-section signal head signals along Orchard Road at Aucutt Road and the existing 3-section head left turn signals along Orchard Road at Rochester Drive will be replaced with a 4-section signal head consisting of a solid red arrow, solid yellow arrow, flashing yellow arrow, and solid green arrow. The flashing yellow arrow indicates that motorists

may make a left-hand turn after yielding to oncoming traffic. Additionally, to meet current traffic signal design guidelines, an additional signal head is needed on the near (north) side of the Orchard Road at Aucutt Road intersection for southbound traffic. A 3-section signal head is proposed on the northwest mast arm to meet this requirement.

As part of a Bike and Pedestrian Plan developed by the Village of Montgomery (Refer to Attachment A-5: Coordination/Documentation), a sidewalk is proposed along the south side of Aucutt Road, and a multi-use path is proposed between Aucutt Road and US Route 30. These future sidewalk and multi-use paths will intersect at the southeast corner of Aucutt Road and Orchard Road. The bike plan indicates Aucutt Road continuing west beyond Orchard Road and terminating at a northern extension of Dickson Road. It also indicates the Village of Montgomery intends to construct a future pedestrian bridge at the Orchard Road and Aucutt Road intersection. As part of this project, ADA ramps, a crosswalk and pedestrian signals with push buttons will be installed across the south side of the intersection to accommodate the Village's future Bike and Pedestrian Plan. The proposed pedestrian accommodations will connect the existing multi-use path along the west side of Orchard Road to the future proposed paths.

Additional information can be found in Attachment A-5: Coordination/Documentation, including the KDOT 2018 HSIP Application and Approval Memos.

Note: Elements of this proposed scope of work that are not part of the HSIP application are identified in the plans and reflected as non-HSIP improvements in the estimate of cost. Federal Participation for this project applies to the HSIP approved improvements only.

Total Cost of Project: $415,800 (Refer to Phase I Estimate)

Federal Participation: $198,900 (Refer to Phase I Estimate)

Categorical Exclusion Statement:

This project will not have any significant impacts on the environment, or involve any unusual circumstances, therefore, it is a Categorical Exclusion I.

Local Agency — 5/31/20 Date

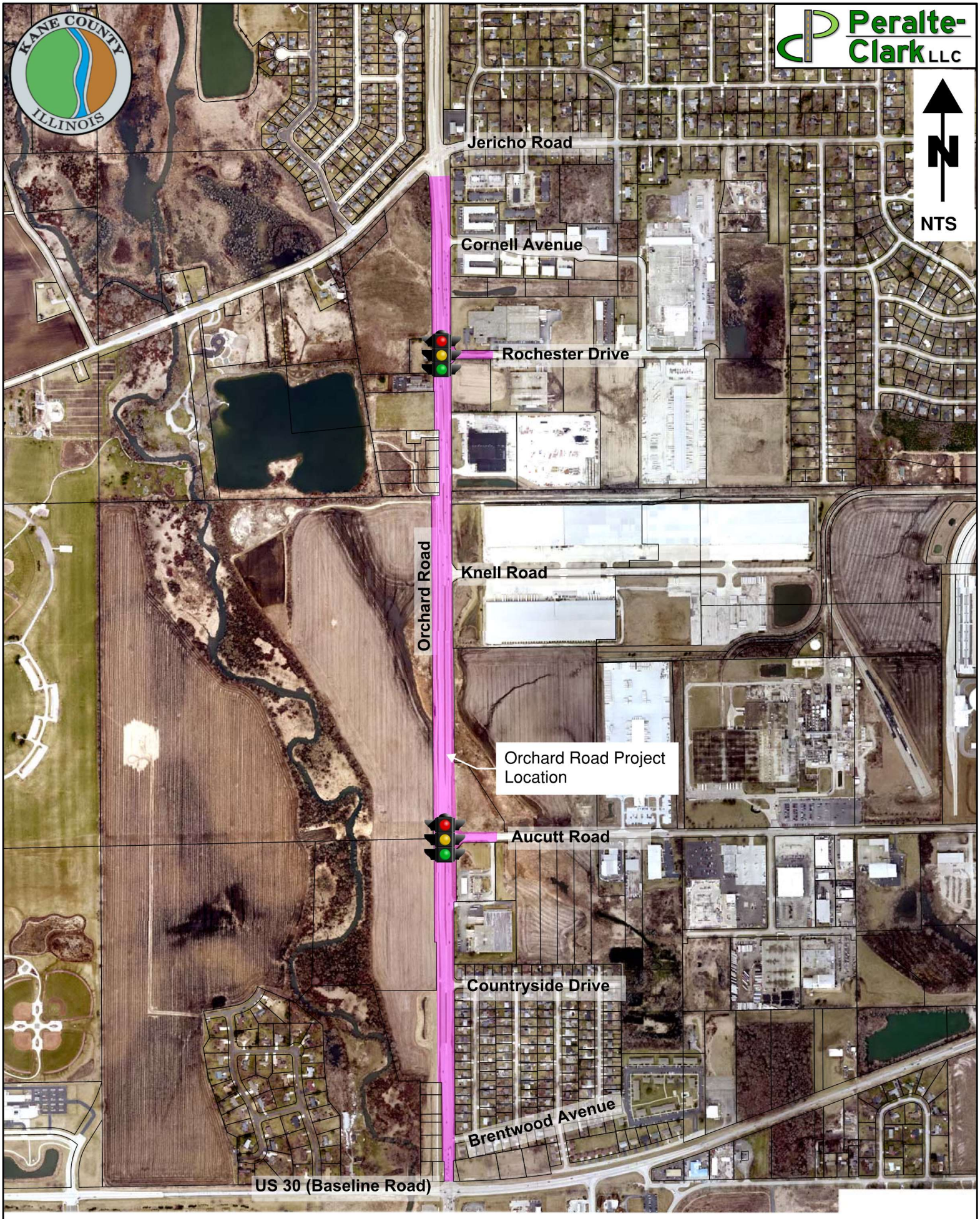
Regional Engineer — 6/23/2020 Date

Required Attachments (when applicable):

- ☒ Location Map
- ☒ Existing and Proposed Typical Sections
- ☐ Coordination with Local Bridge Unit when structures are impacted
- ☐ List of sign quantities for Rural Sign Program projects
- ☒ Layout of sidewalks for sidewalk projects
- ☒ Coordination meeting minutes if applicable
- ☐ Approved BLR 22120 form for approved design variances
- ☒ Other: Environmental Clearances (Cultural, Biological, Special Waste)
  Crash Analysis
  HSIP Application Documents and Approval Memo
  (Refer to Table of Contents)

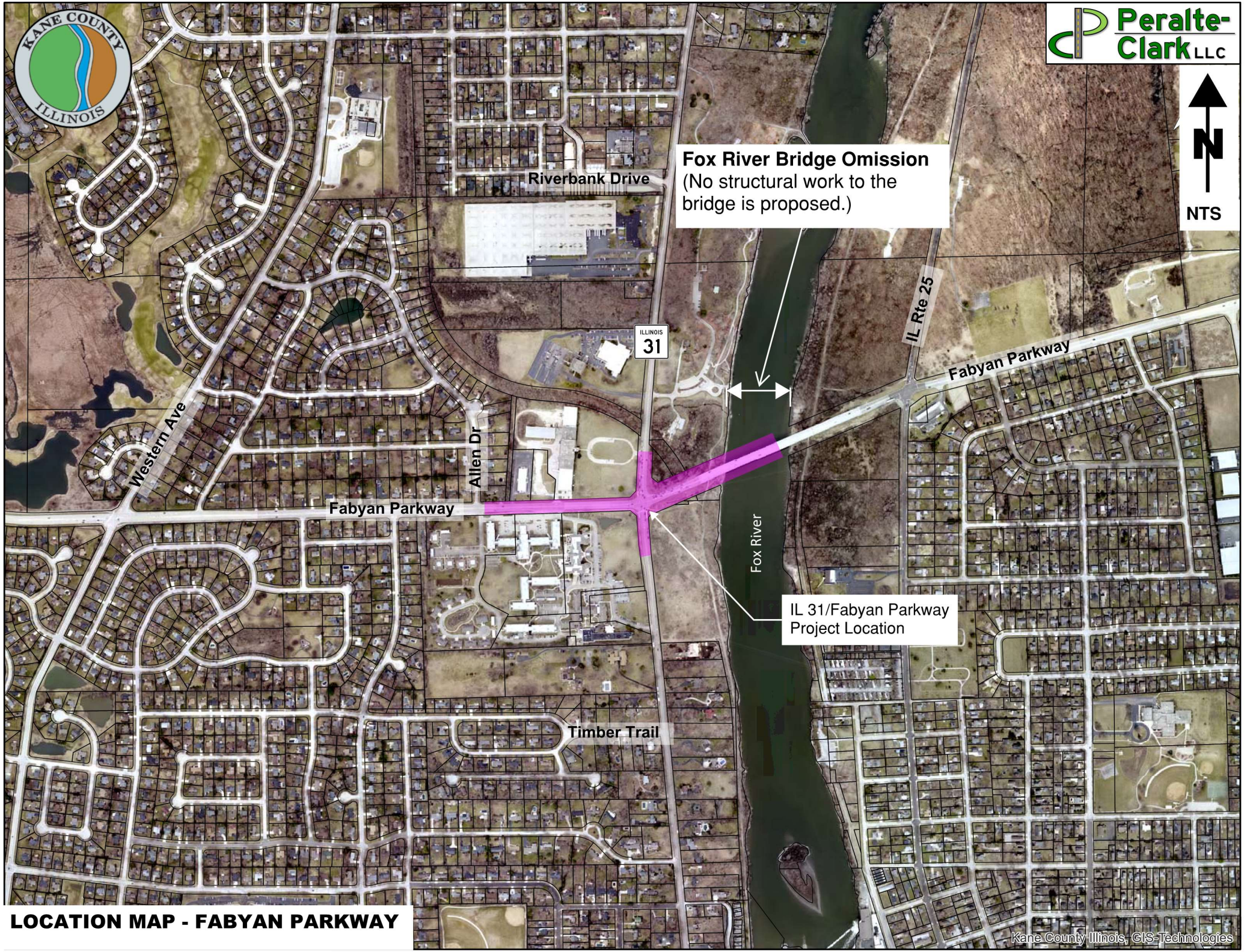
Attachment 1:
Location Maps
Functional Classification Map

Peralte-Clark LLC

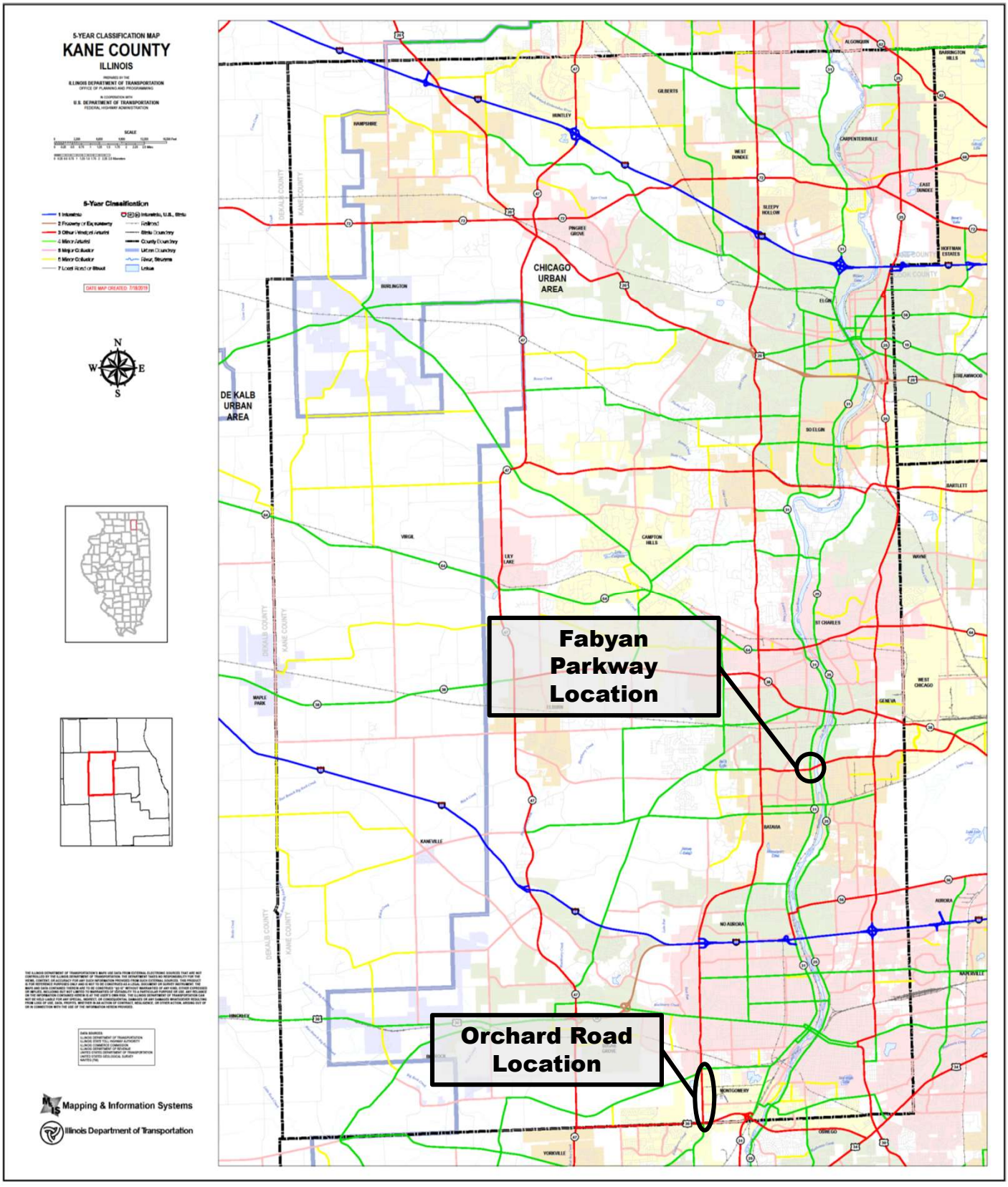


LOCATION MAP - ORCHARD ROAD

Kane County Illinois, GIS-Technologies



LOCATION MAP - FABYAN PARKWAY



Orchard Road = Other Principal Arterial

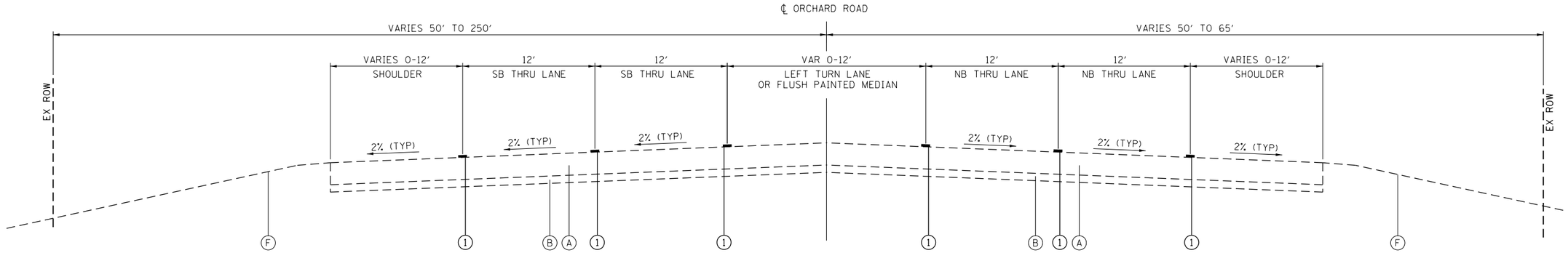
Fabyan Parkway = Other Principal Arterial
IL 31 = Minor Arterial

KDOT 2018 HSIP IMPROVEMENT PROJECT
FUNCTIONAL CLASSIFICATION MAP

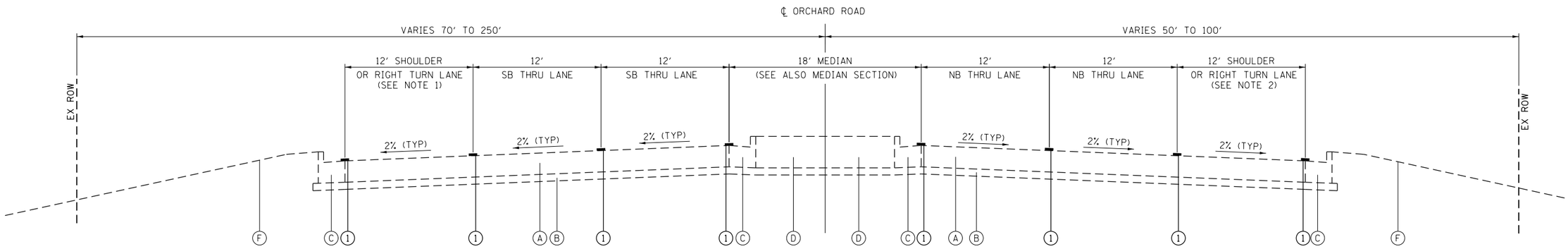
A-1.2

Attachment 2:
Typical Cross Sections

Peralte-Clark LLC



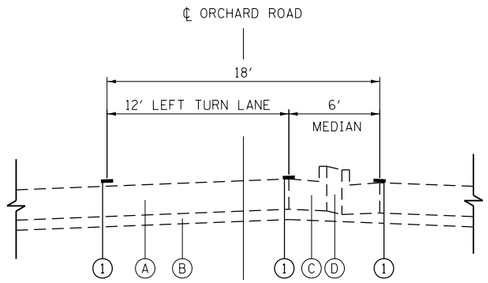
TYPICAL SECTION – ORCHARD ROAD
STA 100+52 TO STA 111+22



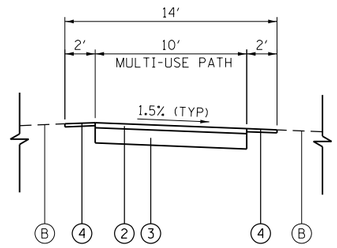
NOTE 1:
REFER TO PLAN SHEETS FOR
RIGHT TURN LANE LOCATIONS
STA 111+22 TO STA 116+45

TYPICAL SECTION – ORCHARD ROAD
STA 111+22 TO STA 178+21

NOTE 2:
REFER TO PLAN SHEETS FOR
RIGHT TURN LANE LOCATIONS
STA 116+70 TO STA 126+13
STA 142+91 TO STA 146+31
STA 150+77 TO STA 154+42
STA 155+46 TO STA 162+96
STA 167+96 TO STA 171+86
STA 174+54 TO STA 178+36



MEDIAN SECTION – ORCHARD ROAD
LEFT TURN LANE
SB STA 116+13 TO STA 120+16
SB STA 127+68 TO STA 132+54
SB STA 147+59 TO STA 151+72
SB STA 155+46 TO STA 159+86
SB STA 164+56 TO STA 168+22
NB STA 160+38 TO STA 162+97
NB STA 173+67 TO STA 178+40



MULTI–USE PATH TYPICAL SECTION
STA 126+10 TO STA 126+48

NOTE 3:
LOCATION IS APPROXIMATE.
REFER TO PROPOSED IMPROVEMENT PLANS AND
ADA RAMP DETAILS FOR ADDITIONAL INFORMATION.

LEGEND – EXISTING TYPICAL SECTIONS

- (A) EXISTING HMA OR PCC PAVEMENT
- (B) EXISTING GRANULAR SUB-BASE
- (C) EXISTING COMBINATION CURB AND GUTTER
- (D) EXISTING CONCRETE BARRIER MEDIAN
- (E) EXISTING SIDEWALK
- (F) EXISTING GRADING

LEGEND – PROPOSED TYPICAL SECTIONS

- (1) PROPOSED PAVEMENT MARKING (REFER TO PLAN SHEETS FOR TYPE AND LOCATION)
- (2) HMA SURFACE COURSE, MIX "C", N50, 2"
- (3) SUBBASE GRANULAR MATERIAL, TYPE B, 8"
- (4) TOPSOIL FURNISH AND PLACE, 4" AND SODDING, SALT TOLERANT

60% REVIEW SET

Peralte-Clark LLC

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	DRAWN -	REVISED -
PLOT SCALE =	CHECKED -	REVISED -
PLOT DATE =	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

KDOT 2018 HSIP: ORCHARD ROAD, US 30 TO JERICHO ROAD
TYPICAL SECTIONS

SCALE: N.T.S. | SHEET 1 OF 2 SHEETS | STA. | TO STA.

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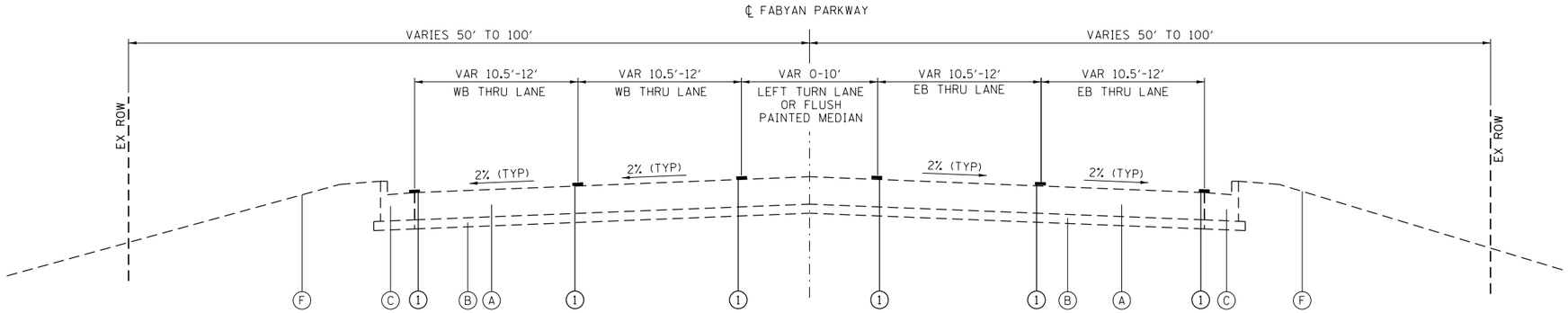
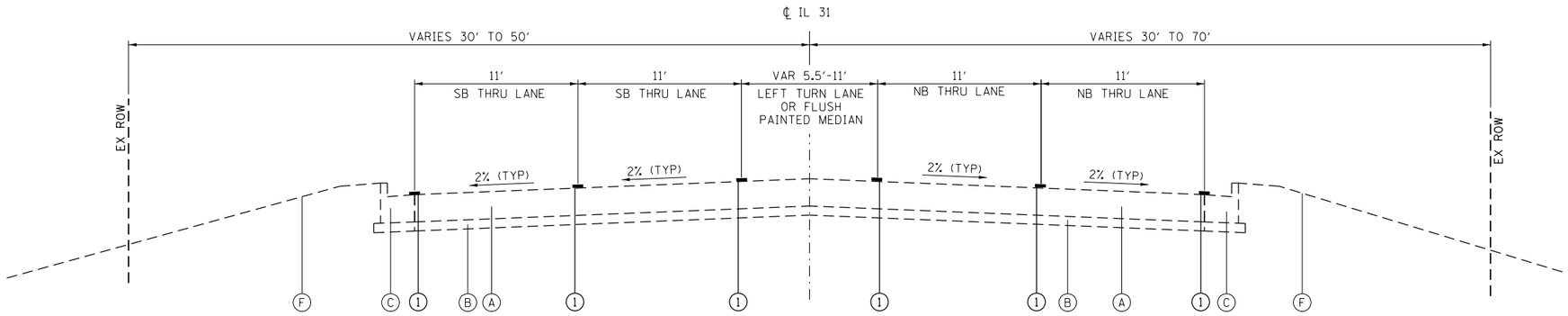


TYPICAL SECTION – FABYAN PARKWAY

STA 60+98 TO STA 81+59
EB LEFT TURN LANE: STA 66+97 TO STA 72+22
WB LEFT TURN LANE: STA 73+75 TO STA 76+75



TYPICAL SECTION – IL 31

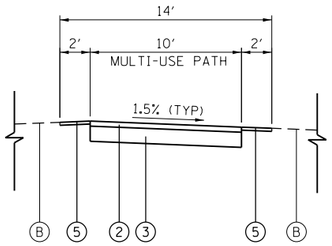
STA 23+83 TO STA 34+76
NB LEFT TURN LANE: STA 26+18 TO STA 29+38
SB LEFT TURN LANE: STA 30+85 TO STA 34+06

LEGEND – EXISTING TYPICAL SECTIONS

- (A) EXISTING HMA OR PCC PAVEMENT
- (B) EXISTING GRANULAR SUB-BASE
- (C) EXISTING COMBINATION CURB AND GUTTER
- (D) EXISTING CONCRETE BARRIER MEDIAN
- (E) EXISTING SIDEWALK
- (F) EXISTING GRADING

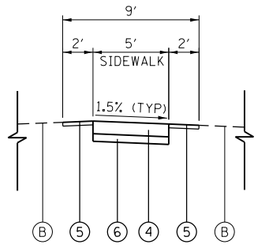
LEGEND – PROPOSED TYPICAL SECTIONS

- (1) PROPOSED PAVEMENT MARKING (REFER TO PLAN SHEETS FOR TYPE AND LOCATION)
- (2) HMA SURFACE COURSE, MIX "C", N50, 2"
- (3) SUBBASE GRANULAR MATERIAL, TYPE B, 8"
- (4) PCC SIDEWALK, 5"
- (5) TOPSOIL FURNISH AND PLACE, 4" AND SODDING, SALT TOLERANT
- (6) SUBBASE GRANULAR MATERIAL, TYPE B, 4"



MULTI–USE PATH TYPICAL SECTION

NW, SW AND SE QUADRANTS OF FABYAN PKWY @ IL 31 INTERSECTION



SIDEWALK TYPICAL SECTION

NW, SW AND SE QUADRANTS OF FABYAN PKWY @ IL 31 INTERSECTION

NOTE:
REFER TO PROPOSED IMPROVEMENT PLANS AND ADA RAMP DETAILS FOR LOCATIONS OF MULTI-USE PATH AND PCC SIDEWALK.

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Peralte-Clark LLC

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

KDOT 2018 HSIP: FABYAN PARKWAY AT IL ROUTE 31
TYPICAL SECTIONS

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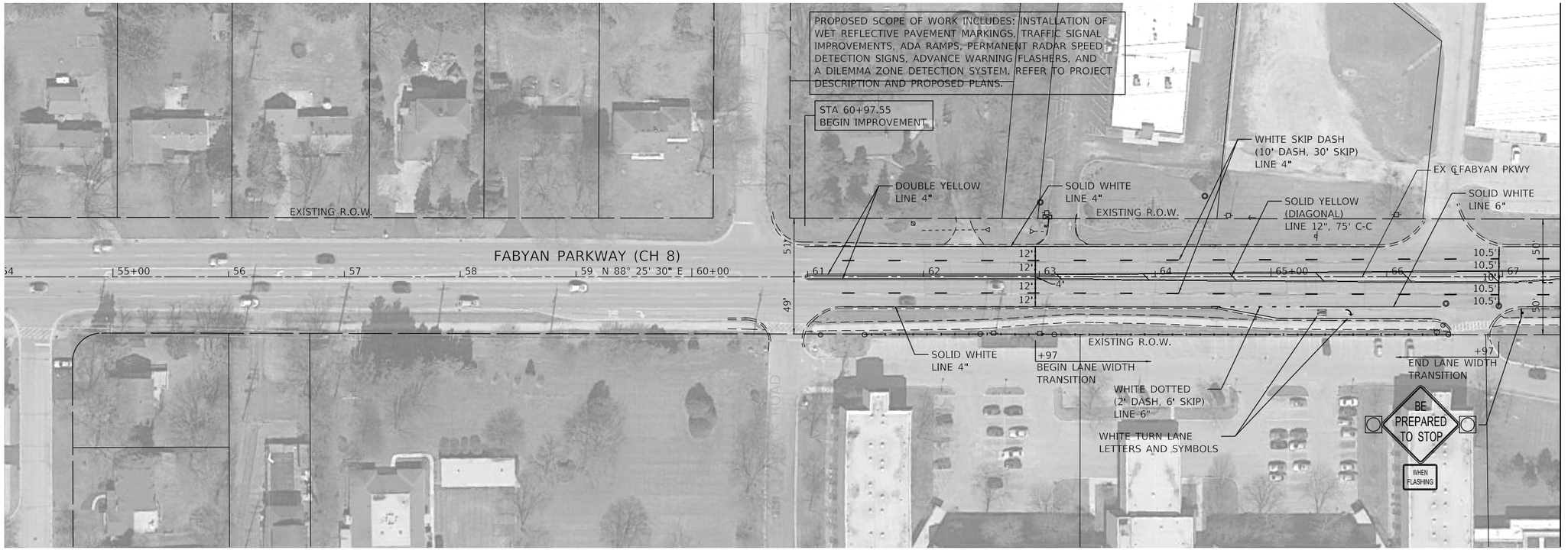
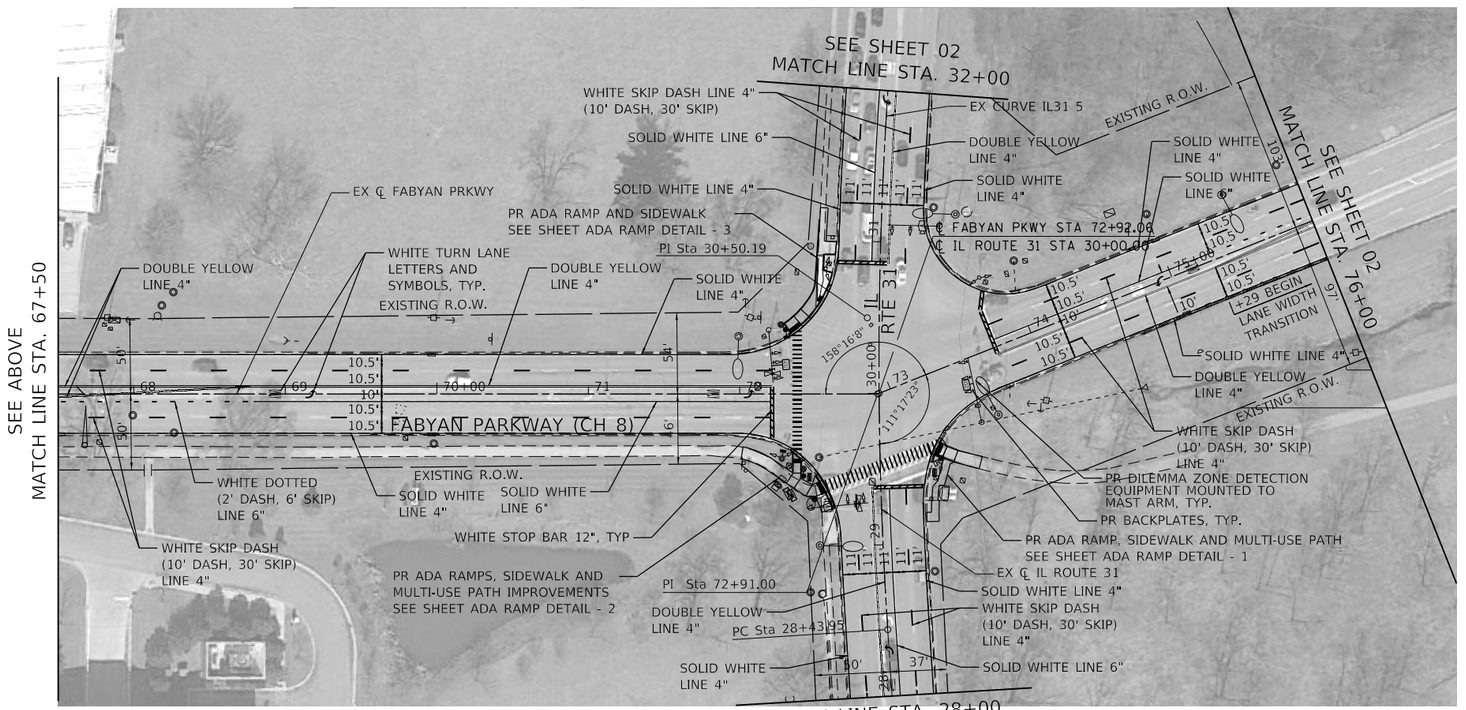
	ILLINOIS	FED. AID PROJECT		

Attachment 3:
Proposed Improvement Plans

Peralte-Clark LLC





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L = 411.69'
E = 7.83'
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T.R. = ____
S.E. RUN = ____
P.C. STA. = 28+43.95
P.T. STA. = 32+55.64

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Peralte-Clark LLC

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN VIEW STA 54+00 TO 76+00
FABYAN PARKWAY

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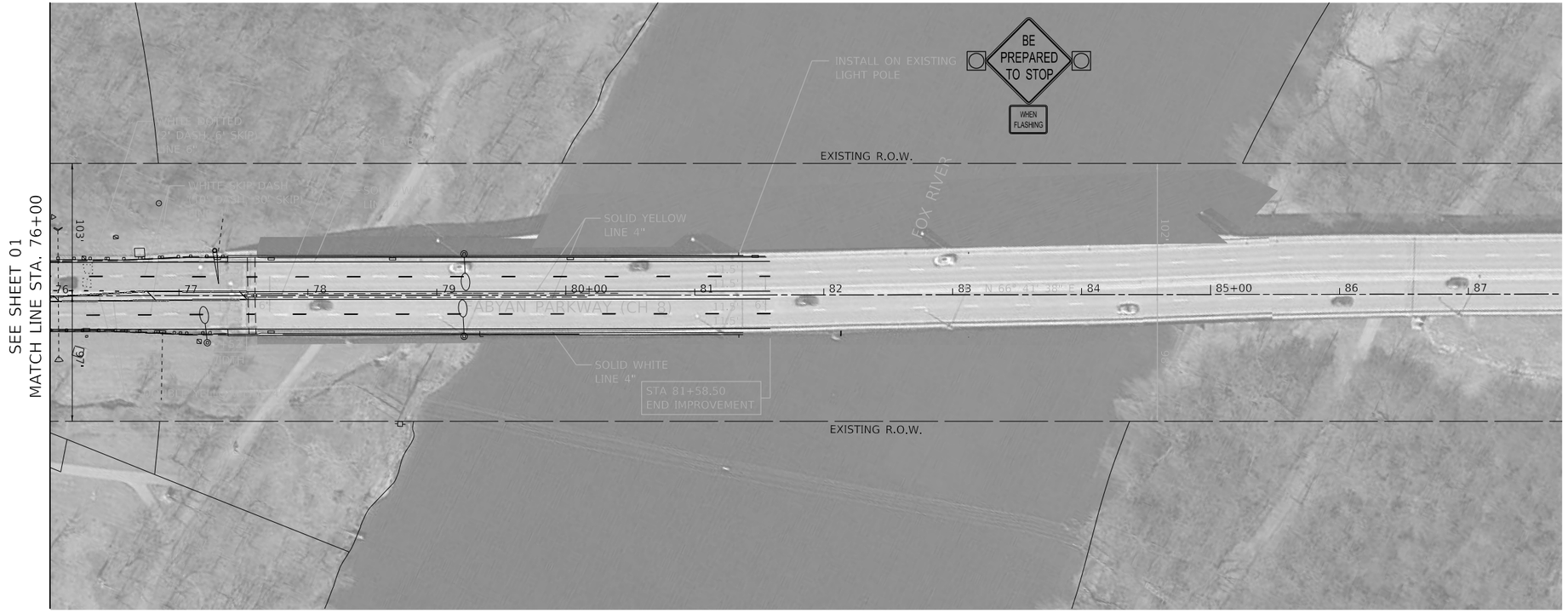

MATCH LINE STA. 28+00
SEE SHEET 01

SEE SHEET 01
MATCH LINE STA. 32+00

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P.T. STA. = 39+93.26

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN VIEW
FABYAN PARKWAY AND BATAVIA AVENUE

SCALE: 1" = 50' | SHEET 2 OF 10 SHEETS | STA. 76+00 TO STA. 86+00

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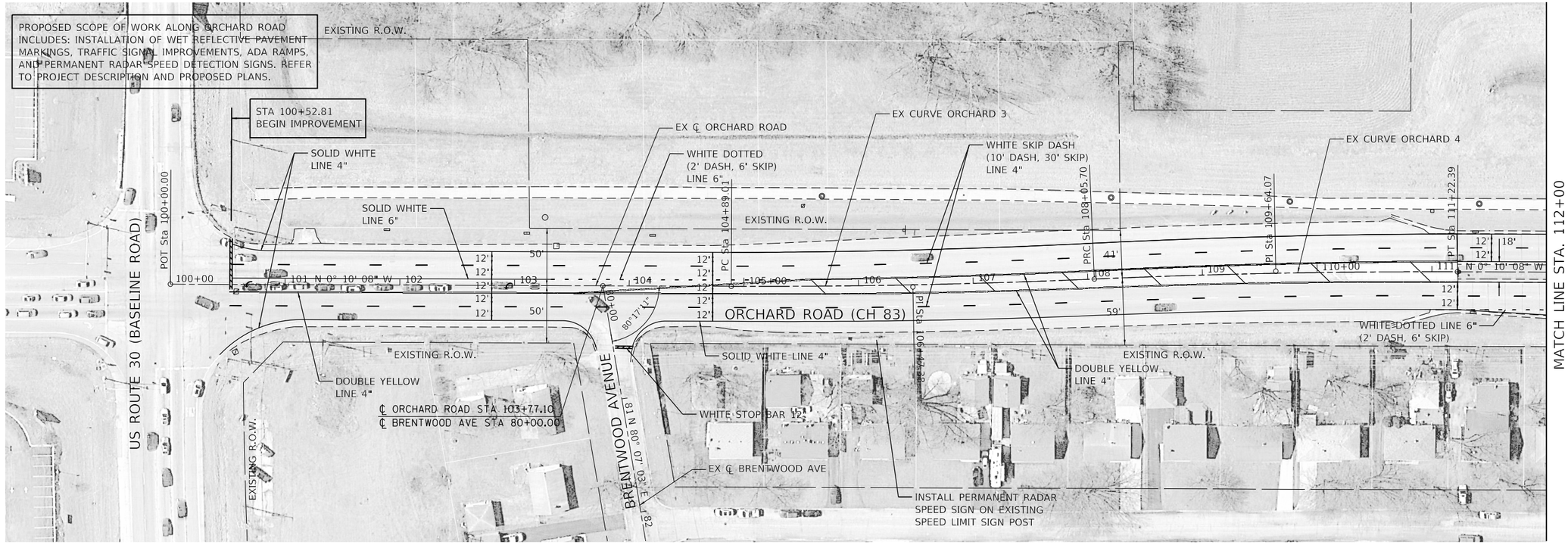

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S.E. RUN = ____
P.C. STA. = 108+05.70
P.T. STA. = 111+22.39

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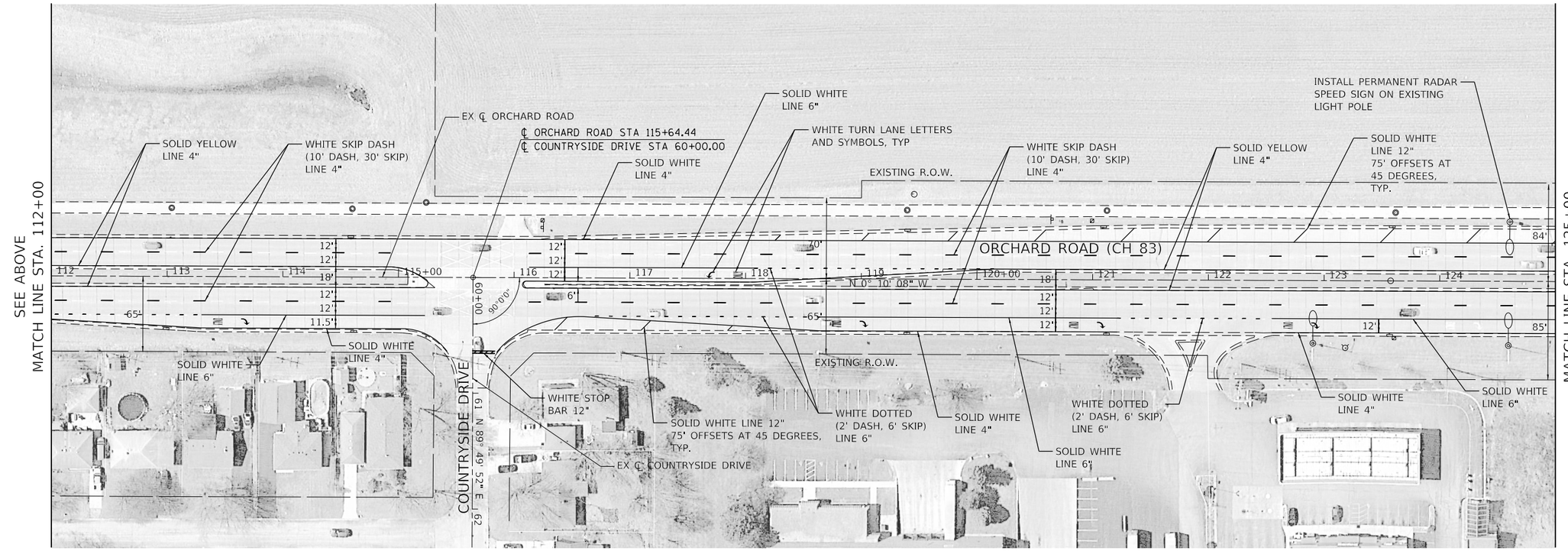
P.T. STA. = 108+05.70



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Peralte-Clark LLC

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN VIEW STA. 100+00 TO 125+00
ORCHARD ROAD

SCALE: 1" = 50' | SHEET 3 OF 10 SHEETS | STA. 100+40 | TO STA. 125+00

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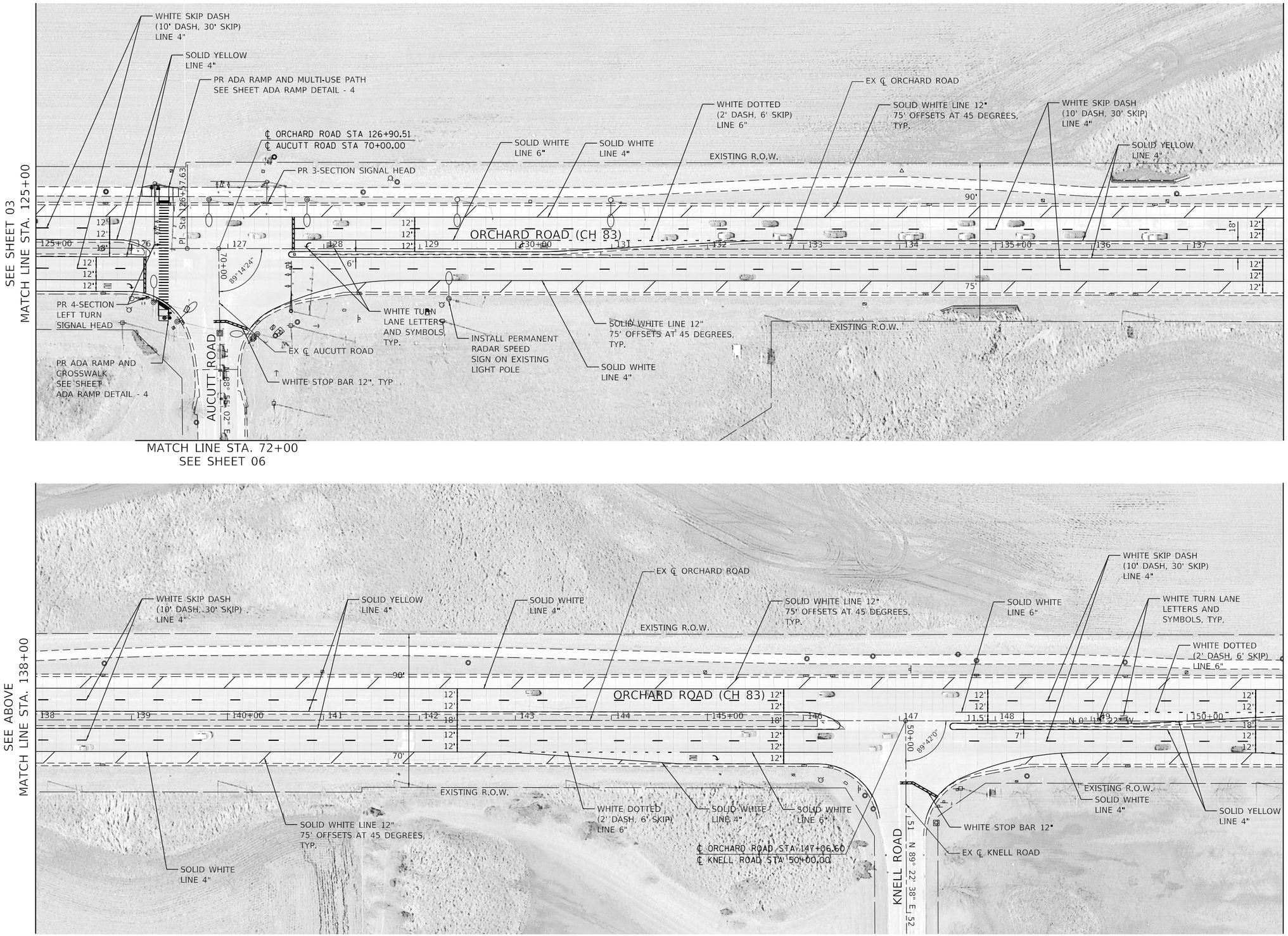

