

Federal Categorical Exclusion

Project Development Report

Dauberman Road Extension

US Route 30 to Granart Road

Section Number 15-00277-01-BR

October 2018

Prepared for:

Kane County Division of Transportation
41W011 Burlington Road
St. Charles, IL 60175

Prepared by:



EXPERIENCE | Transportation

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**Illinois Department
of Transportation**

**Local Project Development Report for Group
II Categorical Exclusions and Design
Approval**

County: Kane County
Local Public Agency: Kane County Division of Transportation
Section Number: 15-00277-01-BR
Route: Kane County Hwy. No. 62

Project Number: _____ Project Length: 1.1 miles

Street/Road Name: Dauberman Road Extension

Termini: US Route 30 to Granart Road

- ☐ For Township or Road District bridge projects: The County Engineer certifies that the project design speed exceeds the minimum design speed recommended for this classification of roadway as provided in the BLRS Manual in order to prevent a deficient NBIS rating for approach roadway alignment appraisal. All elements have been designed to the chosen design speed unless noted otherwise in Section 2(e) and/or the attached BLR 22120.

County Engineer Date

- ☐ Categorical Exclusion and Design Approval Recommended

[Signature] 10.12.18
Local Agency Date

Anthony J. Quigley / CR3 12/4/18
Regional Engineer Date

This project will not have any significant impacts on the human environment; therefore, the FHWA approves the project as a Categorical Exclusion on

11/1/18
Date

- ☐ Design Approval

[Signature] 12/12/18
Bureau of Local Roads & Streets Date

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Appendix B –Clearances and Approvals

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Appendix D – Public Meeting and Public Hearing

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1. LOCATION AND EXISTING CONDITIONS

a. **Location** (attach location map to supplement narrative description)

The project is located within the southwest region of Kane County between the villages of Big Rock and Sugar Grove, west of the City of Aurora. The project consists of the extension of the existing Dauberman Road, located approximately four miles west of Illinois Route 47, from its current southern terminus at US Route 30 south to Granart Road. The proposed improvement limits begin on Dauberman Road, approximately 1,900 feet north of the US Route 30 intersection and continue approximately 6,000 feet south to Granart Road. The project improvement limits are located within the Village of Big Rock and unincorporated Kane County. See **Exhibit 1, Project Limits Map in Appendix A.** (Note that all referenced exhibits in this document are contained in Appendix A.)

The project study area begins on the north where Dauberman Road intersects with the bridge over Welch Creek and continues south, past US Route 30 over the BNSF Railroad and through open farm fields, to Granart Road. Along US Route 30, the project study area begins approximately 3,600 feet west of the proposed Dauberman Road alignment and continues east to approximately 2,600 feet east of the proposed Dauberman Road alignment. Along Granart Road, the project study area begins approximately 3,800 feet west of Mary Drive and continues east to Mary Drive.

b. **Description of Existing Facility** – Give narrative description, including such items as width of travel, parking and turn lanes, sidewalks, alignment, traffic control devices, utilities, jurisdiction, maintenance responsibility, drainage, terrain and current land use (including major public facilities and local landmarks). Attach existing typical sections showing roadway widths, bridge widths, ROW widths, sidewalk widths, guardrail, curb and gutter, and surface types.

Project Roadways

Dauberman Road is functionally classified as a north-south major collector under the jurisdiction and maintenance of Kane County Division of Transportation (KDOT). Dauberman Road begins at Keslinger Road and continues south for approximately eight miles to its current terminus at US Route 30. The existing unchannelized T-intersection of Dauberman Road and US Route 30 is stop controlled on Dauberman Road. Dauberman Road has a rural cross section with one 12-foot lane in each direction and 3-foot wide aggregate shoulders. There were no available archive as-built plan sets to reference, however based on field observations the existing pavement is a bituminous pavement. The existing right-of-way (ROW) width is 60 feet. There is no posted speed limit on Dauberman Road, therefore the speed limit by prescription is 55 mph (miles per hour). Dauberman Road is on a tangent north-south alignment with flat terrain. The existing vertical profile is very flat. There is a high point at approximately Sta. 674+00, sloping southwardly at approximately 1.22 percent to the existing T-intersection. See **Exhibit 2, Existing Typical Cross Sections.**