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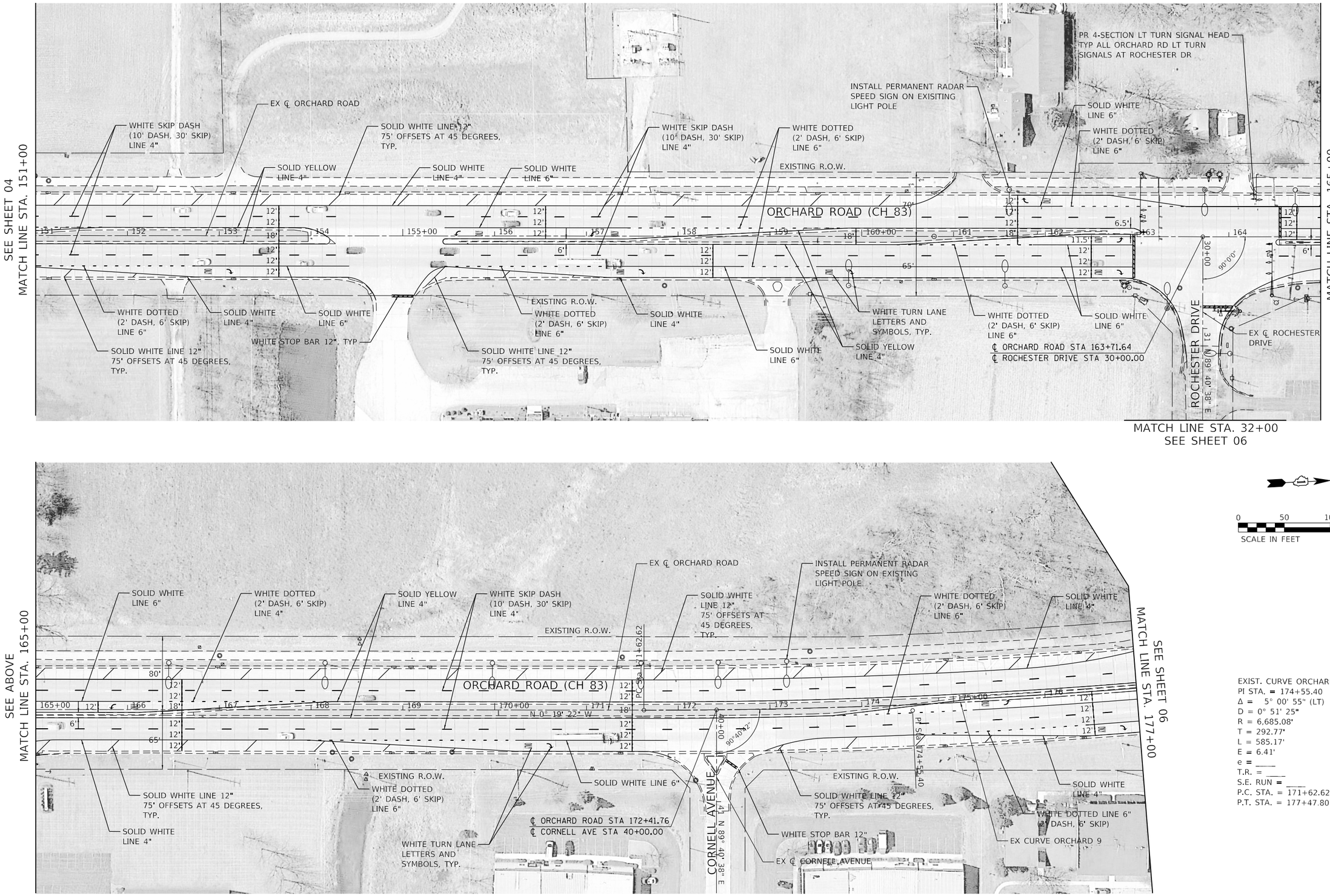
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN VIEW STA 125+00 TO 151+00
ORCHARD ROAD

SCALE: 1" = 50' | SHEET 4 OF 10 SHEETS | STA. 125+00 | TO STA. 151+00

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0336	18-00493-00SP	KANE	10	04
		CONTRACT NO.		
	ILLINOIS FED. AID PROJECT			



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Peralte-Clark LLC

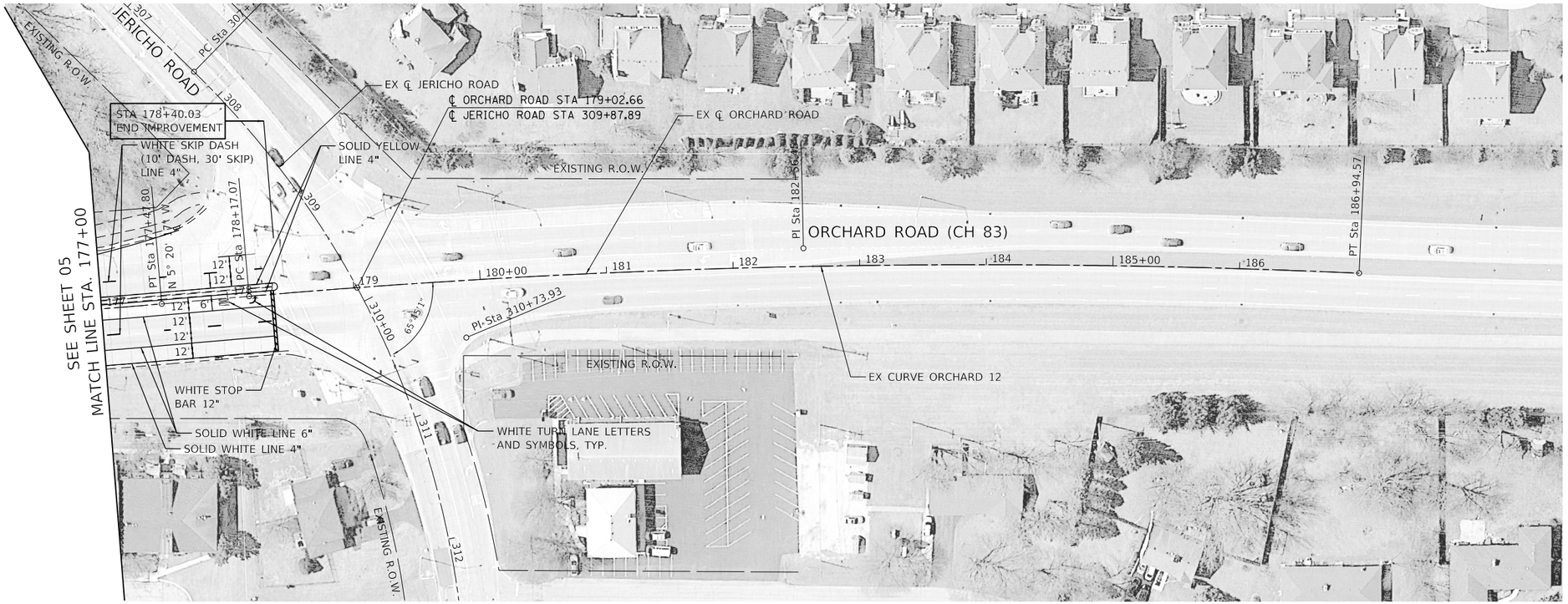
USER NAME = jennifer.morales	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0004 ' / in.	CHECKED -	REVISED -
PLOT DATE = 9/4/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN VIEW STA 151+00 TO 177+00
ORCHARD ROAD

SCALE: 1" = 50' | SHEET 5 OF 10 SHEETS | STA. 151+00 | TO STA. 177+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0336	18-00493-00SP	KANE	10	05
		CONTRACT NO.		
	ILLINOIS FED. AID PROJECT			



N

0 50 100 150
SCALE IN FEET

EXIST. CURVE ORCHARD_12
PI STA. = 182+56.45
Δ = 7° 31' 15" (RT)
D = 0° 51' 25"
R = 6,685.07'
T = 439.38'
L = 877.49'
E = 14.42'
e = ____
T.R. = ____
S.E. RUN = ____
P.C. STA. = 178+17.07
P.T. STA. = 186+94.57

MODEL: Default
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Peralte-Clark LLC

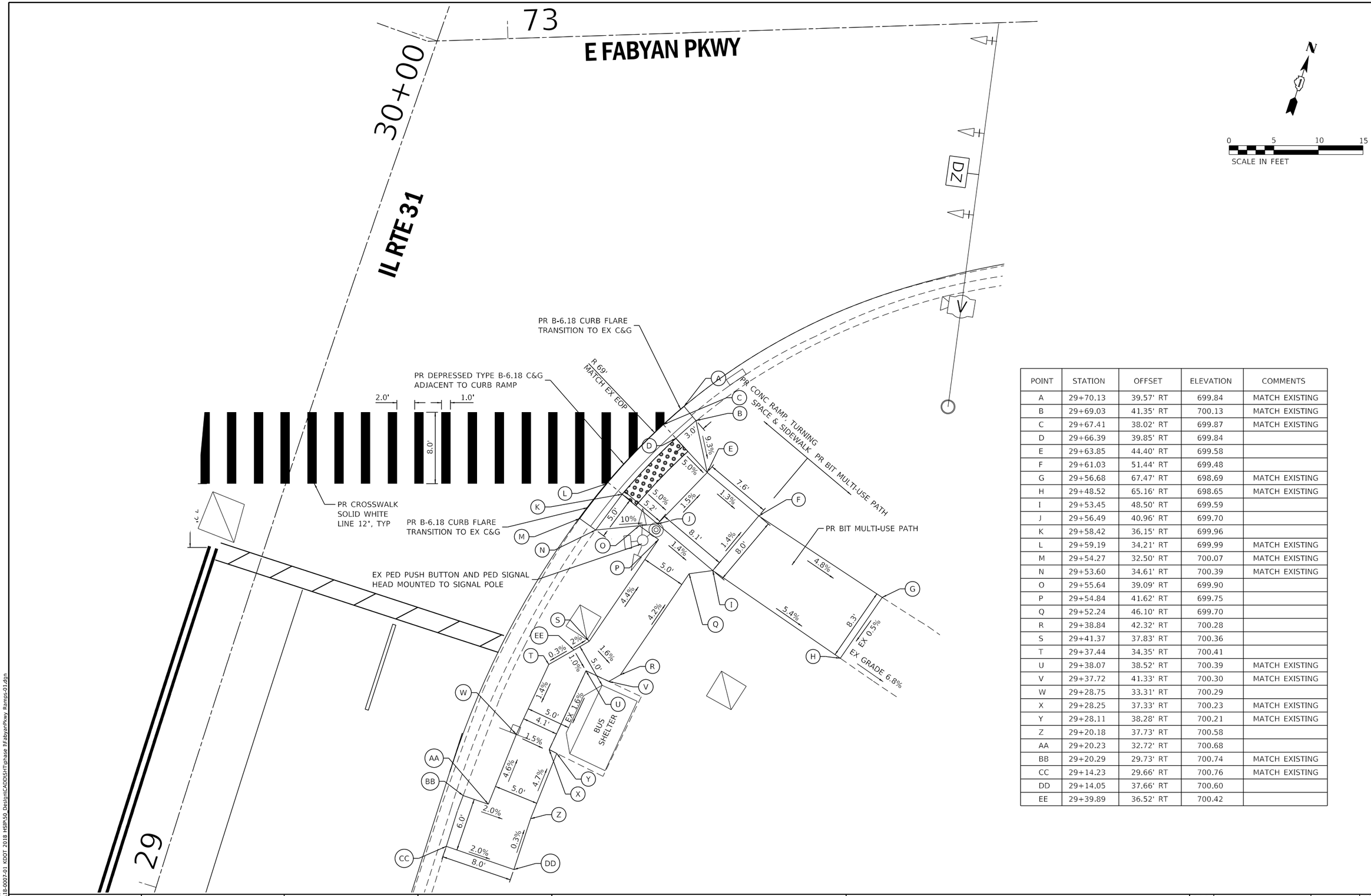
USER NAME = jennifer.morales	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0004 ' / in.	CHECKED -	REVISED -
PLOT DATE = 9/4/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN VIEW STA 177+00 TO 186+00
ORCHARD ROAD

SCALE: 1" = 50' | SHEET 6 OF 10 SHEETS | STA. 177+00 | TO STA. 182+50

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0336	18-00493-00SP	KANE	10	06
		CONTRACT NO.		
	ILLINOIS	FED. AID PROJECT		



POINT	STATION	OFFSET	ELEVATION	COMMENTS
A	29+70.13	39.57' RT	699.84	MATCH EXISTING
B	29+69.03	41.35' RT	700.13	MATCH EXISTING
C	29+67.41	38.02' RT	699.87	MATCH EXISTING
D	29+66.39	39.85' RT	699.84	
E	29+63.85	44.40' RT	699.58	
F	29+61.03	51.44' RT	699.48	
G	29+56.68	67.47' RT	698.69	MATCH EXISTING
H	29+48.52	65.16' RT	698.65	MATCH EXISTING
I	29+53.45	48.50' RT	699.59	
J	29+56.49	40.96' RT	699.70	
K	29+58.42	36.15' RT	699.96	
L	29+59.19	34.21' RT	699.99	MATCH EXISTING
M	29+54.27	32.50' RT	700.07	MATCH EXISTING
N	29+53.60	34.61' RT	700.39	MATCH EXISTING
O	29+55.64	39.09' RT	699.90	
P	29+54.84	41.62' RT	699.75	
Q	29+52.24	46.10' RT	699.70	
R	29+38.84	42.32' RT	700.28	
S	29+41.37	37.83' RT	700.36	
T	29+37.44	34.35' RT	700.41	
U	29+38.07	38.52' RT	700.39	MATCH EXISTING
V	29+37.72	41.33' RT	700.30	MATCH EXISTING
W	29+28.75	33.31' RT	700.29	
X	29+28.25	37.33' RT	700.23	MATCH EXISTING
Y	29+28.11	38.28' RT	700.21	MATCH EXISTING
Z	29+20.18	37.73' RT	700.58	
AA	29+20.23	32.72' RT	700.68	
BB	29+20.29	29.73' RT	700.74	MATCH EXISTING
CC	29+14.23	29.66' RT	700.76	MATCH EXISTING
DD	29+14.05	37.66' RT	700.60	
EE	29+39.89	36.52' RT	700.42	

MODEL: Default
FILE NAME: Z:\18-00072-01 KDOT 2018 HSIP\50 Design\CADD\SHT\phase 1\FabyanPkwy Ramps-01.dgn

Peralte-Clark LLC

USER NAME = Jennifer.morales	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 10.0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 9/3/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAIL - 1
SE CORNER OF FABYAN PKWY AT IL RTE 31

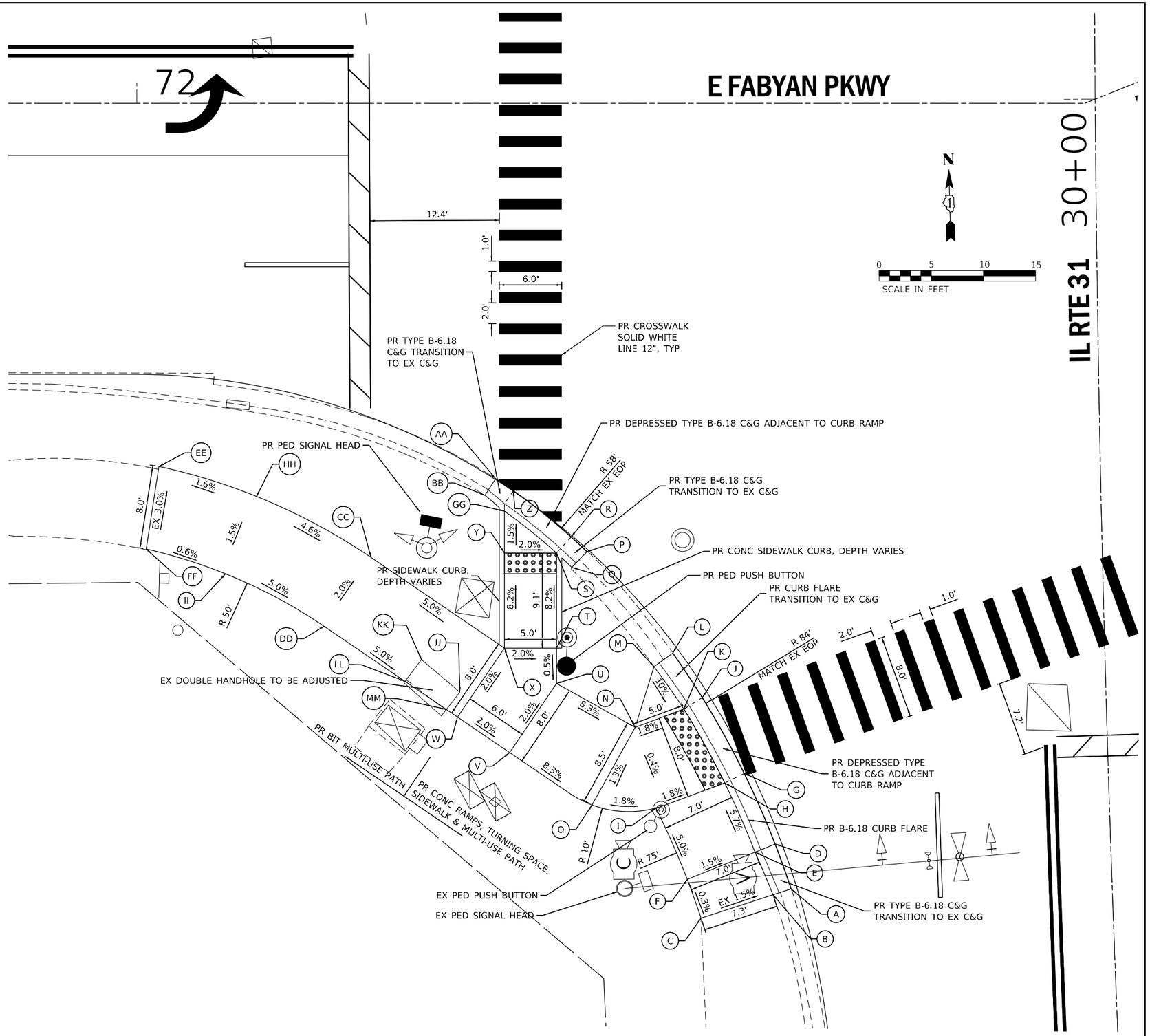
SCALE: 1" = 5' | SHEET 7 OF 10 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0363	18-00493-00SP	KANE	10	07
		CONTRACT NO.		
	ILLINOIS FED. AID PROJECT			

POINT	STATION	OFFSET	ELEVATION	COMMENTS
A	29+25.30	30.81' LT	701.44	MATCH EXISTING
B	29+24.80	32.56' LT	701.85	MATCH EXISTING
C	29+22.83	39.54' LT	701.96	MATCH EXISTING
D	29+29.60	32.19' LT	701.45	MATCH EXISTING
E	29+28.90	34.15' LT	701.86	
F	29+26.54	40.73' LT	701.95	
G	29+36.39	34.93' LT	701.48	MATCH EXISTING
H	29+35.54	36.83' LT	701.45	
I	29+33.37	43.52' LT	701.58	
J	29+43.68	38.74' LT	701.53	MATCH EXISTING
K	29+42.63	40.53' LT	701.50	
L	29+47.96	41.45' LT	701.55	MATCH EXISTING
M	29+46.73	43.29' LT	702.01	MATCH EXISTING
N	29+41.05	45.27' LT	701.59	
O	29+33.86	49.63' LT	701.69	
P	29+57.73	49.45' LT	701.61	MATCH EXISTING
Q	29+56.29	50.93' LT	702.01	MATCH EXISTING
R	29+59.15	50.93' LT	701.61	MATCH EXISTING
S	29+57.67	52.36' LT	701.58	
T	29+48.72	52.59' LT	702.33	
U	29+45.39	52.68' LT	702.31	
V	29+39.04	57.37' LT	702.47	
W	29+42.05	62.27' LT	702.59	
X	29+48.85	57.59' LT	702.43	
Y	29+61.75	57.25' LT	701.68	
Z	29+63.60	56.37' LT	701.65	MATCH EXISTING
AA	29+64.78	58.06' LT	701.66	MATCH EXISTING
BB	29+63.09	59.23' LT	702.10	MATCH EXISTING
CC	29+57.67	70.18' LT	703.20	
DD	29+51.33	74.84' LT	703.36	
EE	29+66.57	90.33' LT	703.61	MATCH EXISTING
FF	29+58.93	91.74' LT	703.84	MATCH EXISTING
GG	29+61.72	57.27' LT	701.62	
HH	29+63.59	81.02' LT	703.77	HIGH POINT
II	29+56.46	84.17' LT	703.89	
JJ	29+44.83	61.98' LT	702.61	EX EL 703.16, DIFF 6.6"
KK	29+47.98	65.57' LT	702.84	EX EL 703.48, DIFF 7.7"
LL	29+45.99	67.21' LT	702.89	EX EL 703.68, DIFF 9.5"
MM	29+43.25	63.32' LT	702.65	EX EL 703.25, DIFF 7.2"

NOTES:

PROPOSED GRADES ARE MAXIMIZED IN THIS DESIGN TO REDUCE THE IMPACT TO THE EXISTING DOUBLE HANDHOLE. CONTRACTOR MUST NOT EXCEED PROPOSED GRADES AND CROSS SLOPES. CONTRACTOR MUST COORDIDNATE WITH THE ELECTRICAL CONTRACTOR TO DETERMINE IF THE PROPOSED DESIGN CAN BE ALTERED AND THE HANDHOLE LOWERED EVEN MORE.



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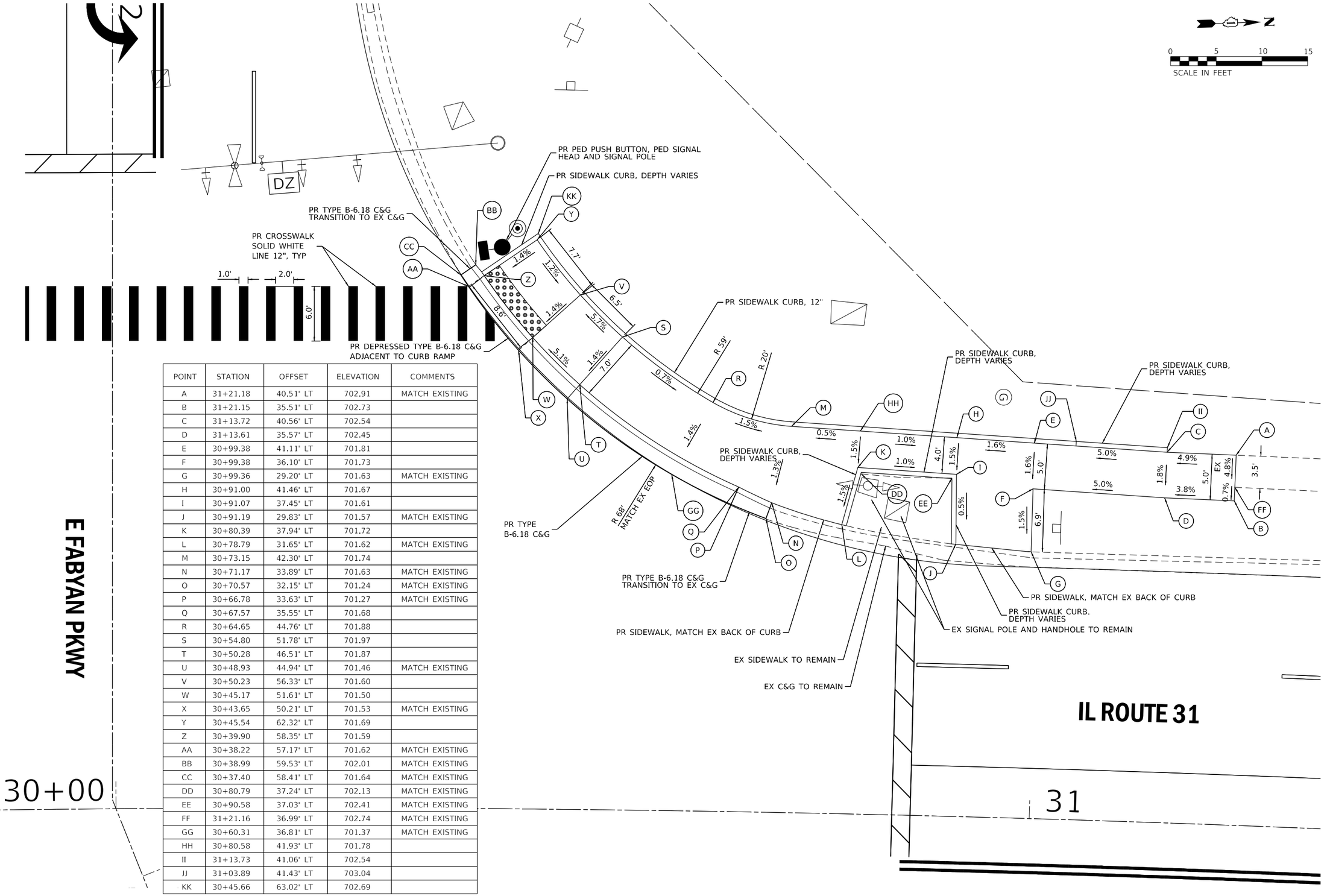
Peralte-Clark LLC	USER NAME = jennifer.morales	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
		DRAWN -	REVISED -	
	PLOT SCALE = 10.0000 ' / in.	CHECKED -	REVISED -	
	PLOT DATE = 9/3/2019	DATE -	REVISED -	

ADA RAMP DETAIL - 2
SW CORNER OF FABYAN PKWY AT IL RTE 31

SCALE: 1" = 5' | SHEET 8 OF 10 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0363	18-00493-00SP	KANE	10	08
		CONTRACT NO.		
	ILLINOIS FED. AID PROJECT			



POINT	STATION	OFFSET	ELEVATION	COMMENTS
A	31+21.18	40.51' LT	702.91	MATCH EXISTING
B	31+21.15	35.51' LT	702.73	
C	31+13.72	40.56' LT	702.54	
D	31+13.61	35.57' LT	702.45	
E	30+99.38	41.11' LT	701.81	
F	30+99.38	36.10' LT	701.73	
G	30+99.36	29.20' LT	701.63	MATCH EXISTING
H	30+91.00	41.46' LT	701.67	
I	30+91.07	37.45' LT	701.61	
J	30+91.19	29.83' LT	701.57	MATCH EXISTING
K	30+80.39	37.94' LT	701.72	
L	30+78.79	31.65' LT	701.62	MATCH EXISTING
M	30+73.15	42.30' LT	701.74	
N	30+71.17	33.89' LT	701.63	MATCH EXISTING
O	30+70.57	32.15' LT	701.24	MATCH EXISTING
P	30+66.78	33.63' LT	701.27	MATCH EXISTING
Q	30+67.57	35.55' LT	701.68	
R	30+64.65	44.76' LT	701.88	
S	30+54.80	51.78' LT	701.97	
T	30+50.28	46.51' LT	701.87	
U	30+48.93	44.94' LT	701.46	MATCH EXISTING
V	30+50.23	56.33' LT	701.60	
W	30+45.17	51.61' LT	701.50	
X	30+43.65	50.21' LT	701.53	MATCH EXISTING
Y	30+45.54	62.32' LT	701.69	
Z	30+39.90	58.35' LT	701.59	
AA	30+38.22	57.17' LT	701.62	MATCH EXISTING
BB	30+38.99	59.53' LT	702.01	MATCH EXISTING
CC	30+37.40	58.41' LT	701.64	MATCH EXISTING
DD	30+80.79	37.24' LT	702.13	MATCH EXISTING
EE	30+90.58	37.03' LT	702.41	MATCH EXISTING
FF	31+21.16	36.99' LT	702.74	MATCH EXISTING
GG	30+60.31	36.81' LT	701.37	MATCH EXISTING
HH	30+80.58	41.93' LT	701.78	
II	31+13.73	41.06' LT	702.54	
JJ	31+03.89	41.43' LT	703.04	
KK	30+45.66	63.02' LT	702.69	

MODEL: Default
FILE NAME: Z:\18-00072-01 KDOT 2018 HSIP\50 Design\CADD\SHTs\phase II\FabyanPkwy_Ramps-03.dgn

Peralte-Clark LLC

USER NAME = Jennifer.morales	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 10.0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 9/3/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAIL - 3
NW CORNER OF FABYAN PKWY AT IL RTE 31

SCALE: 1" = 5' | SHEET 9 OF 10 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0363	18-00493-00SP	KANE	10	09
		CONTRACT NO.		
	ILLINOIS	FED. AID PROJECT		

70+00

AUCUTT ROAD

ORCHARD ROAD

POINT	STATION	OFFSET	ELEVATION	COMMENTS
V	126+20.31	54.00' LT	664.19	
W	126+48.31	63.77' LT	664.43	MATCH EXISTING
X	126+38.31	64.00' LT	664.20	
Y	126+28.31	66.00' LT	664.29	
Z	126+20.31	66.00' LT	664.37	
AA	126+26.50	51.52' RT	663.49	MATCH EXISTING
BB	126+25.32	54.16' RT	663.90	MATCH EXISTING
CC	126+40.31	47.75' LT	664.12	MATCH EXISTING
DD	126+40.31	45.20' LT	663.68	MATCH EXISTING
EE	126+10.31	63.76' LT	664.63	MATCH EXISTING
FF	126+10.31	54.21' LT	664.60	MATCH EXISTING

POINT	STATION	OFFSET	ELEVATION	COMMENTS
A	126+38.31	75.37' RT	663.76	
B	126+28.31	75.37' RT	663.83	
C	126+38.31	70.37' RT	663.69	
D	126+28.31	70.37' RT	663.76	
E	126+38.31	63.77' RT	663.22	
F	126+28.31	63.77' RT	663.40	
G	126+38.31	61.77' RT	663.18	
H	126+28.31	55.25' RT	663.36	
I	126+39.93	59.76' RT	663.24	MATCH EXISTING
J	126+28.31	52.36' RT	663.44	MATCH EXISTING
K	126+39.83	63.06' RT	663.62	MATCH EXISTING
L	126+41.47	61.04' RT	663.21	MATCH EXISTING
M	126+38.31	45.20' LT	663.68	MATCH EXISTING
N	126+28.31	45.20' LT	663.70	MATCH EXISTING
O	126+38.31	47.78' LT	663.62	
P	126+28.31	47.78' LT	663.64	
Q	126+26.31	47.79' LT	664.13	MATCH EXISTING
R	126+26.31	45.20' LT	663.71	MATCH EXISTING
S	126+48.31	54.10' LT	664.48	MATCH EXISTING
T	126+38.31	54.00' LT	664.05	
U	126+28.31	54.00' LT	664.11	

N

0 5 10 15

SCALE IN FEET

PR SIDEWALK CURB DEPTH VARIES

PR TYPE B-6.24 C&G TRANSITION TO EX C&G

PR DEPRESSED TYPE B-6.24 C&G ADJACENT TO CURB RAMP

PR CROSSWALK SOLID WHITE LINE 12", TYP

PR TYPE B-6.24 C&G TRANSITION TO EX C&G

PR SIDEWALK CURB DEPTH VARIES

PR PED PUSH BUTTON, PED SIGNAL HEAD AND SIGNAL POLE

PR TYPE B-6.24 C&G TRANSITION TO EX C&G

PR SIDEWALK CURB DEPTH VARIES

PR PED SIGNAL HEAD MOUNTED TO EX SIGNAL POLE
PR PED PUSH BUTTON

R 61'
MATCH EX EOP

126

MODEL: Default
FILE NAME: Z:\18-00072-01 KDOT 2018 HSIP-50_Design\CADD\SHT\phase II\Orchard Road Ramps.dgn

USER NAME = Jennifer.morales	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
PLOT SCALE = 10.0000 ' / in.	CHECKED -	REVISED -	
PLOT DATE = 9/4/2019	DATE -	REVISED -	

Peralte-Clark LLC

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAIL - 4
ORCHARD ROAD AT AUCUTT ROAD

SCALE: 1" = 5' | SHEET 10 OF 10 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0336	18-00493-00SP	KANE	10	10
		CONTRACT NO.		
		ILLINOIS	FED. AID PROJECT	

Attachment 4:
Environmental Resources

Peralte-Clark LLC

Project Overview

Submittal Date: 01/03/2019 Sequence No: 22205

District: 1 Requesting Agency: Local Kane Co Hwys Project Identifier:

Contract #: Job No.:

Counties: Kane

Route: FAP 363 at FAU 3887, FAP 336 Marked: IL 31

Street: Fabyan Pkwy at Batavia Ave, Orchard Road Section: 18-00493-00-SP

Municipality(ies): Batavia, Geneva, Montgomery, Unincorpor Project Length: 3.2187 km 2 miles

FromTo (At): Fabyan Pkwy at IL 31, Orchard Rd from Jericho Rd to US 30

Quadrangle: North Aurora, Yorkville Township-Range-Section: 39N-8E-15, 38N-7E-36, 38N-7E-25

Survey Target Date: 08/20/2019 Anticipated Design Appr.: 08/21/2019 Anticipated Processing: CE

Funding: ☑ Federal ☐ State ☐ TBP ☑ MFT ☑ Local Non-MFT

Consultant:

PTB No.: Item No.: PTB Date: Prequal Level:

Sequence No: 22205	Biological	Wetlands	Cultural	Special Waste
Entered By	BDE		BDE	BDE
Cleared for DA	4/2/2019		12/6/2019	8/9/2019
Cleared for Letting	4/2/2019		12/6/2019	
Resubmittal				
ResubmittalCleared				

Section: 18-00493-00-SP Job No.:

FromTo (At): Fabyan Pkwy at IL 31, Orchard Rd from Jericho Rd to US 30

Notice of Intent	Project Initiation Ltr to FHWA	Public Info Meeting(s) 1st	Public Info Meeting(s) 2nd	Notice of Availability Draft	Notice of Availability Final	Public Hearing	Draft	ROD/FONSI Approved

Project Phase Comments:

NATURAL RESOURCE / BIOLOGICAL CLEARANCE

Jennifer Morales

From: Allison Eberhardt
Sent: Wednesday, April 3, 2019 2:38 PM
To: KDOT 2018 HSIP
Subject: FW: PMA Seq 22205 - D1 - Kane Co Sec 18-00493-00-SP; Bio. Clearance
Attachments: NRR.pdf; TREC Reports (2 Locs).pdf; USFWS Bat NE Verification Letter - Fabyan Parkway.pdf; USFWS Bat NE Verification Letter - Orchard Road.pdf; USFWS T-E Species Lists (2 Loc's).pdf

Allison Eberhardt, P.E.
Design Engineer

Peralte-Clark, LLC
171 West Wing Street, Unit 204B
Arlington Heights, Illionis 60005
Mobile: 224-577-9025
Email: allison.eberhardt@peralte-clark.com

www.peralte-clark.com

From: Daas, Iyad <Iyad.Daas@illinois.gov>
Sent: Wednesday, April 3, 2019 1:35 PM
To: Mike Zakosek <zakosekmike@co.kane.il.us>; Matt Turk <matt.turk@peralte-clark.com>; Allison Eberhardt <allison.eberhardt@peralte-clark.com>
Cc: Solomon, Marilin D <Marilin.Solomon@illinois.gov>
Subject: FW: PMA Seq 22205 - D1 - Kane Co Sec 18-00493-00-SP; Bio. Clearance

All,

Please see attached for Bio clearance for the subject project.

Thank you,

Iyad Daas
Associate Field Engineer
Illinois Department of Transportation, Dist.1
Bureau of Local Roads and Streets
P: (847) 705 - 4205
E-Mail: Iyad.Daas@illinois.gov

From: DOT.LocalAgencyESR
Sent: Wednesday, April 3, 2019 11:42 AM
To: Daas, Iyad <Iyad.Daas@illinois.gov>
Cc: Solomon, Marilin D <Marilin.Solomon@illinois.gov>; Raffensperger, William <William.Raffensperger@illinois.gov>; DOT.LocalAgencyESR <DOT.LocalAgencyESR@illinois.gov>
Subject: PMA Seq 22205 - D1 - Kane Co Sec 18-00493-00-SP; Bio. Clearance

FYI,

The Natural Resources Review memo, USF&W consultation documents and TREC Reports for the subject section have been posted to your project SharePoint folder and clearance dates have been entered on the ***Biological*** form within the PMA.
The file copies are attached for reference.

Jr

ELMER (JR) PEARCY
CBLRS CONSULTANT
PROJECT DEVELOPMENT UNIT / 217-785-1665
ELMER.PEARCY@ILLINOIS.GOV

From: Hargrove, Susan Dees
Sent: Tuesday, April 02, 2019 3:15 PM
To: Pearcy, Elmer <Elmer.Pearcy@illinois.gov>; Raffensperger, William <William.Raffensperger@illinois.gov>
Subject: PMA Seq. #22205 , 'Cleared for Design Approval' - Biological Form

A 'Cleared for Design Approval' date has been entered on the Biological form for the following PMA record:

Sequence #: 22205
District: 1
Route: FAP 363 at FAU 3887, FAP 336
Marked Route: IL 31
County: Kane

Illinois Department of Transportation

Memorandum

To:	Greg S. Lupton		
From:	Jack A. Elston	By:	Thomas C. Brooks
Subject:	Natural Resources Review		
Date:	April 2, 2019		

Fabyan Parkway at IL 31
Orchard Road
Sec. 18-00493-00-SP
T39N/R8E/S 15 and T38N/R8E/S 30
Seq. No.: 22205
Kane County

There are two proposed projects. The first project is for safety improvements at Fabyan Parkway at Batavia Avenue (IL 31) in Batavia. There will be no structural work on the Fabyan Parkway bridge over the Fox River. The second project is for safety and ADA improvements along Orchard Road from Jericho Road to U.S. 30 in rural Kane County. The scope of work for both projects involves the adjustment of signal heads, application of wet reflective pavement markings, installation of advanced warning flashers, and implementation of a dilemma zone detection system and ADA ramp improvements.

The overall project requires 0.3 acres of land acquisition. There will be no in stream work for either project. There will be no trees to be removed for either project. The land cover in the vicinity of the Fabyan Parkway project is urban and Forest Preserve. The land cover in the vicinity of the Orchard Road project is a mixture of urban and rural.

Review for Illinois Endangered Species Protection and Illinois Natural Areas Preservation – Part 1075

The Illinois Natural Heritage Database contains a record of State-listed endangered and federally listed threatened leafy prairie clover in the vicinity of the Fabyan Parkway project. This species occurs in the southeastern quadrant of the intersection in Forest Preserve of Kane County lands. Examination of project plans, aerial and ground level photographs depict the project area as mowed and bike trail and thus not suitable habitat for this species. There will thus be no adverse effect by the Fabyan Parkway project on the leafy prairie clover. The Illinois Natural Heritage Database contains no record of State-listed threatened or endangered species, Illinois Natural Area Inventory sites, dedicated Illinois Nature Preserves, or registered Land and Water Reserves in the vicinity of the Orchard Road project. **Therefore, consultation under Part 1075 is terminated.**

This review for compliance with 17 Ill. Adm. Code Part 1075 is valid for two years unless new information becomes available that was not previously considered; the proposed improvement is modified; or additional species, essential habitat, or Natural Areas are identified in the vicinity. If the proposed improvement has not been implemented within two years of the date of this memorandum, or any of the above listed conditions develop, a new review will be necessary.

Review for Illinois Interagency Wetland Policy Act – Part 1090

The proposed improvements were not surveyed for wetlands. There are four inventoried wetlands within the ESR limits of the Orchard Road improvement and none within the ESR limits of the Fabyan Parkway improvement. According to the project proponent, there will be no impact to these wetlands due to the minimal scope and location of work in relation to those wetlands. There will be no excavation within 250 feet of the inventoried wetlands. The scope of work for both projects involves the adjustment of signal heads, application of wet reflective pavement markings, installation of advanced warning flashers, and implementation of a dilemma zone detection system and ADA ramp improvements. There will be no instream work in the Fox River. **Our review for compliance under Part 1090 is terminated.**

Review for Endangered Species Act - Section 7

The proposed improvement was reviewed in fulfillment of our obligation under Section 7(a)(2) of the Endangered Species Act. Our review included use of the US Fish and Wildlife Service's Information for Planning and Conservation (IPaC) web-based review tool. Through IPaC, an official species list was received and is saved to the project folder. The list contains the endangered, threatened, proposed and candidate species and proposed and designated critical habitat that may be present within or in the vicinity of the proposed improvement. The following species are listed in Kane and Kendall Counties: Northern long-eared bat (NLEB), Indiana bat (Ibat), Eastern prairie fringed orchid (EPFO), and Rusty patched bumble bee. There is no Critical Habitat in the project vicinity. **Under 50 CFR 402.12(e), the accuracy of the species list is limited to 90 days.**

Northern long-eared bat

Northern long-eared bat suitable summer habitat consists of a wide variety of forested or wooded habitats where they roost, forage, and travel and may also include some adjacent and interspersed non-forested habitats such as emergent wetlands and adjacent edges of agricultural fields, old fields and pastures. This includes forests and woodlots containing potential roosts (i.e., live trees or snags ≥3 inches dbh that have exfoliating bark, cracks, crevices, or hollows) as well as linear features such as fencerows, riparian forests, and other wooded corridors. These wooded areas may be dense or loose aggregates of trees with variable amounts of canopy closure. Individual trees may be considered suitable habitat when they exhibit characteristics of suitable roost trees and are within 1,000 feet of other forested or wooded habitat. Trees found in highly-developed urban areas (e.g., street trees, downtown areas) are extremely unlikely to be suitable NLEB habitat.

There will be no trees removed as a result of these projects. Land use in the project areas is urban and Forest Preserve (Fabyan Parkway) and a mixture of rural and urban (Orchard Road). There are no records of maternity roost trees, maternity colonies or hibernacula in the vicinity of the project corridor.

We assessed the potential for adverse impacts to the NLEB in accordance with the Programmatic Biological Opinion on Final 4(d) Rule for the Northern Long-Eared Bat and Activities Excepted from Take Prohibitions and determined that the proposed improvement will have no effect to the NLEB.

Indiana bat

We assessed the potential for adverse impacts to the Ibat in accordance with the Federal Highway Administration, Federal Railroad Administration, and Federal Transit Administration Programmatic Biological Assessment (BA) for Transportation Projects in the Range of the Ibat and NLEB (11-28-2016) and the Revised US Fish and Wildlife Service Programmatic Biological Opinion (12-15-2016). We have determined that the proposed improvements will have no effect to the Ibat.

Eastern prairie fringed orchid

Eastern prairie fringed orchid occurs in a wide variety of habitats, from mesic prairie to wetland communities such as sedge meadows, marsh edges and even bogs. It requires full sunlight for optimum growth and flowering, which restricts it to grass- and sedge-dominated plant communities. The substrate of the sites where it occurs ranges from neutral to mildly calcareous. Occasionally the orchid colonizes successional habitats or recolonizes previously occupied areas.

We evaluated the limits of the proposed improvements for the presence of potentially suitable EPFO habitat. Our evaluation included the use of EPFO guidance from the US Fish and Wildlife Service, Chicago Ecological Services Field Office. There are no impacted prairies or high quality wetlands in the project corridor. We determined there would be no effect to EPFO from the proposed improvements.

Leafy Prairie Clover

There is an occurrence of the federally threatened leafy prairie clover in the vicinity of the Fabyan Parkway project. This species occurs in the southeastern quadrant of the intersection in Forest Preserve of Kane County lands. Examination of project plans, aerial and ground level photographs depict the project area as mowed and bike trail and thus not suitable habitat for this species. There will thus be no effect by the Fabyan Parkway project on the leafy prairie clover.

Rusty patched bumble bee

We evaluated the limits of the proposed improvements for the presence of potentially suitable Rusty patched bumble bee habitat. Our evaluation included the use of the guidance issued by USFWS dated March 21, 2017 and titled "The

Rusty Patched Bumble Bee (*Bombus affinis*), Interagency Cooperation under Section 7(a)(2) of the Endangered Species Act, Voluntary Implementation Guidance" ("USFWS Interagency Guidance"). According to the guidance, if a project is outside of a high potential zone, then the USFWS advises that the incidental take coverage is not necessary (https://www.fws.gov/midwest/endangered/insects/rpbb/guidance.html). Therefore, if the project is outside of a high potential zone, then a "no effect" determination is appropriate.

We cross referenced the preferred habitat of the Rusty patched bumble bee with our knowledge of the project areas and determined that there is no USFWS High Potential Zone or RPBB record in the project vicinity. USFWS shapefiles dated August 15, 2018, were reviewed February 25, 2019. In accordance with Section 7 of the Endangered Species Act, we determined that there will be no effect to the Rusty patched bumble bee.

Other Federally Listed Species

We cross-referenced the preferred habitat of each of the remaining listed species with our knowledge of the project area and determined that there are no suitable habitats present. We have determined that the proposed improvement will have no effect on any of the remaining listed species.

We have determined that the proposed improvements are not likely to jeopardize the continued existence of any endangered species or threatened species or result in the destruction or adverse modification of any critical habitat.

Should the proposed improvements be modified or new information indicate listed or proposed species may be affected, consultation or additional coordination should be initiated.

Attachment — USFWS species list

SDH

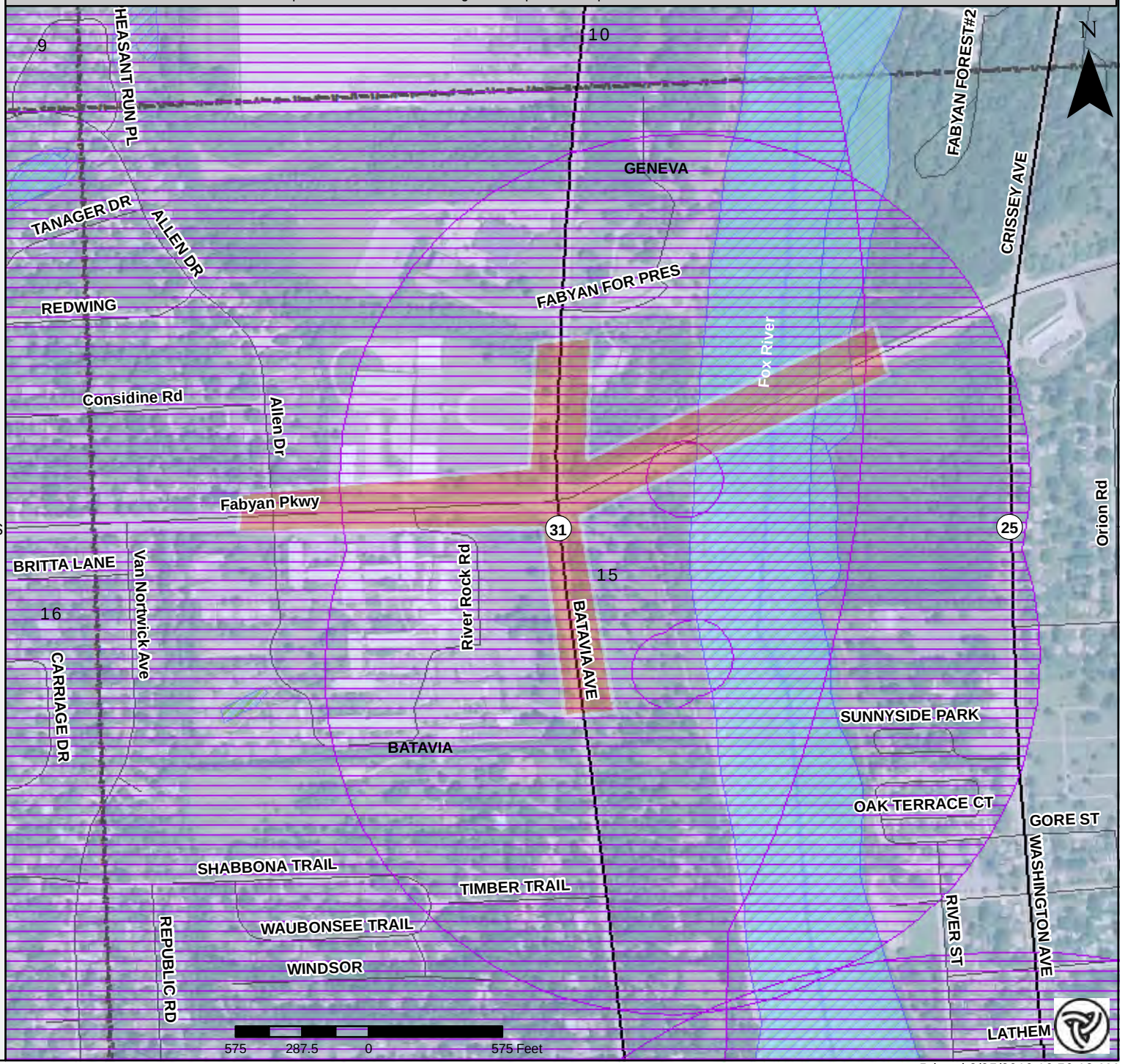
Transportation Review for Ecological Compliance: Report of Possible Resource Conflicts

Sequence #: 22205(1)

Fabyan Pkwy at Batavia Ave
Resource in Vicinity of Project Polygon
*T&E
*Ducks Unlimited Wetlands
*National Wetlands Inventory
INAI & NP w/in 1 mile
*none found

No Resource Found
*INAI
*Nature Preserve
*INHS Wetland
*Roadside Prairie Inventory

County: KANE
Section(PLSS): 3 39N8E15
Area: -0.03328 sq. miles = -21.30178 acres
Report created by Susan Dees Hargrove



Threatened & Endangered Species (T&E)
Nature Preserve (NP)
Illinois Natural Areas Inventory (INAI)
Wetlands
INHS Wetland
Roadside Prairie Inventory

ILLINOIS DEPARTMENT OF NATURAL RESOURCES

Include as additional documentation with permit applications (USACE).

Printed 2/25/2019 10:56:18 AM

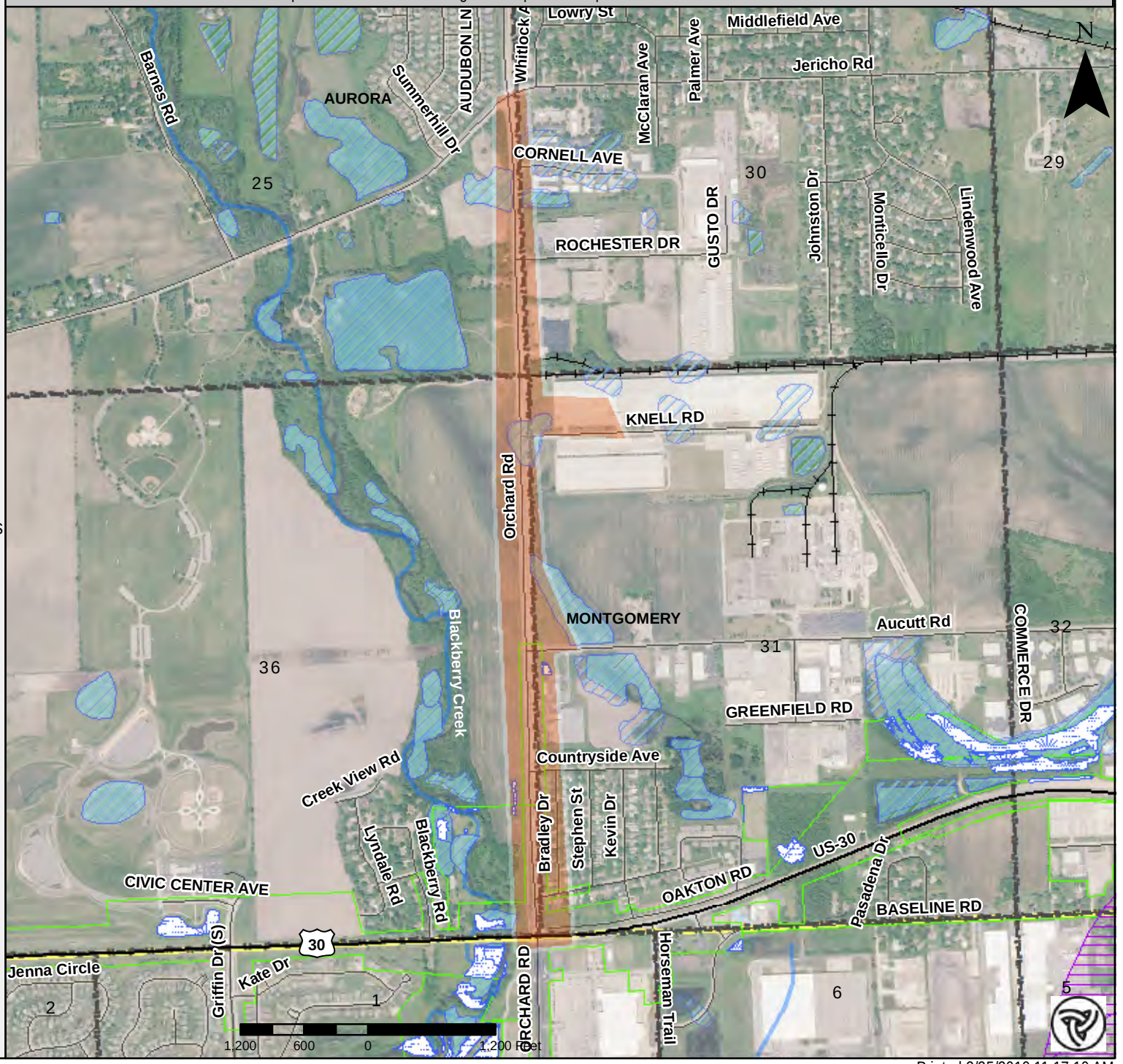
Transportation Review for Ecological Compliance: Report of Possible Resource Conflicts

Sequence #: 22205(2)

Orchard Rd from Jericho Rd to US 30
Resource in Vicinity of Project Polygon
*Ducks Unlimited Wetlands
*INHS Wetland
*National Wetlands Inventory
INAI & NP w/in 1 mile
*none found

No Resource Found
*INAI
*T&E
*Nature Preserve
*Roadside Prairie Inventory

County: KANE
Section(PLSS): 3 38N8E30
Area: -0.14583 sq. miles = -93.33286 acres
Report created by Susan Dees Hargrove



Threatened & Endangered Species (T&E)
Nature Preserve (NP)
Illinois Natural Areas Inventory (INAI)
Wetlands
INHS Wetland
Roadside Prairie Inventory

ILLINOIS DEPARTMENT OF NATURAL RESOURCES

Include as additional documentation with permit applications (USACE).

Printed 2/25/2019 11:17:10 AM

United States Department of the Interior

FISH AND WILDLIFE SERVICE
Chicago Ecological Service Field Office
U.s. Fish And Wildlife Service Chicago Ecological Services Office
230 South Dearborn St., Suite 2938
Chicago, IL 60604-1507
Phone: (312) 216-4720 Fax:
http://www.fws.gov/midwest/endangered/section7/s7process/7a2process.html

IPaC Record Locator: 667-16027441 April 02, 2019

Subject: Consistency letter for the 'Fabyan Parkway at Batavia Avenue, Batavia, Kane Co, seq. 22205 - 1 of 2 projects' project (TAILS 03E13000-2019-R-0179) under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request dated to verify that the **Fabyan Parkway at Batavia Avenue, Batavia, Kane Co, seq. 22205 - 1 of 2 projects** (Proposed Action) may rely on the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat.884, as amended; 16 U.S.C. 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action will have no effect on the endangered Indiana bat (*Myotis sodalis*) or the threatened Northern long-eared bat (*Myotis septentrionalis*). If the Proposed Action is not modified, **no consultation is required for these two species.**

For Proposed Actions that include bridge/structure removal, replacement, and/or maintenance activities: If your initial bridge/structure assessments failed to detect Indiana bats, but you later detect bats during construction, please submit the Post Assessment Discovery of Bats at Bridge/Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action may affect any other federally-listed or proposed species and/or designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act

may also be required. In either of these circumstances, please advise the lead Federal action agency for the Proposed Action accordingly.

The following species may occur in your project area and **are not** covered by this determination:

- Eastern Prairie Fringed Orchid, *Platanthera leucophaea* (Threatened)

Project Description

The following project name and description was collected in IPaC as part of the endangered species review process.

Name

Fabyan Parkway at Batavia Avenue, Batavia, Kane Co, seq. 22205 - 1 of 2 projects

Description

Safety improvements, lighting, etc. No structural work on Fabyan Pkwy bridge over Fox R. New ROW for both projects 0.3 ac, no instream work, no tree removal. Land use urban. Unknown construction date.

Determination Key Result

Based on the information you provided, you have determined that the Proposed Action will have no effect on the endangered Indiana bat and/or the threatened Northern long-eared bat. Therefore, no consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required for these two species.

Qualification Interview

1. Is the project within the range of the Indiana bat[1]?

   [1] See Indiana bat species profile

   Automatically answered
   No

2. Is the project within the range of the Northern long-eared bat[1]?

   [1] See Northern long-eared bat species profile

   Automatically answered
   Yes

3. Which Federal Agency is the lead for the action?

   A) Federal Highway Administration (FHWA)

4. Are *all* project activities limited to non-construction[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

   [1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

   No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces[1]?

   [1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

   No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum[1]?

   [1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

   No

7. Is the project located **within** a karst area?

   No

8. Is there *any* suitable[1] summer habitat for Indiana Bat or NLEB **within** the project action area[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

   [1] See the Service's summer survey guidance for our current definitions of suitable habitat.

   [2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the national consultation FAQs.

   No

9. Does the project include maintenance of the surrounding landscape at existing facilities (e.g., rest areas, stormwater detention basins)?

   No

10. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

    No

11. Does the project include slash pile burning?

    No

12. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

    No

13. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

    No

14. Will the project involve the use of **temporary** lighting *during* the active season?
    No

15. Will the project install new or replace existing **permanent** lighting?
    No

16. Are *all* project activities that are **not associated with** habitat removal, tree removal/ trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

    Examples: lining roadways, unlighted signage , rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

    Yes

17. Will the project raise the road profile **above the tree canopy**?
    No

18. Is the location of this project consistent with a No Effect determination in this key?
    Automatically answered
    Yes, because the project action area not within suitable Indiana bat and/or NLEB summer habitat and is outside of 0.5 miles of a hibernaculum.

Determination Key Description: FHWA, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat

This key was last updated in IPaC on March 16, 2018. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects. The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

United States Department of the Interior

FISH AND WILDLIFE SERVICE
Assistant Director-Ecological Services
1849 C Street Nw
Room 3345
Washington, DC 20240-0001
Phone: (202) 208-4646 Fax: (202) 208-5618

IPaC Record Locator: 509-16027597 April 02, 2019

Subject: Consistency letter for the 'Orchard Rd from Jericho Rd to US 30, Montgomery/ Aurora, Kane Co, seq. 22205 - 2 of 2 projects' project (TAILS 03E18000-2019-R-0685, 03E13000-2019-R-0180) under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request dated to verify that the **Orchard Rd from Jericho Rd to US 30, Montgomery/Aurora, Kane Co, seq. 22205 - 2 of 2 projects** (Proposed Action) may rely on the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat.884, as amended; 16 U.S.C. 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action will have no effect on the endangered Indiana bat (*Myotis sodalis*) or the threatened Northern long-eared bat (*Myotis septentrionalis*). If the Proposed Action is not modified, **no consultation is required for these two species.**

For Proposed Actions that include bridge/structure removal, replacement, and/or maintenance activities: If your initial bridge/structure assessments failed to detect Indiana bats, but you later detect bats during construction, please submit the Post Assessment Discovery of Bats at Bridge/Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action may affect any other federally-listed or proposed species and/or designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act

may also be required. In either of these circumstances, please advise the lead Federal action agency for the Proposed Action accordingly.

The following species may occur in your project area and **are not** covered by this determination:

- Eastern Prairie Fringed Orchid, *Platanthera leucophaea* (Threatened)

Project Description

The following project name and description was collected in IPaC as part of the endangered species review process.

Name

Orchard Rd from Jericho Rd to US 30, Montgomery/Aurora, Kane Co, seq. 22205 - 2 of 2 projects

Description

Safety improvements and ADA, improvements. Total new ROW for both projects 0.3 ac, no instream work, no tree removal. Land use mix of urban and rural. Unknown construction date.

Determination Key Result

Based on the information you provided, you have determined that the Proposed Action will have no effect on the endangered Indiana bat and/or the threatened Northern long-eared bat. Therefore, no consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required for these two species.

Qualification Interview

1. Is the project within the range of the Indiana bat[1]?

   [1] See Indiana bat species profile

   Automatically answered
   Yes

2. Is the project within the range of the Northern long-eared bat[1]?

   [1] See Northern long-eared bat species profile

   Automatically answered
   Yes

3. Which Federal Agency is the lead for the action?

   A) Federal Highway Administration (FHWA)

4. Are *all* project activities limited to non-construction[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

   [1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

   No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces[1]?

   [1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

   No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum[1]?

   [1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

   No

7. Is the project located **within** a karst area?

   No

8. Is there *any* suitable[1] summer habitat for Indiana Bat or NLEB **within** the project action area[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

   [1] See the Service's summer survey guidance for our current definitions of suitable habitat.

   [2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the national consultation FAQs.

   No

9. Does the project include maintenance of the surrounding landscape at existing facilities (e.g., rest areas, stormwater detention basins)?

   No

10. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

    No

11. Does the project include slash pile burning?

    No

12. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

    No

13. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

    No

14. Will the project involve the use of **temporary** lighting *during* the active season?

    No

15. Will the project install new or replace existing **permanent** lighting?

    No

16. Are *all* project activities that are **not associated with** habitat removal, tree removal/ trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

    Examples: lining roadways, unlighted signage , rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

    Yes

17. Will the project raise the road profile **above the tree canopy**?

    No

18. Is the location of this project consistent with a No Effect determination in this key?

    Automatically answered

    Yes, because the project action area not within suitable Indiana bat and/or NLEB summer habitat and is outside of 0.5 miles of a hibernaculum.

Determination Key Description: FHWA, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat

This key was last updated in IPaC on March 16, 2018. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects. The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

U.S. Fish & Wildlife Service Contact List

Illinois-Iowa Ecological Services Field Office
Illinois & Iowa Ecological Services Field Office
1511 47th Ave
Moline, IL 61265-7022
(309) 757-5800

Chicago Ecological Service Field Office
U.s. Fish And Wildlife Service Chicago Ecological Services Office
230 South Dearborn St., Suite 2938
Chicago, IL 60604-1507
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United States Department of the Interior

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http://www.fws.gov/midwest/endangered/section7/s7process/7a2process.html

In Reply Refer To: March 21, 2019
Consultation Code: 03E13000-2019-SLI-0179
Event Code: 03E13000-2019-E-00468
Project Name: Fabyan Parkway at Batavia Avenue, Batavia, Kane Co, seq. 22205 - 1 of 2 projects

Subject: List of threatened and endangered species that may occur in your proposed project location, and/or may be affected by your proposed project

To Whom It May Concern:

To Whom It May Concern:

The attached species list identifies any federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Please note! For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

For all other projects, continue the Section 7 Consultation process by going to our Section 7 Technical Assistance website at http://www.fws.gov/midwest/endangered/section7/s7process/index.html. If you are familiar with this website, you may want to go to Step 2 of the Section 7 Consultation process at http://www.fws.gov/midwest/endangered/section7/s7process/step2.html.

Under 50 CFR 402.12(e) (the regulations that implement Section 7 of the Endangered Species Act) the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally. You may verify the list by visiting the ECOS-IPaC website

http://ecos.fws.gov/ipac/ at regular intervals during project planning and implementation and completing the same process you used to receive the attached list. As an alternative, you may contact this Ecological Services Field Office for updates.

Although no longer protected under the Endangered Species Act, be aware that bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*), as are golden eagles. Projects affecting these species may require measures to avoid harming eagles or may require a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website at http://www.fws.gov/midwest/midwestbird/EaglePermits/index.html to help you determine if you can avoid impacting eagles or if a permit may be necessary.

We appreciate your concern for threatened and endangered species. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List

Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Chicago Ecological Service Field Office
U.s. Fish And Wildlife Service Chicago Ecological Services Office
230 South Dearborn St., Suite 2938
Chicago, IL 60604-1507
(312) 216-4720

Project Summary

Consultation Code: 03E13000-2019-SLI-0179

Event Code: 03E13000-2019-E-00468

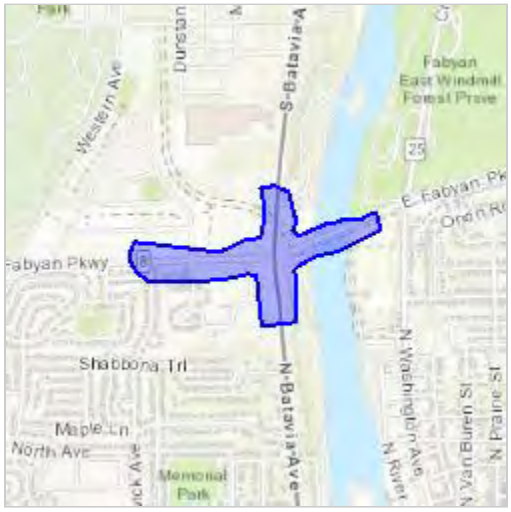
Project Name: Fabyan Parkway at Batavia Avenue, Batavia, Kane Co, seq. 22205 - 1 of 2 projects

Project Type: TRANSPORTATION

Project Description: Safety improvements, lighting, etc. No structural work on Fabyan Pkwy bridge over Fox R. New ROW for both projects 0.3 ac, no instream work, no tree removal. Land use urban. Unknown construction date.

Project Location:

Approximate location of the project can be viewed in Google Maps: https://www.google.com/maps/place/41.86433548058181N88.31659947837187W



Counties: Kane, IL

Endangered Species Act Species

There is a total of 2 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries[1], as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

---

1. NOAA Fisheries, also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

Mammals

NAME	STATUS
Northern Long-eared Bat *Myotis septentrionalis* No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/9045	Threatened

Flowering Plants

NAME	STATUS
Eastern Prairie Fringed Orchid *Platanthera leucophaea* No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: ▪ Follow the guidance provided at https://www.fws.gov/midwest/endangered/section7/s7process/plants/epfos7guide.html Species profile: https://ecos.fws.gov/ecp/species/601 Species survey guidelines: https://ecos.fws.gov/ipac/guideline/survey/population/984/office/31131.pdf	Threatened

Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

United States Department of the Interior

FISH AND WILDLIFE SERVICE
Chicago Ecological Service Field Office
U.s. Fish And Wildlife Service Chicago Ecological Services Office
230 South Dearborn St., Suite 2938
Chicago, IL 60604-1507
Phone: (312) 216-4720 Fax:
http://www.fws.gov/midwest/endangered/section7/s7process/7a2process.html

In Reply Refer To: March 21, 2019
Consultation Code: 03E13000-2019-SLI-0180
Event Code: 03E13000-2019-E-00470
Project Name: Orchard Rd from Jericho Rd to US 30, Montgomery/Aurora, Kane Co, seq. 22205 - 2 of 2 projects

Subject: List of threatened and endangered species that may occur in your proposed project location, and/or may be affected by your proposed project

To Whom It May Concern:

To Whom It May Concern:

The attached species list identifies any federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Please note! For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

For all other projects, continue the Section 7 Consultation process by going to our Section 7 Technical Assistance website at http://www.fws.gov/midwest/endangered/section7/s7process/index.html. If you are familiar with this website, you may want to go to Step 2 of the Section 7 Consultation process at http://www.fws.gov/midwest/endangered/section7/s7process/step2.html.

Under 50 CFR 402.12(e) (the regulations that implement Section 7 of the Endangered Species Act) the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally. You may verify the list by visiting the ECOS-IPaC website

http://ecos.fws.gov/ipac/ at regular intervals during project planning and implementation and completing the same process you used to receive the attached list. As an alternative, you may contact this Ecological Services Field Office for updates.

Although no longer protected under the Endangered Species Act, be aware that bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*), as are golden eagles. Projects affecting these species may require measures to avoid harming eagles or may require a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website at http://www.fws.gov/midwest/midwestbird/EaglePermits/index.html to help you determine if you can avoid impacting eagles or if a permit may be necessary.

We appreciate your concern for threatened and endangered species. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List

Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Chicago Ecological Service Field Office
U.s. Fish And Wildlife Service Chicago Ecological Services Office
230 South Dearborn St., Suite 2938
Chicago, IL 60604-1507
(312) 216-4720

This project's location is within the jurisdiction of multiple offices. Expect additional species list documents from the following office, and expect that the species and critical habitats in each document reflect only those that fall in the office's jurisdiction:

Illinois-Iowa Ecological Services Field Office
Illinois & Iowa Ecological Services Field Office
1511 47th Ave
Moline, IL 61265-7022
(309) 757-5800

Project Summary

Consultation Code: 03E13000-2019-SLI-0180

Event Code: 03E13000-2019-E-00470

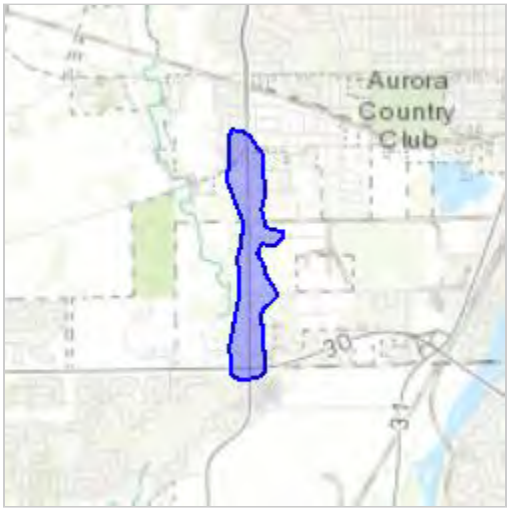
Project Name: Orchard Rd from Jericho Rd to US 30, Montgomery/Aurora, Kane Co, seq. 22205 - 2 of 2 projects

Project Type: TRANSPORTATION

Project Description: Safety improvements and ADA, improvements. Total new ROW for both projects 0.3 ac, no instream work, no tree removal. Land use mix of urban and rural. Unknown construction date.

Project Location:

Approximate location of the project can be viewed in Google Maps: https://www.google.com/maps/place/41.73365958242697N88.37549318758244W



Counties: Kane, IL | Kendall, IL

Endangered Species Act Species

There is a total of 2 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries[1], as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

---

1. NOAA Fisheries, also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

Mammals

NAME	STATUS
Northern Long-eared Bat *Myotis septentrionalis* No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/9045	Threatened

Flowering Plants

NAME	STATUS
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Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

CULTURAL CLEARANCE

Illinois Department of Transportation

Memorandum

To:	Bureau of Local Roads	Attn: William Raffensperger
From:	Jack Elston	By: Brad Koldehoff
Subject:	Cultural Resources - No Historic Properties Affected Clearance	
Date:	December 6, 2019	

Kane County
FAP 363 FAU 3887, FAP 336, IL 31, Fabyan Parkway at Batavia Avenue, Orchard Roadad Batavia, Geneva, Montgomery, Unincorporated Kane County
Sec. 18-00493-00-SP
Seq. 22205

For the above referenced undertaking, IDOT's qualified Cultural Resources staff hereby make a **"No Historic Properties Affected"** finding pursuant to Section 106 of the National Historic Preservation Act.

This finding concludes the Section 106 process in accordance with the stipulations of the Programmatic Agreement Regarding Section 106 Implementation for Federal-Aid Transportation Projects in the State of Illinois, executed March 6, 2018 by FHWA, Illinois SHPO, IDOT and the Advisory Council on Historic Preservation.

No further cultural resources coordination is required for this undertaking, unless design modifications or new information indicate that historic properties may be affected. If so, then, additional coordination with my office is required.

Brad H. Koldehoff
Cultural Resources Unit Chief
Bureau of Design & Environment

LETTER OF NO WETLAND IMPACT

KANE COUNTY
DIVISION OF TRANSPORTATION

Carl Schoedel, P.E.
Director of Transportation
County Engineer

41W011 Burlington Road
St. Charles, IL 60175
Phone: (630) 584-1170
Fax: (630) 584-5265

March 14, 2019

Illinois Department of Transportation
Bureau of Design and Environment
2300 South Dirksen Parkway, Room 330
Springfield, Illinois 62764

Attn: Susan Dees Hargrove
Biological Resources Specialist

Re: Wetland Impact Evaluation Request
State Route IL 31 at Fabyan Parkway and Orchard Road from Jericho Road to US Route 30
Highway Safety Improvement Project
IDOT District One - Kane County, Illinois
Section No. 18-00493-00-SP
ESR Seq. No 22205

Dear Ms. Hargrove,

The purpose of this letter is to certify that no wetlands will be impacted by the proposed Kane County 2018 Highway Safety Improvement Program project. The project involves safety improvements at the intersection of Fabyan Parkway and IL Route 31 (Batavia Avenue) in Batavia, IL and safety improvements along Orchard Road from Jericho Road to US Route 30 (Baseline Road) in Montgomery, IL. The scope of the proposed work includes adjustment of signal heads, application of wet reflective pavement markings, installation of advanced warning flashers, implementation of a dilemma zone detection system and ADA ramp improvements. Excavation is not anticipated within 250 ft. of the wetlands outlined in the attached TREC Report.

If you have any questions or require additional information, please contact Mike Zakosek at our office at (630) 584-1170.

Sincerely,

Carl Schoedel, P.E.
Director of Transportation/County Engineer

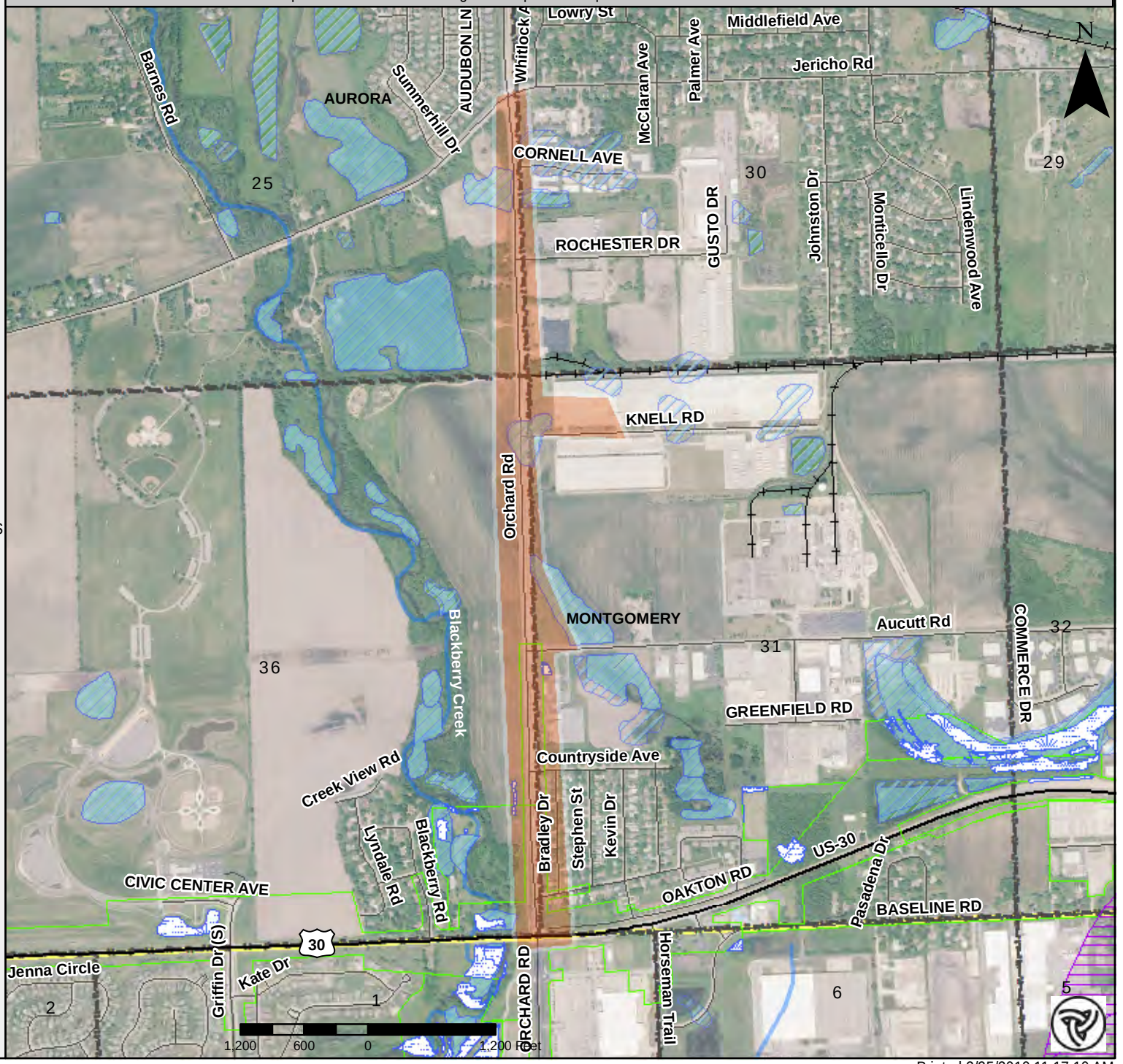
Transportation Review for Ecological Compliance: Report of Possible Resource Conflicts

Sequence #: 22205(2)

Orchard Rd from Jericho Rd to US 30
Resource in Vicinity of Project Polygon
*Ducks Unlimited Wetlands
*INHS Wetland
*National Wetlands Inventory
INAI & NP w/in 1 mile
*none found

No Resource Found
*INAI
*T&E
*Nature Preserve
*Roadside Prairie Inventory

County: KANE
Section(PLSS): 3 38N8E30
Area: -0.14583 sq. miles = -93.33286 acres
Report created by Susan Dees Hargrove



Threatened & Endangered Species (T&E)
Nature Preserve (NP)
Illinois Natural Areas Inventory (INAI)
Wetlands
INHS Wetland
Roadside Prairie Inventory

ILLINOIS DEPARTMENT OF NATURAL RESOURCES

Include as additional documentation with permit applications (USACE).

Printed 2/25/2019 11:17:10 AM

SPECIAL WASTE (PESA) CLEARANCE

Illinois Department of Transportation

Memorandum

To:	Christopher Holt	Attn:	Irma Romiti-Johnson
From:	Jack A. Elston	By:	Scott E. Stitt
Subject:	PESA Review		*Scott E. Stitt*
Date:	August 9, 2019		

Project:	FAP 363 at FAU 3887, FAP 336 (IL 31)		
District 1:	Kane County	Job #:	Not provided
Requesting Agency:	Kane Co Highways	Contract #:	Not provided
Survey Target Date:	08/20/2019	Anticipated DA:	08/21/2019
Anticipated Letting:	Not provided	Section:	18-00493-00-SP
BDE Sequence #:	22205	ISGS PESA #:	3721

Attached is a copy of the Preliminary Environmental Site Assessment (PESA) report prepared by the Illinois State Geological Survey (ISGS) for the subject project as described in your Special Waste Environmental Survey Request (ESR). Table 1 identifies sites along the project route that were determined to contain recognized environmental conditions (RECs). It is the opinion of this office, in consultation with the Chief Counsel's Office, that a preliminary site investigation (PSI) is required if any site identified in Table 1 of the PESA report involves any of the following situations:

- New right of way or easement (temporary or permanent);
- Railroad right-of-way, other than single rail rural with no maintenance facilities; or
- Building demolition / modification.

Additionally, a PSI is required if the project will have excavation or subsurface utility relocation on existing right-of-way adjoining a site identified in Table 1 of the PESA report.

If the district determines that they can avoid all the sites that contain RECs, then a PSI is not required and the project will be in compliance with Departmental Policy D&E-11. If the district determines that the project will involve a site containing a REC(s), then a PSI is required and the statewide special waste consultant should be requested to perform the PSI. Please notify this office of any actions you may decide to take concerning these sites (avoidance or further investigation). The PESA Response form can be found on PMA.

The District's Bureau of Land Acquisition (DBLA) should determine if any new right-of-way or easement will involve any site identified in Table 1 or any site adjoining a site listed in Table 4 of the PESA report. On those identified situations, DBLA shall coordinate the acquisition with this office, Central Bureau of Land Acquisition, and the Chief Counsel's Office to determine if an "All Appropriate Inquiries" (AAI) assessment is required for additional liability protection under CERCLA.

Other findings and recommendations of the report should be carefully considered. If you have any questions regarding this letter report, please contact Josh Venaas at 217/785-4181 or James R. Curtis at 217/558-4653.

Attachments

cc: Office of Chief Counsel – Rm. 313 District Utility Coordinator
District Bureau of Land Acquisition

PESA

Submittal Date: **Sequence No:** 22205

District: 1 **Requesting Agency:** Local Kane Co Hwys **Project No:**

Contract #: **Job No.:**

Counties: Kane

Route: FAP 363 at FAU 3887, FAP 336 **Marked:** IL 31

Street: Fabyan Pkwy at Batavia Ave, Orchard Road **Section:** 18-00493-00-SP

Municipality(ies): Batavia, Geneva, Montgomery, Unincorpora **Project Length:** 3.2187 **km** 2 **miles**

FromTo (At): Fabyan Pkwy at IL 31, Orchard Rd from Jericho Rd to US 30

Quadrangle: North Aurora, Yorkville **Township-Range-Section:** 39N-8E-15, 38N-7E-36, 38N-7E-25

Survey Target Date: 08/20/2019 **Anticipated DA:** 08/21/2019 **Cleared for DA:** 08/09/2019

Anticipated Letting Date: **Cleared for Letting:** ☐ **Target Memo** **Antic. Process.** CE

☐ **Validation**

Sent for Survey	PESA Report Received	District Notified	Letter Report Received	District Notified	PESA Number	Risk Level	Survey Results to Fire Marshal	Survey Results to IEPA	PESA Response Date
02/26/2019	08/06/2019	08/09/2019			3721				

☐ **Waiver** **PESA Number:** 3721 **Waiver Request Approved:**

Comments:

LOCAL AGENCY PESA EXECUTIVE SUMMARY

Huff & Huff incorporated

A Subsidiary of GZA

Preliminary Environmental Site Assessment

FABYAN PARKWAY HSIP
FROM RIVER ROCK ROAD TO FOX RIVER
BATAVIA, KANE COUNTY, ILLINOIS

June 21, 2019
81.0220635.00

PREPARED FOR:
Peralte - Clark, LLC
171 West Wing Street, Suite 204B
Arlington Heights, Illinois, 60005

Huff & Huff, A Subsidiary of GZA
915 Harger Road | Oak Brook, IL 60523
630-648-9100

GZA has 31 Offices Nationwide
www.huffnhuff.com www.gza.com

Huff & Huff incorporated
A Subsidiary of GZA

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WATER
CONSTRUCTION MANAGEMENT

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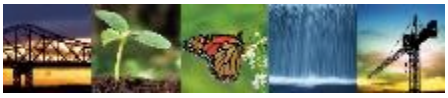

June 21, 2019
File No. 81.02206235.00

John Clark, P.E.
Vice President
Peralte - Clark, LLC
171 West Wing Street, Suite 204B
Arlington Heights, IL 60005

Re: Preliminary Environmental Site Assessment Report
Fabyan Parkway HSIP
Batavia, Kane County, Illinois

Dear Mr. Clark:

Huff & Huff Inc., a subsidiary of GZA GeoEnvironmental, Inc. (GZA), is pleased to present the attached Preliminary Environmental Site Assessment Report ("Report") for the proposed Fabyan Parkway Highway Safety Improvement Project from River Road to the Fox River in Batavia, Kane County, Illinois. Please note that the portions of the planned improvements within the IL Route 31 right-of-way were not investigated as part of this project. It is anticipated that the Illinois Department of Transportation will request the Illinois State Geological Survey to complete work within the areas of IDOT ROW.

A total of six (6) potentially impacted properties were identified during this assessment; however, one site (the Fox River) is considered avoided as no in-stream work is planned as part of this project. As potential impacts to groundwater and soils exist, a series of soil borings through a Preliminary Site Investigation (PSI) is recommended along the Project Corridor to specifically address the five (5) PIPs not considered avoided and to document the soil conditions from an environmental perspective. The information obtained from soil sampling will also aid in developing potential pay items for soil management within areas of anticipated excavation.

In the areas with identified PIPs, testing will be necessary for the earth excavation and off-site disposal of soil spoils to be considered acceptable for CCDD or soil-only facilities. The remaining soil from non-PIP areas along the Project Corridor is expected to be acceptable for a CCDD or soil-only facility without further analytical, except for soil pH testing to satisfy the acceptance criteria. However, it should be noted that since 2017, some CCDD facilities are not accepting the LPC-662 (soil pH sampling only) approach and in many cases also include mandatory minimum sampling requirements regardless of the status of due diligence.

We appreciate the opportunity to provide services for this project. Please feel free to contact us with any questions.
Very truly yours,

Huff & Huff, Inc.

Tim Huff, AICP
Project Manager

Shane Cuplin, P.G.
Senior Project Manager

Jeremy J. Reynolds, P.G
Associate Principal
Attachments: Fabyan HSIP PESA Report

EXECUTIVE SUMMARY

This preliminary environmental site assessment (PESA) identifies man-made hazards that may be encountered within the proposed project area of the proposed Fabyan Parkway Highway Safety Improvement Project from River Rock Road to the Fox River (Project Corridor) in Batavia, Kane County, Illinois (Township 39N, Range 8E, Section 15). The Project Corridor is approximately 0.5 miles in length. Please note that the portions of the planned improvements within the IL Route 31 right-of-way were not investigated as part of this project. It is anticipated that the Illinois Department of Transportation (IDOT) will request the Illinois State Geological Survey (ISGS) to complete work within the areas of IDOT ROW.

The screening process, used to identify sites that may pose a hazard to the Project Corridor, included a historical review, database search, review of other applicable information, and site reconnaissance. Historical resources included historical aerial photos and historical topographic maps of the Project Corridor. Both were reviewed for evidence of former sites that may pose a hazard to the Project Corridor. The database search provided information (on a local, state, or federal level) on properties that may pose a hazard to the Project Corridor. Information not provided in the database search, such as water quality data, solid waste disposal sites, and the national pipeline mapping system was also reviewed with regards to the Project Corridor. Site reconnaissance was conducted on May 31, 2019 to inspect the sites identified through the screening process, and to also identify additional sites adjacent to the Project Corridor with storage areas, spills, staining, or other indications of potential environmental concern.

Sites identified through the screening process were then further reviewed to determine their status as a potentially impacted property (PIP) in connection to the Project Corridor. The following tables (Tables ES-1 through ES-4) summarize these sites. Based on the information presented in this PESA and data collected during the screening process, this assessment has revealed evidence of six (6) potentially impacted properties (PIPs) in connection to the Project Corridor.

A total of six (6) potentially impacted properties were identified during this assessment; however, one site (the Fox River) is considered avoided as no in-stream work is planned as part of this project. As potential impacts to groundwater and soils exist, a series of soil borings through a Preliminary Site Investigation (PSI) is recommended along the Project Corridor to specifically address the five (5) PIPs not considered avoided and to document the soil conditions from an environmental perspective. The PIPs identified are listed in Table ES-1.

Table ES-1 Summary of Sites Determined to be PIPS

Site ID	Site Name	Address	Reason(s)
2	Covenant Retirement Communities	700 Fabyan Pkwy	UST
4	RJ Ward & Company, Landmark Communicatns [*sic*] Inc	901 N. Batavia Ave	RCRA, LUST, SPILLS, UST
5	Michaelsen Health Ctr	831 Batavia Ave	UST
6	Open Space/Fox River Trail/Former Railroad	NA (Crosses Project Corridor West of Fox River)	Historic Railroad
7	Fabyan Forest Preserve/Former Railroad	1925 Batavia Ave	Historic Railroad
8	Fox River	NA	303(d) Listing

Table ES-2 Summary of Sites Identified Adjacent to the Project Corridor with De Minimis Conditions[1]

Site ID	Site Name	Address	Reason(s)
---	None Identified	None Identified	None Identified

[1]De minimis based on definition included in ASTM Standard E 1527-13

For the purposes of this report, the following are considered to be de minimis conditions:

- Transformers in normal use, unless the transformers were observed to be leaking, appear on an environmental regulatory list, or were otherwise determined to pose a hazard not related to normal use
- Lead-based paint
- Asbestos-Containing Material (ACM)
- Pesticides/herbicides used on farmland
- Soil Piles
- Radon and Biological Hazards

The conditions listed above were considered while developing the report. However, as sites with these conditions are seen often, each site was not specifically referenced as de minimis based on these conditions alone.

Table ES-3 Summary of Sites Identified Adjacent to the Project Corridor with No Status

Site ID	Site Name	Address	Reason(s)
1	Residence	Various	Site Reconnaissance
3	Fabyan Medical Plaza	725 Fabyan Pkwy	Site Reconnaissance

Table ES-4 Summary of Sites Identified Off the Project Corridor with No Status[1]

Site ID	Site Name	Address	Database	Reason
---	None Identified	None Identified	None Identified	---

[1]Only sites identified off the Project Corridor, via the screening process, are included.

Huff & Huff incorporated

A Subsidiary of GZA

Preliminary Environmental Site Assessment

ORCHARD ROAD IMPROVEMENTS FROM JERICHO ROAD TO US 30 MONTGOMERY AND UNINCORPORATED KANE COUNTY, ILLINOIS

June 21, 2019
81.0220635.00

PREPARED FOR:
Peralte - Clark, LLC
171 West Wing Street, Suite 204B
Arlington Heights, Illinois, 60005

Huff & Huff, A Subsidiary of GZA
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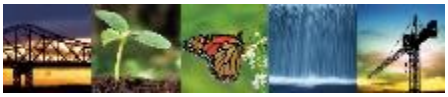


June 21, 2019
File No. 81.0220635.00

John Clark, P.E.
Vice President
Peralte - Clark, LLC
171 West Wing Street, Suite 204B
Arlington Heights, IL 60005

Re: Preliminary Environmental Site Assessment Report
Orchard Road HSIP
Montgomery and unincorporated Kane County, Illinois

Dear Mr. Clark:

Huff & Huff Inc., a subsidiary of GZA GeoEnvironmental, Inc. (GZA), is pleased to present the attached Preliminary Environmental Site Assessment Report ("Report") for the proposed Orchard Road Highway Safety Improvement Project from US 30 to Jericho Road in Montgomery and unincorporated Kane County, Illinois.

A total of eight (8) potentially impacted properties were identified during this assessment. As potential impacts to groundwater and soils exist, a series of soil borings through a Preliminary Site Investigation (PSI) is recommended along the Project Corridor to specifically address the eight (8) PIPs and to document the soil conditions from an environmental perspective. The information obtained from soil sampling will also aid in developing potential pay items for soil management within areas of anticipated excavation.

In the areas with identified PIPs, testing will be necessary for the earth excavation and off-site disposal of soil spoils to be considered acceptable for CCDD or soil-only facilities. The remaining soil from non-PIP areas along the Project Corridor is expected to be acceptable for a CCDD or soil-only facility without further analytical, except for soil pH testing to satisfy the acceptance criteria. However, it should be noted that since 2017, some CCDD facilities are not accepting the LPC-662 (soil pH sampling only) approach and in many cases also include mandatory minimum sampling requirements regardless of the status of due diligence.

We appreciate the opportunity to provide services for this project. Please feel free to contact us with any questions.
Very truly yours,

Huff & Huff, Inc.

Tim Huff, AICP
Project Manager

Shane Cuplin, P.G.
Senior Project Manager

Jeremy J. Reynolds, P.G
Associate Principal

Attachments: Orchard Road HSIP PESA Report

EXECUTIVE SUMMARY

This preliminary environmental site assessment (PESA) identifies man-made hazards that may be encountered within the proposed project area of the Orchard Road Highway Safety Improvement Project from US 30 to Jericho Road (Project Corridor) in Montgomery and unincorporated Kane County, Illinois. The Project Corridor is approximately 1.5 miles in length.