US Route 30 is functionally classified as an east-west minor arterial under the jurisdiction and maintenance of the Illinois Department of Transportation (IDOT). US Route 30, in Illinois, begins at the western border of Illinois/Iowa and extends to the eastern border of Illinois/Indiana. In the project study area, US Route 30 has a rural cross section with one 12-foot lane in each direction and 10-foot wide aggregate shoulders. From available archive plans, the pavement structure of US Route 30 in 1944 consisted of an existing PCC pavement with a 6-inch bituminous overlay. In 1984, US Route 30 was overlaid with 1.5 inches of binder course and 1.5 inches of surface course.¹ The ROW width is 80 feet. The speed limit along US Route 30 varies. East of Dauberman Road is unposted; therefore, the speed limit by

¹ Source: IDOT archive plan set, FAP-573 (US RTE 30), Section (30 & 31)WRS-1(80), Kane County, Project No. IX-573(16), C-91-294-83.

prescription is 55 mph. West of Dauberman Road the eastbound speed limit is 55 mph exiting the limits of downtown Big Rock. In the westbound direction, the speed limit becomes 45 mph entering the village limits of Big Rock. US Route 30 is mostly on a tangent east-west alignment with flat terrain, however at the east project limit there is a slight curve. This existing horizontal curve is approximately 440 feet long, with a radius of approximately 5,730 feet. The existing vertical profile is very flat. There is a minor high point at approximately Sta. 363+50, with approach slopes of approximately 0.59 percent to the west and -0.2 percent to the east. US Route 30 is a designated Class II truck route² within the project limits. See **Exhibit 2, Existing Typical Cross Sections**.

Granart Road is functionally classified as a northeast-southwest minor arterial with multiple jurisdictions depending on the location. Big Rock Township, Village of Big Rock, Kane County, and Village of Sugar Grove all possess sections of jurisdiction. Within the project improvement limits, Granart Road is under the jurisdiction and maintenance of Big Rock Township (from the western limit to approximately Sta. 142+50) and the Village of Big Rock (from approximately Sta. 142+50 to the eastern limit). Granart Road begins at Galena Road, in Kendall County, and extends northeast to Dugan Road. Granart Road has a rural cross section with an 11.5-foot lane in each direction and 3 to 5-foot wide aggregate shoulders. There were no available archive as-built plan sets to reference, however based on field observations the existing roadway has a bituminous pavement. The ROW width is 66 feet. The posted speed limit is 55 mph. Granart Road is on a slight curved alignment (900-foot radius) with long tangents on either side. The terrain is flat. The vertical profile is very flat with slopes being less than one percent. There are no existing turn lanes, intersections, and/or traffic control devices within the project study area along Granart Road. See **Exhibit 2, Existing Typical Cross Sections**.

BNSF Railroad

The BNSF Railroad (BNSF RR) runs east-west and parallels US Route 30 approximately 530 feet south of the roadway. Within the project limits, there is one mainline track. The railroad ROW is 100 feet wide. Approximately 0.3 miles west of Dauberman Road, a second spur track exists within the BNSF RR ROW. The BNSF has an at-grade crossing at Rhodes Avenue, approximately one mile west of Dauberman Road, in downtown Big Rock. The next closest at-grade crossing is approximately two miles east, at Dugan Road, within the Village of Sugar Grove. The nearest grade-separated crossing of the railroad tracks is 4.5 miles east, at Illinois Route 47, in the Village of Sugar Grove.