The screening process, used to identify sites that may pose a hazard to the Project Corridor, included a historical review, database search, review of other applicable information, and site reconnaissance. Historical resources included historical aerial photos and historical topographic maps of the Project Corridor. Both were reviewed for evidence of former sites that may pose a hazard to the Project Corridor. The database search provided information (on a local, state, or federal level) on properties that may pose a hazard to the Project Corridor. Information not provided in the database search, such as water quality data, solid waste disposal sites, and the national pipeline mapping system was also reviewed with regards to the Project Corridor. Site reconnaissance was conducted on May 31, 2019 to inspect the sites identified through the screening process, and to also identify additional sites adjacent to the Project Corridor with storage areas, spills, staining, or other indications of potential environmental concern.

Sites identified through the screening process were then further reviewed to determine their status as a potentially impacted property (PIP) in connection to the Project Corridor. The following tables (Tables ES-1 through ES-4) summarize these sites. Based on the information presented in this PESA and data collected during the screening process, this assessment has revealed evidence of eight (8) potentially impacted properties (PIPs) in connection to the Project Corridor.

A total of eight (8) potentially impacted properties were identified during this assessment. As potential impacts to groundwater and soils exist, a series of soil borings through a Preliminary Site Investigation (PSI) is recommended along the Project Corridor to specifically address the eight (8) PIPs and to document the soil conditions from an environmental perspective. The PIPs identified are listed in Table ES-1.

Table ES-1 Summary of Sites Determined to be PIPS

Site ID	Site Name	Address	Reason(s)
5	Unknown (Roadside Spill)	Orchard Road & Countryside	SPILLS
7	Agricultural Field / Chicago Burlington and Quincy RR NIPC 327	NA	NIPC
8	Orchard Property 2, LLC / BP	1700 Orchard Rd	UST, Gas Station
10	Agricultural Field / Chicago Burlington and Quincy RR NIPC 327	NA	NIPC
12	Former Railroad Spur	RR Crossing Near Central Portion of Project Corridor	Historic Railroad
13	Transformers	NA	Potential for PCBs
14	Michels Corporation / Steiner Company Inc	1266 Orchard Rd	RCRA
17	Superior Beverage	1070 Orchard Rd	AST

Table ES-2 Summary of Sites Identified Adjacent to the Project Corridor with De Minimis Conditions[1]

Site ID	Site Name	Address	Reason(s)
9	Agricultural Field / Open Space	NA	Potential chemical use associated with farming activities

[1]De minimis based on definition included in ASTM Standard E 1527-13

For the purposes of this report, the following are considered to be de minimis conditions:

- Transformers in normal use, unless the transformers were observed to be leaking, appear on an environmental regulatory list, or were otherwise determined to pose a hazard not related to normal use
- Lead-based paint
- Asbestos-Containing Material (ACM)
- Pesticides/herbicides used on farmland
- Soil Piles
- Radon and Biological Hazards

The conditions listed above were considered while developing the report. However, as sites with these conditions are seen often, each site was not specifically referenced as de minimis based on these conditions alone.

Table ES-3 Summary of Sites Identified Adjacent to the Project Corridor with No Status

Site ID	Site Name	Address	Reason(s)
1	Mattress Firm	2007 Orchard Road	Site Reconnaissance
2	Western Union	1991 US 30	Site Reconnaissance
3	Open Space	NA	Site Reconnaissance
4	Residential	2330 Brentwood Ave, 1829-1939 Bradley Dr	Site Reconnaissance
6	Newly Weds Foods	1750 Orchard Rd	Site Reconnaissance
11	United Facilities	1390 Orchard Rd	Site Reconnaissance
15	Open Space	NA	Site Reconnaissance
16	Orchard Road Animal Hospital	1175 Orchard Rd	Site Reconnaissance
18	Open Space	NA	Site Reconnaissance
19	Canine Physical Rehab, See Spot Swim, Spillane Fire Protection, Horse Guard, Ideal Auto Sales	2214 Cornell Ave	Site Reconnaissance
20	Crown Trophy, Ramp Now, Liquidation Warehouse, Brian & Sons, Citron	2282 Cornell Ave	Site Reconnaissance
21	Residential	2270-2274 Jericho Road	Site Reconnaissance
22	Residence	2350 Jericho Road	Site Reconnaissance
23	Residential	934 Audubon Lane	Site Reconnaissance
24	Orchard Valley Baptist Church	2301 Jericho Road	Site Reconnaissance

Table ES-4 Summary of Sites Identified Off the Project Corridor with No Status[1]

Site ID	Site Name	Address	Database	Reason
A	Jericho Lake / Former Gravel Pit	NA	Former Gravel Pit	Separation Distance (500 ft)
B	Rochester Midland Corp	2200 Rochester Dr	UST, RCRA, SSTS	Separation Distance (500 ft)

[1]Only sites identified off the Project Corridor, via the screening process, are included.

Attachment 5:
Coordination/Documentation

Peralte-Clark LLC

IDOT BLR KICK-OFF MEETING MINUTES 11/29/2018

Peralte-Clark LLC

Meeting Minutes

PROJECT:	**KANE COUNTY HSIP**
MEETING DATE/TIME:	**Thursday, November 29, 2018 @ 10:00am**
MEETING LOCATION:	**IDOT District 1 Office**
MEETING PURPOSE:	**IDOT BLR Coordination**
ATTENDEES:	**See attached agenda**

Peralte-Clark Project #18-0007-01

ITEM#		**Action (Due Date)**
1.0	The meeting attendees provided introductions and noted their respective roles, responsibilities and the firm or agency they represent. A copy of the meeting attendance roster and agenda is attached for reference.	N/A
2.0	An overview of the project was discussed. Pavement striping improvements and flashing left turn arrows on the Orchard Road corridor (non-IDOT jurisdiction). Dilemma zone detection installation, advanced warning flashers (for Fabyan Parkway), and ADA improvements at the IL Route 31 and Fabyan Parkway intersection (IDOT jurisdiction). IDOT is currently not interested in installing flashing left turn arrows at IL 31 and Fabyan Parkway. KDOT agreed to install reflectorized backplates for the signal at this intersection.	N/A
3.0	P-C/KDOT inquired IDOT if any planned improvements were scheduled or anticipated within project limits. IDOT to follow up.	IDOT
4.0	Kane County has obtained HSIP funding for this project. HSIP funding may only be used for specific scope items that are spelled out in the approval letter. If there is are changes in that scope that desire HSIP funding, KDOT would need to resubmit the application.	N/A
5.0	This project anticipates no excavation outside of IDOT or KDOT right-of-way. KDOT to provide P-C of delineation of KDOT/IDOT right-of-way through IL Route 31 and Fabyan Parkway intersection. KDOT also to provide a letter confirming no wetlands within project limits.	KDOT
6.0	The ESR submittal process was discussed. IDOT provided ESR Submittal checklist (attached) and stated that biological, cultural, and special waste screenings would be required. The following items were also noted concerning the ESR: • In addition to hard copies, electronic copies should be provided to IDOT BLR on CDs. • Google Streetview can be used in lieu of site photos, P-C does not have to submit photos of potentially impacted properties that are older than 40 yrs.	P-C
7.0	Separate Special Waste PESA's are required for IDOT and Kane County jurisdictions.	P-C

8.0	IDOT BLR agreed that the Phase I would be processed as a State Approved Categorical Exclusion. IDOT BLR provided P-C with a State Approved CE with no report procedures checklist (attached). The "report" will consist of BLR 19100 form with the required attachments and supporting documentation.	N/A
9.0	The following items were discussed and agreed upon: • No public involvement is anticipated with this project. • No FHWA Coordination is required. • No detours are anticipated with the scoped improvements. • No proposed right-of-way nor any easements are anticipated. • No major utility conflicts are anticipated. • The project locations are not located within the floodplain.	N/A
10.0	IDOT BLR does not require ADA compliance to be achieved based on the existing scope (no intersection resurfacing currently anticipated); however, KDOT wishes to still upgrade the existing pedestrian accommodations. IDOT will review compliance for any shared-use path (SE corner of IL 31 and Fabyan) improvements.	N/A
11.0	P-C will send project location maps to IDOT to obtain available crash data and crash diagrams. IDOT will confirm that there are no 5% crash locations within project limits.	P-C/IDOT
12.0	IDOT stated that they do have a dilemma zone detection system installed at the IL Route 53 and Manhattan Road intersection. If the dilemma zone detection system is installed at the IL 31 and Fabyan intersection that were to impact green times, then a timing analysis would need to be completed to determine traffic impacts from the altered green times.	P-C
13.0	IDOT BLR requested 6 printed sets of the Phase I Report. IDOT anticipated a 4-week review period for the Phase I submittal.	P-C

The meeting adjourned at 11:10 a.m.

The above constitutes the author's understanding of items discussed and conclusions reached. Participants are requested to notify the author of any errors or omissions within five (5) days of the issued date.

Author: John A. Clark, P.E., ENV SP

Submitted by:

John A. Clark, P.E., ENV SP
Project Manager

cc: Attendees
Peralte-Clark: File 18-0007-01

Illinois Department of Transportation

Attendance Roster

Bureau: Local Roads and Streets **Section:** 18-00493-00-SP

Project/Topic: Kane County; Phase 1 KO Meeting for 2 HSIP combining into one Project; Fabyan Pkwy. at IL Route 31 and Orchard Rd. at Rochester and Aucutt Roads

Date: Thursday, November 29, 2018

Time: 10:00 AM

Location: Local Roads Conference Room A

	Attendees	Representing	Phone Number	Email Address
1.	Marilin Solomon MS	IDOT – D1 – BLRS	(847) 705 - 4407	Marilin. Solomon@illinois.gov
2.	Moe Kawash MK	IDOT – D1 – BLRS	(847) 705 - 4205	Mohammad.Kawash@illinois.gov
3.	Jonathan M. Lloyd JL	IDOT – D1 – Traffic	(847) 705 - 4135	Jonathan.Lloyd@illinois.gov
4.	Jason R. Salley JS	IDOT – D1 – Geometrics	(847) 705 - 4085	Jason.Salley@illinois.gov
5.	Jackie Forbes	KDOT	630-444-3142	forbesjackie@co.kane.il.us
6.	John Clark	Peralte-Clark, LLC	312-965-9845	john.clark@peralte-clark.com
7.	MATT TURK	PERALTE - CLARK, LLC	224-577-9028	matt.turk@peralte-clark.com
8.	MIKE ZAKOSEK	KDOT	630-584-1170	ZAKOSEKMIKE@CO.KANE.IL.US
9.				
10.				
11.				
12.				
13.				
14.				
15.				

KDOT 2018 HSIP APPLICATION AND APPROVAL MEMO

Illinois Department of Transportation
2300 South Dirksen Parkway / Springfield, Illinois / 62764

July 27, 2017

Mr. Carl Schoedel
Kane County Engineer
41W011 Burlington Road
St. Charles, IL 60175

Dear Mr. Schoedel,

The Illinois Department of Transportation is pleased to inform you that your project has been selected for local Highway Safety Improvement Program (HSIP) funding. The project includes the placement of signal heads at the center of each lane, yellow flashing arrows, the installation of advanced warning flashers with a wig-wag light, and the installation of a Dilemma Zone Detection System at the intersection of Fabyan Parkway and Illinois Route 31. Please note the Department will not approve HSIP funding for any work completed on the IDOT legs of the intersection. This project will be identified by the Department as HSIP # 201712031. Congratulations on your successful application.

The federal HSIP commitment for this project will not exceed $605,273. The deadline for this award to be federally authorized is July 1, 2020 or funds will be rescinded.

Please contact Mr. Christopher Holt, District 1 Local Roads Engineer by telephone at (847) 705-4201 to discuss program requirements and preparation of any agreements and / or contracts. Projects located within a Metropolitan Planning Organization (MPO) planning boundary are required to be listed in the local MPO's Transportation Improvement Program (TIP). Questions regarding the HSIP may be directed to Ms. Melinda Kos in the Central Bureau of Local Roads and Streets by telephone at (217) 785-5178.

All HSIP grant recipients must be registered with the State of Illinois in order to comply with the Grant Accountability and Transparency Act (GATA) 30 ILCS 708. Additional pre-award paperwork is also required, including: (1) a Notice of State Award (NOSA); and (2) a Uniform Grant Budget. The Illinois Department of Transportation will be contacting you shortly to accept or deny the terms of your NOSA. The Uniform Grant Budget Form will be sent directly to your email and must be returned to Melinda Kos at Melinda.Kos@illinois.gov by August 24, 2017. You may also send inquiries to DOT.GATA@illinois.gov for further assistance.

Sincerely,

Maureen E Kastl

Maureen E. Kastl, P.E.
Engineer of Local Roads and Streets

cc: Alan Ho, FHWA – Illinois Division
Paul Lorton, IDOT – Bureau of Safety Programs and Engineering
Christopher Holt, IDOT District 1 Local Roads Engineer
File

KANE COUNTY
DIVISION of TRANSPORTATION

Carl Schoedel, P.E.
Director of Transportation
County Engineer

41W011 Burlington Road
St. Charles, IL 60175
Phone: (630) 584-1170
Fax: (630) 584-5265

MEMORANDUM

DATE: June 2, 2017

TO: IDOT District 1 Bureau of Local Roads and Streets

FROM: Carl Schoedel

RE: 2017 Highway Safety Improvement Program Call for Applications

Dear Sir or Madam:

Please find our completed application package for Fabyan Parkway at Illinois Route 31 which includes the HSIP Candidate Application Form, Benefit/Cost Ratio form, project location map, project photographs, estimated cost breakdown, project timeline, project narrative, uniform application for state grant assistance, and the programmatic risk assessment questionnaire as well as crash data from the past five years.

Thank you for considering our application.

Illinois Department of Transportation

HSIP Candidate Form

FY

ID:	**Contract:**	**Award Date:**	**Completion Date:**
District: 1	**County:** Kane		**City:**
Key route: Fabyan Parkway	**Marked route:** CH #8		
Road Name: Fabyan Parkway	**Intersecting Roadway:** IL Route 31 N/A		☐
Length: ☒ N/A		**Mile station:** to	

Location Description: Fabyan Parkway at the intersection of IL Route 31.

☐ **Rural**	☒ **Urban**	**Lanes:** 4	
AADT(Segment):	**Total Entering AADT (Intersection): 26,667-31,945**		**Speed Limit:** 40-45 mph
Friction Test Results:	☐ N/A	**Lighting Present:** ☒ Y ☐ N	

CHSP Emphasis Area(s): ☐ District Documentation ☐ Systematic Improvements ☐ N/A

Peer Group: ☐ N/A

Other:

Crashes Details

Year	Total Crashes	Fatal Crashes	Fatalities	A-Injury Crashes	A-Injuries	B-Injury Crashes	B-Injuries	C-Injury Crashes	C-Injuries	PDO	Wet-Weather Crashes	Darkness (Not lighted) Crashes
2011	16	0	0	0	0	4	4	4	8	8	3	0
2012	23	0	0	1	2	4	7	4	5	14	7	2
2013	21	0	0	3	4	2	3	3	4	13	10	0
2014	20	0	0	2	2	2	3	1	1	15	8	0
2015	18	1	1	1	2	2	5	2	3	12	7	0
Total	98	1	1	7	10	14	22	14	21	62	35	2

Location Description: Fabyan Parkway, which is a prinicipal arterial, at the intersection of IL Route 31 which is a minor arterial. This intersection is next to residential, commercial, and forest preserve land.

Problem Description: There has been a fatal crash and several A Injury Crashes at this intersection, primarily because of the offset intersection. There are many turning and rear end type crashes.

Previous Safety Improvements: Signage advising drivers that a left turn on green is permitted. Directional arrrows on eastbound and westbound Fabyan approaching IL Route 31.

Collision Diagram: ☐ Y ☒ N **Images:** ☒ Y ☐ N

Predominant Crash Types: The predominant crash types are turning and rear-end.

Proposed Improvement(s): Placing signal heads at the center of each lane; utilzing flashing yellow arrow signal; installing an advanced warning flasher with a wig-wag light, over each leg of Fabyan Parkway; Installing a Dilemma Zone Detection System

Estimated Project Cost ($000's): $673 **Benefit-Cost Ratio:** 21.49

Local Projects:

Annual Fatal Crash Rate (Fatal Crashes/100 Miles): **Annual A-Injury Crash Rate (A-Injury Crashes/100 Miles):**

Local Roads Rural Functional Class: Other Principal Arterial

Approved: **Central HSIP Approval Date:**

Signed:

State Safety Engineer

Funding: ☒ HSIP ☐ HRRR ☐ RAIL

Comment:

Distribution: ☐ OPP ☐ District ☐ BSPE ☐ LRS ☐ BDE

PROJECT DESCRIPTION - PROJECT DATA INPUT (INTERSECTIONS)

Project:	Illinois Route 31 and Fabyan Parkway				*Prepared by:*		Stephen Zulkowski
District:	1	*County:*	Kane	*City:*	Batavia	*Date*	6/1/2017
Key Route:	Fabyan Parkway	*Marked Route:*	CH # 8	*MilePost:*		*Current AADT:* Major Street	29200
Location Description:	Intersection of Illinois Route 31 and Fabyan Parkway					Minor Street	13100
Crash data:	5 Years				*Traffic Growth factor:*		2.0%
	From 2011	to	2015		*Interest rate:*		4.0%

Control type

Peer Group 7 - Urban Signalized Intersection — 7 — Urban — Signal

PROCESS STEPS - Benefit Cost Calculations (INTERSECTIONS)

User will input data only in Highlighted Cells

Enter Project Description Data.
Select Peer Group
Input crash data for the analysis period based on crash severity by crash type **(Note: If the countermeasure selected does not affect all legs of the intersection then enter only crash data for the affected legs)**
Enter the list of potential countermeasures selected from the drop down menus***
Enter **"Unit Cost"** for the countermeasure selected
Update the **"Quantity"** for each countermeasure selected for cost calculations
(Example: If adding a Left Turn Lane is the selected countermeasure and applied to 2-Legs of the Intersection, then the "Quantity = 2")
Calculate cost of countermensures selected by clicking on the **"Cost Calculation"** button
The B/C will be reported in Cell E45 based on the analysis

INTERSECTION CRASH SEVERITY DISTRIBUTION BY CRASH TYPE FOR ANALYSIS PERIOD

	All Crashes	Angle	Animal	Fixed Object	Head On	Left Turn	Other Noncollision	Other Object	Overturned	Pedestrian	Pedalcyclist	Parked Vehicle	Rear End	Right Turn	Sideswipe Same Direction	Sideswipe Opposite Direction	Turning	Train	Night Time crash	Wet Pavement
	ALL	*AG*	*AN*	*FO*	*HO*	*LT*	*OtherNC*	*OtherO*	*OVT*	*PD*	*PDC*	*PKV*	*RE*	*RT*	*SSD*	*SOD*	*T*	*TR*	*NGT*	*WP*
Fatal Crashes	1																1			
A-Injury Crashes	7				1											1	5			
B-Injury Crashes	14	2							1				1				10			
C-Injury Crashes	14	1			2								7				4			
PDO Crashes	62	1		2	1		1					1	31		6		19			

INTERSECTION BENEFIT COST ANALYSIS

BENEFIT CALCULATIONS / COUNTERMEASURE COST CALCULATIONS

COUNTERMEASURE	CMF *	Crash Type affected by this improvement	Unit Cost	Quantity	Units	Total Cost	Service Life	Present Worth	EUAC **
1.4.11.I7.3 - Signalization - Add Mast Arms and Signal Head per Lane	0.75	RE,AG,LT,RT,T	250000	1	Unit Qnty	$250,000	15	$250,000	$18,395
Require new Mast arms and Signal heads all 4 legs									
1.4.14.AL.1 Protected LT only flashing yellow arrow (signalization)	0.30	LT,RE	25000	1	Per Signal Loc	$25,000	20	$25,000	$1,840
Install FYA (Protected/Permissive)									
1.4.4.I7.1 - Signing - Install Advance Signal Warning with Flashers	0.64, 0.38	RE= 0.64, AG (AG, RT, LT, T, SOD)= 0.38	108750	2	Unit Qnty	$217,500	6	$525,243	$38,648
One for EB, one for WB, in the advanced									
1.4.15.AL.1 CMF # 4857,4854 - Dilemma Zone Protection System	0.08	ALL=.08, AG (AG,RT,LT,T,SOD)=.43	41250	1	Per Signal Loc	$41,250	10	$69,117	$5,086
May include new Intersection Controller, Ensure compadibility with TOD Coord plans									
TOTAL BENEFIT	*$1,374,382*		*TOTAL COST*						*$63,969*

Cost Calculation

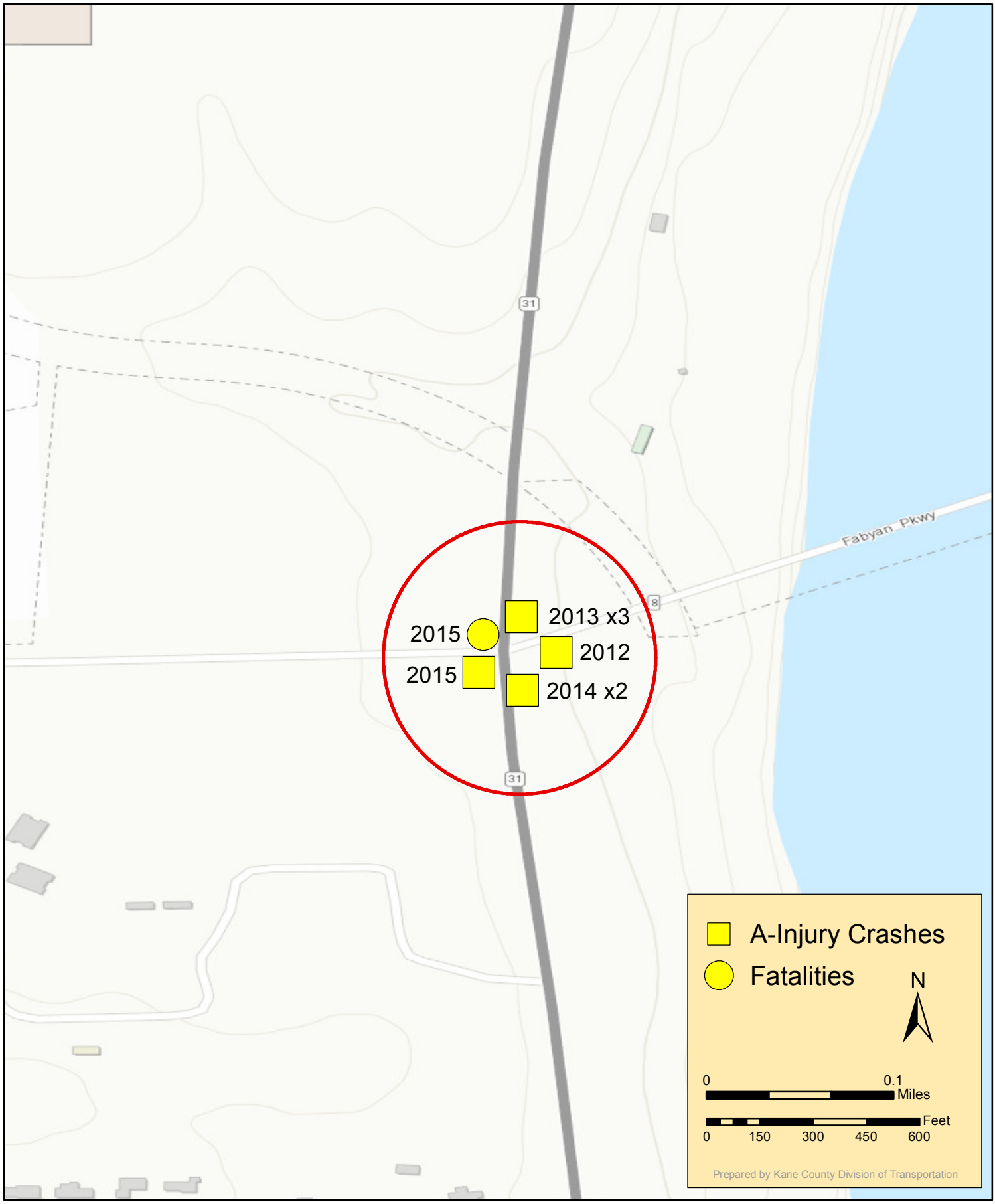
BENEFIT/ COST	21.49

NOTE: IF THE NUMBER OF LEGS AFFECTED VARIES BY COUNTERMEASURES SELECTED, THEN CALCULATE THE BENEFIT-COST RATIO FOR EACH COUNTERMEASURE SEPARATELY (Use separate spreadsheets for each countermeasure applied).

* CMF = Crash Modification Factor
** EUAC = Estimated Uniform Annual Cost

2017 HSIP Project Location

Fabyan Parkway at Route 31



Kane County HSIP Application
Fabyan Parkway at Illinois Route 31

These pictures from May 2017 demonstrate the offset intersection and approach from the west. The current signals, turn lanes, and proximity to forest preserve property is shown in the following photos.

Fabyan Parkway heading east approaching Il Route 31. Note how Fabyan curves to the left immediately following intersection.

Another view looking east on Fabyan, note the curve. Turning left from Fabyan to northbound Il Route 31 is where the majority of crashes occur.

Fabyan Parkway looking east. Note current safety measures on signals. Kane County Forest Preserve owns the property on the north and south corners of the east side of the intersection, which is seen here.

Fabyan parkway looking southeast.

Note signs advising left turn on green must yield

Fabyan Parkway approaching Route 31 from the east. This section is a bridge over the Fox River, which limits certain safety improvements on the east side of the intersection.

Example of directional arrows approaching Route 31

Fabyan parkway approaching Route 31 from the east. Note curve at intersection.

Another view of Fabyan Parkway westbound approaching Route 31

Kane County HSIP Application 2017
Project Cost Breakdown
Fabyan Parkway at Illinois Route 31

Phase	**Total**	**HSIP Funding**	**Local Share**
Phase 1 Engineering	$ 42,700	$ 38,430	$ 4,270
Phase 2 Engineering	$ 42,700	$ 38,430	$ 4,270
CE	$ 53,375	$ 48,038	$ 5,338
Construction	$ 533,750	$ 480,375	$ 53,375
	Total	$ 605,273	$ 67,253

	Includes
Mast arm signal head/lane	$ 250,000
flashing yellow arrrow	$ 25,000
advance signal warning	$ 217,500
dilemma zone protection system	$ 41,250
	$ 533,750

Total Project Cost $ 672,525

PROJECT MILESTONE SCHEDULE

Municipality:	Kane County DOT
Project:	Fabyan Parkway at IL 31
Scope of Work:	Intersection Improvement – HSIP application 6/17
TIP #:	
TIP Years (Ph II / Const):	
Section #:	
Last Constr & E3 Cost (Date):	
Current Constr & E2 Cost (Date):	$

Contact Information		
	Name	**Telephone**
Municipality		
Council/Liaison		
Consultant		
IDOT		

Date Prepared: 6/2/17 Date Revised:

		Projected Dates			Notes
		Initial Est.	**Kick-Off**	**Revised/Actual**	
1.	Project Scoping				
2.	IDOT Phase I Kick-Off Meeting	6/1/18			
3.	Environmental Field Review	7/1/18			
4.	ESR Submittal	9/2018			
5.	Submit IDS	10/2018			
6.	1st State/Federal Coordination Meeting – concurrence on environmental processing:	10/2018			
7.	Right-of-Way Kick-Off Meeting (or N/A)	N/A			No ROW needed
8.	Submit Draft LDS	12/2018			
9.	Submit Draft Phase I Report (PDR) to IDOT[(a)]	2/2019			
10.	Public Hearing (or N/A)	N/A			
11.	Submit Final Phase I Report (PDR) to IDOT[(b)]	4/2019			
12.	**Phase I Design Approval**	5/2019			
13.	ROW Acquisition Initiation (or N/A)[(c)]	N/A			
14.	Phase II Engr. Agreement Approval (or N/A)	7/2019			
15.	Submit Pre-Final Plans and Estimates 85% Min.[(d)]	9/2020			
16.	Submit Phase III Engr. Agreement to IDOT				
17.	Submit Final Plans, Specs & Estimates (PS&E)[(e)]	10/2020			
18.	ROW Acquisition Complete				
19.	**Construction Letting**	12/2019			Estimated completion date 10/2020

s:

(a) 3 to 6 month review required per complexity and submittal quality
(b) 1 to 3 month review
(c) Minimum 9 to 18 months required from Plats to Acquisition
(d) 1 to 4 month review
(e) 7 to 10 days before Springfield BLR due date

See IDOT Local Roads Mechanics of Project Management Federal Aid Project Initiation to Completion Flow Chart for sequence of events and estimated review times.

2017 Kane County HSIP Application
Fabyan Parkway at Illinois Route 31
Project Narrative

Kane County Division of Transportation (DOT) has recognized the safety issues at this intersection for many years. Currently, Fabyan Parkway on the east side of the intersection is at an angle to Illinois Route 31 because of how a bridge over the Fox River was constructed decades ago. This bridge required the angle approach, but now is contributing to crashes at that intersection because of reduced sight distance and difficulty of drivers judging speed and distance of oncoming traffic on an angle. The intersection geometry cannot be changed because of the bridge.

The County has added safety measures, including new signage on traffic signal poles and masts reminding drivers making a left turn onto Route 31 that they must yield to oncoming traffic on a green light. There is also new informational signage on both directions of Fabyan upon approaching Route 31, which shows the angle intersection approaching and a separate left turn lane. See pictures for examples.

In late 2016, Kane County DOT conducted an online survey to gather input for its Long Range Transportation Plan update. As part of that update, all respondents were asked to locate areas on an online map where there were issues and to make specific comments about it. This intersection was overwhelmingly identified as a dangerous place in the county and residents requested that Kane County examine it to make it safer.

Kane County DOT studied the crash data and the types of crashes found at the intersection. In 2015, a fatal crash happened during a turn and was caused by a failure to yield right of way. The seven A Injury Crashes were mostly caused by a failure to yield right of way. The majority of all crashes in the past five years are turning and rear-end crashes. A careful review of potential countermeasures provided several options to address these crashes:

- Placing signal heads at the center of each lane
- Utilizing a flashing yellow arrow to further remind turning drivers to yield to oncoming traffic
- An advanced warning flasher, with a wig-wag light, over each leg of Fabyan Parkway that is placed 6-10 seconds of travel time from the stop bar. This will activate 6-10 seconds prior to the end of the green and continue flashing while the signal is red.
- Installing a Dilemma Zone Detection System that will hold the intersection at an “all red” state when a detector senses that a vehicle will be in the intersection and needs extra time to clear the intersection

Kane County DOT has used all but the last countermeasure in other areas of the County and is familiar with their installation and operation and drivers are also familiar with them. This experience with other HSIP-funded projects and these suggested countermeasures will ensure the project will be completed on time and will be evaluated continuously. Kane County DOT will continue to work closely with IDOT on the introduction of any countermeasures proposed for their approval.

Uniform Application for State Grant Assistance

Agency Completed Section

1.	Type of Submission	☐ Pre-application ☒ Application ☐ Changed / Corrected Application
2.	Type of Application	☒ New ☐ Continuation (i.e. multiple year grant) ☐ Revision (modification to initial application)
3.	Date / Time Received by State	Completed by State Agency upon Receipt of Application
4.	Name of the Awarding State Agency	Illinois Department of Transportation
5.	Catalog of State Financial Assistance (CSFA) Number	494-00-1004
6.	CSFA Title	Local Highway Safety Improvement Program
Catalog of Federal Domestic Assistance (CFDA) ☒ Not applicable (No federal funding)		
7.	CFDA Number	20.205
8.	CFDA Title	Highway Planning and Construction
9.	CFDA Number	
10.	CFDA Title	
Funding Opportunity Information		
11.	Funding Opportunity Number	19-1004-01
12.	Funding Opportunity Title	Local HIghway Safety Improvement Program
Competition Identification ☒ Not Applicable		
13.	Competition Identification Number	
14.	Competition Identification Title	

Applicant Completed Section		
Applicant Information		
15.	Legal Name	Kane County Division of Transportation
16.	Common Name (DBA)	
17.	Employer / Taxpayer Identification Number (EIN, TIN)	36-6006585
18.	Organizational DUNS number	945248565
19.	SAM Cage Code	63RN4
20.	Business Address	Street address: 41W011 Burlington Road City: St. Charles State: IL County: Kane Zip + 4: 60175-8412
Applicant's Organizational Unit		
21.	Department Name	Transportation
22.	Division Name	Design
Applicant's Name and Contact Information for Person to be Contacted for *Program* Matters involving this Application		
23.	First Name	Jennifer
24.	Last Name	Becker
25.	Suffix	Ms.
26.	Title	Chief of Planning and Programming
27.	Organizational Affiliation	Kane County Division of Transportation
28.	Telephone Number	630 444 2957
29.	Fax Number	630 584 5239
30.	Email address	beckerjennifer@co.kane.il.us
Applicant's Name and Contact Information for Person to be Contacted for *Business/Administrative Office* Matters involving this Application		
31.	First Name	Kathleen
32.	Last Name	Hopkinson
33.	Suffix	Ms.
34.	Title	Chief of Finance
35.	Organizational Affiliation	Kane County Division of Transportation
36.	Telephone Number	630 584 1170
37.	Fax Number	630 584 5239
38.	Email address	hopkinsonkathleen@co.kane.il.us

Areas Affected		
39.	Areas Affected by the Project (cities, counties, state-wide)	City of Geneva, City of Batavia Add Attachments (e.g., maps)
40.	Legislative and Congressional Districts of Applicant	Illinois Senatorial Districts 33, 35, 25, 42 Illinois Representative District 43, 49, 50, 53, 66, 70, 83
41.	Legislative and Congressional Districts of Program / Project	Illinois Senatorial Districts 25, 33 Illinois Representative Districts 49, 65
Applicant's Project		
42.	Description Title of Applicant's Project	Fabyan Parkway at IL Route 31
43.	Proposed Project Term	Start Date: 2018 End Date: 2019
44.	Estimated Funding (include all that apply)	☒ Amount Requested from the State: 605,273 ☒ Applicant Contribution (e.g., in kind, matching): 67,253 ☐ Local Contribution: ☐ Other Source of Contribution: ☐ Program Income: Total Amount 672,525

Applicant Certification:

By signing this application, I certify (1) to the statements contained in the list of certifications* and (2) that the statements herein are true, complete and accurate to the best of my knowledge. I also provide the required assurances* and agree to comply with any resulting terms if I accept an award. I am aware that any false, fictitious, or fraudulent statements or claims may subject me to criminal, civil or administrative penalties. (U.S. Code, Title 218, Section 1001)

(*) The list of certification and assurances, or an internet site where you may obtain this list is contained in the Notice of Funding Opportunity.

☒ I agree

Authorized Representative		
45.	First Name	Kathleen
46.	Last Name	Hopkinson
47.	Suffix	Ms.
48.	Title	Chief of Finance
49.	Telephone Number	630 584 1170
50.	Fax Number	630 584 5265
51.	Email Address	hopkinsonkathleen@co.kane.il.us
52.	Signature of Authorized Representative	Kathleen T. Hopkinson
53.	Date Signed	6-2-17

Programmatic Risk Assessment Questionnaire

The purpose of this assessment is to evaluate the programmatic risk of the applicant. Limited program experience, protocols and internal control governing program delivery will increase an applicant's degree of risk but will not preclude the applicant from becoming a grantee. The applicant's degree of risk may require additional conditions to be incorporated into the grant award pursuant to 2 CFR 200.207.

Patterns or trends in programmatic risk will influence GATA training as well as the agency's monitoring plan. Appropriate support must be provided by GATU and the agency to build grantee capacity.

Process:

A. The questionnaire (including the agency and/or grant-specific questions) is distributed to the applicant by the agency prior to an awarding decision.
B. The applicant returns the completed questionnaire to the agency. The agency scores the questionnaire based on the responses provided by the applicant.
C. The calculated responses equate to a risk profile for each of the 4 risk categories.
D. The agency aligns the risk profile to the applicable specific condition(s) for medium and high risk applicants in each of the 4 risk categories.
E. The agency communicates the applicable specific condition(s) within the Notice of State Award.

In response to the requirements of 2 CFR 200.205, the awarding agency is required to review the programmatic risk posed by applicants. Five risk categories are assessed through this questionnaire:

1. **Quality of management systems and ability to meet the management standards**
2. **History of performance**
3. **Reports and findings from audits performed under Subpart F—Audit Requirements of this part or the reports and findings of any other available audit**
4. **The applicant's ability to effectively implement statutory, regulatory, or other requirements imposed on awardees.**

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1. **Quality of management systems and ability to meet the management standards**

1.1. Do you have written policies and procedures that guide program delivery on the topics of:

a. Quality assurance ☒ YES/☐ NO
b. Outcome tracking and reporting mechanisms ☒ YES/☐ NO
c. Relevant documentation of services/goods delivered ☒ YES/☐ NO
d. Staff performance management policies and procedures ☒ YES/☐ NO
Personnel policies and procedures that include conflict of interest statements ☒ YES/☐ NO
e. Complaint/grievance resolution policies and procedures ☒ YES/☐ NO

f. Governing body policies and procedures that include conflict of interest statements ☒ YES/☐ NO

g. Safeguarding funds, property and other assets against loss from unauthorized use or disposition ☒ YES/☐ NO

h. Management of grant term extensions, where applicable ☒ YES/☐ NO

1.2. Do you have internal controls that govern program delivery on the topics of:

a. Quality assurance reporting ☒ YES/☐ NO

b. Appropriate (to industry) supervision of staff ☒ YES/☐ NO

c. Unit costs analysis and management ☒ YES/☐ NO

d. Accreditation/licensing compliance program ☐ YES/☐ NO /☒ NOT APPLICABLE

1.3. Does the organization have written standards of conduct covering real or perceived conflict of interest related to actions of employees engaged in the selection, award or administration of contracts supported by grant awards? ☒ YES/☐ NO

1.4. How many years of experience does the project leader have managing the scope of services required under this program?

☒ More than five years (low risk)

☐ One to five years (medium risk)

☐ Less than one year (high risk)

1.5. Does the organization have a time and effort system that:

a. Records all time worked, including time not charged to awards? ☒ YES / ☐ NO

b. Is signed-off by the employee and a supervisor? ☒ YES/☐ NO

c. Includes an approved methodology? ☒ YES/☐ NO/☐ NOT APPLICABLE

☐ Question is not applicable because grants are based on a set rate or a per unit of service. Go to question 1.6.

1.6. Does the organization have controls for invoicing grants paid based on a rate or unit of service?

☒ YES/☐ NO

1.7. Does the organization apply the same standard for match requirements as it does for expenses?

☒ YES/☐ NO/☐ NOT APPLICABLE - WE'VE NOT BEEN SUBJECT TO MATCH REQUIREMENTS

1.8. To what extent are you able to produce periodic grant status reports to inform stakeholders about program outcomes?

☒ Reports are an established part of grant management procedures (low risk)

☐ We're developing reports as part of grant management procedures (medium risk)

☐ We do not currently have established reports as part of grant management (high risk)

2. **History of performance** (The applicant's record in managing grant awards, if it is a prior recipient of awards, including timeliness of compliance with applicable reporting requirements, conformance to the terms and conditions of previous awards, and if applicable, the extent to which any previously awarded amounts will be expended prior to future awards)

2.1. How many years of experience does your organization have with grants of comparable scope and/or capacity?

- [x] More than five years (low risk)
- [ ] One to five years (medium risk)
- [ ] Less than one year (high risk)
- [ ] No experience (high risk) GO TO QUESTION 3.3

2.2. If your organization has experience with grants of comparable scope and/or capacity, provide a brief description of similar project goals and outcomes; specify the applicable year: (Text response)

- see attached

2.3. During your last two fiscal years, how frequently has your organization submitted project performance reports on time?

- [x] Always (low risk)
- [ ] Reported late up to three times (medium risk)
- [ ] Reported late four or more times (high risk)
- [ ] Not applicable – not a requirement of awards previously received

2.4. Have there been any significant changes in your organization in the last fiscal year related to:

a.	Leadership change(s)	☐ YES/☒ NO
b.	Significant program / grant initiative(s)	☐ YES/☒ NO
c.	Structural changes	☐ YES/☒ NO
d.	Fiscal changes	☐ YES/☒ NO
e.	Statutory or regulatory requirements	☐ YES/☒ NO
f.	Other	☐ YES/☒ NO

2.5. Provide a brief explanation for all "YES" responses to question 2.4. (Text response) N/A

2.6. Does the organization utilize a sub-grantee/sub-recipient / sub-award to manage, administer or complete a project? ☐ YES/☒ NO If NO, go to question 2.10.

2.7. What responsibilities does the sub-grantee/sub-recipient/sub-award perform?

a.	Participant eligibility determination	☐ YES/☐ NO
b.	Performance reporting	☐ YES/☐ NO
c.	Program delivery functions	☐ YES/☐ NO
d.	Financial reporting	☐ YES/☐ NO
e.	Other	☐ YES/☐ NO

2.8. What percentage of grant funds does the organization pass on to sub-grantees/sub-recipients/sub-awards?

- ☐ Less than 10% (low risk)
- ☐ 10-20% (medium risk)
- ☐ More than 20% (high risk)

2.9. Does your organization have an implemented policy for sub-grantee monitoring? ☐ YES/☐ NO

If NO, go to 2.10. If YES, does it include:

- ☐ on-site review (low risk)
- ☐ review of prior monitoring (low risk)
- ☐ desk / quantitative review (medium risk)

2.10 Do you obtain prior written approval from the funding agency when:

a. The scope or objective of the program changes ☒ YES/☐ NO
b. Key personnel specified in the application change ☒ YES/☐ NO
c. The approved project director disengages for more than 3 months or reduces 25% of time devoted to the project ☒ YES/☐ NO

☐ Question is not applicable because organization has not been subject to these requirements

2.11 Does your organization have performance measurements that tie to financial data?

☒ YES/☐ NO

3. Reports and findings from audits performed under Subpart F—Audit Requirements of this part or the reports and findings of any other available audit

3.1. During the last two fiscal years, has your organization been out of compliance with *programmatic* terms and conditions of awards?

- ☐ Organization has not been audited; Go to Question 3.6
- ☒ No occurrences of non-compliance; Go to Question 3.6 (low risk)
- ☐ One to three occurrences of non-compliance (medium risk)
- ☐ Four or more occurrences of non-compliance (high risk)

3.2. If your organization had at least one occurrence of non-compliance with programmatic terms and conditions, summarize each occurrence. (Text response)

3.3. Have corrective actions been implemented within the specified timeframe? ☐ YES/☐ NO

3.4. Provide explanation for any corrective actions that were not implemented within the timeframe specified and for any corrective actions that remain open. (Text response)

3.5. Have there been conflict of interest-related findings within the last two fiscal years? ☐ YES/☐ NO

a. If NO, go to question 3.6. (low risk)
b. If YES, specify the conflict of interest-related finding and your response to the finding.

(Text response)

3.6. Has your organization been subject to conditional approvals due to program issues? ☐ YES/☒ NO

a. If NO, to go question 4.1.
b. If YES, specify the terms of the special condition and whether or not the special condition is still applicable. (Text response)

4. The applicant's ability to effectively implement statutory, regulatory, or other requirements imposed on awardees.

4.1. To what extent does your organization have policies to ensure programmatic expenses are reasonable, necessary and prudent (allowable)?

☒ Policies are implemented and followed (low risk)
☐ Policies are not fully implemented (high risk)
☐ The organization does not currently have these types of policies (high risk)

4.2. To what extent does your organization have policies to ensure programmatic activities are allowable?

☒ Policies are implemented and followed (low risk)
☐ Policies are not fully implemented (high risk)
☐ The organization does not currently have these types of policies (high risk)

4.3. To what extent is your organization able to comply with all statutory requirements of this program?

☒ Fully able to comply with all statutory requirements (low risk)
☐ With the following exception(s), the organization is able to comply: Text response of exception(s) (medium to high risk depending on the exceptions)

4.4. Has the organization been out of compliance with any statutory, regulatory or other requirements of grant funding within the last two fiscal years? ☐ YES/☒ NO

If YES, provide explanation. (Text response)

Certification Section - Add wording to validate that the responses provided are true and accurate and that all occurrence of non-compliance with programmatic requirements has been disclosed.

Kathleen T. Hopkinson — 6-1-17
Authorized Signature Chief, Finance — Date

Programmatic Risk Assessment Questionnaire - Extended Response to Question 2.2

Kane County has received HSIP funding in 2012, 2013, 2014, and 2016 and therefore has experience managing this fund source. In addition, Kane County received Surface Transportation Program (STP) funding annually; Congestion Mitigation Air Quality (CMAQ) funding in 2011, 2014, 2015; Truck Access Route Program (TARP) funding in 2014; Federal Lands Access Program (FLAP) in 2015. These are only recent examples. The HSIP funding received in past years is providing safety improvements at a corridor level and intersection-specific level which is what this current request will implement. The currently funded HSIP projects are under construction or recently constructed and sufficient data to evaluate the outcomes is not available yet.

Annual Crash Summary Sheet
Kane County HSIP Application 2017
Road Fabyan Parkway
Limits at Route 31

Type of Crash	2011	2012	2013	2014	2015
Total Number of Crashes	16	23	21	20	18
Fatal Crashes	0	0	0	0	1
Fatalities	0	0	0	0	1
A-Injury Crashes	0	1	3	2	1
A-Injuries	0	2	4	2	2
B-Injury Crashes	4	4	2	2	2
B-Injuries	4	7	3	3	5
C-Injury Crashes	4	4	3	1	2
C-Injuries	8	5	4	1	3
Propert Damage Only	8	14	13	15	12
Wet Weather Crashes	3	7	10	8	7
Darkness (not lighted) Crashes	0	2	0	0	0

Pedestrian	0	0	0	0	0
Animal	0	0	0	0	0
Overturned	0	1	0	0	0
Fixed Object	0	1	0	0	1
Other	0	1	0	0	1
Turning	6	8	8	8	9
Rear End	7	6	10	10	6
Sideswipe Same Dir	1	2	1	1	1
Sideswipe Opposite Dir	0	1	0	0	0
Head on	1	2	1	0	0
Angle	1	1	1	1	0
Pedalcyclist	0	0	0	0	0

2015 Data

ID	Crash Month	Crash Day	Total Killed	A Injuries	B Injuries	C Injuries	Crash Severity	Type Of Crash	Cause 1	Cause 2	Device Condition	Roadway Surface	Road Defect	Crash Injury Severity	Light Condition	Weather Code 2
1	4	24	0	1	1	0	Injury	Turning	Failing to Yield Right of Way	Failing to Reduce Speed to Avoid Crash	Functioning Properly	Dry	No Defects	A Injury Crash	Daylight	Clear
2	6	27	0	0	3	0	Injury	Turning	Failing to Yield Right of Way	Road Construction/Maintenance	Functioning Properly	Dry	No Defects	B Injury Crash	Daylight	Clear
3	12	23	0	0	1	0	Injury	Turning	Failing to Yield Right of Way	NA	Functioning Properly	Wet	No Defects	B Injury Crash	Daylight	Clear
4	11	28	0	0	0	1	Injury	Rear End	Failing to Reduce Speed to Avoid Crash	Under Influence of Alcohol/Drugs	Functioning Properly	Dry	No Defects	C Injury Crash	Darkness / Lighted Road	Clear
5	6	26	0	0	0	1	Injury	Rear End	Failing to Reduce Speed to Avoid Crash	Distraction - From Inside Vehicle	Functioning Properly	Wet	No Defects	C Injury Crash	Daylight	Rain
6	10	11	1	1	0	1	Fatal	Turning	Failing to Yield Right of Way	NA	Functioning Properly	Dry	No Defects	Fatal Crash	Darkness / Lighted Road	Clear
7	10	20	0	0	0	0	Property Damage	Fixed Object	Distraction - From Inside Vehicle	Driving Skills/Knowledge/Experience	No Controls	Dry	No Defects	No Injuries	Darkness / Lighted Road	Clear
8	7	9	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Following Too Closely	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear
9	7	17	0	0	0	0	Property Damage	Rear End	Driving Skills/Knowledge/Experience	NA	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear
10	7	24	0	0	0	0	Property Damage	Sideswipe Same Direction	Driving Skills/Knowledge/Experience	Unable to Determine	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear
11	4	20	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	NA	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear
12	8	5	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	NA	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear
13	8	1	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	NA	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear
14	1	5	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	Improper Turning/No Signal	Functioning Properly	Ice	No Defects	No Injuries	Darkness / Lighted Road	Clear
15	1	8	0	0	0	0	Property Damage	Parked Motor Vehicle	Weather	Exceeding Safe Speed For Conditions	No Controls	Snow or Sl	No Defects	No Injuries	Daylight	Snow
16	1	9	0	0	0	0	Property Damage	Rear End	Exceeding Safe Speed For Conditions	Failing to Reduce Speed to Avoid Crash	Functioning Properly	Snow or Sl	No Defects	No Injuries	Daylight	Clear
17	12	30	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Weather	Functioning Properly	Snow or Sl	No Defects	No Injuries	Daylight	Snow
18	11	25	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	NA	Functioning Improperly	Wet	No Defects	No Injuries	Daylight	Clear
			1	2	5	3										

Pedestrian	0			Fatal	1
Animal	0			A crash	1
Overturned	0		7	B crash	2
Fixed Object	1			C Crash	2
Other	1			PDO	12
Turning	9				18
Rear End	6				
Sideswipe Same Dir	1			Darkness	0
Sideswipe Opposite Dir	0			Darkness/Lighted	2
Head on	0				
Angle	0				
Pedalcyclist	0				
	18				

2014 Data

ID	Crash Month	CrashDay	Total Killed	A Injuries	B Injuries	C Injuries	Crash Severity	Type Of Crash	Cause 1	Cause 2	Traffic Device	Device Condition	Roadway Surface	Road Defects	Crash Injury Severity	Light Condition	Weather Code 2
1	10	15	0	1	0	0	Injury	Turning	Failing to Yield Right of Way	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	A Injury Crash	Daylight	Clear
2	3	10	0	1	0	0	Injury	Turning	Failing to Yield Right of Way	Failing to Reduce Speed to Avoid Crash	Traffic Signal	Functioning Properly	Wet	No Defects	A Injury Crash	Daylight	Clear
3	1	26	0	0	2	0	Injury	Angle	Under Influence of Alcohol/Drugs	Disregarding Traffic Signals	Traffic Signal	Functioning Properly	Snow or Slush	No Defects	B Injury Crash	Darkness/ Lighted Road	Snow
4	1	23	0	0	1	0	Injury	Turning	Failing to Yield Right of Way	Driving Skills/Knowledge/Experience	Traffic Signal	Functioning Properly	Wet	No Defects	B Injury Crash	Darkness/ Lighted Road	Clear
5	3	21	0	0	0	1	Injury	Rear End	Failing to Reduce Speed to Avoid Crash	Equipment-Vehicle Condition	Traffic Signal	Functioning Properly	Dry	No Defects	C Injury Crash	Daylight	Clear
6	11	12	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Darkness/ Lighted Road	Clear
7	7	9	0	0	0	0	Property Damage	Rear End	Distraction - From Inside Vehicle	Failing to Reduce Speed to Avoid Crash	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear
8	6	4	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Unable to Determine	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear
9	12	6	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Improper Lane Usage	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Darkness/ Lighted Road	Clear
10	10	28	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear
11	4	17	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear
12	7	19	0	0	0	0	Property Damage	Sideswipe Same Direction	Improper Overtaking/Passing	(N/A)	Lane Use Marking	Functioning Properly	Dry	No Defects	No Injuries	Darkness/ Lighted Road	Clear
13	11	21	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear
14	2	27	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Dusk	Clear
15	6	30	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	Failing to Reduce Speed to Avoid Crash	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear
16	2	21	0	0	0	0	Property Damage	Rear End	Weather	Exceeding Safe Speed For Conditions	Lane Use Marking	Functioning Properly	Ice	Rut, Holes	No Injuries	Daylight	Other
17	4	1	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	(N/A)	Traffic Signal	Functioning Properly	Wet	Unknown	No Injuries	Daylight	Rain
18	5	15	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	(N/A)	Traffic Signal	Functioning Properly	Wet	Unknown	No Injuries	Daylight	Clear
19	4	8	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	(N/A)	Traffic Signal	Functioning Properly	Wet	No Defects	No Injuries	Daylight	Clear
20	6	7	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	Cell Phone Use Other Than Texting	Traffic Signal	No Controls	Wet	No Defects	No Injuries	Darkness	Rain
			0	2	3	1											

Pedestrian	0			wet	6	Fatal	0
Animal	0			snow/ice	2	A crash	2
Overturned	0				8	B crash	2
Fixed Object	0					C Crash	1
Other	0					PDO	15
Turning	8						20
Rear End	10						
Sideswipe Same Dir	1					Darkness	0
Sideswipe Opposite Dir	0					Darkness/Lighted	5
Head on	0						
Angle	1						
Pedalcyclist	0						
	20						

2013 Data

ID	Crash Month	Crash Day	Total Killed	A Injuries	B Injuries	C Injuries	Crash Severity	Type Of Crash	Cause 1	Cause 2	Traffic Device	Device Condition	Roadway Surface	Road Defects	Crash Injury Severity	Light Condition	Weather Code 2
1	8	31	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Darkness/ Lighted Road	Clear
2	12	11	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	(N/A)	Traffic Signal	Functioning Properly	Unknown	Unknown	No Injuries	Darkness/ Lighted Road	Clear
3	4	11	0	0	0	0	Property Damage	Angle	Disregarding Traffic Signals	Failing to Yield Right of Way	Traffic Signal	Functioning Properly	Wet	No Defects	No Injuries	Dawn	Rain
4	4	13	0	2	0	1	Injury	Head On	Physical Condition of Driver	Driving On Wrong Side/Wrong Way	Traffic Signal	Functioning Properly	Dry	No Defects	A Injury Crash	Daylight	Clear
5	12	16	0	1	0	0	Injury	Turning	Disregarding Traffic Signals	(N/A)	Traffic Signal	Functioning Properly	Wet	No Defects	A Injury Crash	Daylight	Snow
6	12	4	0	1	0	0	Injury	Turning	Failing to Yield Right of Way	(N/A)	Traffic Signal	Functioning Properly	Wet	No Defects	A Injury Crash	Daylight	Rain
7	7	6	0	0	1	0	Injury	Turning	Failing to Yield Right of Way	Failing to Reduce Speed to Avoid Crash	Traffic Signal	Functioning Properly	Dry	No Defects	B Injury Crash	Daylight	Clear
8	12	9	0	0	2	0	Injury	Turning	Failing to Yield Right of Way	Driving Skills/Knowledge/Experience	Traffic Signal	Functioning Properly	Dry	No Defects	B Injury Crash	Daylight	Clear
9	5	28	0	0	0	1	Injury	Turning	Failing to Yield Right of Way	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	C Injury Crash	Daylight	Clear
10	10	19	0	0	0	1	Injury	Turning	Failing to Yield Right of Way	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	C Injury Crash	Daylight	Clear
11	1	25	0	0	0	1	Injury	Rear End	Exceeding Safe Speed For Conditions	(N/A)	Traffic Signal	Functioning Properly	Snow or Slush	No Defects	C Injury Crash	Daylight	Snow
12	8	1	0	0	0	0	Property Damage	Rear End	Equipment-Vehicle Condition	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear
13	9	16	0	0	0	0	Property Damage	Rear End	Driving Skills/Knowledge/Experience	Unable to Determine	Traffic Signal	Functioning Properly	Dry	Unknown	No Injuries	Daylight	Clear
14	10	25	0	0	0	0	Property Damage	Sideswipe Same Direction	Improper Lane Usage	Disregarding Road Markings	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear
15	8	24	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear
16	12	18	0	0	0	0	Property Damage	Rear End	Exceeding Safe Speed For Conditions	(N/A)	Traffic Signal	Functioning Properly	Ice	No Defects	No Injuries	Daylight	Clear
17	3	5	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Exceeding Safe Speed For Conditions	Traffic Signal	Functioning Properly	Snow or Slush	No Defects	No Injuries	Daylight	Snow
18	5	9	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Exceeding Safe Speed For Conditions	Traffic Signal	Functioning Properly	Wet	No Defects	No Injuries	Daylight	Rain
19	6	11	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	(N/A)	No Controls	No Controls	Wet	No Defects	No Injuries	Daylight	Rain
20	10	31	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Exceeding Safe Speed For Conditions	Traffic Signal	Functioning Properly	Wet	No Defects	No Injuries	Daylight	Rain
21	10	17	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	(N/A)	Traffic Signal	Functioning Properly	Wet	No Defects	No Injuries	Daylight	Rain
			0	4	3	4											

Pedestrian	0
Animal	0
Overturned	0
Fixed Object	0
Other	0
Turning	8
Rear End	10
Sideswipe Same Dir	1
Sideswipe Opposite Dir	0
Head on	1
Angle	1
Pedalcyclist	0
	21

wet	7
snow/ice	3
	10

Fatal	
A crash	3
B crash	2
C Crash	3
PDO	13
	21

Darkness	0
Darkness/Lighted	2

2012 Data

ID	Total Killed	A Injuries	B Injuries	C Injuries	Crash Severity	Type Of Crash	Cause 1	Cause 2	Traffic Device	Device Condition	Roadway Surface	Road Defects	Crash Injury Severity	Light Condition	Weather Code 2
1	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	Unable to Determine	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Darkness	Clear
2	0	0	0	0	Property Damage	Sideswipe Same Direction	(N/A)	(N/A)	Traffic Signal	Functioning Properly	Wet	No Defects	No Injuries	Darkness	Rain
3	0	2	0	0	Injury	Sideswipe Opposite Direction	Improper Lane Usage	Had Been Drinking	No Controls	No Controls	Dry	No Defects	A Injury Crash	Darkness, Lighted Road	Clear
4	0	0	0	1	Injury	Head On	Failing to Yield Right of Way	Improper Turning/No Signal	Traffic Signal	Functioning Properly	Dry	No Defects	C Injury Crash	Darkness, Lighted Road	Clear
5	0	0	0	0	Property Damage	Fixed Object	Had Been Drinking	Exceeding Authorized Speed Limit	Traffic Signal	Functioning Properly	Wet	No Defects	No Injuries	Darkness, Lighted Road	Clear
6	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Distraction - From Inside Vehicle	Traffic Signal	Functioning Properly	Wet	No Defects	No Injuries	Darkness, Lighted Road	Rain
7	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	(N/A)	Traffic Signal	Functioning Properly	Wet	No Defects	No Injuries	Darkness, Lighted Road	Clear
8	0	0	3	0	Injury	Angle	Disregarding Other Traffic Signs	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	B Injury Crash	Daylight	Clear
9	0	0	1	0	Injury	Overturned	Unable to Determine	(N/A)	No Controls	No Controls	Dry	No Defects	B Injury Crash	Daylight	Clear
10	0	0	2	0	Injury	Turning	Failing to Yield Right of Way	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	B Injury Crash	Daylight	Clear
11	0	0	1	0	Injury	Turning	Failing to Yield Right of Way	(N/A)	Traffic Signal	Functioning Properly	Wet	No Defects	B Injury Crash	Daylight	Clear
12	0	0	0	2	Injury	Head On	Unable to Determine	Improper Lane Usage	Traffic Signal	Functioning Properly	Dry	No Defects	C Injury Crash	Daylight	Clear
13	0	0	0	1	Injury	Rear End	Improper Lane Usage	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	C Injury Crash	Daylight	Clear
14	0	0	0	1	Injury	Turning	Failing to Yield Right of Way	Improper Turning/No Signal	Traffic Signal	Functioning Properly	Dry	No Defects	C Injury Crash	Daylight	Clear
15	0	0	0	0	Property Damage	Rear End	Distraction Î"Ã‡Ã´ operating a wireless phone	Failing to Reduce Speed to Avoid Crash	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear
16	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear
17	0	0	0	0	Property Damage	Sideswipe Same Direction	Improper Lane Usage	(N/A)	No Controls	No Controls	Dry	Unknown	No Injuries	Daylight	Clear
18	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	Improper Turning/No Signal	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear
19	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	Improper Lane Usage	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear
20	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear
21	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	Failing to Reduce Speed to Avoid Crash	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear
22	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	(N/A)	Traffic Signal	Functioning Properly	Snow or Slush	No Defects	No Injuries	Daylight	Snow
23	0	0	0	0	Property Damage	Other Non-Collision	Equipment-Vehicle Condition	(N/A)	No Controls	No Controls	Wet	No Defects	No Injuries	Daylight	Clear
	0	2	7	5											

Pedestrian	0			wet	6	Fatal 0
Animal	0			snow/ice	1	A Crash 1
Overturned	1					B Crash 4
Fixed Object	1					C Crash 4
Other	1					PDO 14
Turning	8					23
Rear End	6					
Sideswipe Same Dir	2					Darkness 2
Sideswipe Opposite Dir	1					Darkness/Lighted 5
Head on	2					
Angle	1					
Pedalcyclist	0					
	23					

2011 Data

ID	Crash Month	Crash Day	Total Killed	A Injuries	B Injuries	C Injuries	Crash Severity	Type Of Crash	Cause1	Cause 2	Traffic Device	Device Condition	Roadway Surface	Crash Injury Severity	Light Condition	Weather Code 2
1	6	4	0	0	1	0	Injury	10-Turning	Failing to yield right-of-way	Not applicable	Traffic signal	Function properly	Dry	B Injury Crash	Daylight	Clear
2	7	27	0	0	1	2	Injury	10-Turning	Failing to yield right-of-way	Not applicable	Traffic signal	Function properly	Dry	B Injury Crash	Daylight	Clear
3	7	27	0	0	1	1	Injury	10-Turning	Failing to yield right-of-way	Driving skills/knowledge/experience	Traffic signal	Function properly	Dry	B Injury Crash	Daylight	Clear
4	8	17	0	0	1	0	Injury	11-Rear end	Following too closely	Not applicable	No controls	No controls	Dry	B Injury Crash	Daylight	Clear
5	3	28	0	0	0	2	Injury	10-Turning	Failing to yield right-of-way	Not applicable	Traffic signal	Function properly	Dry	C Injury Crash	Daylight	Clear
6	5	9	0	0	0	1	Injury	11-Rear end	Distraction from inside vehicle	Failing to reduce speed to avoid crash	Traffic signal	Function properly	Dry	C Injury Crash	Daylight	Clear
7	7	31	0	0	0	1	Injury	15-Angle	Disregarding traffic signals	Under the influence of alcohol/drugs	Traffic signal	Function properly	Dry	C Injury Crash	Daylight	Clear
8	11	4	0	0	0	1	Injury	11-Rear end	Following too closely	Failing to reduce speed to avoid crash	Traffic signal	Function properly	Unknown	C Injury Crash	Darkness/Lighted road	Clear
9	8	18	0	0	0	0	Property Damage	10-Turning	Disregarding traffic signals	Failing to yield right-of-way	Traffic signal	Function properly	Dry	No Injuries	Daylight	Clear
10	12	6	0	0	0	0	Property Damage	10-Turning	Failing to yield right-of-way	Not applicable	Traffic signal	Function properly	Dry	No Injuries	Darkness/Lighted road	Clear
11	7	21	0	0	0	0	Property Damage	11-Rear end	Following too closely	Failing to reduce speed to avoid crash	Traffic signal	Function properly	Dry	No Injuries	Daylight	Clear
12	11	7	0	0	0	0	Property Damage	11-Rear end	Failing to reduce speed to avoid crash	Unable to determine	No controls	No controls	Dry	No Injuries	Darkness/Lighted road	Clear
13	12	15	0	0	0	0	Property Damage	12-Sideswipe same direction	Improper lane usage	Distraction Ã¢â‚¬â€œ from inside vehicle	No controls	No controls	Dry	No Injuries	Daylight	Clear
14	5	21	0	0	0	0	Property Damage	11-Rear end	Exceeding safe speed for conditions	Not applicable	Traffic signal	Function properly	Wet	No Injuries	Daylight	Rain
15	12	17	0	0	0	0	Property Damage	11-Rear end	Failing to reduce speed to avoid crash	Not applicable	Traffic signal	Functioning improperly	Wet	No Injuries	Daylight	Clear
16	12	14	0	0	0	0	Property Damage	14-Head on	Failing to yield right-of-way	Not applicable	Traffic signal	Function properly	Wet	No Injuries	Darkness/Lighted road	Rain
			0	0	4	8										

Pedestrian	0	wet	3	Fatal	0
Animal	0	snow/ice	0	A crash	0
Overturned	0			B crash	4
Fixed Object	0			C Crash	4
Other	0			PDO	8
Turning	6				16
Rear End	7				
Sideswipe Same Dir	1			Darkness	0
Sideswipe Opposite Dir	0			Darkness/Lighted	4
Head on	1				
Angle	1				
Pedalcyclist	0				
	16				

Illinois Department of Transportation
2300 South Dirksen Parkway / Springfield, Illinois / 62764

July 27, 2017

Mr. Carl Schoedel
Kane County Engineer
41W011 Burlington Road
St. Charles, IL 60175

Dear Mr. Schoedel,

The Illinois Department of Transportation is pleased to inform you that your project has been selected for local Highway Safety Improvement Program (HSIP) funding. The project includes the installation of flashing yellow arrows at intersections and wet reflective pavement markings along Orchard Road from south of Jericho Road to north of U.S. Route 30 and will be identified by the Department as HSIP # 201712032. The installation of dynamic speed signs was deemed ineligible for HSIP funding by the Department. Congratulations on your successful application.

The federal HSIP commitment for this project will not exceed $395,555. The deadline for this award to be federally authorized is July 1, 2020 or funds will be rescinded.

Please contact Mr. Christopher Holt, District 1 Local Roads Engineer by telephone at (847) 705-4201 to discuss program requirements and preparation of any agreements and / or contracts. Projects located within a Metropolitan Planning Organization (MPO) planning boundary are required to be listed in the local MPO's Transportation Improvement Program (TIP). Questions regarding the HSIP may be directed to Ms. Melinda Kos in the Central Bureau of Local Roads and Streets by telephone at (217) 785-5178.

All HSIP grant recipients must be registered with the State of Illinois in order to comply with the Grant Accountability and Transparency Act (GATA) 30 ILCS 708. Additional pre-award paperwork is also required, including: (1) a Notice of State Award (NOSA); and (2) a Uniform Grant Budget. The Illinois Department of Transportation will be contacting you shortly to accept or deny the terms of your NOSA. The Uniform Grant Budget Form will be sent directly to your email and must be returned to Melinda Kos at Melinda.Kos@illinois.gov by August 24, 2017. You may also send inquiries to DOT.GATA@illinois.gov for further assistance.

Sincerely,

Maureen E. Kastl, P.E.
Engineer of Local Roads and Streets

cc: Alan Ho, FHWA – Illinois Division
Paul Lorton, IDOT – Bureau of Safety Programs and Engineering
Christopher Holt, IDOT District 1 Local Roads Engineer
File

KANE COUNTY
DIVISION of TRANSPORTATION

Carl Schoedel, P.E.
Director of Transportation
County Engineer

41W011 Burlington Road
St. Charles, IL 60175
Phone: (630) 584-1170
Fax: (630) 584-5265

MEMORANDUM

DATE: June 2, 2017

TO: IDOT District 1 Bureau of Local Roads and Streets

FROM: Carl Schoedel

RE: 2017 Highway Safety Improvement Program Call for Applications

Dear Sir or Madam:

Please find our completed application package for Orchard Road from south of Jericho Road to north of U.S. Route 30 which includes the HSIP Candidate Application Form, Benefit/Cost Ratio form, project location map, project photographs, estimated cost breakdown, project timeline, project narrative, uniform application for state grant assistance, and the programmatic risk assessment questionnaire as well as crash data from the past five years.

Thank you for considering our application.

Illinois Department of Transportation

HSIP Candidate Form

FY

ID:	**Contract:**	**Award Date:**	**Completion Date:**
District: 1	**County:** Kane		**City:**
Key route: Orchard Road	**Marked route:** CH #83		
Road Name: Orchard Road	**Intersecting Roadway:** N/A ☐		
Length: 1.4 ☐ N/A		**Mile station:** to	

Location Description: Orchard Road from south of Jericho Road to north of U.S. Route 30.

☐ **Rural**	☒ **Urban**	**Lanes:** 4	
AADT(Segment): 29,900	**Total Entering AADT (Intersection):**		**Speed Limit:** 45 mph
Friction Test Results:	☐ N/A	**Lighting Present:** ☒ Y ☐ N	

CHSP Emphasis Area(s): ☐ District Documentation ☐ Systematic Improvements ☐ N/A

Peer Group: ☐ N/A

Other:

Crashes Details

Year	Total Crashes	Fatal Crashes	Fatalities	A-Injury Crashes	A-Injuries	B-Injury Crashes	B-Injuries	C-Injury Crashes	C-Injuries	PDO	Wet-Weather Crashes	Darkness (Not lighted) Crashes
2011	52	0	0	2	2	9	12	7	14	34	10	6
2012	29	0	0	1	2	3	4	7	8	18	4	2
2013	35	0	0	0	0	1	1	7	9	27	11	2
2014	48	0	0	1	1	5	7	8	10	34	14	4
2015	41	1	1	1	1	3	5	3	5	33	13	5
Total	205	1	1	5	6	21	29	32	46	146	52	19

Location Description: Strategic Regional Arterial (SRA) highway Orchard Road from south of Jericho Road to just north of U.S. Route 30. This roadway is adjacent to industrial, residential and commercial land use and is located in the cities of Montgomery and Aurora.

Problem Description: There have been five A-Injury and one fatal crash in this corridor in the past five years. These are mostly rear end type crashes.

Previous Safety Improvements: Signal upgrades, grooved concrete pavement, separate muliti-use path.

Collision Diagram: ☐ Y ☒ N **Images:** ☒ Y ☐ N

Predominant Crash Types: The predominant crash types are rear end followed by turning

Proposed Improvement(s): Flashing yellow arrow at intersections, wet reflective pavement markings

Estimated Project Cost ($000's): $440 **Benefit-Cost Ratio:** 19.57

Local Projects:

Annual Fatal Crash Rate (Fatal Crashes/100 Miles): 72 **Annual A-Injury Crash Rate (A-Injury Crashes/100 Miles):** 362

Local Roads Rural Functional Class: Other Principal Arterial

Approved: **Central HSIP Approval Date:**

Signed:

State Safety Engineer

Funding: ☒ HSIP ☐ HRRR ☐ RAIL

Comment:

Distribution: ☐ OPP ☐ District ☐ BSPE ☐ LRS ☐ BDE

PROJECT DESCRIPTION - PROJECT DATA INPUT (SEGMENTS)

Project:	Orchard Road HSIP Countermeasure extension (South of previously awarded limits)					*Prepared by:*	Stephen Zulkowski	Stephen Zulkowski
District:	1	*County:*	Kane	*City:*	Montgomery	*Date*	6/1/2017	REV 7/12/17
Key Route:	Orchard	*Marked Route:*	CH# 83	*MilePost:*		*Current AADT:*	29000	
Location Description:	Orchard Road (Jericho Road to North of US Route 30)							
						Length:	1.5	Miles
Crash data:	5 Years					*Traffic Growth factor*	2.0%	
	From 2011	to	2015			*Interest rate*	4.0%	

Highway Peer Group:

Peer Group 9 - Urban Multilane Divided Highway | 9 | Urban | Multi-Divided

PROCESS STEPS - Benefit Cost Calculations (SEGMENTS)

User will input data only in Highlighted Cells

SEGMENTS CRASH SEVERITY DISTRIBUTION BY CRASH TYPE FOR ANALYSIS PERIOD

	All Crashes	Angle	Animal	Fixed Object	Head On	Left Turn	Other Noncollision	Other Object	Overturned	Pedestrian	Pedalcyclist	Parked Vehicle	Rear End	Right Turn	Sideswipe Same Direction	Sideswipe Opposite Direction	Turning	Train	Night Time crash	Wet Pavement
	ALL	*AG*	*AN*	*FO*	*HO*	*LT*	*OtherNC*	*OtherO*	*OVT*	*PD*	*PDC*	*PKV*	*RE*	*RT*	*SSD*	*SOD*	*T*	*TR*	*NGT*	*WP*
Fatal Crashes	1												1							
A-Injury Crashes	5				1								2				2			
B-Injury Crashes	21	3		2									8		1		7			
C-Injury Crashes	32		1						1				22		1		7			
PDO Crashes	146	5	1	7								1	96		10	4	22			

SEGMENTS BENEFIT COST ANALYSIS

BENEFIT CALCULATIONS | **COUNTERMEASURE COST CALCULATIONS**

COUNTERMEASURE	CMF *	Crash Type affected by this improvement	Unit Cost	Quantity	Units	Total Cost	Service Life	Present worth	EUAC **
2.8.12.AL.1 CMF# 4176,4177 - PPLT to Flashing yellow arrow (signalization) (At Aucutt and Rochester Signals)	0.92	ALL=0.922, LT=0.806	68,750	2	Per Signal Loc	$137,500	20	$137,500	$10,117
2.8.15.AL.1 CMF Clearing House # 8134 - Wet Reflective Markings Longitudenal Pavement Markings, on Existing pavement	0.88	All	148,500	1.5	Mile	$222,750	5	$680,001	$50,036
		All				$0		$0	$0
		All				$0		$0	$0
						$360,250			
TOTAL BENEFIT $1,177,201			*TOTAL COST*						*$60,153*

Cost Calculation

BENEFIT/ COST	19.57

* CMF = Crash Reduction Factor

** EUAC = Estimated Uniform Annual Cost

2017 HSIP Project Location

Orchard Road Corridor from Jericho Road to Route 30

Kane County HSIP Application
Orchard Road from Jericho to U.S. Route 30

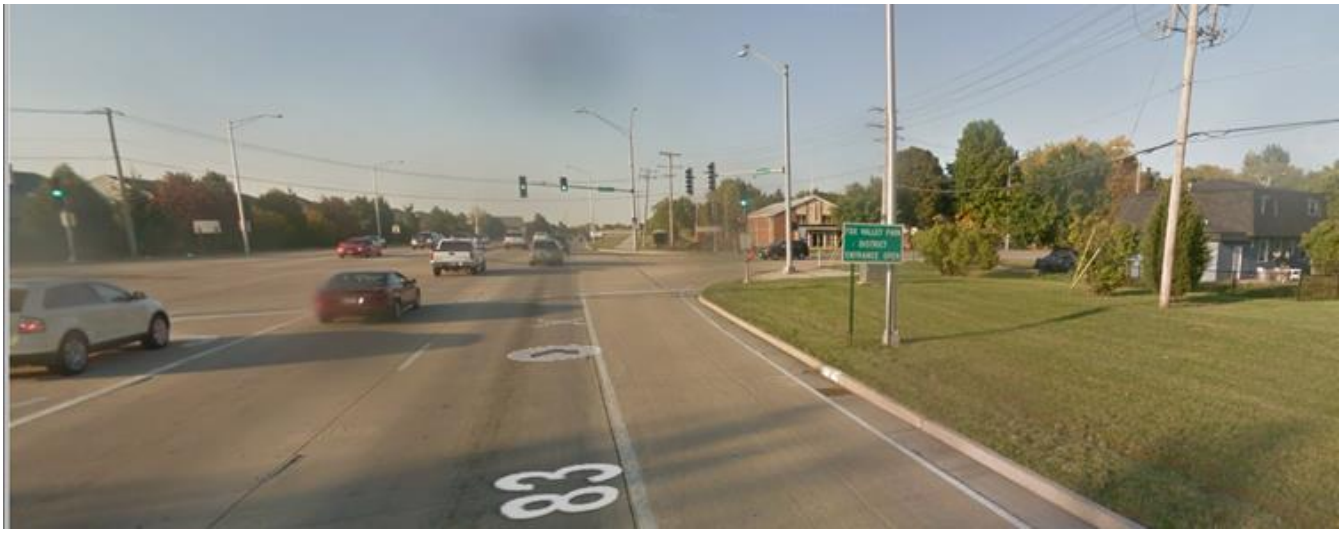
These pictures from May 2017 demonstrate the current conditions on this segment of Orchard Road from south of Jericho Road to north of U.S. Route 30.

Orchard Road facing north approaching Aucutt Road.

Another view looking north on Orchard Road approaching Aucutt Road.

Orchard Road facing south, approaching Countryside Drive.

Southbound Orchard Road approaching Jericho Road.



Northbound Orchard Road approaching Jericho Road.

Northbound Jericho Road approaching Rochester Drive.

Kane County HSIP Application 2017
Project Cost Breakdown
Orchard Road Corridor

Phase	**Total**	**HSIP Funding**	**Local Share**
Phase 1 Engineering	21,615	$ 19,454	$ 2,162
Phase 2 Engineering	21,615	$ 19,454	$ 2,162
CE	36,025	$ 32,423	$ 3,603
Construction	360,250	$ 324,225	$ 36,025
	Total	$ 395,555	$ 43,951

Includes

flashing yellow arrrow	$	137,500
wet reflective pavement markings	$	222,750
	$	360,250

Total Project Cost 439,505

PROJECT MILESTONE SCHEDULE

Municipality: Kane County DOT
Project: Orchard Road – Jericho to north of US 30
Scope of Work: Corridor Improvements– HSIP application 6/17
TIP #:
TIP Years (Ph II / Const):
Section #:
Last Constr & E3 Cost (Date):
Current Constr & E2 Cost (Date): $

Contact Information		
	Name	**Telephone**
Municipality		
Council/Liaison		
Consultant		
IDOT		

Date Prepared: 6/2/17 Date Revised:

		Projected Dates			Notes
		Initial Est.	**Kick-Off**	**Revised/Actual**	
1.	Project Scoping				
2.	IDOT Phase I Kick-Off Meeting	6/1/18			
3.	Environmental Field Review	7/1/18			
4.	ESR Submittal	9/2018			
5.	Submit IDS	10/2018			
6.	1st State/Federal Coordination Meeting – concurrence on environmental processing:	10/2018			
7.	Right-of-Way Kick-Off Meeting (or N/A)	N/A			No ROW needed
8.	Submit Draft LDS	12/2018			
9.	Submit Draft Phase I Report (PDR) to IDOT[(a)]	2/2019			
10.	Public Hearing (or N/A)	N/A			
11.	Submit Final Phase I Report (PDR) to IDOT[(b)]	4/2019			
12.	**Phase I Design Approval**	5/2019			
13.	ROW Acquisition Initiation (or N/A)[(c)]	N/A			
14.	Phase II Engr. Agreement Approval (or N/A)	7/2019			
15.	Submit Pre-Final Plans and Estimates 85% Min.[(d)]	9/2020			
16.	Submit Phase III Engr. Agreement to IDOT				
17.	Submit Final Plans, Specs & Estimates (PS&E)[(e)]	10/2020			
18.	ROW Acquisition Complete				
19.	**Construction Letting**	12/2019			Estimated completion date 9/2020

s:

(a) 3 to 6 month review required per complexity and submittal quality
(b) 1 to 3 month review
(c) Minimum 9 to 18 months required from Plats to Acquisition
(d) 1 to 4 month review
(e) 7 to 10 days before Springfield BLR due date

See IDOT Local Roads Mechanics of Project Management Federal Aid Project Initiation to Completion Flow Chart for sequence of events and estimated review times.

2017 Kane County HSIP Application
Orchard Road from south of Jericho Road to north of U.S. Route 30
Project Narrative

Kane County Division of Transportation (DOT) has been updating the county highway system to use the most up-to-date safety measures and provide access for all users of roadways through the County's complete streets policy. Users of Kane County highways are familiar with the "look" of arterial roads and it is good practice to keep the corridors consistent.

This section of Orchard Road has not been updated yet and there are several crashes that continue in this area that could be mitigated by countermeasures used on the other areas of the Orchard Corridor. Kane County DOT studied the crash data and the types of crashes found in this area of Orchard. The vast majority of crashes (129 of 205 total) are rear-end crashes, with excessive speed contributing to the crashes as well as pavement condition (wet). A careful review of potential countermeasures provided several options to address these crashes:

- Utilizing a flashing yellow arrow to further remind turning drivers to yield to oncoming traffic
- Wet reflective pavement markings, which will assist drivers in rain/low visibility driving conditions.

Kane County DOT has used these countermeasures in other areas of the County and is familiar with their installation and operation and drivers are also familiar with them. This experience with other HSIP-funded projects and these suggested countermeasures will ensure the project will be completed on time and will be evaluated continuously.

Uniform Application for State Grant Assistance

Agency Completed Section

1.	Type of Submission	☐ Pre-application ☒ Application ☐ Changed / Corrected Application
2.	Type of Application	☒ New ☐ Continuation (i.e. multiple year grant) ☐ Revision (modification to initial application)
3.	Date / Time Received by State	Completed by State Agency upon Receipt of Application
4.	Name of the Awarding State Agency	Illinois Department of Transportation
5.	Catalog of State Financial Assistance (CSFA) Number	494-00-1004
6.	CSFA Title	Local Highway Safety Improvement Program
Catalog of Federal Domestic Assistance (CFDA) ☒ Not applicable (No federal funding)		
7.	CFDA Number	20.205
8.	CFDA Title	Highway Planning and Construction
9.	CFDA Number	
10.	CFDA Title	
Funding Opportunity Information		
11.	Funding Opportunity Number	19-1004-01
12.	Funding Opportunity Title	Local HIghway Safety Improvement Program
Competition Identification ☒ Not Applicable		
13.	Competition Identification Number	
14.	Competition Identification Title	

Applicant Completed Section		
Applicant Information		
15.	Legal Name	Kane County Division of Transportation
16.	Common Name (DBA)	
17.	Employer / Taxpayer Identification Number (EIN, TIN)	36-6006585
18.	Organizational DUNS number	945248565
19.	SAM Cage Code	63RN4
20.	Business Address	Street address: 41W011 Burlington Road City: St. Charles State: IL County: Kane Zip + 4: 60175-8412
Applicant's Organizational Unit		
21.	Department Name	Transportation
22.	Division Name	Design
Applicant's Name and Contact Information for Person to be Contacted for *Program* Matters involving this Application		
23.	First Name	Jennifer
24.	Last Name	Becker
25.	Suffix	Ms.
26.	Title	Chief of Planning and Programming
27.	Organizational Affiliation	Kane County Division of Transportation
28.	Telephone Number	630 444 2957
29.	Fax Number	630 584 5239
30.	Email address	beckerjennifer@co.kane.il.us
Applicant's Name and Contact Information for Person to be Contacted for *Business/Administrative Office* Matters involving this Application		
31.	First Name	Kathleen
32.	Last Name	Hopkinson
33.	Suffix	Ms.
34.	Title	Chief of Finance
35.	Organizational Affiliation	Kane County Division of Transportation
36.	Telephone Number	630 584 1170
37.	Fax Number	630 584 5239
38.	Email address	hopkinsonkathleen@co.kane.il.us

Areas Affected		
39.	Areas Affected by the Project (cities, counties, state-wide)	City of Aurora, Village of Montgomery Add Attachments (e.g., maps)
40.	Legislative and Congressional Districts of Applicant	Illinois Senatorial Districts 33, 35, 25, 42 Illinois Representative District 43, 49, 50, 53, 66, 70, 83
41.	Legislative and Congressional Districts of Program / Project	Illinois Senatorial District 25 Illinois Representative District 50
Applicant's Project		
42.	Description Title of Applicant's Project	Orchard Road Corridor
43.	Proposed Project Term	Start Date: 2018 End Date: 2019
44.	Estimated Funding (include all that apply)	☒ Amount Requested from the State: 395,555 ☒ Applicant Contribution (e.g., in kind, matching): 43,951 ☐ Local Contribution: ☐ Other Source of Contribution: ☐ Program Income: Total Amount 439,505

Applicant Certification:

By signing this application, I certify (1) to the statements contained in the list of certifications* and (2) that the statements herein are true, complete and accurate to the best of my knowledge. I also provide the required assurances* and agree to comply with any resulting terms if I accept an award. I am aware that any false, fictitious, or fraudulent statements or claims may subject me to criminal, civil or administrative penalties. (U.S. Code, Title 218, Section 1001)

(*) The list of certification and assurances, or an internet site where you may obtain this list is contained in the Notice of Funding Opportunity.

☒ I agree

Authorized Representative		
45.	First Name	Kathleen
46.	Last Name	Hopkinson
47.	Suffix	Ms.
48.	Title	Chief of Finance
49.	Telephone Number	630 584 1170
50.	Fax Number	630 584 5265
51.	Email Address	hopkinsonkathleen@co.kane.il.us
52.	Signature of Authorized Representative	Kathleen T. Hopkinson
53.	Date Signed	6-2-17

Programmatic Risk Assessment Questionnaire

The purpose of this assessment is to evaluate the programmatic risk of the applicant. Limited program experience, protocols and internal control governing program delivery will increase an applicant's degree of risk but will not preclude the applicant from becoming a grantee. The applicant's degree of risk may require additional conditions to be incorporated into the grant award pursuant to 2 CFR 200.207.

Patterns or trends in programmatic risk will influence GATA training as well as the agency's monitoring plan. Appropriate support must be provided by GATU and the agency to build grantee capacity.

Process:

A. The questionnaire (including the agency and/or grant-specific questions) is distributed to the applicant by the agency prior to an awarding decision.
B. The applicant returns the completed questionnaire to the agency. The agency scores the questionnaire based on the responses provided by the applicant.
C. The calculated responses equate to a risk profile for each of the 4 risk categories.
D. The agency aligns the risk profile to the applicable specific condition(s) for medium and high risk applicants in each of the 4 risk categories.
E. The agency communicates the applicable specific condition(s) within the Notice of State Award.

In response to the requirements of 2 CFR 200.205, the awarding agency is required to review the programmatic risk posed by applicants. Five risk categories are assessed through this questionnaire:

1. **Quality of management systems and ability to meet the management standards**
2. **History of performance**
3. **Reports and findings from audits performed under Subpart F—Audit Requirements of this part or the reports and findings of any other available audit**
4. **The applicant's ability to effectively implement statutory, regulatory, or other requirements imposed on awardees.**

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1. **Quality of management systems and ability to meet the management standards**

1.1. Do you have written policies and procedures that guide program delivery on the topics of:

a. Quality assurance ☒ YES/☐ NO
b. Outcome tracking and reporting mechanisms ☒ YES/☐ NO
c. Relevant documentation of services/goods delivered ☒ YES/☐ NO
d. Staff performance management policies and procedures ☒ YES/☐ NO
Personnel policies and procedures that include conflict of interest statements ☒ YES/☐ NO
e. Complaint/grievance resolution policies and procedures ☒ YES/☐ NO

f. Governing body policies and procedures that include conflict of interest statements ☒ YES/☐ NO

g. Safeguarding funds, property and other assets against loss from unauthorized use or disposition ☒ YES/☐ NO

h. Management of grant term extensions, where applicable ☒ YES/☐ NO

1.2. Do you have internal controls that govern program delivery on the topics of:

a. Quality assurance reporting ☒ YES/☐ NO

b. Appropriate (to industry) supervision of staff ☒ YES/☐ NO

c. Unit costs analysis and management ☒ YES/☐ NO

d. Accreditation/licensing compliance program ☐ YES/☐ NO /☒ NOT APPLICABLE

1.3. Does the organization have written standards of conduct covering real or perceived conflict of interest related to actions of employees engaged in the selection, award or administration of contracts supported by grant awards? ☒ YES/☐ NO

1.4. How many years of experience does the project leader have managing the scope of services required under this program?

☒ More than five years (low risk)

☐ One to five years (medium risk)

☐ Less than one year (high risk)

1.5. Does the organization have a time and effort system that:

a. Records all time worked, including time not charged to awards? ☒ YES / ☐ NO

b. Is signed-off by the employee and a supervisor? ☒ YES/☐ NO

c. Includes an approved methodology? ☒ YES/☐ NO/☐ NOT APPLICABLE

☐ Question is not applicable because grants are based on a set rate or a per unit of service. Go to question 1.6.

1.6. Does the organization have controls for invoicing grants paid based on a rate or unit of service?

☒ YES/☐ NO

1.7. Does the organization apply the same standard for match requirements as it does for expenses?

☒ YES/☐ NO/☐ NOT APPLICABLE - WE'VE NOT BEEN SUBJECT TO MATCH REQUIREMENTS

1.8. To what extent are you able to produce periodic grant status reports to inform stakeholders about program outcomes?

☒ Reports are an established part of grant management procedures (low risk)

☐ We're developing reports as part of grant management procedures (medium risk)

☐ We do not currently have established reports as part of grant management (high risk)

2. **History of performance** (The applicant's record in managing grant awards, if it is a prior recipient of awards, including timeliness of compliance with applicable reporting requirements, conformance to the terms and conditions of previous awards, and if applicable, the extent to which any previously awarded amounts will be expended prior to future awards)

2.1. How many years of experience does your organization have with grants of comparable scope and/or capacity?

- [x] More than five years (low risk)
- [ ] One to five years (medium risk)
- [ ] Less than one year (high risk)
- [ ] No experience (high risk) GO TO QUESTION 3.3

2.2. If your organization has experience with grants of comparable scope and/or capacity, provide a brief description of similar project goals and outcomes; specify the applicable year: (Text response)

- see attached

2.3. During your last two fiscal years, how frequently has your organization submitted project performance reports on time?

- [x] Always (low risk)
- [ ] Reported late up to three times (medium risk)
- [ ] Reported late four or more times (high risk)
- [ ] Not applicable – not a requirement of awards previously received

2.4. Have there been any significant changes in your organization in the last fiscal year related to:

a.	Leadership change(s)	☐ YES/☒ NO
b.	Significant program / grant initiative(s)	☐ YES/☒ NO
c.	Structural changes	☐ YES/☒ NO
d.	Fiscal changes	☐ YES/☒ NO
e.	Statutory or regulatory requirements	☐ YES/☒ NO
f.	Other	☐ YES/☒ NO

2.5. Provide a brief explanation for all "YES" responses to question 2.4. (Text response) N/A

2.6. Does the organization utilize a sub-grantee/sub-recipient / sub-award to manage, administer or complete a project? ☐ YES/☒ NO If NO, go to question 2.10.

2.7. What responsibilities does the sub-grantee/sub-recipient/sub-award perform?

a.	Participant eligibility determination	☐ YES/☐ NO
b.	Performance reporting	☐ YES/☐ NO
c.	Program delivery functions	☐ YES/☐ NO
d.	Financial reporting	☐ YES/☐ NO
e.	Other	☐ YES/☐ NO

2.8. What percentage of grant funds does the organization pass on to sub-grantees/sub-recipients/sub-awards?

- ☐ Less than 10% (low risk)
- ☐ 10-20% (medium risk)
- ☐ More than 20% (high risk)

2.9. Does your organization have an implemented policy for sub-grantee monitoring? ☐ YES/☐ NO

If NO, go to 2.10. If YES, does it include:

- ☐ on-site review (low risk)
- ☐ review of prior monitoring (low risk)
- ☐ desk / quantitative review (medium risk)

2.10 Do you obtain prior written approval from the funding agency when:

a. The scope or objective of the program changes ☒ YES/☐ NO
b. Key personnel specified in the application change ☒ YES/☐ NO
c. The approved project director disengages for more than 3 months or reduces 25% of time devoted to the project ☒ YES/☐ NO

☐ Question is not applicable because organization has not been subject to these requirements

2.11 Does your organization have performance measurements that tie to financial data?

☒ YES/☐ NO

3. Reports and findings from audits performed under Subpart F—Audit Requirements of this part or the reports and findings of any other available audit

3.1. During the last two fiscal years, has your organization been out of compliance with *programmatic* terms and conditions of awards?

- ☐ Organization has not been audited; Go to Question 3.6
- ☒ No occurrences of non-compliance; Go to Question 3.6 (low risk)
- ☐ One to three occurrences of non-compliance (medium risk)
- ☐ Four or more occurrences of non-compliance (high risk)

3.2. If your organization had at least one occurrence of non-compliance with programmatic terms and conditions, summarize each occurrence. (Text response)

3.3. Have corrective actions been implemented within the specified timeframe? ☐ YES/☐ NO

3.4. Provide explanation for any corrective actions that were not implemented within the timeframe specified and for any corrective actions that remain open. (Text response)

3.5. Have there been conflict of interest-related findings within the last two fiscal years? ☐ YES/☐ NO

a. If NO, go to question 3.6. (low risk)
b. If YES, specify the conflict of interest-related finding and your response to the finding.

(Text response)

3.6. Has your organization been subject to conditional approvals due to program issues? ☐ YES/☒ NO

a. If NO, to go question 4.1.
b. If YES, specify the terms of the special condition and whether or not the special condition is still applicable. (Text response)

4. The applicant's ability to effectively implement statutory, regulatory, or other requirements imposed on awardees.

4.1. To what extent does your organization have policies to ensure programmatic expenses are reasonable, necessary and prudent (allowable)?

☒ Policies are implemented and followed (low risk)
☐ Policies are not fully implemented (high risk)
☐ The organization does not currently have these types of policies (high risk)

4.2. To what extent does your organization have policies to ensure programmatic activities are allowable?

☒ Policies are implemented and followed (low risk)
☐ Policies are not fully implemented (high risk)
☐ The organization does not currently have these types of policies (high risk)

4.3. To what extent is your organization able to comply with all statutory requirements of this program?

☒ Fully able to comply with all statutory requirements (low risk)
☐ With the following exception(s), the organization is able to comply: Text response of exception(s) (medium to high risk depending on the exceptions)

4.4. Has the organization been out of compliance with any statutory, regulatory or other requirements of grant funding within the last two fiscal years? ☐ YES/☒ NO

If YES, provide explanation. (Text response)

Certification Section - Add wording to validate that the responses provided are true and accurate and that all occurrence of non-compliance with programmatic requirements has been disclosed.

Kathleen T. Hopkinson — 6-1-17
Authorized Signature Chief, Finance — Date

Programmatic Risk Assessment Questionnaire - Extended Response to Question 2.2

Kane County has received HSIP funding in 2012, 2013, 2014, and 2016 and therefore has experience managing this fund source. In addition, Kane County received Surface Transportation Program (STP) funding annually; Congestion Mitigation Air Quality (CMAQ) funding in 2011, 2014, 2015; Truck Access Route Program (TARP) funding in 2014; Federal Lands Access Program (FLAP) in 2015. These are only recent examples. The HSIP funding received in past years is providing safety improvements at a corridor level and intersection-specific level which is what this current request will implement. The currently funded HSIP projects are under construction or recently constructed and sufficient data to evaluate the outcomes is not available yet.

Annual Crash Summary Sheet
Kane County HSIP Application 2017
Road Orchard Road
Limits Jericho to US 30

Type of Crash	2011	2012	2013	2014	2015
Total Number of Crashes	52	29	35	48	41
Fatal Crashes	0	0	0	0	1
Fatalities	0	0	0	0	1
A-Injury Crashes	2	1	0	1	1
A-Injuries	2	2	0	1	1
B-Injury Crashes	9	3	1	5	3
B-Injuries	12	4	1	7	5
C-Injury Crashes	7	7	7	8	3
C-Injuries	14	8	9	10	5
Propert Damage Only	34	18	27	34	33
Wet Weather Crashes	10	4	11	14	13
Darkness (not lighted) Crashes	6	2	2	4	5

Pedestrian	0	0	0	0	0
Animal	0	0	0	2	0
Overturned	0	1	0	0	0
Fixed Object	0	1	2	4	2
Other	0	0	0	0	1
Turning	5	2	12	9	10
Rear End	44	17	18	27	23
Sideswipe Same Dir	1	2	1	5	3
Sideswipe Opposite Dir	1	1	0	0	2
Head on	0	1	0	0	0
Angle	1	4	2	1	0
Pedalcyclist	0	0	0	0	0

2015 Data

ID	Crash Month	Crash Day	Total Killed	A Injuries	B Injuries	C Injuries	Crash Severity	Type Of Crash	Cause 1	Cause 2	Device Condition	Roadway Surface	Road Defects	Crash Injury Severity	Light Condition	Weather Code 2	hlink	prfixear	calcyear
1	12	7	0	1	0	0	Injury	Turning	Failing to Yield Right of Way	Under Influence of Alcohol/Drugs	Functioning Properly	Dry	No Defects	A Injury Crash	Darkness	Fog/Smoke/Haze	R:\CrashPD	20	2015
2	1	4	0	0	1	0	Injury	Turning	Improper Turning/No Signal	Failing to Yield Right of Way	Functioning Properly	Wet	No Defects	B Injury Crash	Darkness	Snow	R:\CrashPD	20	2015
3	3	13	0	0	0	1	Injury	Rear End	Failing to Reduce Speed to Avoid Crash	Following Too Closely	No Controls	Dry	No Defects	C Injury Crash	Darkness	Clear	R:\CrashPD	20	2015
4	11	17	0	0	0	1	Injury	Rear End	Distraction - From Inside Vehicle	Failing to Reduce Speed to Avoid Crash	No Controls	Wet	No Defects	C Injury Crash	Darkness	Rain	R:\CrashPD	20	2015
5	1	9	0	0	0	0	Property Damage	Sideswipe Same Direction	Weather	Exceeding Safe Speed For Conditions	No Controls	Ice	No Defects	No Injuries	Darkness	Snow	R:\CrashPD	20	2015
6	1	16	0	0	3	0	Injury	Turning	Failing to Reduce Speed to Avoid Crash	Exceeding Safe Speed For Conditions	Functioning Properly	Dry	No Defects	B Injury Crash	Darkness / Lighted Road	Clear	R:\CrashPD	20	2015
7	2	12	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Following Too Closely	No Controls	Dry	No Defects	No Injuries	Darkness / Lighted Road	Clear	R:\CrashPD	20	2015
8	12	31	0	0	0	0	Property Damage	Rear End	Improper Backing	NA	Functioning Properly	Dry	No Defects	No Injuries	Darkness / Lighted Road	Clear	R:\CrashPD	20	2015
9	1	7	0	0	0	0	Property Damage	Fixed Object	Driving Skills/Knowledge/Experience	Weather	No Controls	Snow or Slush	No Defects	No Injuries	Darkness / Lighted Road	Severe Cross Wind	R:\CrashPD	20	2015
10	2	25	0	0	0	0	Property Damage	Rear End	Weather	Following Too Closely	No Controls	Snow or Slush	No Defects	No Injuries	Darkness / Lighted Road	Snow	R:\CrashPD	20	2015
11	2	25	0	0	0	0	Property Damage	Turning	Weather	NA	Functioning Improperly	Snow or Slush	No Defects	No Injuries	Darkness / Lighted Road	Snow	R:\CrashPD	20	2015
12	12	20	0	0	1	1	Injury	Turning	Failing to Yield Right of Way	Failing to Reduce Speed to Avoid Crash	Functioning Properly	Dry	No Defects	B Injury Crash	Daylight	Clear	R:\CrashPD	20	2015
13	12	16	0	0	0	2	Injury	Turning	Failing to Yield Right of Way	Disregarding Traffic Signals	Functioning Properly	Wet	No Defects	C Injury Crash	Daylight	Rain	R:\CrashPD	20	2015
14	12	4	1	0	0	0	Fatal	Rear End	Failing to Reduce Speed to Avoid Crash	Unable to Determine	Functioning Properly	Dry	No Defects	Fatal Crash	Daylight	Clear	R:\CrashPD	20	2015
15	7	23	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	NA	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\CrashPD	20	2015
16	6	2	0	0	0	0	Property Damage	Rear End	Following Too Closely	Failing to Reduce Speed to Avoid Crash	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\CrashPD	20	2015
17	4	6	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Following Too Closely	No Controls	Dry	No Defects	No Injuries	Daylight	Clear	R:\CrashPD	20	2015
18	7	28	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Distraction - From Outside Vehicle	No Controls	Dry	No Defects	No Injuries	Daylight	Clear	R:\CrashPD	20	2015
19	3	30	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Failing to Yield Right of Way	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\CrashPD	20	2015
20	2	23	0	0	0	0	Property Damage	Rear End	Following Too Closely	Failing to Reduce Speed to Avoid Crash	No Controls	Dry	No Defects	No Injuries	Daylight	Clear	R:\CrashPD	20	2015
21	9	3	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Disregarding Stop Sign	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\CrashPD	20	2015
22	3	10	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	NA	No Controls	Dry	No Defects	No Injuries	Daylight	Clear	R:\CrashPD	20	2015
23	5	15	0	0	0	0	Property Damage	Rear End	Following Too Closely	Failing to Reduce Speed to Avoid Crash	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\CrashPD	20	2015
24	6	8	0	0	0	0	Property Damage	Rear End	Following Too Closely	Failing to Reduce Speed to Avoid Crash	No Controls	Dry	No Defects	No Injuries	Daylight	Clear	R:\CrashPD	20	2015
25	5	22	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	NA	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\CrashPD	20	2015
26	4	14	0	0	0	0	Property Damage	Rear End	Distraction - From Inside Vehicle	Failing to Reduce Speed to Avoid Crash	No Controls	Dry	No Defects	No Injuries	Daylight	Clear	R:\CrashPD	20	2015
27	5	7	0	0	0	0	Property Damage	Rear End	Physical Condition of Driver	NA	No Controls	Dry	No Defects	No Injuries	Daylight	Clear	R:\CrashPD	20	2015
28	4	30	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	NA	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\CrashPD	20	2015
29	6	23	0	0	0	0	Property Damage	Rear End	Following Too Closely	Failing to Reduce Speed to Avoid Crash	No Controls	Dry	No Defects	No Injuries	Daylight	Clear	R:\CrashPD	20	2015
30	5	15	0	0	0	0	Property Damage	Rear End	Cell Phone Use Other Than Texting	Failing to Reduce Speed to Avoid Crash	No Controls	Dry	No Defects	No Injuries	Daylight	Clear	R:\CrashPD	20	2015
31	5	23	0	0	0	0	Property Damage	Sideswipe Same Direction	Unable to Determine	NA	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\CrashPD	20	2015
32	8	6	0	0	0	0	Property Damage	Sideswipe Same Direction	Improper Overtaking/Passing	NA	No Controls	Dry	No Defects	No Injuries	Daylight	Clear	R:\CrashPD	20	2015
33	4	24	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	Driving Skills/Knowledge/Experience	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\CrashPD	20	2015
34	10	8	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	NA	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\CrashPD	20	2015
35	1	9	0	0	0	0	Property Damage	Fixed Object	Weather	Exceeding Safe Speed For Conditions	Functioning Properly	Ice	No Defects	No Injuries	Daylight	Clear	R:\CrashPD	20	2015
36	1	9	0	0	0	0	Property Damage	Sideswipe Opposite Direction	Exceeding Safe Speed For Conditions	NA	No Controls	Ice	Other	No Injuries	Daylight	Clear	R:\CrashPD	20	2015
37	1	4	0	0	0	0	Property Damage	Parked Motor Vehicle	Failing to Reduce Speed to Avoid Crash	Physical Condition of Driver	No Controls	Snow or Slush	No Defects	No Injuries	Daylight	Snow	R:\CrashPD	20	2015
38	1	9	0	0	0	0	Property Damage	Rear End	Following Too Closely	Failing to Reduce Speed to Avoid Crash	Functioning Properly	Snow or Slush	No Defects	No Injuries	Daylight	Clear	R:\CrashPD	20	2015
39	1	8	0	0	0	0	Property Damage	Turning	Exceeding Safe Speed For Conditions	Weather	Functioning Properly	Snow or Slush	No Defects	No Injuries	Daylight	Snow	R:\CrashPD	20	2015
40	1	13	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	Failing to Reduce Speed to Avoid Crash	Functioning Properly	Wet	No Defects	No Injuries	Daylight	Clear	R:\CrashPD	20	2015
41	1	9	0	0	0	0	Property Damage	Sideswipe Opposite Direction	Weather	Exceeding Safe Speed For Conditions	Functioning Improperly	Ice	No Defects	No Injuries	Dusk	Snow	R:\CrashPD	20	2015
			1	1	5	5													

Pedestrian				wet	4	Fatal	1
Animal				snow/ice	9	A crash	1
Overturned					13	B crash	3
Fixed Object	2					C Crash	3
Other	1					PDO	33
Turning	10						41
Rear End	23						
Sideswipe Same Dir	3					Darkness	5
Sideswipe Opposite Dir	2					Darkness/Lighted	6
Head on							
Angle							
Pedalcyclist							
	41						

2014 Data

ID	Crash Month	Crash Day	Total Killed	A Injuries	B Injuries	C Injuries	Crash Severity	Type Of Crash	Cause 1	Cause 2	Traffic Device	Device Condition	Roadway Surface	Road Defects	Crash Injury Severity	Light Condition	Weather Code 2	hlink	prfixear	calcyear
1	9	4	0	0	0	1	Injury	Animal	Animal	(N/A)	No Controls	No Controls	Dry	No Defects	C Injury Crash	Darkness	Clear	R:\crashPE	20	2014
2	2	12	0	0	0	1	Injury	Sideswipe Same Direction	Under Influence of Alcohol/Drugs	Failing to Reduce Speed to Avoid Crash	Traffic Signal	Functioning Properly	Dry	No Defects	C Injury Crash	Darkness	Clear	R:\crashPE	20	2014
3	9	18	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Driving Skills/Knowledge/Experience	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Darkness	Clear	R:\crashPE	20	2014
4	2	20	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Unable to Determine	No Controls	No Controls	Wet	No Defects	No Injuries	Darkness	Rain	R:\crashPE	20	2014
5	11	15	0	0	1	0	Injury	Fixed Object	Weather	Exceeding Safe Speed For Conditions	No Controls	No Controls	Ice	No Defects	B Injury Crash	Darkness/ Lighted Road	Snow	R:\crashPE	20	2014
6	1	20	0	0	1	0	Injury	Rear End	Failing to Reduce Speed to Avoid Crash	Unable to Determine	No Controls	No Controls	Wet	No Defects	B Injury Crash	Darkness/ Lighted Road	Snow	R:\crashPE	20	2014
7	5	2	0	0	0	1	Injury	Rear End	Failing to Reduce Speed to Avoid Crash	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	C Injury Crash	Darkness/ Lighted Road	Clear	R:\crashPE	20	2014
8	7	21	0	0	0	1	Injury	Rear End	Failing to Reduce Speed to Avoid Crash	(N/A)	No Controls	No Controls	Dry	No Defects	C Injury Crash	Darkness/ Lighted Road	Clear	R:\crashPE	20	2014
9	2	18	0	0	0	1	Injury	Turning	Failing to Yield Right of Way	Weather	No Controls	No Controls	Snow or Slush	No Defects	C Injury Crash	Darkness/ Lighted Road	Snow	R:\crashPE	20	2014
10	7	4	0	0	0	0	Property Damage	Animal	Animal	(N/A)	No Controls	No Controls	Dry	No Defects	No Injuries	Darkness/ Lighted Road	Clear	R:\crashPE	20	2014
11	12	4	0	0	0	0	Property Damage	Rear End	Following Too Closely	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Darkness/ Lighted Road	Clear	R:\crashPE	20	2014
12	11	19	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	Failing to Reduce Speed to Avoid Crash	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Darkness/ Lighted Road	Clear	R:\crashPE	20	2014
13	1	22	0	0	0	0	Property Damage	Turning	Improper Turning/No Signal	Weather	Traffic Signal	Functioning Properly	Snow or Slush	No Defects	No Injuries	Darkness/ Lighted Road	Other	R:\crashPE	20	2014
14	11	21	0	1	1	0	Injury	Turning	Failing to Yield Right of Way	(N/A)	No Controls	No Controls	Dry	No Defects	A Injury Crash	Daylight	Clear	R:\crashPE	20	2014
15	10	17	0	0	1	0	Injury	Rear End	Following Too Closely	Failing to Reduce Speed to Avoid Crash	No Controls	No Controls	Dry	No Defects	B Injury Crash	Daylight	Clear	R:\crashPE	20	2014
16	4	3	0	0	1	0	Injury	Sideswipe Same Direction	Improper Lane Usage	Failing to Reduce Speed to Avoid Crash	Traffic Signal	Functioning Properly	Dry	No Defects	B Injury Crash	Daylight	Clear	R:\crashPE	20	2014
17	9	13	0	0	2	0	Injury	Turning	Failing to Yield Right of Way	Vision Obscured	Traffic Signal	Functioning Properly	Dry	No Defects	B Injury Crash	Daylight	Clear	R:\crashPE	20	2014
18	3	6	0	0	0	1	Injury	Rear End	Failing to Reduce Speed to Avoid Crash	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	C Injury Crash	Daylight	Clear	R:\crashPE	20	2014
19	5	29	0	0	0	3	Injury	Turning	Improper Overtaking/Passing	Exceeding Safe Speed For Conditions	No Controls	No Controls	Dry	No Defects	C Injury Crash	Daylight	Clear	R:\crashPE	20	2014
20	2	5	0	0	0	1	Injury	Rear End	Failing to Reduce Speed to Avoid Crash	Weather	Traffic Signal	Functioning Properly	Snow or Slush	No Defects	C Injury Crash	Daylight	Snow	R:\crashPE	20	2014
21	5	24	0	0	0	0	Property Damage	Angle	Improper Backing	Failing to Reduce Speed to Avoid Crash	No Controls	No Controls	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2014
22	11	26	0	0	0	0	Property Damage	Fixed Object	Improper Lane Usage	(N/A)	No Controls	No Controls	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2014
23	10	27	0	0	0	0	Property Damage	Rear End	Following Too Closely	Failing to Reduce Speed to Avoid Crash	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2014
24	11	21	0	0	0	0	Property Damage	Rear End	Distraction - From Inside Vehicle	Failing to Reduce Speed to Avoid Crash	No Controls	No Controls	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2014
25	12	12	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Following Too Closely	No Controls	No Controls	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2014
26	11	5	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Following Too Closely	No Controls	No Controls	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2014
27	4	21	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Following Too Closely	No Controls	No Controls	Dry	No Defects	No Injuries	Daylight	Cloudy/Overcast	R:\crashPE	20	2014
28	8	12	0	0	0	0	Property Damage	Rear End	Following Too Closely	Failing to Reduce Speed to Avoid Crash	No Controls	No Controls	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2014
29	7	30	0	0	0	0	Property Damage	Rear End	Following Too Closely	Failing to Reduce Speed to Avoid Crash	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2014
30	11	7	0	0	0	0	Property Damage	Rear End	Following Too Closely	Failing to Reduce Speed to Avoid Crash	Lane Use Marking	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2014
31	5	3	0	0	0	0	Property Damage	Rear End	Following Too Closely	Failing to Reduce Speed to Avoid Crash	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2014
32	10	22	0	0	0	0	Property Damage	Rear End	Following Too Closely	Failing to Reduce Speed to Avoid Crash	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2014
33	10	1	0	0	0	0	Property Damage	Rear End	Following Too Closely	Failing to Reduce Speed to Avoid Crash	No Controls	No Controls	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2014
34	7	30	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Following Too Closely	No Controls	No Controls	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2014
35	8	12	0	0	0	0	Property Damage	Rear End	Following Too Closely	Failing to Reduce Speed to Avoid Crash	No Controls	No Controls	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2014
36	6	22	0	0	0	0	Property Damage	Rear End	Distraction - From Inside Vehicle	Failing to Yield Right of Way	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2014
37	7	18	0	0	0	0	Property Damage	Rear End	Driving Skills/Knowledge/Experience	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2014
38	12	11	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Following Too Closely	No Controls	No Controls	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2014
39	7	2	0	0	0	0	Property Damage	Sideswipe Same Direction	Evasive Action Due to Animal / Object / Non-Motorist	(N/A)	No Controls	No Controls	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2014
40	11	21	0	0	0	0	Property Damage	Turning	Following Too Closely	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2014
41	1	6	0	0	0	0	Property Damage	Rear End	Cell Phone Use Other Than Texting	Failing to Reduce Speed to Avoid Crash	Traffic Signal	Functioning Properly	Ice	No Defects	No Injuries	Daylight	Severe Cross Wind	R:\crashPE	20	2014
42	1	8	0	0	0	0	Property Damage	Sideswipe Same Direction	Weather	Exceeding Safe Speed For Conditions	No Controls	No Controls	Ice	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2014
43	1	27	0	0	0	0	Property Damage	Turning	Exceeding Safe Speed For Conditions	Weather	Traffic Signal	Functioning Properly	Ice	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2014
44	1	26	0	0	0	0	Property Damage	Fixed Object	Weather	(N/A)	Stop Sign/Flasher	Functioning Properly	Snow or Slush	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2014
45	4	28	0	0	0	0	Property Damage	Fixed Object	Equipment-Vehicle Condition	Exceeding Safe Speed For Conditions	Traffic Signal	Functioning Properly	Wet	No Defects	No Injuries	Daylight	Rain	R:\crashPE	20	2014
46	6	30	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	(N/A)	Traffic Signal	Functioning Properly	Wet	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2014
47	3	12	0	0	0	0	Property Damage	Sideswipe Same Direction	Exceeding Safe Speed For Conditions	Following Too Closely	Traffic Signal	Other	Wet	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2014
48	10	15	0	0	0	0	Property Damage	Turning	Improper Overtaking/Passing	Unable to Determine	Traffic Signal	Functioning Properly	Wet	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2014
			0	1	7	10														

Pedestrian	0			wet	6
Animal	2			snow/ice	8
Overturned	0				14
Fixed Object	4				
Other	0				
Turning	9				
Rear End	27				
Sideswipe Same Dir	5				
Sideswipe Opposite Dir	0				
Head on	0				
Angle	1				
Pedalcyclist	0				
	48				

Fatal	0
A crash	1
B crash	5
C Crash	8
PDO	34
	48

Darkness	4
Darkness/Lighted	9

2013 Data

ID	Crash Month	Crash Day	Total Killed	A Injuries	B Injuries	C Injuries	Crash Severity	Type Of Crash	Cause 1	Cause 2	Traffic Device	Device Condition	Roadway Surface	Road Defects	Crash Injury Severity	Light Condition	Weather Code 2	hlink	prfixear	calcyear
1	10	31	0	0	0	2	Injury	Turning	Disregarding Traffic Signals	Weather	Traffic Signal	Functioning Properly	Wet	No Defects	C Injury Crash	Darkness	Rain	R:\crashPE	20	2013
2	1	10	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Weather	Traffic Signal	Functioning Properly	Wet	No Defects	No Injuries	Darkness	Rain	R:\crashPE	20	2013
3	12	23	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Unable to Determine	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Darkness/ Lighted Road	Clear	R:\crashPE	20	2013
4	7	27	0	0	0	0	Property Damage	Rear End	Distraction - From Inside Vehicle	Failing to Reduce Speed to Avoid Crash	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Darkness/ Lighted Road	Clear	R:\crashPE	20	2013
5	12	30	0	0	0	0	Property Damage	Sideswipe Same Direction	Unable to Determine	(N/A)	Traffic Signal	Functioning Properly	Snow or Slush	No Defects	No Injuries	Darkness/ Lighted Road	Snow	R:\crashPE	20	2013
6	1	26	0	0	0	0	Property Damage	Fixed Object	Distraction - From Inside Vehicle	Weather	No Controls	No Controls	Wet	No Defects	No Injuries	Darkness/ Lighted Road	Rain	R:\crashPE	20	2013
7	10	31	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	Failing to Reduce Speed to Avoid Crash	Traffic Signal	Functioning Properly	Wet	No Defects	No Injuries	Darkness/ Lighted Road	Rain	R:\crashPE	20	2013
8	6	11	0	0	0	2	Injury	Turning	Driving Skills/Knowledge/Experience	Failing to Yield Right of Way	Traffic Signal	Functioning Properly	Dry	No Defects	C Injury Crash	Dawn	Clear	R:\crashPE	20	2013
9	6	13	0	0	0	0	Property Damage	Angle	Failing to Reduce Speed to Avoid Crash	(N/A)	Traffic Signal	Functioning Properly	Wet	Unknown	No Injuries	Dawn	Clear	R:\crashPE	20	2013
10	8	17	0	0	1	0	Injury	Fixed Object	Animal	Unable to Determine	No Controls	No Controls	Dry	No Defects	B Injury Crash	Daylight	Clear	R:\crashPE	20	2013
11	12	24	0	0	0	1	Injury	Rear End	Failing to Reduce Speed to Avoid Crash	Unable to Determine	Traffic Signal	Functioning Properly	Dry	No Defects	C Injury Crash	Daylight	Clear	R:\crashPE	20	2013
12	7	23	0	0	0	1	Injury	Turning	Failing to Yield Right of Way	Improper Turning/No Signal	Traffic Signal	Functioning Properly	Dry	No Defects	C Injury Crash	Daylight	Clear	R:\crashPE	20	2013
13	1	26	0	0	0	1	Injury	Rear End	Unable to Determine	Unable to Determine	Traffic Signal	Functioning Properly	Unknown	Unknown	C Injury Crash	Daylight	Snow	R:\crashPE	20	2013
14	8	22	0	0	0	1	Injury	Rear End	Following Too Closely	Failing to Yield Right of Way	Traffic Signal	Functioning Properly	Wet	No Defects	C Injury Crash	Daylight	Rain	R:\crashPE	20	2013
15	7	8	0	0	0	1	Injury	Rear End	Distraction - From Inside Vehicle	Failing to Reduce Speed to Avoid Crash	Traffic Signal	Functioning Properly	Wet	No Defects	C Injury Crash	Daylight	Rain	R:\crashPE	20	2013
16	10	5	0	0	0	0	Property Damage	Angle	Failing to Yield Right of Way	Failing to Reduce Speed to Avoid Crash	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2013
17	7	9	0	0	0	0	Property Damage	Rear End	Improper Overtaking/Passing	Unable to Determine	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2013
18	9	23	0	0	0	0	Property Damage	Rear End	Following Too Closely	(N/A)	No Controls	No Controls	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2013
19	5	19	0	0	0	0	Property Damage	Rear End	Improper Backing	Failing to Reduce Speed to Avoid Crash	Stop Sign/Flasher	Unknown	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2013
20	11	20	0	0	0	0	Property Damage	Rear End	Physical Condition of Driver	Failing to Reduce Speed to Avoid Crash	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2013
21	7	25	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Failing to Yield Right of Way	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2013
22	8	10	0	0	0	0	Property Damage	Rear End	Physical Condition of Driver	Failing to Reduce Speed to Avoid Crash	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2013
23	4	5	0	0	0	0	Property Damage	Rear End	Following Too Closely	(N/A)	No Controls	No Controls	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2013
24	2	24	0	0	0	0	Property Damage	Rear End	Following Too Closely	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2013
25	6	4	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Following Too Closely	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2013
26	10	2	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	Turning Right On Red	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2013
27	10	15	0	0	0	0	Property Damage	Turning	Exceeding Safe Speed For Conditions	Unable to Determine	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2013
28	9	6	0	0	0	0	Property Damage	Turning	Following Too Closely	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2013
29	6	18	0	0	0	0	Property Damage	Turning	Weather	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2013
30	12	6	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	Unable to Determine	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2013
31	7	1	0	0	0	0	Property Damage	Turning	Disregarding Traffic Signals	Failing to Yield Right of Way	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2013
32	9	3	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2013
33	12	8	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	(N/A)	Traffic Signal	Functioning Properly	Snow or Slush	No Defects	No Injuries	Daylight	Snow	R:\crashPE	20	2013
34	8	6	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Following Too Closely	Traffic Signal	Functioning Properly	Wet	No Defects	No Injuries	Daylight	Clear	R:\crashPE	20	2013
35	5	22	0	0	0	0	Property Damage	Turning	Failing to Yield Right of Way	Disregarding Stop Sign	Stop Sign/Flasher	Functioning Properly	Wet	No Defects	No Injuries	Daylight	Rain	R:\crashPE	20	2013
			0	0	1	9														

Pedestrian	0				
Animal	0				
Overturned	0				
Fixed Object	2				
Other	0				
Turning	12				
Rear End	18				
Sideswipe Same Dir	1				
Sideswipe Opposite Dir	0				
Head on	0				
Angle	2				
Pedalcyclist	0				
	35				

wet	9	Fatal	0
snow/ice	2	A crash	0
	11	B crash	1
		C Crash	7
		PDO	27
			35
		Darkness	2
		Darkness/Lighted	5

2012 Data

ID	Crash Month	Crash Day	Total Killed	A Injuries	B Injuries	C Injuries	Crash Severity	Type Of Crash	Cause 1	Cause 2	Traffic Device	Device Condition	Roadway Surface	Road Defects	Crash Injury Severity	Light Condition	Weather Code 2	hlink	prfixear	calcyear
1	8	13	0	0	0	0	Property Damage	Rear End	Distraction - From Inside Vehicle	(N/A)	Traffic Signal	Functioning Properly	Dry	Construction Zone	No Injuries	Darkness	Fog/Smoke/Haze	M:\Dotsen	20	2012
2	8	13	0	0	0	0	Property Damage	Fixed Object	Weather	Road Construction/Maintenance	No Controls	No Controls	Wet	Construction Zone	No Injuries	Darkness	Fog/Smoke/Haze	M:\Dotsen	20	2012
3	8	6	0	2	0	0	Injury	Head On	Under Influence of Alcohol/Drugs	Failing to Reduce Speed to Avoid Crash	No Controls	No Controls	Dry	No Defects	A Injury Crash	Darkness, Lighted Road	Clear	M:\Dotsen	20	2012
4	7	19	0	0	2	0	Injury	Angle	Disregarding Traffic Signals	Failing to Yield Right of Way	Traffic Signal	Functioning Properly	Dry	Maintenance Zone	B Injury Crash	Darkness, Lighted Road	Clear	M:\Dotsen	20	2012
5	8	5	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Unable to Determine	Traffic Signal	Functioning Properly	Dry	Construction Zone	No Injuries	Darkness, Lighted Road	Clear	M:\Dotsen	20	2012
6	5	3	0	0	1	0	Injury	Angle	Failing to Yield Right of Way	Unable to Determine	Traffic Signal	Functioning Properly	Dry	No Defects	B Injury Crash	Daylight	Clear	M:\Dotsen	20	2012
7	12	11	0	0	1	0	Injury	Rear End	Failing to Reduce Speed to Avoid Crash	Following Too Closely	Traffic Signal	Functioning Properly	Dry	No Defects	B Injury Crash	Daylight	Clear	M:\Dotsen	20	2012
8	4	16	0	0	0	1	Injury	Rear End	Distraction - From Inside Vehicle	Failing to Reduce Speed to Avoid Crash	No Controls	No Controls	Dry	Construction Zone	C Injury Crash	Daylight	Clear	M:\Dotsen	20	2012
9	5	4	0	0	0	1	Injury	Rear End	Failing to Reduce Speed to Avoid Crash	Unable to Determine	Traffic Signal	Functioning Properly	Dry	Construction Zone	C Injury Crash	Daylight	Clear	M:\Dotsen	20	2012
10	11	26	0	0	0	1	Injury	Rear End	Failing to Reduce Speed to Avoid Crash	Distraction - From Inside Vehicle	Traffic Signal	Functioning Properly	Dry	No Defects	C Injury Crash	Daylight	Clear	M:\Dotsen	20	2012
11	8	14	0	0	0	1	Injury	Rear End	Distraction Ã¯Â¿Â½ from outside vehicle	Failing to Reduce Speed to Avoid Crash	Lane Use Marking	Functioning Properly	Dry	Construction Zone	C Injury Crash	Daylight	Clear	M:\Dotsen	20	2012
12	7	5	0	0	0	2	Injury	Rear End	Failing to Reduce Speed to Avoid Crash	Road Construction/Maintenance	Other Regualtory Sig	Functioning Properly	Dry	Construction Zone	C Injury Crash	Daylight	Clear	M:\Dotsen	20	2012
13	11	7	0	0	0	1	Injury	Overturned	Exceeding Safe Speed For Conditions	Failing to Reduce Speed to Avoid Crash	Traffic Signal	Functioning Properly	Wet	No Defects	C Injury Crash	Daylight	Clear	M:\Dotsen	20	2012
14	10	19	0	0	0	1	Injury	Turning	Failing to Yield Right of Way	(N/A)	No Controls	No Controls	Wet	Construction Zone	C Injury Crash	Daylight	Clear	M:\Dotsen	20	2012
15	7	13	0	0	0	0	Property Damage	Angle	Failing to Reduce Speed to Avoid Crash	(N/A)	Traffic Signal	Functioning Improperly	Dry	No Defects	No Injuries	Daylight	Clear	M:\Dotsen	20	2012
16	7	18	0	0	0	0	Property Damage	Angle	Failing to Yield Right of Way	(N/A)	No Controls	No Controls	Dry	Construction Zone	No Injuries	Daylight	Clear	M:\Dotsen	20	2012
17	4	3	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Following Too Closely	Traffic Signal	Functioning Properly	Dry	Construction Zone	No Injuries	Daylight	Clear	M:\Dotsen	20	2012
18	3	18	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Following Too Closely	Lane Use Marking	Functioning Properly	Dry	Construction Zone	No Injuries	Daylight	Clear	M:\Dotsen	20	2012
19	10	29	0	0	0	0	Property Damage	Rear End	Distraction - From Inside Vehicle	Failing to Reduce Speed to Avoid Crash	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	M:\Dotsen	20	2012
20	3	23	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Improper Backing	Traffic Signal	Functioning Properly	Dry	Construction Zone	No Injuries	Daylight	Clear	M:\Dotsen	20	2012
21	8	31	0	0	0	0	Property Damage	Rear End	Distraction Ã¯Â¿Â½ from outside vehicle	(N/A)	Traffic Signal	Functioning Properly	Dry	Construction Zone	No Injuries	Daylight	Clear	M:\Dotsen	20	2012
22	5	9	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	(N/A)	Lane Use Marking	Functioning Properly	Dry	Construction Zone	No Injuries	Daylight	Clear	M:\Dotsen	20	2012
23	3	31	0	0	0	0	Property Damage	Rear End	Distraction - From Inside Vehicle	Following Too Closely	Traffic Signal	Functioning Properly	Dry	Construction Zone	No Injuries	Daylight	Clear	M:\Dotsen	20	2012
24	1	5	0	0	0	0	Property Damage	Rear End	Failing to Reduce Speed to Avoid Crash	Distraction Ã¯Â¿Â½ from outside vehicle	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	M:\Dotsen	20	2012
25	8	28	0	0	0	0	Property Damage	Sideswipe Opposite Direction	Physical Condition of Driver	Driving On Wrong Side/Wrong Way	Traffic Signal	Functioning Properly	Dry	Construction Zone	No Injuries	Daylight	Clear	M:\Dotsen	20	2012
26	9	8	0	0	0	0	Property Damage	Sideswipe Same Direction	Unable to Determine	(N/A)	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	M:\Dotsen	20	2012
27	5	4	0	0	0	0	Property Damage	Sideswipe Same Direction	Improper Lane Usage	Improper Lane Usage	No Controls	No Controls	Dry	Construction Zone	No Injuries	Daylight	Clear	M:\Dotsen	20	2012
28	1	8	0	0	0	0	Property Damage	Turning	Improper Turning/No Signal	Exceeding Safe Speed For Conditions	Traffic Signal	Functioning Properly	Dry	No Defects	No Injuries	Daylight	Clear	M:\Dotsen	20	2012
29	9	21	0	0	0	0	Property Damage	Rear End	Following Too Closely	Weather	Traffic Signal	Functioning Properly	Wet	Construction Zone	No Injuries	Daylight	Rain	M:\Dotsen	20	2012
			0	2	4	8														

Pedestrian	0			wet	4
Animal	0			snow/ice	0
Overturned	1				4
Fixed Object	1				
Other	0				
Turning	2				
Rear End	17				
Sideswipe Same Dir	2				
Sideswipe Opposite Dir	1				
Head on	1				
Angle	4				
Pedalcyclist	0				
	29				

Fatal	0
A crash	1
B crash	3
C Crash	7
PDO	18
	29
Darkness	2
Darkness/Lighted	3

2011 Data

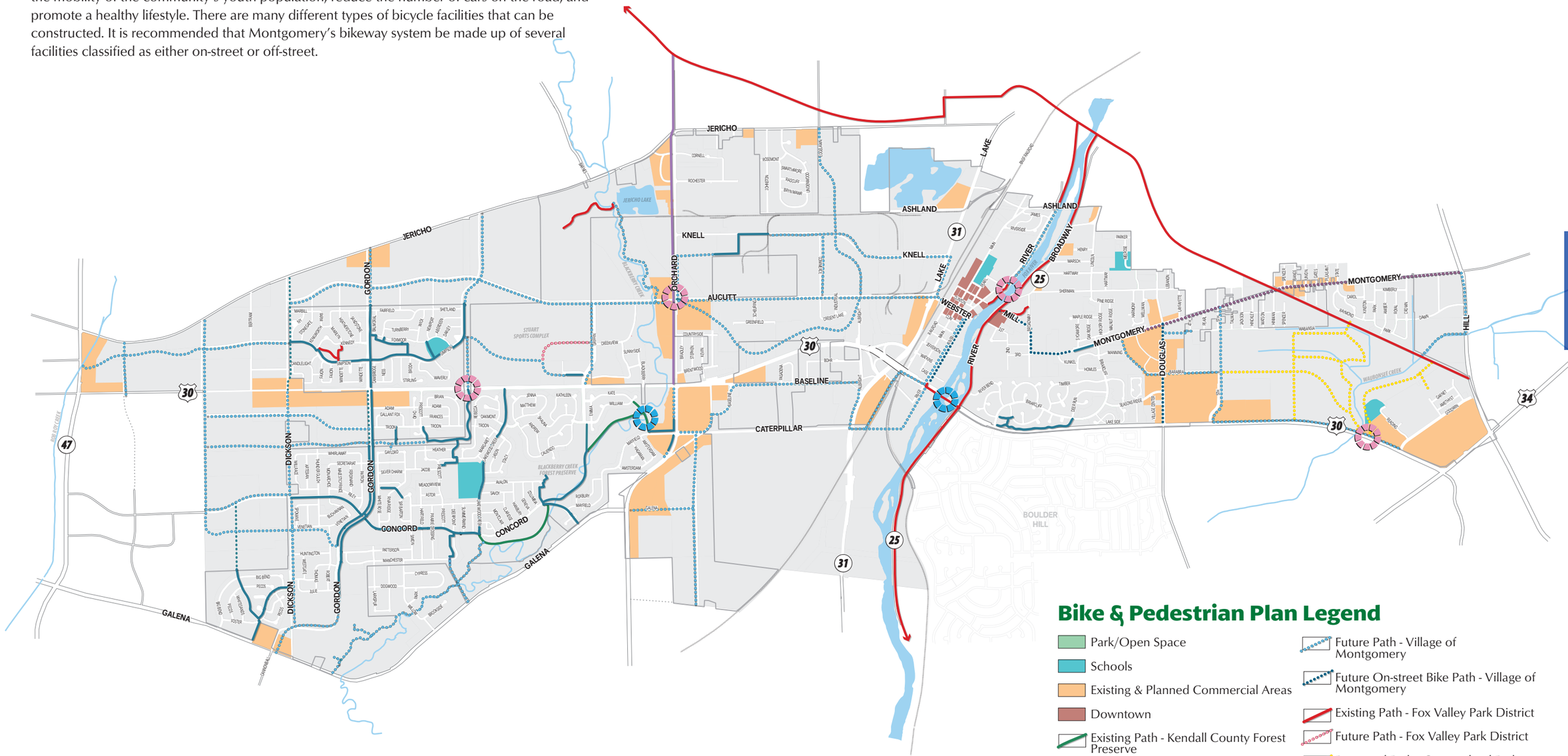
ID	Crash Month	Crash Day	Total Killed	A Injuries	B Injuries	C Injuries	Crash Severity	Type Of Crash	Cause 1	Cause 2	Traffic Device	Device Condition	Roadway Surface	Road Defects	Crash Injury Severity	Light Condition	Weather Code 2	hlink	prfixear	calcyear
1	11	19	0	0	2	0	Injury	11-Rear end	Failing to reduce speed to avoid crash	Following too closely	No controls	No controls	Dry	Construction zone	B Injury Crash	Darkness	Clear	M:\Dotsen	20	2011
2	11	21	0	0	0	3	Injury	11-Rear end	Failing to reduce speed to avoid crash	Not applicable	No controls	No controls	Dry	Construction zone	C Injury Crash	Darkness	Clear	M:\Dotsen	20	2011
3	10	6	0	0	0	0	Property Damage	11-Rear end	Not applicable	Not applicable	Traffic signal	Function properly	Dry	Construction zone	No Injuries	Darkness	Clear	M:\Dotsen	20	2011
4	12	2	0	0	0	0	Property Damage	11-Rear end	Failing to reduce speed to avoid crash	Following too closely	Lane use marking	Function properly	Dry	Construction zone	No Injuries	Darkness	Clear	M:\Dotsen	20	2011
5	5	22	0	0	1	2	Injury	15-Angle	Had been drinking	Exceeding safe speed for conditions	Lane use marking	Function properly	Wet	No defects	B Injury Crash	Darkness	Clear	M:\Dotsen	20	2011
6	2	19	0	0	0	0	Property Damage	13-Sideswipe opp. direction	Equipment Ã¢â‚¬Å“ vehicle condition	Not applicable	No controls	No controls	Dry	No defects	No Injuries	Darkness	Clear	M:\Dotsen	20	2011
7	4	9	0	0	1	0	Injury	10-Turning	Failing to yield right-of-way	Not applicable	Traffic signal	Function properly	Dry	No defects	B Injury Crash	Darkness/Lighted road	Clear	M:\Dotsen	20	2011
8	12	6	0	0	0	1	Injury	11-Rear end	Following too closely	Unable to determine	Traffic signal	Function properly	Dry	No defects	C Injury Crash	Darkness/Lighted road	Clear	M:\Dotsen	20	2011
9	10	25	0	0	1	0	Injury	11-Rear end	Failing to reduce speed to avoid crash	Not applicable	No controls	No controls	Dry	Construction zone	B Injury Crash	Daylight	Clear	M:\Dotsen	20	2011
10	12	12	0	0	2	0	Injury	11-Rear end	Following too closely	Failing to reduce speed to avoid crash	No controls	No controls	Dry	Construction zone	B Injury Crash	Daylight	Clear	M:\Dotsen	20	2011
11	12	5	0	0	0	1	Injury	11-Rear end	Unable to determine	Not applicable	No controls	No controls	Dry	Construction zone	C Injury Crash	Daylight	Clear	M:\Dotsen	20	2011
12	10	13	0	0	0	1	Injury	11-Rear end	Following too closely	Not applicable	Traffic signal	Function properly	Wet	Construction zone	C Injury Crash	Daylight	Rain	M:\Dotsen	20	2011
13	7	19	0	0	0	0	Property Damage	11-Rear end	Failing to reduce speed to avoid crash	Following too closely	No controls	No controls	Dry	Construction zone	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
14	7	24	0	0	0	0	Property Damage	11-Rear end	Distraction Ã¢â‚¬Å“ from inside vehicle	Following too closely	No controls	No controls	Dry	Construction zone	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
15	7	22	0	0	0	0	Property Damage	11-Rear end	Failing to reduce speed to avoid crash	Following too closely	No controls	No controls	Dry	Construction zone	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
16	7	23	0	0	0	0	Property Damage	11-Rear end	Failing to reduce speed to avoid crash	Not applicable	Traffic signal	Function properly	Dry	Construction zone	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
17	8	24	0	0	0	0	Property Damage	11-Rear end	Failing to reduce speed to avoid crash	Unable to determine	Traffic signal	Function properly	Dry	Construction zone	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
18	8	25	0	0	0	0	Property Damage	11-Rear end	Unable to determine	Not applicable	No controls	No controls	Dry	Construction zone	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
19	9	15	0	0	0	0	Property Damage	11-Rear end	Failing to reduce speed to avoid crash	Following too closely	Police/flagman	Worn reflect. material	Dry	Construction zone	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
20	10	12	0	0	0	0	Property Damage	11-Rear end	Distraction Ã¢â‚¬Å“ from outside vehicle	Failing to reduce speed to avoid crash	Police/flagman	Function properly	Dry	Construction zone	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
21	10	18	0	0	0	0	Property Damage	11-Rear end	Unable to determine	Not applicable	No controls	No controls	Dry	Construction zone	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
22	12	5	0	0	0	0	Property Damage	11-Rear end	Failing to reduce speed to avoid crash	Not applicable	No controls	No controls	Dry	Construction zone	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
23	12	2	0	0	0	0	Property Damage	11-Rear end	Failing to reduce speed to avoid crash	Following too closely	Lane use marking	Function properly	Dry	Construction zone	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
24	11	28	0	0	0	0	Property Damage	12-Sideswipe same direction	Improper overtaking/passing	Failing to reduce speed to avoid crash	No controls	No controls	Dry	Construction zone	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
25	5	5	0	1	0	0	Injury	11-Rear end	Failing to reduce speed to avoid crash	Unable to determine	No controls	No controls	Dry	No defects	A Injury Crash	Daylight	Clear	M:\Dotsen	20	2011
26	3	23	0	1	0	0	Injury	11-Rear end	Failing to reduce speed to avoid crash	Not applicable	Traffic signal	Function properly	Wet	No defects	A Injury Crash	Daylight	Clear	M:\Dotsen	20	2011
27	6	2	0	0	1	1	Injury	10-Turning	Failing to yield right-of-way	Failing to reduce speed to avoid crash	Traffic signal	Function properly	Dry	No defects	B Injury Crash	Daylight	Clear	M:\Dotsen	20	2011
28	4	15	0	0	2	0	Injury	10-Turning	Failing to yield right-of-way	Failing to reduce speed to avoid crash	Traffic signal	Function properly	Wet	No defects	B Injury Crash	Daylight	Rain	M:\Dotsen	20	2011
29	3	5	0	0	1	0	Injury	11-Rear end	Failing to reduce speed to avoid crash	Exceeding safe speed for conditions	Traffic signal	Function properly	Wet	No defects	B Injury Crash	Daylight	Snow	M:\Dotsen	20	2011
30	6	15	0	0	1	0	Injury	11-Rear end	Exceeding safe speed for conditions	Weather	Traffic signal	Function properly	Wet	No defects	B Injury Crash	Daylight	Rain	M:\Dotsen	20	2011
31	3	29	0	0	0	1	Injury	11-Rear end	Unable to determine	Not applicable	No controls	No controls	Dry	No defects	C Injury Crash	Daylight	Clear	M:\Dotsen	20	2011
32	8	11	0	0	0	3	Injury	11-Rear end	Distraction Ã¢â‚¬Å“ from inside vehicle	Failing to reduce speed to avoid crash	Traffic signal	Function properly	Dry	No defects	C Injury Crash	Daylight	Clear	M:\Dotsen	20	2011
33	5	20	0	0	0	0	Property Damage	10-Turning	Failing to yield right-of-way	Improper turning/no signal	Traffic signal	Function properly	Dry	No defects	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
34	2	21	0	0	0	0	Property Damage	11-Rear end	Failing to reduce speed to avoid crash	Unable to determine	Traffic signal	Function properly	Dry	No defects	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
35	1	25	0	0	0	0	Property Damage	11-Rear end	Following too closely	Failing to reduce speed to avoid crash	No controls	No controls	Dry	No defects	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
36	4	1	0	0	0	0	Property Damage	11-Rear end	Failing to reduce speed to avoid crash	Not applicable	Traffic signal	Function properly	Dry	No defects	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
37	3	7	0	0	0	0	Property Damage	11-Rear end	Distraction Ã¢â‚¬Å“ from inside vehicle	Failing to reduce speed to avoid crash	No controls	No controls	Dry	No defects	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
38	3	14	0	0	0	0	Property Damage	11-Rear end	Failing to reduce speed to avoid crash	Not applicable	Traffic signal	Function properly	Dry	No defects	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
39	4	4	0	0	0	0	Property Damage	11-Rear end	Failing to reduce speed to avoid crash	Unable to determine	Traffic signal	Function properly	Dry	No defects	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
40	4	20	0	0	0	0	Property Damage	11-Rear end	Distraction Ã¢â‚¬Å“ from outside vehicle	Failing to reduce speed to avoid crash	Traffic signal	Function properly	Dry	No defects	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
41	6	16	0	0	0	0	Property Damage	11-Rear end	Failing to reduce speed to avoid crash	Following too closely	Lane use marking	Function properly	Dry	No defects	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
42	6	29	0	0	0	0	Property Damage	11-Rear end	Failing to reduce speed to avoid crash	Distraction Ã¢â‚¬Å“ from inside vehicle	No controls	No controls	Dry	No defects	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
43	7	10	0	0	0	0	Property Damage	11-Rear end	Distraction Ã¢â‚¬Å“ from inside vehicle	Unable to determine	Traffic signal	Function properly	Dry	No defects	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
44	7	5	0	0	0	0	Property Damage	11-Rear end	Distraction Ã¢â‚¬Å“ from inside vehicle	Unable to determine	Stop sign/flasher	Function properly	Dry	No defects	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
45	11	28	0	0	0	0	Property Damage	11-Rear end	Failing to reduce speed to avoid crash	Operating vehicle in erractic, reckless, careless, negligent or aggressive manner	Traffic signal	Function properly	Dry	No defects	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
46	2	4	0	0	0	0	Property Damage	10-Turning	Failing to yield right-of-way	Failing to reduce speed to avoid crash	Traffic signal	Function properly	Wet	No defects	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
47	2	16	0	0	0	0	Property Damage	11-Rear end	Failing to reduce speed to avoid crash	Not applicable	Traffic signal	Function properly	Wet	No defects	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
48	3	9	0	0	0	0	Property Damage	11-Rear end	Following too closely	Failing to reduce speed to avoid crash	No controls	No controls	Wet	No defects	No Injuries	Daylight	Rain	M:\Dotsen	20	2011
49	4	1	0	0	0	0	Property Damage	11-Rear end	Failing to reduce speed to avoid crash	Exceeding safe speed for conditions	Traffic signal	Function properly	Wet	No defects	No Injuries	Daylight	Rain	M:\Dotsen	20	2011
50	9	17	0	0	0	0	Property Damage	11-Rear end	Following too closely	Operating vehicle in erractic, reckless, careless, negligent or aggressive manner	No controls	No controls	Dry	Unknown	No Injuries	Daylight	Clear	M:\Dotsen	20	2011
51	9	16	0	0	0	1	Injury	11-Rear end	Distraction Ã¢â‚¬Å“ from outside vehicle	Failing to reduce speed to avoid crash	No controls	No controls	Dry	Construction zone	C Injury Crash	Dusk	Clear	M:\Dotsen	20	2011
52	11	29	0	0	0	0	Property Damage	11-Rear end	Distraction Ã¢â‚¬Å“ from inside vehicle	Failing to reduce speed to avoid crash	No controls	No controls	Dry	Construction zone	No Injuries	Dusk	Clear	M:\Dotsen	20	2011
			0	2	12	14														

Pedestrian	0	wet	10	Fatal	0
Animal	0	snow/ice	0	A crash	2
Overturned	0		10	B crash	9
Fixed Object	0			C Crash	7
Other	0			PDO	34
Turning	5				52
Rear End	44				
Sideswipe Same Dir	1			Darkness	6
Sideswipe Opposite Dir	1			Darkness/Lighted	2
Head on	0				
Angle	1				
Pedalcyclist	0			Construction Zone	22
	52				

VILLAGE OF MONTGOMERY BIKE AND PEDESTRIAN PLAN

Bike & Pedestrian Plan

Montgomery's parks and open spaces provide the framework for a comprehensive bike network throughout the Village. Such a network would provide an alternative to motorized travel, increase the mobility of the community's youth population, reduce the number of cars on the road, and promote a healthy lifestyle. There are many different types of bicycle facilities that can be constructed. It is recommended that Montgomery's bikeway system be made up of several facilities classified as either on-street or off-street.



Bike & Pedestrian Plan Legend

- Park/Open Space
- Schools
- Existing & Planned Commercial Areas
- Downtown
- Existing Path - Kendall County Forest Preserve
- Existing Path - Kane County
- Future Path - Kane County
- Existing Path - Village of Montgomery
- Future Path - Village of Montgomery
- Future On-street Bike Path - Village of Montgomery
- Existing Path - Fox Valley Park District
- Future Path - Fox Valley Park District
- Proposed Path - Oswegoland Park District
- Existing Pedestrian Bridge
- Proposed Pedestrian Bridge

Attachment 6:
District One Correspondence

Peralte-Clark LLC

HYDRAULICS APPROVAL

DISTRICT ONE - HYDRAULICS CONCURRENCE

Jennifer Morales

From: Solomon, Marilin D <Marilin.Solomon@illinois.gov>
Sent: Tuesday, June 2, 2020 10:31 AM
To: Jennifer Morales
Cc: Kawash, Mohammad
Subject: FW: Kane Co Sec 18-00493-00-SP; Phase I (State Routes IL 31 & US 30); HSIP Federal Funding

Hi Jennifer,

Kindly attach the email response from D-1 Hydraulics Section Chief (Bureau of Programming) in the final PDR as a concurrence (that no review is required). Please include at TAB A-6 along with other State coordination.

Thanks,
Marilin (Mari) Solomon
847-705-4643

From: Masouridis, Eleftherios P <Eleftherios.Masouridis@illinois.gov>
Sent: Friday, May 29, 2020 9:48 PM
To: Solomon, Marilin D <Marilin.Solomon@illinois.gov>
Cc: Kawash, Mohammad <Mohammad.Kawash@illinois.gov>; Jennifer Morales <jennifer.morales@peralte-clark.com>
Subject: RE: Kane Co Sec 18-00493-00-SP; Phase I (State Routes IL 31 & US 30)

Marilin,

For Orchard @ US 30 the project limits end at the intersection and there are no significant changes.

For Il 31 @ Fabyan the changes seem more but looking at the grades it doesn't seem like much is changing.

We don't need to review it anymore prior to DA but in the future there should always be some sort of LDTM just like we do.

Perry

E. Perry Masouridis, P.E.
Hydraulics Section Chief
Bureau of Programming/District 1
Illinois Department of Transportation

Email: eleftherios.masouridis@illinois.gov
Tel: (847) 705-4474

From: Solomon, Marilin D <Marilin.Solomon@illinois.gov>
Sent: Friday, May 29, 2020 8:08 PM
To: Masouridis, Eleftherios P <Eleftherios.Masouridis@illinois.gov>
Cc: Kawash, Mohammad <Mohammad.Kawash@illinois.gov>; Jennifer Morales <jennifer.morales@peralte-clark.com>
Subject: Kane Co Sec 18-00493-00-SP; Phase I (State Routes IL 31 & US 30)

Hi Perry,

Trust all is well with you.

We would like to ask a favor: can you please check if a review from your Unit is required for the above subject project? (2 – State Routes)

Scope:

Fabyan Parkway at IL 31: installation of retroreflective, back plates, curb ramp & bus boarding area

Orchard at US 30: Reflective Pavement, New ADA Ramp, Traffic Signal Improvement & Radar Speed Limit

Attached are the Project description and Plans/ADA Ramp Detail (approved by Geometrics & ADA Coordinator).

Kindly advise if a review is required for the installation/upgrade of ADA Ramps. The project is ready for the submittal for Design Approval; however, we noticed that no coordination was submitted to you. Thank you, much appreciated.

Thanks,
Marilin (Mari) Solomon, P.E. (X-54643)
IDOT, D-1, BLRS West Division Field Engineer
847-705-4643

Note: Working remotely due COVID-19 crisis
Work Hours: 8:45 AM – 5:00 PM

GEOMETRIC APPROVAL

Illinois Department of Transportation

Informal Transmittal

To:	Steve Travia
Attn:	Marilin Solomon / Iyad Daas
Bureau:	Local Roads & Streets
Phone:	(847) 705-4407
Date:	May 9, 2019

From:	Jason Salley
Bureau:	Programming / Geometrics Unit
Phone:	(847) 705-4085
Subject:	Fabyan Pkwy at IL 31 Orchard Rd, Jericho Rd to US 30 LR&S Section # 18-00493-00-SP
	Geometric & IDS Approval

Please check appropriate box below:

- ☐ Take Necessary Action
- ☐ For Your Comments
- ☒ Per Your Request
- ☐ For Your Approval
- ☐ For Your Information
- ☐ See Me About the Attached
- ☐ Draft (Letter)(Memo) For My signature
- ☐ Reply
- ☐ Return
- ☐ Route
- ☐ File

Message

Marilin & Iyad,
All the design elements for this project meet current BDE Standards.

Therefore, I approve this project's geometry.

This project's BDE 2602 Form will be forwarded to IDOT BDE for their records.

Please contact me if you have any questions or comments.

Thanks,

Jason Salley, P.E.
Signature

Copies to

File	BDE	BOT	

Response

Signature

S:\WP\p&es\GEO\Geometric Approvals\2019\Fabyan Pkwy at IL 31 & Orchard Rd from Jericho Rd to US 30 (LR&S) 20190509.docx

Illinois Department of Transportation

Informal Transmittal

201 West Center Court
Schaumburg, IL 60196-1096

JNS/SES
DUE→5/22/19

To:	Issam Rayyan
Bureau:	Programming (Geometrics)
Attention:	Jason Salley
	1ST SUBMITTAL
Date:	April 10, 2019 ✓

From:	Marilin Solomon
Bureau:	Local Roads & Streets
By:	Iyad Daas
Subject:	Kane County
Section:	18-00493-00-SP

Please check appropriate box below:

- ☒ Take Necessary Action
- ☐ For Your Comments
- ☐ Per Your Request
- ☒ For Your Approval
- ☐ For Your Information
- ☐ See Me About the Attached
- ☐ Draft (Letter)(Memo) For My signature
- ☒ Reply
- ☒ Return
- ☐ Route
- ☐ File

Message

Jason,

- Fabyan Pkwy at IL Route 31 and Orchard Rd. from Jericho Rd. to US 30.

Attached is Phase I report for the subject federal aid project. Please review and provide your comments.
Should you have any questions please contact Marilin Solomon at 4407.
Thanks.

Feel free to call me at x4205 or email me at Iyad.Daas@Illinois.gov

Thank you.

Iyad Daas IYD
Signature

Copies to | File | | |

Response 5/9/19

MARILIN & IYAD - THIS PROJECT'S GEOMETRY IS APPROVED.

THANKS,

X4085
Signature

CC: JK & YH & JL(BOE)
DF(BOE)

transmem - Revised 2/19/98

Print Form | E-mail | Reset Form

1	Peralte-Clark LLC

☐ ☐

☐ ☑ Plan Sheets

FAU 363 / FAP 336		Fabyan Pkwy / Orchard Rd
FAU 3887 / FAU 1542 to FAP 349	IL 31 / NA to US 30	Batavia Ave / Jericho Rd to Baseline Rd

Kane	Various

Kane County DOT	18-00493-00-SP

Minor Safety Improvements To Both Roadways & Intersections.

05/09/2019

BUREAU OF TRAFFIC APPROVAL

VL

Illinois Department of Transportation

Memorandum

JML 2/3/2020

from To: Bureau of Traffic Operations Attn: C. LaRocco-Stresino

to From: Bureau of Local Roads

Subject: Plan Review

Date: December 20, 2019

SUBMITTAL/TYPE OF REVIEW

☒ Phase I	☐ LOI/LOU/Agreements	☐ Detector Loop Request	☐ Preliminary PS&E
☐ Warrants	☐ Cost Estimates	☐ Traffic Signal Design Request	☐ Pre-final PS&E
☐ TIS	☐ Work Zone Safety	☐ Railroad Report	☐ Final PS&E
☐ IDS	☐ Utility Permits	☐ Catalog Cuts	☐ HSIP

CONTACT PERSON: Iyad Daas SECTION CHIEF: Marilin Solomon
ROUTE: Fabyan Pkwy at IL Route 31., Orchard Rd.
LIMITS: Jericho Rd to US 30 TOWN/VILLAGE: Kane D.O.T
SECTION: 18-00493-00-SP COUNTY: Kane
JOB NUMBER: NA CONTRACT #: NA
LETTING DATE:
WERE COMMENTS PREVIOUSLY RECEIVED: ☒ YES ☐ NO
If yes, please attach a copy of the disposition of comments.

LOCAL ROADS RECEIVED FEB 05 2020

SECTION(S) TO REVIEW
(see attachment for section description)
Each section requires their own set of plans.

☐ PERMITS	ELECTRICAL DESIGN
☐ EXPRESSWAYS	☐ Lighting
☐ ELECTRICAL MAINTENANCE/TSC	☐ ITS
☐ STRUCTURAL OPS.	☐ Surveillance
☐ ARTERIALS	TRAFFIC PROGRAMS
☐ TRAFFIC CONTROL STAGING	☐ Design
	☒ Studies
	☐ Signals

Please allow up to three (3) weeks from the date of this request for review process.

For Traffic Operations only

Return to Connie by: 01-09-20 File Name: 19-2234

Comments:

Traffic Studies - No further comments

EMAIL CORRESPONDENCE WITH BUREAU OF TRAFFIC - OPERATIONS

John Clark

From: Lloyd, Jonathan M. <Jonathan.Lloyd@illinois.gov>
Sent: Friday, March 1, 2019 11:19 AM
To: John Clark; Solomon, Marilin D
Cc: Allison Eberhardt; Zulkowski, Stephen; zakosekmike@co.kane.il.us; Matt Turk
Subject: RE: State Route (IL Route 31) - KDOT 2018 HSIP Sec No. 18-00493-00SP - Fabyan Parkway at IL Route 31 Intersection

Dear John,

After discussing this internally, we are not going to require a capacity analysis. We will need to review the plans and proposed operation of the dilemma zone system.

Sincerely,

Jonathan M. Lloyd, P.E.
Traffic Studies Engineer
IDOT – District 1: Traffic Operations
847-705-4135

A goal we can all live with
Drive Zero Fatalities to a Reality
Please consider the environment before printing this e-mail.

From: John Clark <john.clark@peralte-clark.com>
Sent: Friday, February 08, 2019 12:02 PM
To: Solomon, Marilin D <Marilin.Solomon@illinois.gov>
Cc: Allison Eberhardt <allison.eberhardt@peralte-clark.com>; Lloyd, Jonathan M. <Jonathan.Lloyd@illinois.gov>; Zulkowski, Stephen <ZulkowskiStephen@co.kane.il.us>; zakosekmike@co.kane.il.us; Matt Turk <matt.turk@peralte-clark.com>
Subject: [External] State Route (IL Route 31) - KDOT 2018 HSIP Sec No. 18-00493-00SP - Fabyan Parkway at IL Route 31 Intersection

Hi Marilin,

Good morning. We have a question regarding required coordination with IDOT regarding the Kane County project, KDOT 2018 HSIP Sec No. 18-00493-00SP. At our project kick-off meeting at the IDOT BLR on November 29, 2018, Jonathan (Bureau of Traffic) mentioned that a signal timing analysis would be required if Kane County wished to utilize the planned installation of a dilemma zone detection system to adjust green time at the intersection. I have attached the meeting minutes from that meeting for your reference.

At this time, the dilemma zone detection system equipment is planned to be installed to monitor approaching traffic on Fabyan Parkway. No equipment is anticipated to be installed to monitor approaching traffic on IL Route 31.

We have spoken at length with Stephen Zulkowski, the Traffic Operations Engineer at KDOT, to discuss the implementation of a dilemma zone detection system at the intersection. We discussed signal phasing at this intersection and the current minimum and maximum green times when the coordinated route is in operation (weekdays

from 6am-11pm, Saturday from 7am-11pm and Sunday from 9am-9pm) and when it is not in operation (all other times). Fabyan Parkway is the coordinated route at this location. KDOT is planning on installing the dilemma zone detection system in a manner as to not impact the IL Route 31 signal timing. With the dilemma zone detection system, only the green time for Fabyan Parkway through movement will be affected. This green time will be reduced and the additional time allocated while the dilemma zone detection system is running will extend to the current max green time if the presence of vehicles is continuously detected.

Our question is - based on this approach, if the dilemma zone detection system is not going to adversely impact effective green time provided at this intersection for IL Route 31, is a signal timing study still required as part of this project?

We would be happy to schedule a time to discuss in person or on the phone, if it would help.

Thank you,

John A. Clark, P.E., ENV SP
Vice President

Peralte-Clark, LLC
171 West Wing Street, Suite 204B
Arlington Heights, Illinois 60005-5803
Main: 847-485-8069
Mobile: 312-965-9845
E-mail: john.clark@peralte-clark.com

www.peralte-clark.com

PROJECT ALERT FORM AND D1 ADA COORDINATOR CORRESPONDENCE

Jennifer Morales

From: Mate, Amruta P. <Amruta.Mate@Illinois.gov>
Sent: Friday, January 31, 2020 9:06 AM
To: Solomon, Marilin D
Subject: RE: PMA Seq 22205-D1-Kane Co Sec 18-00493-00-SP; PROJECT ALERT FORM D1 PD0038

Thank you. Please send to DOT.D1.ADA@illinois.gov in the future 😊

From: Solomon, Marilin D <Marilin.Solomon@illinois.gov>
Sent: Thursday, January 30, 2020 10:17 AM
To: Mate, Amruta P. <Amruta.Mate@Illinois.gov>
Cc: Kawash, Mohammad <Mohammad.Kawash@illinois.gov>; Jennifer Morales <jennifer.morales@peralte-clark.com>
Subject: FW: PMA Seq 22205-D1-Kane Co Sec 18-00493-00-SP; PROJECT ALERT FORM D1 PD0038

Hi Amruta,

We are forwarding the Project Alert Forms & Location maps of the above subject project for your review and concurrence. Kindly let us know if you have any comments.

Thanks,
Marilin (Mari) Solomon (54643)
West Division Field Engineer
847-705-4643

From: Jennifer Morales <jennifer.morales@peralte-clark.com>
Sent: Thursday, January 30, 2020 10:01 AM
To: Solomon, Marilin D <Marilin.Solomon@illinois.gov>; Kawash, Mohammad <Mohammad.Kawash@illinois.gov>
Cc: John Clark <john.clark@peralte-clark.com>; KDOT 2018 HSIP <18-0007-01@peralte-clark.com>; Mike Zakosek <zakosekmike@co.kane.il.us>
Subject: [External] PMA Seq 22205-D1-Kane Co Sec 18-00493-00-SP; PROJECT ALERT FORM D1 PD0038

Good Morning Marilin and Moe,

Attached are the Project Alert Forms for the KDOT 2018 HSIP Project with the requested project location maps. Separate forms were created for the Orchard Road Corridor location and the Fabyan Parkway at IL 31 location. Please let me know if you need additional information in order to forward to the IDOT D1 ADA Coordinator.

Thank you,

Jennifer

Jennifer Morales, P.E., ENV SP
Senior Project Manager
Peralte-Clark, LLC
171 West Wing Street, Suite 204B
Arlington Heights, Illinois 60005-5803
Mobile: 224-577-9016
E-mail: jennifer.morales@peralte-clark.com

Illinois Department of Transportation

ADA/PROWAG Project Alert

Date	From	Location/Bureau	Phone	Ext
01/24/20	Mike Zakosek, PE	Kane County DOT	(630) 406-7346	

Marked Route/Street

Name	Limits	Project/Permit Number
Fabyan Parkway	at Illinois Route 31	

Contract Number	Section Number
	18-00493-00-SP

Type of Work

☐ Reconstruction ☐ 3R/W&RS ☐ 3P/Resurfacing ☒ Other (explain) HSIP/Signal Improvements

Scope of Work

The purpose of the Kane County 2018 Highway Safety Improvement Program (HSIP) project is to provide safety countermeasures at the intersection of Fabyan Parkway at IL Route 31 in Batavia, IL. Proposed improvements at the intersection are confined within the existing right-of-way. Improvements include the installation of retro-reflective backplates on the existing signal heads, installation of wet reflective pavement markings, installation of advanced warning flashers, a dilemma zone detection system, and curb ramp, sidewalk and bus boarding area improvements. There are no geometric revisions proposed to the intersection, and the proposed pavement markings will match the existing land widths and configurations. Curb ramps in the northwest, southwest and southeast corners of the intersection have been redesigned to meet ADA requirements. A new crosswalk is proposed along the west side of IL Route 31 and a new curb ramp is proposed in the southwest quadrant of the intersection connecting the sidewalk from the northwest leg of the intersection to the sidewalk and multi-use path on the southwest leg of the intersection.

Municipality

Batavia

County

☐ Cook ☒ Kane ☐ Lake ☐ Various
☐ Du-Page ☐ Mchenry ☐ Will

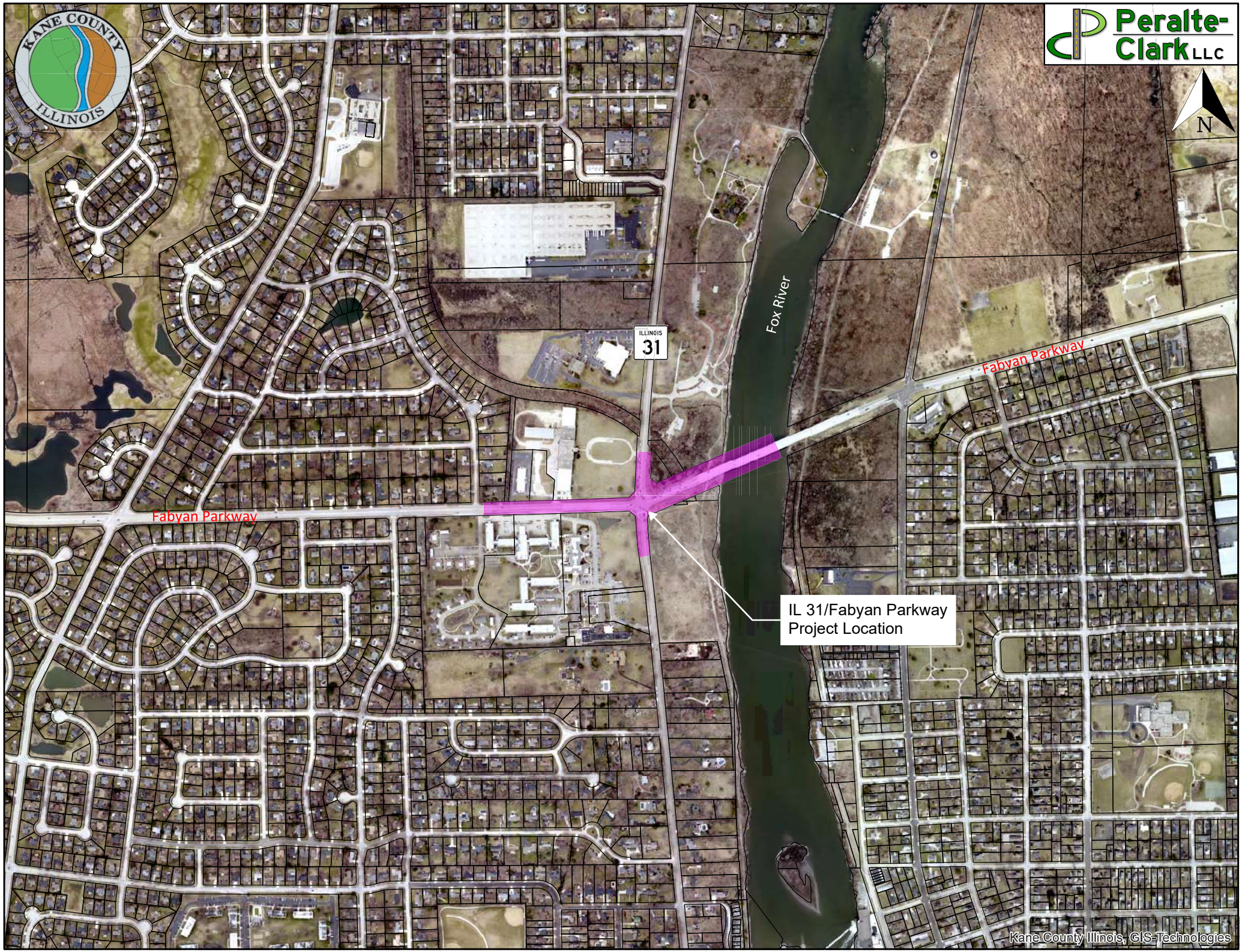
Letting Target	Design Approval Target Date
09-18-2020	03/01/20

*****Attach Location Map with alteration boundaries marked or ESR Exhibit*****

FOR ADA COORDINATOR USE ONLY

Date Uploaded into Inventory	Uploaded By

ADA Coordinator will update the GIS ADA Inventory points with Project Alert Form Information


KANE COUNTY
ILLINOIS
Peralte-Clark LLC
N
Fox River
ILLINOIS
31
Fabyan Parkway
Fabyan Parkway
IL 31/Fabyan Parkway
Project Location
Kane County Illinois, GIS-Technologies

Illinois Department of Transportation

ADA/PROWAG Project Alert

Date	From	Location/Bureau	Phone	Ext
01/30/20	Mike Zakosek, PE	Kane County DOT	(630) 406-7346	

Marked Route/Street

Name	Limits	Project/Permit Number
Orchard Road	US Route 30 to Jericho Road	

Contract Number	Section Number
	18-00493-00-SP

Type of Work

☐ Reconstruction ☐ 3R/W&RS ☐ 3P/Resurfacing ☒ Other (explain) HSIP/Signal Improvements

Scope of Work

The purpose of the Kane County 2018 Highway Safety Improvement Program (HSIP) project is to provide safety countermeasures along the Orchard Road corridor from US Route 30 to Jericho Road in Montgomery, IL and Unincorporated Kane County. Proposed improvements to the Orchard Road corridor are confined within the existing right-of-way. Improvements include the installation of wet reflective pavement markings, traffic signal improvements, the installation of radar speed limit signs, and new ADA ramps. Signals for the southbound Orchard Road at Aucutt Road left turn movement and the southbound and northbound Orchard Road at Rochester Drive left turn movement will be replaced with a 4-section signal head / flashing yellow arrow. As part of a Bike and Pedestrian Plan developed by the Village of Montgomery, a sidewalk is proposed along the south side of Aucutt Road and a multi-use path is proposed between Aucutt Road and US Route 30. As part of this HSIP project, ADA ramps, a crosswalk and pedestrian signals with push buttons will be installed across the south side of the intersection to accommodate the Village's future Bike and Pedestrian Plan. The proposed pedestrian accommodations will connect the existing multi-use path along the west side of Orchard Road to the future proposed paths. New ADA ramps are proposed at the south leg of the Orchard Road and Aucutt Road intersection.

Municipality

Montgomery and Unincorporated Kane County

County

☐ Cook ☒ Kane ☐ Lake ☐ Various
☐ Du-Page ☐ Mchenry ☐ Will

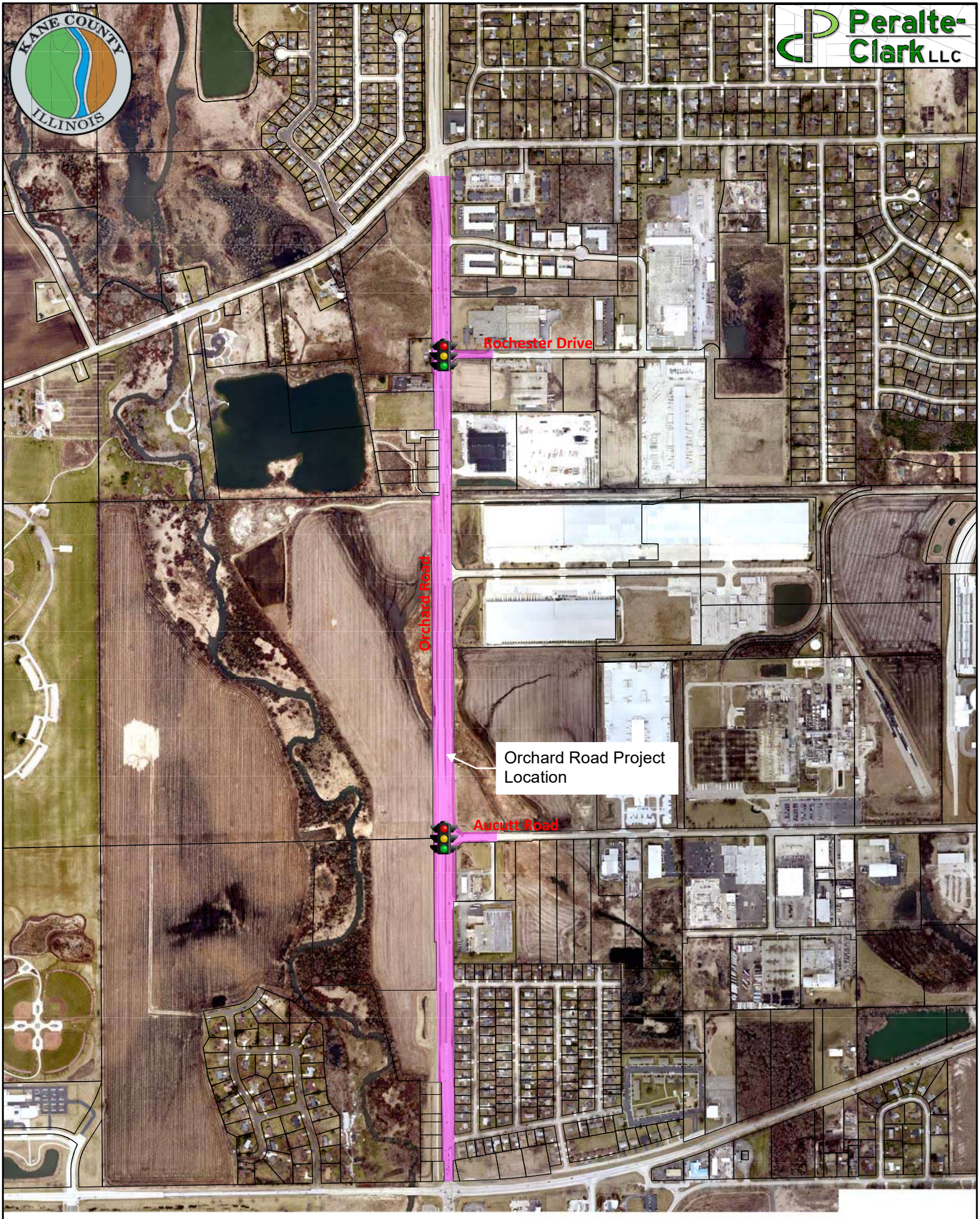
Letting Target	Design Approval Target Date
09-18-2020	03/01/20

*****Attach Location Map with alteration boundaries marked or ESR Exhibit*****

FOR ADA COORDINATOR USE ONLY

Date Uploaded into Inventory	Uploaded By

ADA Coordinator will update the GIS ADA Inventory points with Project Alert Form Information


KANE COUNTY
ILLINOIS
Peralte-Clark LLC
Rochester Drive
Orchard Road
Orchard Road Project Location
Aucutt Road
Kane County Illinois, GIS-Technologies

Jennifer Morales

From: Solomon, Marilin D <Marilin.Solomon@illinois.gov>
Sent: Friday, January 31, 2020 9:30 AM
To: Mate, Amruta P.
Cc: Kawash, Mohammad; Jennifer Morales
Subject: RE: PMA Seq 22205-D1-Kane Co Sec 18-00493-00-SP; PROJECT ALERT FORM D1 PD0038

Good am Amruta,

Thank you, will do.

Thanks,
Marilin (Mari) Solomon (54643)
West Division Field Engineer
847-705-4643

From: Mate, Amruta P. <Amruta.Mate@Illinois.gov>
Sent: Friday, January 31, 2020 9:06 AM
To: Solomon, Marilin D <Marilin.Solomon@illinois.gov>
Subject: RE: PMA Seq 22205-D1-Kane Co Sec 18-00493-00-SP; PROJECT ALERT FORM D1 PD0038

Thank you. Please send to DOT.D1.ADA@illinois.gov in the future 😊

From: Solomon, Marilin D <Marilin.Solomon@illinois.gov>
Sent: Thursday, January 30, 2020 10:17 AM
To: Mate, Amruta P. <Amruta.Mate@Illinois.gov>
Cc: Kawash, Mohammad <Mohammad.Kawash@illinois.gov>; Jennifer Morales <jennifer.morales@peralte-clark.com>
Subject: FW: PMA Seq 22205-D1-Kane Co Sec 18-00493-00-SP; PROJECT ALERT FORM D1 PD0038

Hi Amruta,

We are forwarding the Project Alert Forms & Location maps of the above subject project for your review and concurrence. Kindly let us know if you have any comments.

Thanks,
Marilin (Mari) Solomon (54643)
West Division Field Engineer
847-705-4643

From: Jennifer Morales <jennifer.morales@peralte-clark.com>
Sent: Thursday, January 30, 2020 10:01 AM
To: Solomon, Marilin D <Marilin.Solomon@illinois.gov>; Kawash, Mohammad <Mohammad.Kawash@illinois.gov>
Cc: John Clark <john.clark@peralte-clark.com>; KDOT 2018 HSIP <18-0007-01@peralte-clark.com>; Mike Zakosek <zakosekmike@co.kane.il.us>
Subject: [External] PMA Seq 22205-D1-Kane Co Sec 18-00493-00-SP; PROJECT ALERT FORM D1 PD0038

Good Morning Marilin and Moe,

Attached are the Project Alert Forms for the KDOT 2018 HSIP Project with the requested project location maps. Separate forms were created for the Orchard Road Corridor location and the Fabyan Parkway at IL 31 location. Please let me know if you need additional information in order to forward to the IDOT D1 ADA Coordinator.

Thank you,

Jennifer

Jennifer Morales, P.E., ENV SP
Senior Project Manager
Peralte-Clark, LLC
171 West Wing Street, Suite 204B
Arlington Heights, Illinois 60005-5803
Mobile: 224-577-9016
E-mail: jennifer.morales@peralte-clark.com
www.peralte-clark.com

Attachment 7:
Crash Analysis

Peralte-Clark LLC

Crash Analysis

Kane County Division of Transportation 2018 Highway Safety Improvement Program

Fabyan Parkway at IL Route 31 and Orchard Road from Jericho to US Route 30

Section No. 18-00493-00-SP

For the purpose of this Crash Analysis, information relating to the intersection of Fabyan Parkway at IL Route 31 was studied separately from the Orchard Road corridor from Jericho Road to US Route 30. Crash data for Fabyan Parkway at IL Route 31 is presented first, followed by crash data for Orchard Road. All crash data was derived from police crash reports obtained by Kane County Division of Transportation and the Illinois Department of Transportation.

Fabyan Parkway at IL 31 Crash Analysis

Crash data was analyzed for the five-year period between 2012 and 2016. A total of one-hundred crashes were recorded during this time. Turning and rear end crashes were the two most common types (see Table 1). Six other types of crashes occurred at the intersection, but none of them account for more than five percent of the total. It does not appear that a disproportionate number of crashes occur during wet pavement conditions (see Table 2). Since most of the crashes occur during daylight, and lighting is present at the intersection and some of the approaches, lighting is not a variable to the cause of the crashes (see Table 3).

Crashes were more common during weekdays than weekends. Seventy-nine percent of the one-hundred crashes occurred during weekdays (see Table 4). Evening rush hour between 3 PM and 7 PM was the most common time of the day for crashes to occur. Figure 1 shows the number of crashes for each one-hour time period during the day.

Turning Movements

Forty-two turning crashes occurred in the five-year period, accounting for forty-two percent of the total. Thirty-three of the turning movement crashes (seventy-nine percent of all turning crashes) occurred when a vehicle was turning left from eastbound Fabyan Parkway onto northbound IL Route 31. Table 5 documents the vehicle turning movements involved in the turning crashes. The severity of these crashes is shown in Table 6. In the study period, there was one fatality and five incapacitating, Type A, injuries that occurred during turning crashes. The fatality and three of the incapacitating injuries occurred during EB-NB turning movements (see Table 7).

The fatal crash occurred in October 2015 and involved a collision between a vehicle traveling westbound on Fabyan Parkway approaching the IL 31 intersection with a vehicle traveling eastbound on Fabyan Parkway preparing to turn northbound on to IL 31. Weather conditions were recorded as clear, roadway surface condition as dry, and the crash occurred in the evening while roadway lighting was illuminated. A review of the detailed police report indicates the crash occurred during a green light cycle. Turning movement crashes from EB to NB at this intersection make up 79% of the turning movement crashes, thereby justifying the need for safety improvements.

Rear End Collisions

Thirty-nine rear end crashes occurred during the study period, accounting for thirty-nine percent of the reported total. Table 8 details vehicle direction of travel when the crashes occurred. The severity of the rear end crashes is shown in Table 9.

Proposed Improvement

Various safety measures are proposed at this intersection to help reduce the number of crashes. Wet reflective pavement markings are proposed to increase motorists' visibility of the lane lines, stop bars and crosswalks. Advanced warning signs and flashers are proposed to notify drivers along Fabyan Parkway when the signal is about to turn red and that they should slow down and be cautious as they approach the intersection. This may reduce the number of rear-end crashes and turning movement crashes that occur during the signal phase change. Additionally, a dilemma zone detection system is proposed as part of this improvement to adjust the signal green time along Fabyan Parkway if traffic is detected within the dilemma zone. The dilemma zone is the area in advance of the intersection where motorists must to choose between going through the intersection or stopping abruptly when the signal turns yellow. The installation of the dilemma zone detection system will reduce the number of vehicles that run a red light through Fabyan Parkway. Thus, the number of crashes that occur due to motorists running red lights will be reduced as well.

Table 1 – Fabyan Parkway & IL 31 - Crash Type Split

Crash Type	Count	% of Total
Angle	5	5%
Fixed Object	5	5%
Head On	1	1%
Other Object	2	2%
Rear End	39	39%
Sideswipe Opposite Direction	1	1%
Sideswipe Same Direction	5	5%
Turning	42	42%
Grand Total	100	100%

Table 2 – Fabyan Parkway & IL 31 - Road Surface Condition Split

Road Surface Condition	Count	% of Total
Dry	58	58%
Ice	4	4%
Unknown	1	1%
Snow or Slush	8	8%
Wet	29	29%
Grand Total	100	100%

Table 3 – Fabyan Parkway & IL 31 – Daylight Conditions Split

Road Surface Condition	Count	% of Total
Darkness	1	1%
Darkness, Lighted Road	23	23%
Dawn	2	2%
Daylight	72	72%
Dusk	2	2%
Grand Total	100	100%

Table 4 - Fabyan Parkway & IL 31 – Day of Week Split

Crash Type	Count	% of Total
Monday	16	16%
Tuesday	12	12%
Wednesday	16	16%
Thursday	19	19%
Friday	16	16%
Saturday	12	12%
Sunday	9	9%
Grand Total	100	100%

Table 5 – Fabyan Parkway & IL 31 - Direction of Movement Split for Turning Movement Crashes

Movement	Count	% of Total
EB-NB Left	33	79%
NB-EB Right	1	2%
NB-WB Left	3	7%
SB-EB Left	2	5%
SB-EB Left, NB-EB Right	1	2%
WB-SB Left	2	5%
Grand Total	**42**	**100%**

Table 6 – Fabyan Parkway & IL 31 - Turning Movement Crash Severity

Crash Severity	Count	% of Total
A	5	12%
B	9	21%
C	7	17%
K	1	2%
PDO	20	48%
Grand Total	**42**	**100%**

Table 7 – Fabyan Parkway & IL 31 – EB-NB Left Turning Movement Crash Severity

Crash Severity	Count	% of Total
A	3	9%
B	7	21%
C	7	21%
K	1	3%
PDO	15	45%
Grand Total	**33**	**100%**

Table 8 – Fabyan Parkway & IL 31 – Direction of Movement Split for Rear End Crashes

Movement	Count	% of Total
EB	11	28%
EB-SB Right	1	3%
NB	10	26%
Not Given	4	10%
SB	5	13%
WB	8	21%
Grand Total	**39**	**100%**

Table 9 – Fabyan Parkway & IL 31 -Rear End Crash Severity

Crash Severity	Count	% of Total
A	0	0%
B	1	3%
C	7	18%
K	0	0%
PDO	31	79%
Grand Total	**39**	**100%**

Figure 1: Fabyan Parkway & IL 31 Crash Time Split

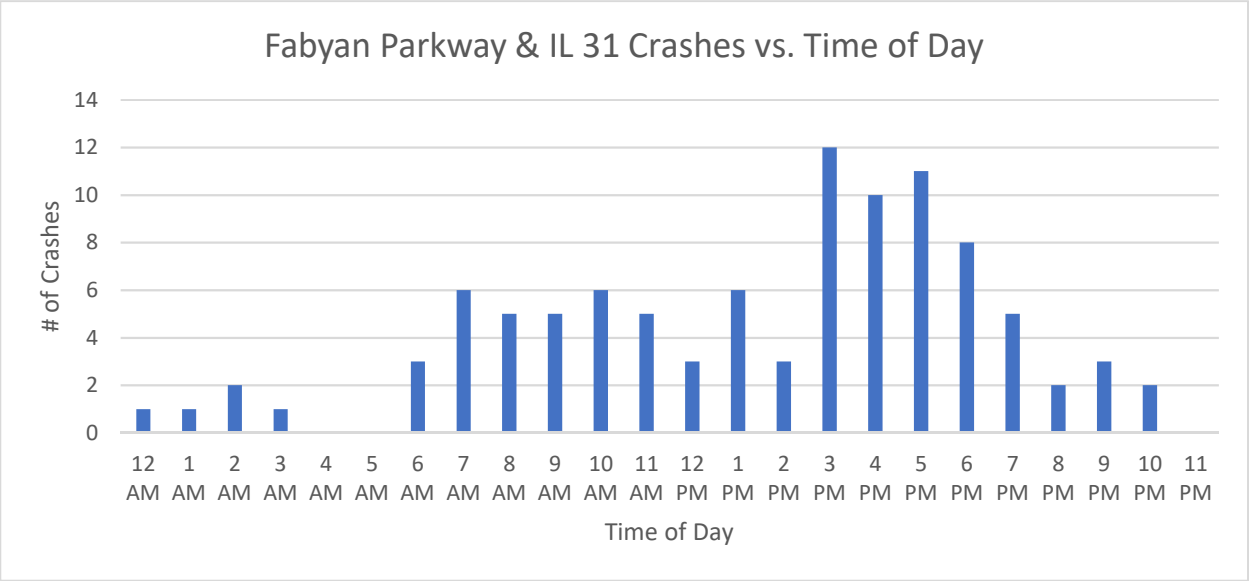


Table 10 - Crash Data for Fabyan Parkway at IL Route 31 from 2012 to 2016

						Number of Injuries						
	Day	Date	Time	Crash Type	Other Info, Direction From - To	Severity	A	B	C	Fatal	Daylight Conditions	Weather	Roadway Surface Conditions
1	Thursday	1/12/2012	9:43 AM	Rear End	NB	PDO					Daylight	Snow	Snow or Slush
2	Monday	1/23/2012	6:17 PM	Rear End	EB	PDO					Darkness, Lighted Road	Clear	Wet
3	Friday	2/10/2012	2:41 PM	Other Object	Wheel Fell Off	PDO					Daylight	Clear	Wet
4	Thursday	2/16/2012	8:22 AM	Turning	SB-EB Left	B		1			Daylight	Clear	Wet
5	Monday	3/19/2012	3:57 PM	Turning	EB-NB Left	PDO					Daylight	Clear	Dry
6	Tuesday	5/8/2012	6:08 PM	Angle	NB WB SB	B		3			Daylight	Clear	Dry
7	Monday	5/14/2012	10:02 AM	Other Object	NB	B		1			Daylight	Clear	Dry
8	Tuesday	5/15/2012	9:10 PM	Sideswipe Same Direction	NB	PDO					Darkness	Rain	Wet
9	Wednesday	5/16/2012	10:49 AM	Rear End	WB	C			1		Daylight	Clear	Dry
10	Tuesday	5/29/2012	3:56 PM	Rear End	EB	PDO					Daylight	Clear	Dry
11	Thursday	6/7/2012	7:22 AM	Turning	SB-EB Left NB-EB Right	PDO					Daylight	Clear	Dry
12	Sunday	7/15/2012	9:49 AM	Turning	EB-NB Left	C			1		Daylight	Clear	Dry
13	Wednesday	8/1/2012	4:23 PM	Turning	EB-NB Left	PDO					Daylight	Clear	Dry
14	Sunday	8/5/2012	1:33 PM	Rear End	EB	PDO					Daylight	Clear	Dry
15	Friday	8/17/2012	12:10 PM	Sideswipe Same Direction	EB	PDO					Daylight	Clear	Dry
16	Friday	8/31/2012	8:40 AM	Turning	EB-NB Left	PDO					Daylight	Clear	Dry
17	Monday	9/10/2012	11:43 AM	Turning	NB-WB Left	B		1			Daylight	Clear	Dry
18	Friday	9/21/2012	6:27 PM	Rear End	WB	PDO					Darkness, Lighted Road	Rain	Wet
19	Wednesday	9/26/2012	8:23 PM	Turning	EB-NB Left	C			1		Darkness, Lighted Road	Clear	Dry
20	Thursday	9/27/2012	1:31 PM	Turning	EB-NB Left	C			2		Daylight	Clear	Dry
21	Thursday	10/4/2012	2:25 AM	Fixed Object	EB	B		1			Darkness, Lighted Road	Clear	Wet
22	Thursday	12/6/2012	5:12 PM	Turning	EB-NB Left	PDO					Darkness, Lighted Road	Clear	Dry
23	Sunday	12/30/2012	7:28 PM	Sideswipe Opposite Direction	EB/WB	A	2				Darkness, Lighted Road	Clear	Dry
24	Friday	1/25/2013	7:25 AM	Rear End	SB	C			1		Daylight	Snow	Snow or Slush
25	Tuesday	3/5/2013	9:00 AM	Rear End	WB	PDO					Daylight	Snow	Snow or Slush
26	Thursday	4/11/2013	6:20 AM	Angle	EB/NB	A	1				Dawn	Rain	Wet
27	Saturday	4/13/2013	1:07 PM	Head On	NB and SB	A	2		1		Daylight	Clear	Dry
28	Thursday	5/9/2013	3:41 PM	Rear End	WB	PDO					Daylight	Rain	Wet
29	Tuesday	5/28/2013	12:08 PM	Turning	EB-NB Left	C			1		Daylight	Clear	Dry
30	Tuesday	6/11/2013	3:51 PM	Rear End	NB	PDO					Daylight	Rain	Wet
31	Saturday	7/6/2013	12:25 PM	Turning	EB-NB Left	B		1			Daylight	Clear	Dry
32	Thursday	8/1/2013	8:10 AM	Rear End	NB	PDO					Daylight	Clear	Dry
33	Saturday	8/24/2013	8:30 AM	Turning	EB-NB Left	PDO					Daylight	Clear	Dry
34	Saturday	8/31/2013	10:29 PM	Rear End	NB	PDO					Darkness, Lighted Road	Clear	Dry
35	Monday	9/16/2013	5:00 PM	Rear End	Not Given	PDO					Daylight	Clear	Dry
36	Thursday	10/17/2013	7:05 AM	Turning	EB-NB Left	PDO					Daylight	Rain	Wet
37	Saturday	10/19/2013	11:31 AM	Turning	EB-NB Left	C			1		Daylight	Clear	Dry
38	Friday	10/25/2013	4:40 PM	Sideswipe Same Direction	EB	PDO					Daylight	Clear	Dry
39	Thursday	10/31/2013	4:01 PM	Rear End	SB	PDO					Daylight	Rain	Wet
40	Wednesday	12/4/2013	3:25 PM	Turning	NB-WB Left	A	1				Daylight	Rain	Wet
41	Monday	12/9/2013	6:29 PM	Turning	EB-NB Left	B		2			Daylight	Clear	Dry
42	Wednesday	12/11/2013	4:47 PM	Rear End	Not Given	PDO					Darkness, Lighted Road	Clear	Dry
43	Monday	12/16/2013	3:52 PM	Turning	NB-WB Left	A	1				Daylight	Snow	Wet

Table 10 - Crash Data for Fabyan Parkway at IL Route 31 from 2012 to 2016

							Number of Injuries						
	Day	Date	Time	Crash Type	Other Info, Direction From - To	Severity	A	B	C	Fatal	Daylight Conditions	Weather	Roadway Surface Conditions
44	Wednesday	12/18/2013	7:46 AM	Rear End	EB	PDO					Daylight	Clear	Ice
45	Thursday	1/23/2014	6:01 PM	Turning	EB-NB Left	B		1			Darkness, Lighted Road	Sleet/hail	Wet
46	Sunday	1/26/2014	2:30 AM	Angle	WB and SB	B		2			Darkness, Lighted Road	Snow	Snow or Slush
47	Friday	2/21/2014	8:45 AM	Rear End	WB	PDO					Daylight	Other	Ice
48	Thursday	2/27/2014	5:24 PM	Turning	EB-NB Left	PDO					Dusk	Clear	Dry
49	Monday	3/10/2014	5:45 PM	Turning	EB-NB Left	A	1				Daylight	Clear	Wet
50	Friday	3/21/2014	4:25 PM	Rear End	WB	C			1		Daylight	Clear	Dry
51	Tuesday	4/1/2014	10:00 AM	Rear End	NB	PDO					Daylight	Rain	Wet
52	Tuesday	4/8/2014	6:50 AM	Turning	EB-NB Left	PDO					Daylight	Clear	Wet
53	Thursday	4/17/2014	7:20 AM	Rear End	EB-SB Right	PDO					Daylight	Clear	Dry
54	Thursday	5/15/2014	1:15 PM	Rear End	NB	PDO					Daylight	Clear	Wet
55	Wednesday	6/4/2014	5:40 PM	Rear End	Not Given	PDO					Daylight	Clear	Dry
56	Saturday	6/7/2014	9:04 PM	Turning	NB-EB Right	PDO					Darkness, Lighted Road	Rain	Wet
57	Monday	6/30/2014	3:04 PM	Turning	EB-NB Left	PDO					Daylight	Clear	Dry
58	Wednesday	7/9/2014	3:52 PM	Rear End	EB	PDO					Daylight	Clear	Dry
59	Saturday	7/19/2014	10:03 PM	Sideswipe Same Direction	NB	PDO					Darkness, Lighted Road	Clear	Dry
60	Wednesday	10/15/2014	3:29 PM	Turning	EB-NB Left	A					Daylight	Clear	Dry
61	Tuesday	10/28/2014	5:02 PM	Rear End	NB	PDO					Daylight	Clear	Dry
62	Wednesday	11/12/2014	5:30 PM	Rear End	Not Given	PDO					Darkness, Lighted Road	Clear	Dry
63	Friday	11/21/2014	10:24 AM	Turning	SB-EB Left	PDO					Daylight	Clear	Dry
64	Saturday	12/6/2014	8:49 PM	Rear End	NB	PDO					Darkness, Lighted Road	Clear	Dry
65	Monday	1/5/2015	6:02 PM	Turning	EB-NB Left	PDO					Darkness, Lighted Road	Clear	Ice
66	Friday	1/9/2015	10:03 AM	Rear End	SB	PDO					Daylight	Clear	Snow or Slush
67	Monday	4/20/2015	3:46 PM	Turning	EB-NB Left	PDO					Daylight	Clear	Dry
68	Friday	4/24/2015	11:01 AM	Turning	EB-NB Left	A	1	1			Daylight	Clear	Dry
69	Friday	6/26/2015	5:05 PM	Rear End	EB	C			1		Daylight	Rain	Wet
70	Saturday	6/27/2015	6:42 PM	Turning	EB-NB Left	B		3			Daylight	Clear	Dry
71	Thursday	7/9/2015	4:54 PM	Rear End	EB	PDO					Daylight	Clear	Dry
72	Friday	7/17/2015	11:54 AM	Rear End	WB	PDO					Daylight	Clear	Dry
73	Friday	7/24/2015	1:32 PM	Sideswipe Same Direction	EB	PDO					Daylight	Clear	Dry
74	Saturday	8/1/2015	4:26 PM	Turning	EB-NB Left	PDO					Daylight	Clear	Dry
75	Wednesday	8/5/2015	10:23 AM	Turning	EB-NB Left	PDO					Daylight	Clear	Dry
76	Sunday	10/11/2015	7:20 PM	Turning	EB-NB Left	K	1		1	1	Darkness, Lighted Road	Clear	Dry
77	Tuesday	10/20/2015	1:45 AM	Fixed Object	Not Given	PDO					Darkness, Lighted Road	Clear	Dry
78	Wednesday	11/25/2015	3:32 PM	Turning	WB-SB Left	PDO					Daylight	Clear	Wet
79	Saturday	11/28/2015	7:27 PM	Rear End	SB	C			1		Darkness, Lighted Road	Clear	Dry
80	Wednesday	12/23/2015	2:02 PM	Turning	EB-NB Left	B		1			Daylight	Clear	Wet
81	Wednesday	12/30/2015	7:43 AM	Rear End	EB	PDO					Daylight	Snow	Snow or Slush
82	Wednesday	1/13/2016	6:34 PM	Turning	EB-NB Left	B		2			Darkness, Lighted Road	Clear	Wet
83	Sunday	1/17/2016	2:10 PM	Rear End	NB	C			1		Daylight	Clear	Dry
84	Thursday	1/21/2016	9:00 AM	Rear End	EB	PDO					Daylight	Clear	Wet
85	Sunday	2/14/2016	4:38 PM	Fixed Object	EB	PDO					Daylight	Snow	Snow or Slush
86	Sunday	2/14/2016	9:15 PM	Angle	NB and EB	PDO					Darkness, Lighted Road	Snow	Snow or Slush

Table 10 - Crash Data for Fabyan Parkway at IL Route 31 from 2012 to 2016

							Number of Injuries						
	Day	Date	Time	Crash Type	Other Info, Direction From - To	Severity	A	B	C	Fatal	Daylight Conditions	Weather	Roadway Surface Conditions
87	Tuesday	3/1/2016	9:58 AM	Angle	SB-WB Right	PDO					Daylight	Clear	Wet
88	Tuesday	3/15/2016	4:53 PM	Rear End	EB	B		3			Daylight	Clear	N/A
89	Thursday	3/24/2016	4:50 PM	Turning	EB-NB Left	C			1		Daylight	Rain	Wet
90	Monday	3/28/2016	5:27 PM	Turning	EB-NB Left	B		2			Daylight	Clear	Dry
91	Sunday	4/10/2016	12:55 AM	Turning	WB-SB Left	PDO					Daylight	Rain	Wet
92	Friday	4/29/2016	1:40 PM	Turning	EB-NB Left	C			1		Daylight	Clear	Dry
93	Monday	5/2/2016	5:33 PM	Rear End	SB	PDO					Daylight	Clear	Dry
94	Monday	5/9/2016	7:03 PM	Rear End	WB	C			2		Dusk	Rain	Wet
95	Monday	6/6/2016	3:14 PM	Rear End	EB	PDO					Daylight	Clear	Dry
96	Wednesday	7/13/2016	3:08 AM	Fixed Object	EB	B		1			Darkness, Lighted Road	Clear	Dry
97	Friday	9/2/2016	11:03 AM	Rear End	NB	PDO					Daylight	Clear	Dry
98	Saturday	10/29/2016	7:46 PM	Turning	EB-NB Left	PDO					Darkness, Lighted Road	Clear	Dry
99	Monday	12/12/2016	5:15 PM	Turning	EB-NB Left	PDO					Darkness, Lighted Road	Clear	Ice
100	Thursday	12/22/2016	6:30 AM	Fixed Object	EB	PDO					Dawn	Clear	Wet

Orchard Road Corridor Crash Analysis

Crash data was analyzed for the five-year period between 2012 and 2016. Initially, the data was analyzed holistically to include crashes within the entire Orchard Road corridor and crashes on side streets that occurred at their intersections with Orchard Road. The side streets studied include Cornell Ave, Rochester Road, Knell Road, Aucutt Road, Countryside Drive and Brentwood Ave. Crashes that occurred at the intersection of Orchard Road at Jericho Road and Orchard Road at US Route 30 were not included in this study. Later, the crash data for the Orchard Road at Aucutt Road and Orchard Road at Rochester Road intersections were analyzed independently. Refer to Figure 8 through Figure 12 for Intersection Crash Diagrams and Table 22 for a complete list of all recorded crashes in this study.

Orchard Road Corridor, Including Side Streets, from Jericho Road to US Route 30

A total of one hundred thirty-two crashes were reported within the corridor during the five-year span. Ninety-seven of the crashes occurred at intersections while thirty-five occurred between intersections on roadway segments. Sixty percent of crashes were rear end crashes and seventeen percent of the crashes were turning movement crashes (see Table 11). It does not appear as if a disproportionate number of accidents occurred during wet pavement conditions (see Table 12). Crashes occurred more frequently on weekdays than weekends (see Table 13). The most common time for crashes to occur were from 7-8 AM and 4-7 PM (see Figure 7).

More than half of the rear end crashes occurred with cars moving in the SB direction (see Table 14). Most rear end collisions occurred at the Orchard Road at Aucutt Road and Orchard Road at Rochester Road intersections (see Table 15). The majority of the rear-end collisions resulted only in property damage, but it is of note that one rear-end crash in 2015 at the Rochester Road intersection resulted in a fatality (see Table 16).

Table 17 shows the number of turning movement crashes broken up by intersection.

Aucutt Road Intersection

Thirty-six of the one hundred thirty-two crashes occurred at the intersection of Orchard Road at Aucutt Road (see Table 18). Most of the crashes at this intersection (fifty-six percent) were rear end collisions and turning movement crashes (twenty-four percent) were the second most common crash type (see Table 19). Table 20 details the crash severity of the rear end and turning movement collisions for both the Aucutt Road and Rochester Road intersections. It does not appear that lighting is an issue, as a majority of crashes occur during daylight and roadway lighting is present at the intersection (see Table 21).

Rochester Road Intersection

Twenty-two of the one hundred thirty-two crashes occurred at the intersection of Orchard Road at Rochester Road (see Table 18). The majority of the crashes at this intersection (seventy-seven percent) were rear end crashes and the second most common crash type was turning movement (fourteen percent) (see Table 19). Table 20 details the crash severity of the rear end and turning movement collisions at the intersection. It does not appear that lighting is an issue, as most crashes occur during daylight and roadway lighting is present at this intersection (see Table 21).

One fatality at the Orchard-Rochester intersection occurred during the study period. A review of the detailed police report indicates both vehicles were traveling southbound on Orchard Road. One vehicle was stopped during a red-light cycle when struck from behind. This rear-end crash occurred during the day under clear weather conditions and dry roadway surface conditions.

Proposed Improvement

Safety measures are proposed along Orchard Road as part of this project. Wet reflective pavement markings will increase driver's visibility of the lane lines, stop bars and crosswalks throughout the corridor. Four-section flashing yellow arrow signals are proposed along Orchard Road at the existing signalized intersections, Orchard Road at Aucutt Road and Orchard Road at Rochester Drive. The flashing yellow arrow signifies that drivers must yield to oncoming traffic before proceeding to make the left-turn. The addition of the four-section flashing yellow arrow signal heads provides a more intuitive indication of permitted turning movements and may reduce the number of left-turning crashes at these intersections.

Table 11 – Orchard Road Corridor - Crash Type Split

Crash Type	Count	% of Total
Animal	3	2%
Fixed Object	9	7%
Head On	1	1%
Other Non-collision	1	1%
Rear End	79	60%
Sideswipe Opposite Direction	3	2%
Sideswipe Same Direction	12	9%
Turning	23	17%
Parked Motor Vehicle	1	1%
Grand Total	**132**	**100%**

Table 12 – Orchard Road Corridor Crashes – Road Surface Condition

Road Surface Condition	Count	% of Total
Dry	102	77%
Ice	8	6%
Snow or slush	7	5%
Wet	15	11%
Grand Total	**132**	**100%**

Table 13 – Orchard Road Corridor – Day of Week Split

Day	Count	% of Total
Monday	18	14%
Tuesday	22	17%
Wednesday	26	20%
Thursday	23	17%
Friday	27	20%
Saturday	9	7%
Sunday	7	5%
Grand Total	**132**	**100%**

Table 14 – Orchard Road Corridor - Rear End Collision Directional Split

Movement	Count	% of Total
EB	1	1%
NB	19	24%
SB	42	53%
WB	16	20%
N/A	1	1%
Grand Total	**79**	**100%**

Tables 15A & 15B – Orchard Road Corridor - Rear End Collision Intersection/Segment Split

Intersection	Count	% of Total
Cornell	2	3%
Rochester	17	29%
Knell	1	2%
Aucutt	22	38%
Countryside	9	16%
Brentwood	7	12%
Grand Total	**58**	**100%**

Segment	Count	% of Total
Jericho to Cornell	1	5%
Cornell to Rochester	5	24%
Rochester to Knell	4	19%
Knell to Aucutt	4	19%
Aucutt to Countryside	2	10%
Countryside to Brentwood	5	24%
Grand Total	**21**	**100%**

Table 16 – Orchard Road Corridor - Rear End Collision Crash Severity

Crash Severity	Count	% of Total
A	0	0%
B	3	4%
C	12	15%
K	1	1%
PDO	63	80%
Grand Total	**79**	**100%**

Table 17 – Orchard Road Corridor - Turning Movement Collision Intersection Split

Intersection	Count	% of Total
Cornell	0	0%
Rochester	3	13%
Knell	2	9%
Aucutt	10	43%
Countryside	6	26%
Brentwood	2	9%
Grand Total	**23**	**100%**

Tables 18A & 18B – Orchard Road Corridor - Crash Split by Intersection/Segment

Intersection	Count	% of Total
Cornell	2	2%
Rochester	22	23%
Knell	5	5%
Aucutt	36	37%
Countryside	20	21%
Brentwood	12	12%
Grand Total	**97**	**100%**

Segment	Count	% of Total
Aucutt to Countryside	3	9%
Cornell to Rochester	7	20%
Countryside to Brentwood	6	17%
Jericho to Cornell	3	9%
Knell to Aucutt	8	23%
Rochester to Knell	8	23%
Grand Total	**35**	**100%**

Table 19 – Aucutt Road & Rochester Road Intersections - Crash Type Split

	Aucutt Road		Rochester Road	
Crash Type	**Count**	**% of Total**	**Count**	**% of Total**
Angle	0	0%	0	0%
Animal	1	3%	0	0%
Fixed Object	1	3%	1	5%
Head On	0	0%	0	0%
Rear End	22	61%	17	77%
Sideswipe Opposite Direction	2	6%	0	0%
Sideswipe Same Direction	0	0%	1	5%
Turning	10	28%	3	14%
Grand Total	**36**	**100%**	**22**	**100%**

Table 20 – Aucutt Road & Rochester Road Intersections - Crash Severity for Rear End and Turning Movement Collisions

	Aucutt Road		Rochester Road	
Crash Severity	**Rear End**	**Turning**	**Rear End**	**Turning**
A	-	1	-	-
B	1	5	-	-
C	3	1	3	-
PDO	18	3	13	3
K	-	-	1	-
Grand Total	**22**	**10**	**17**	**3**

Table 21 – Aucutt Road & Rochester Road Intersections – Daylight Conditions During Crash

	Aucutt Road		Rochester Road	
Daylight Condition	**Crash Count**	**% of Total**	**Crash Count**	**% of Total**
Darkness	3	8%	0	0%
Darkness, lighted road	4	11%	2	9%
Daylight	28	78%	20	91%
Dusk	1	3%	0	0%
Grand Total	**36**	**100%**	**22**	**100%**

Figure 7 – Orchard Road Corridor Crash Time Split

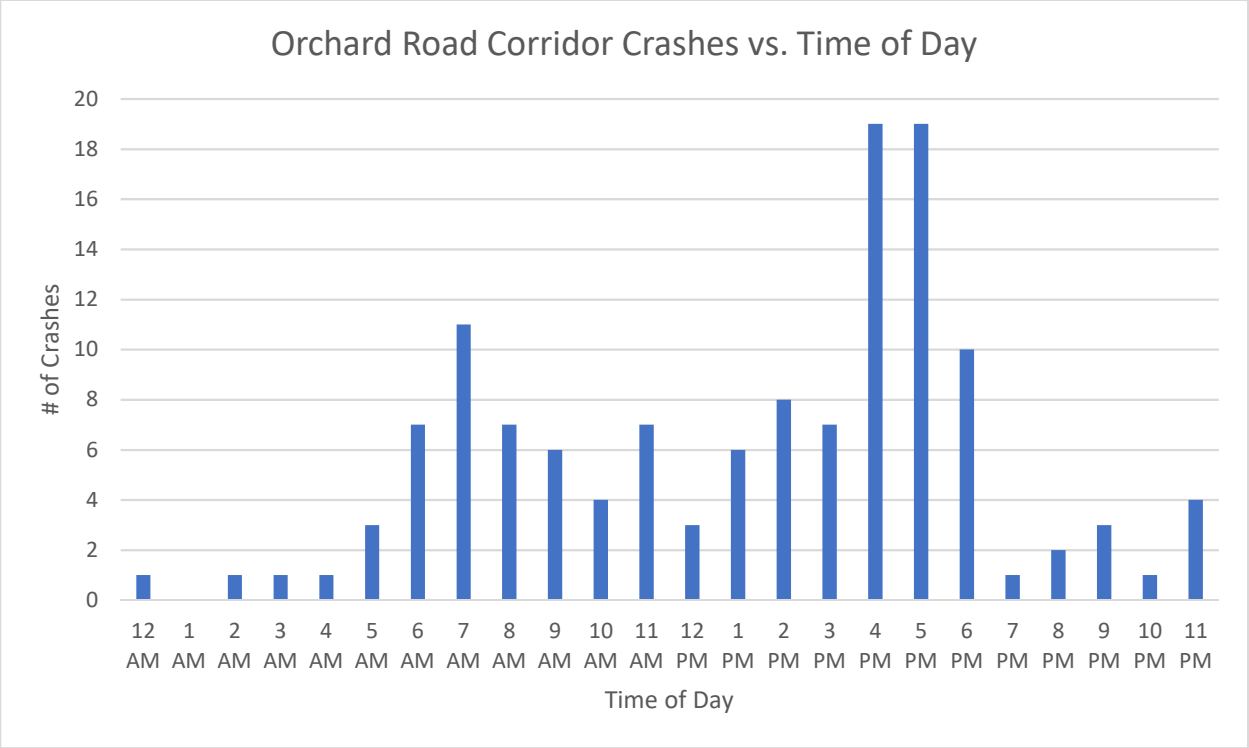


Table 22 - Crash Data for Orchard Road from South of Jericho Road to North of US Route 30

								Number of Injuries							
	Segment	Intersection	Date	Day	Time	Crash Type	Severity	A	B	C	Fatal	Daylight Conditions	Weather	Roadway Surface Conditions	Other Info, Direction From - To
1	Jericho to Cornell		5/4/2012	Friday	6:15:00 PM	Sideswipe Same Direction	PDO					Daylight	Clear	Dry	SB
2	Jericho to Cornell		11/9/2016	Wednesday	5:30:00 PM	Rear End	C			4		Darkness, lighted road	Clear	Dry	
3	Jericho to Cornell		8/15/2016	Monday	11:30:00 AM	Fixed Object	B		1			Daylight	Clear	Dry	NB
4		Cornell	12/11/2014	Thursday	4:00:00 PM	Rear End	PDO					Daylight	Clear	Dry	NB
5		Cornell	11/16/2016	Wednesday	7:30:00 AM	Rear End	C			1		Daylight	Clear	Dry	NB
6	Cornell to Rochester		4/30/2015	Thursday	7:31:00 AM	Rear End	PDO					Daylight	Clear	Dry	NB
7	Cornell to Rochester		7/4/2014	Friday	11:47:00 PM	Animal	PDO					Darkness, lighted road	Clear	Dry	SB
8	Cornell to Rochester		5/23/2015	Saturday	1:54:00 PM	Sideswipe Same Direction	PDO					Daylight	Clear	Dry	SB
9	Cornell to Rochester		7/9/2013	Tuesday	6:35:00 PM	Rear End	PDO					Daylight	Clear	Dry	NB
10	Cornell to Rochester		9/24/2016	Saturday	4:20:00 PM	Rear End	PDO					Daylight	Clear	Dry	NB
11	Cornell to Rochester		11/7/2014	Friday	8:25:00 AM	Rear End	PDO					Daylight	Clear	Dry	NB
12	Cornell to Rochester		11/16/2016	Wednesday	7:20:00 AM	Rear End	PDO					Daylight	Clear	Dry	NB
13		Rochester	7/30/2014	Wednesday	6:36:00 PM	Rear End	PDO					Daylight	Clear	Dry	SB
14		Rochester	3/23/2012	Friday	2:15:00 PM	Rear End	PDO					Daylight	Clear	Dry	WB
15		Rochester	4/3/2012	Tuesday	5:37:00 PM	Rear End	PDO					Daylight	Clear	Dry	SB
16		Rochester	8/5/2012	Sunday	10:38:00 PM	Rear End	PDO					Darkness, lighted road	Clear	Dry	SB
17		Rochester	4/5/2013	Friday	2:01:00 PM	Rear End	PDO					Daylight	Clear	Dry	SB
18		Rochester	7/1/2013	Monday	8:07:00 AM	Turning	PDO					Daylight	Clear	Dry	WB-SB Left
19		Rochester	1/22/2014	Wednesday	5:47:00 PM	Turning	PDO					Darkness, lighted road	Other	Snow or slush	SB-EB Left
20		Rochester	3/6/2014	Thursday	4:14:00 PM	Rear End	C			1		Daylight	Clear	Dry	NB
21		Rochester	3/12/2014	Wednesday	10:45:00 AM	Sideswipe Same Direction	PDO					Daylight	Clear	Wet	SB
22		Rochester	4/28/2014	Monday	6:04:00 AM	Fixed Object	PDO					Daylight	Rain	Wet	SB-EB Left
23		Rochester	10/22/2014	Wednesday	2:45:00 PM	Rear End	PDO					Daylight	Clear	Dry	NB
24		Rochester	5/12/2015	Tuesday	4:03:00 PM	Rear End	PDO					Daylight	Clear	Dry	WB
25		Rochester	5/15/2015	Friday	4:22:00 PM	Rear End	PDO					Daylight	Clear	Dry	WB
26		Rochester	6/23/2015	Tuesday	12:44:00 PM	Rear End	PDO					Daylight	Clear	Dry	NB
27		Rochester	12/4/2015	Friday	9:45:00 AM	Rear End	K				1	Daylight	Clear	Dry	SB
28		Rochester	2/16/2016	Tuesday	4:26:00 PM	Rear End	PDO					Daylight	Snow	Wet	SB
29		Rochester	8/26/2016	Friday	3:00:00 PM	Rear End	PDO					Daylight	Clear	Dry	SB
30		Rochester	12/8/2016	Thursday	7:28:00 AM	Rear End	C			2		Daylight	8 (?)	Dry	NB
31		Rochester	9/6/2013	Friday	5:15:00 PM	Rear End	PDO					Daylight	Clear	Dry	WB
32		Rochester	10/1/2014	Wednesday	11:29:00 AM	Rear End	PDO					Daylight	Clear	Dry	NB
33		Rochester	1/9/2015	Friday	8:36:00 AM	Turning	PDO					Daylight	Clear	Ice	SB-EB Left
34		Rochester	8/14/2012	Tuesday	9:46:00 AM	Rear End	C			1		Daylight	Clear	Dry	SB
35	Rochester to Knell		1/5/2016	Tuesday	5:41:00 PM	Rear End	PDO					Darkness	Clear	Dry	SB
36	Rochester to Knell		8/6/2012	Monday	9:23:00 PM	Head On	A	2				Darkness, lighted road	Clear	Dry	NB-SB
37	Rochester to Knell		7/2/2014	Wednesday	11:24:00 AM	Sideswipe Same Direction	PDO					Daylight	Clear	Dry	NB
38	Rochester to Knell		8/17/2013	Saturday	1:44:00 PM	Fixed Object	B		1			Daylight	Clear	Dry	NB
39	Rochester to Knell		5/7/2015	Thursday	5:30:00 PM	Rear End	PDO					Daylight	Clear	Dry	SB
40	Rochester to Knell		3/31/2012	Saturday	3:40:00 PM	Rear End	PDO					Daylight	Clear	Dry	NB
41	Rochester to Knell		11/15/2016	Tuesday	2:55:00 AM	Other noncollision	A	1				Darkness, lighted road	Clear	Dry	SB
42	Rochester to Knell		4/16/2012	Monday	2:10:00 PM	Rear End	C			1		Daylight	Clear	Dry	NB

Table 22 - Crash Data for Orchard Road from South of Jericho Road to North of US Route 30

								Number of Injuries							
	Segment	Intersection	Date	Day	Time	Crash Type	Severity	A	B	C	Fatal	Daylight Conditions	Weather	Roadway Surface Conditions	Other Info, Direction From - To
43		Knell	7/18/2012	Wednesday	5:59:00 AM	Turning	PDO					Daylight	Clear	Dry	WB-SB Left
44		Knell	5/29/2014	Thursday	7:08:00 AM	Turning	C			3		Daylight	Clear	Dry	SB-EB Left
45		Knell	1/9/2015	Friday	7:45:00 AM	Sideswipe Opposite Direction	PDO					Daylight	Clear	Ice	NB-SB
46		Knell	6/8/2015	Monday	4:20:00 PM	Rear End	PDO					Daylight	Clear	Dry	SB
47		Knell	1/7/2015	Wednesday	4:43:00 AM	Fixed Object	PDO					Darkness, lighted road	Severe cross wind	Snow or slush	NB
48	Knell to Aucutt		11/21/2014	Friday	3:47:00 PM	Rear End	PDO					Daylight	Clear	Dry	SB
49	Knell to Aucutt		5/15/2015	Friday	4:50:00 PM	Rear End	PDO					Daylight	Clear	Dry	SB
50	Knell to Aucutt		1/8/2014	Wednesday	8:36:00 AM	Sideswipe Same Direction	PDO					Daylight	Clear	Ice	SB
51	Knell to Aucutt		8/12/2014	Tuesday	4:32:00 PM	Rear End	PDO					Daylight	Clear	Dry	SB
52	Knell to Aucutt		9/4/2014	Thursday	12:40:00 AM	Animal	C			1		Darkness	Clear	Dry	SB
53	Knell to Aucutt		3/16/2016	Wednesday	2:33:00 PM	Rear End	PDO					Daylight	Clear	Dry	NB
54	Knell to Aucutt		11/15/2014	Saturday	8:40:00 PM	Fixed Object	B		1			Darkness, lighted road	Snow	Ice	NB
55	Knell to Aucutt		1/9/2015	Friday	5:33:00 AM	Sideswipe Same Direction	PDO					Darkness	Snow	Ice	NB
56		Aucutt	11/5/2014	Wednesday	4:15:00 PM	Rear End	PDO					Daylight	Clear	Dry	SB
57		Aucutt	3/10/2016	Thursday	5:35:00 PM	Rear End	PDO					Daylight	Clear	Dry	SB
58		Aucutt	9/16/2016	Friday	9:27:00 AM	Rear End	PDO					Daylight	Clear	Dry	SB
59		Aucutt	5/4/2012	Friday	11:27:00 AM	Rear End	C			1		Daylight	Clear	Dry	WB
60		Aucutt	7/19/2012	Thursday	8:41:00 PM	Turning	B		2			Darkness, lighted road	Clear	Dry	WB-SB Left
61		Aucutt	9/21/2012	Friday	6:20:00 PM	Rear End	PDO					Daylight	Rain	Wet	SB
62		Aucutt	12/11/2012	Tuesday	6:40:00 AM	Rear End	B		1			Daylight	Clear	Dry	WB
63		Aucutt	6/18/2013	Tuesday	3:34:00 PM	Turning	PDO					Daylight	Clear	Dry	WB-SB Left
64		Aucutt	11/20/2013	Wednesday	7:45:00 AM	Rear End	PDO					Daylight	Clear	Dry	SB
65		Aucutt	9/13/2014	Saturday	1:00:00 PM	Turning	B		2			Daylight	Clear	Dry	SB-EB Left
66		Aucutt	9/18/2014	Thursday	9:00:00 PM	Rear End	PDO					Darkness	Clear	Dry	WB
67		Aucutt	10/27/2014	Monday	8:58:00 AM	Rear End	PDO					Daylight	Clear	Dry	WB
68		Aucutt	11/21/2014	Friday	1:36:00 PM	Rear End	PDO					Daylight	Clear	Dry	WB
69		Aucutt	12/4/2014	Thursday	5:35:00 AM	Rear End	PDO					Darkness, lighted road	Clear	Dry	SB
70		Aucutt	1/9/2015	Friday	6:25:00 AM	Sideswipe Opposite Direction	PDO					Dusk	Snow	Ice	NB-SB
71		Aucutt	6/2/2015	Tuesday	7:57:00 AM	Rear End	PDO					Daylight	Clear	Dry	WB
72		Aucutt	7/23/2015	Thursday	4:34:00 PM	Rear End	PDO					Daylight	Clear	Dry	WB
73		Aucutt	12/7/2015	Monday	6:17:00 AM	Turning	A	1				Darkness	Fog/Smoke/Haze	Dry	SB-EB Left
74		Aucutt	12/16/2015	Wednesday	7:29:00 AM	Turning	C			2		Daylight	Rain	Wet	SB-EB Left
75		Aucutt	7/14/2016	Thursday	7:44:00 AM	Rear End	PDO					Daylight	Clear	Dry	WB
76		Aucutt	7/27/2016	Wednesday	8:30:00 AM	Rear End	C			1		Daylight	Clear	Dry	SB
77		Aucutt	10/28/2016	Friday	7:51:00 AM	Turning	PDO					Daylight	Clear	Dry	WB-NB Right
78		Aucutt	10/28/2016	Friday	5:04:00 PM	Rear End	C			1		Daylight	Clear	Dry	SB
79		Aucutt	11/22/2016	Tuesday	12:39:00 PM	Rear End	PDO					Daylight	? (8)	Dry	WB
80		Aucutt	11/29/2016	Tuesday	11:34:00 PM	Animal	PDO					Darkness, lighted road	Clear	Dry	NB
81		Aucutt	8/13/2012	Monday	11:28:00 PM	Rear End	PDO					Darkness	Fog/Smoke/Haze	Dry	NB
82		Aucutt	1/27/2014	Monday	8:05:00 AM	Turning	PDO					Daylight	Clear	Ice	NB-EB Right
83		Aucutt	2/23/2016	Tuesday	3:52:00 PM	Rear End	PDO					Daylight	Clear	Dry	WB
84		Aucutt	5/5/2016	Thursday	4:16:00 PM	Turning	B		1			Daylight	Clear	Dry	SB-EB Left

Table 22 - Crash Data for Orchard Road from South of Jericho Road to North of US Route 30

								Number of Injuries							
	Segment	Intersection	Date	Day	Time	Crash Type	Severity	A	B	C	Fatal	Daylight Conditions	Weather	Roadway Surface Conditions	Other Info, Direction From - To
85		Aucutt	5/18/2016	Wednesday	3:16:00 PM	Turning	B		1			Daylight	Clear	Dry	SB-EB Left
86		Aucutt	8/22/2016	Monday	10:11:00 AM	Turning	B		1			Daylight	Clear	Dry	SB-EB Left
87		Aucutt	1/6/2014	Monday	11:43:00 AM	Rear End	PDO					Daylight	Severe cross wind	Ice	EB
88		Aucutt	5/9/2012	Wednesday	2:45:00 PM	Rear End	PDO					Daylight	Clear	Dry	SB
89		Aucutt	7/21/2014	Monday	9:00:00 PM	Rear End	PDO					Darkness, lighted road	Clear	Dry	SB
90		Aucutt	8/28/2012	Tuesday	6:42:00 AM	Sideswipe Opposite Direction	PDO					Daylight	Clear	Dry	NB-SB
91		Aucutt	11/26/2014	Wednesday	2:18:00 PM	Fixed Object	PDO					Daylight	Clear	Dry	NB
92	Aucutt to Countryside		2/25/2015	Wednesday	6:48:00 PM	Rear End	PDO					Darkness, lighted road	Snow	Snow or slush	SB
93	Aucutt to Countryside		8/13/2012	Monday	11:19:00 PM	Fixed Object	PDO					Darkness	Fog/Smoke/Haze	Wet	NB
94	Aucutt to Countryside		12/4/2016	Sunday	12:40:00 PM	Rear End	PDO					Daylight	Snow	Snow or slush	NB
95		Countryside	7/30/2014	Wednesday	6:00:00 PM	Rear End	PDO					Daylight	Clear	Dry	SB
96		Countryside	2/5/2016	Friday	4:37:00 PM	Sideswipe Same Direction	PDO					Dusk	Clear	Dry	SB
97		Countryside	4/13/2016	Wednesday	3:53:00 PM	Rear End	PDO					Daylight	Clear	Dry	SB
98		Countryside	7/5/2012	Thursday	6:50:00 AM	Rear End	C			1		Daylight	Clear	Dry	SB
99		Countryside	12/12/2014	Friday	5:50:00 PM	Rear End	PDO					Daylight	Clear	Dry	SB
100		Countryside	9/20/2016	Tuesday	5:09:00 PM	Sideswipe Same Direction	PDO					Daylight	Clear	Dry	SB
101		Countryside	10/19/2012	Friday	9:35:00 AM	Turning	C			1		Daylight	Clear	Wet	WB-SB Left
102		Countryside	1/26/2013	Saturday	3:00:00 AM	Fixed Object	C			1		Darkness, lighted road	Rain	Wet	SB
103		Countryside	5/22/2013	Wednesday	6:43:00 AM	Sideswipe Same Direction	PDO					Daylight	Rain	Wet	SB
104		Countryside	4/21/2014	Monday	4:25:00 PM	Rear End	PDO					Daylight	? (8)	Dry	SB
105		Countryside	8/12/2014	Tuesday	4:09:00 PM	Rear End	PDO					Daylight	Clear	Dry	SB
106		Countryside	11/21/2014	Friday	2:31:00 PM	Turning	A	1	1			Daylight	Clear	Dry	SB-EB Left
107		Countryside	10/8/2015	Thursday	10:34:00 AM	Turning	PDO					Daylight	Clear	Dry	WB-SB Left
108		Countryside	1/5/2016	Tuesday	5:23:00 PM	Turning	PDO					Darkness, lighted road	Clear	Dry	SB-EB Left
109		Countryside	7/7/2016	Thursday	4:53:00 PM	Rear End	PDO					Daylight	Rain	Wet	SB
110		Countryside	7/7/2016	Thursday	5:47:00 PM	Rear End	PDO					Daylight	Rain	Wet	SB
111		Countryside	3/18/2012	Sunday	10:44:00 AM	Rear End	PDO					Daylight	Clear	Dry	SB
112		Countryside	2/18/2014	Tuesday	5:08:00 PM	Turning	C			1		Darkness, lighted road	Snow	Snow or slush	SB-EB Left
113		Countryside	1/31/2016	Sunday	1:25:00 PM	Turning	PDO					Daylight	Rain	Wet	WB-SB Left
114		Countryside	4/13/2016	Wednesday	4:40:00 PM	Sideswipe Same Direction	PDO					Daylight	Clear	Dry	SB
115	Countryside to Brentwood		7/25/2013	Thursday	5:34:00 PM	Rear End	PDO					Daylight	Clear	Dry	SB
116	Countryside to Brentwood		1/20/2014	Monday	5:44:00 PM	Rear End	B		1			Darkness, lighted road	Snow	Wet	SB
117	Countryside to Brentwood		2/23/2015	Monday	5:40:00 PM	Rear End	PDO					Daylight	Clear	Dry	SB
118	Countryside to Brentwood		10/28/2016	Friday	4:04:00 PM	Rear End	B		1			Daylight	Clear	Dry	SB
119	Countryside to Brentwood		8/25/2016	Thursday	5:27:00 PM	Rear End	PDO					Daylight	Clear	Dry	SB
120	Countryside to Brentwood		2/12/2014	Wednesday	6:18:00 PM	Sideswipe Same Direction	C			1		Darkness	Clear	Dry	SB
121		Brentwood	11/17/2015	Tuesday	6:30:00 PM	Rear End	C			1		Darkness	Rain	Wet	SB
122		Brentwood	5/19/2013	Sunday	5:28:00 PM	Rear End	PDO					Daylight	Clear	Dry	WB
123		Brentwood	9/23/2013	Monday	9:16:00 AM	Rear End	PDO					Daylight	Clear	Dry	NB
124		Brentwood	1/26/2014	Sunday	9:50:00 AM	Fixed Object	PDO					Daylight	Clear	Snow or slush	NB
125		Brentwood	2/12/2015	Thursday	7:08:00 PM	Rear End	PDO					Darkness, lighted road	Clear	Dry	NB
126		Brentwood	7/28/2015	Tuesday	6:16:00 PM	Rear End	PDO					Daylight	Clear	Dry	SB

Table 22 - Crash Data for Orchard Road from South of Jericho Road to North of US Route 30

								Number of Injuries							
	Segment	Intersection	Date	Day	Time	Crash Type	Severity	A	B	C	Fatal	Daylight Conditions	Weather	Roadway Surface Conditions	Other Info, Direction From - To
127		Brentwood	9/3/2015	Thursday	6:44:00 PM	Rear End	PDO					Daylight	Clear	Dry	WB
128		Brentwood	11/18/2016	Friday	5:46:00 PM	Turning	B		1			Darkness, lighted road	Clear	Dry	WB-SB Left
129		Brentwood	12/22/2016	Thursday	1:46:00 PM	Rear End	C			1		Daylight	Clear	Dry	SB
130		Brentwood	7/23/2016	Saturday	4:18:00 PM	Sideswipe Same Direction	PDO					Daylight	Clear	Dry	SB
131		Brentwood	1/9/2016	Saturday	11:28:00 AM	Turning	PDO					Daylight	Sleet/hail	Wet	NB-EB Right
132		Brentwood	1/4/2015	Sunday	11:48:00 AM	Parked Motor Vehicle	PDO					Daylight	Snow	Snow or slush	NB

Attachment 8:
Cost Estimate

Peralte-Clark LLC

KANE COUNTY DIVISION OF TRANSPORTATION
KDOT 2018 HSIP
FABYAN PARKWAY AT IL ROUTE 31 AND ORCHARD ROAD FROM JERICHO TO US ROUTE 30
PHASE I COST ESTIMATE

ITEM NO	ITEM	QUANTITY	UNIT	UNIT COST	TOTAL
1	REMOVALS				
	Curb and Gutter	129	FOOT	$12.00	$1,548.00
	Sidewalk	455	SQ FT	$5.00	$2,275.00
	Bike Path	118	SQ YD	$25.00	$2,950.00
	Adjust Handhole	1	EACH	$1,100.00	$1,100.00
	Pavement Marking	22,396	SQ FT	$1.00	$22,395.63
				TOTAL REMOVALS =	$30,268.63
2	PROPOSED ROADWAY				
	Sidewalk	1,757	SQ FT	$10.00	$17,570.80
	Sidewalk Curb	165	FOOT	$35.00	$5,757.50
	Multi-Use Path	70	SQ YD	$60.00	$4,220.00
	Detectable Warnings	112	SQ FT	$35.00	$3,930.00
	Curb and Gutter	163	FOOT	$50.00	$8,160.00
	Pavement Markings	22,396	SQ FT	$4.00	$89,582.51
				TOTAL PR ROADWAY =	$129,220.81
3	DRAINAGE	N/A	LS		$0.00
4	STRUCTURAL	N/A	LS		$0.00
5	EARTHWORK (20% of Proposed Roadway Cost)	1	LS		$25,850.00
6	INCIDENTALS (15% of Removal and Roadway Cost)	1	LS		$24,000.00
7	PROPOSED TRAFFIC				
	Signals				
	3 Section Signal Head	1	EACH	$950.00	$950.00
	Pedestrian Push Buttons	4	EACH	$1,700.00	$6,800.00
	Pedestrian Signal	4	EACH	$800.00	$3,200.00
	Signal Post for Ped Signals	3	EACH	$1,100.00	$3,300.00
	4 Section Signal Head with Left Arrows	6	EACH	$950.00	$5,700.00
	Advance Warning Flashing Beacons	6	EACH	$400.00	$2,400.00
	Signs	46	SQ FT	$50.00	$2,300.00
	Conduit	1,600	FOOT	$10.00	$16,000.00
	Dilemma Zone Detection System Equipment	1	LS		$15,100.00
	Dilemma Zone Detection Installation	2	EACH	$ 3,000.00	$6,000.00
	Controller Maintenance and Reoptimization	3	EACH	$ 5,000.00	$15,000.00
				TOTAL PR TRAFFIC =	$76,750.00
8	MAINTENANCE OF TRAFFIC (15% of Items 1-6)	1	LS		$31,500.00
9	MOBILIZATION (15% of Items 1-6)	1	LS		$31,500.00
10	CONTINGENCY (25% of Items 1-7)	1	LS		$71,600.00
11	RIGHT-OF-WAY	N/A	ACRE		-
				TOTAL PROJECT COST =	$420,700.00

Attachment 9:
Project Commitments

Peralte-Clark LLC

Project Commitments

1. All Special Waste related pay items must be included in the Final PS&E submittal in Phase II.

2. A Preliminary Site Investigation (PSI) will be completed in Phase II.