Land Use

Land use within the project limits is mostly farmed agricultural lands. Along Dauberman Road, the J. Maddock Air Strip is located on the west side of the roadway, near the north project limits, at approximately Sta. 669+35. This air strip is privately owned, it has one grass landing strip typically used by small-size planes. Downtown Big Rock is approximately one mile west. Located near downtown Big Rock are the Hinckley-Big Rock Middle School (along Galena Street/US Route 30, near Rhodes Avenue) and the Big Rock Fire Department (on 2nd Street, south of the railroad tracks). Along US Route 30 there are pockets of industrial land uses – there is a farm equipment dealer, mobile home and trailer dealer, and storage facility. Directly west of the storage facility is Cyrus Ministries International; it shares a driveway with the storage facility. Three miles to the east is the Aurora Municipal Airport. Along Granart Road, the adjacent land use is mainly agricultural, with a residential subdivision, on the north side of the roadway, approximately 0.5 miles to the east of the project limits at Mary Drive. Along the south side of Granart Road is Big Rock Campground, owned by the Forest Preserve District of Kane County. South of Big Rock Campground is the Camp Dean Girl Scouts facility which is accessed from Camp Dean Road, east of the project improvement limits. Welch Creek diagonally traverses the vicinity of the project area but the

² Source: <https://idot.maps.arcgis.com/apps/webappviewer/index.html?id=7dd67e99799a4c59a5c21e5175b8a97f>. Accessed October 31, 2017.

crossings are outside of the project limits. The creek crosses Dauberman Road approximately 0.5 miles north of US Route 30 and crosses US Route 30 approximately 0.5 miles east of Dauberman Road. See **Exhibit 3, Environmental Resources Map**.

Utilities

There are several utilities along the project roadways. Most are underground, including electric, fiber optic, gas lines, telephone, sanitary sewers and drain tiles. There are aerial electric and telephone lines along the south side of US Route 30 and along the north and south sides of Granart Road. Coordination has been initiated with the various utility companies. A summary table of the pertinent utility coordination can be found in **Appendix E**.

Drainage

The project passes through two drainage watersheds. North of the future intersection of Dauberman Road and Granart Road, the project is within the Welch Creek watershed. South of the intersection, the project is within the Big Rock Creek watershed. There are eleven existing outlets within and directly adjacent to the project limits. There is no floodplain or floodway located within the project limits. See **Exhibit 4, Floodplain Map FEMA Firmette**. Watershed limits can be found in the Location Drainage Study which is available under separate cover.

The project is split into four subareas, which includes the area north of US Route 30, the area between US Route 30 and the BNSF RR, the area between the BNSF RR and Granart Road, and the area south of Granart Road. Areas north of US Route 30 drain west to east through shallow ditches within existing ROW or overland to an existing drainage channel. This channel enters Dauberman Road ROW from the west at Sta. 666+60, turns south parallel to Dauberman Road and crosses through multiple pipe culverts under Dauberman Road (double 42-inch x 29-inch culverts at Sta. 661+00, 24-inch culvert at Sta. 665+55, and a 42-inch x 29-inch culvert at Sta. 665+64). From there, the channel flows southeast to multiple pipe/box culverts under US Route 30 (30-inch culvert at Sta. 354+53, 30-inch culvert at Sta. 354+59, and 4-foot x 2.5-foot box culvert at Sta. 354+69). Finally, the channel flows south through an existing channel to multiple pipe/box culverts under the BNSF RR (6-foot x 4-foot box culvert at Sta. 260+43, and 24-inch/30-inch culverts at Sta. 263+20 LT), where it ultimately outlets at Welch Creek, to the east.

Areas between US Route 30 and the BNSF RR drain overland west to east to the existing drainage channel stated above. Areas between the BNSF RR and Granart Road drain overland west to east to depressional areas, which overflow east to Welch Creek. Areas south of Granart Road drain north to south to depressional areas, which overflows south to Big Rock Creek. These depressional areas are drained by drain tiles ranging between 3 and 8 inches in diameter.

Additionally, there are also two large drain tiles (one 18-inch and one 20-inch) which cross the project improvement limits, draining offsite agricultural fields. The 18-inch drain tile runs diagonally bisecting the north quadrants of the Dauberman/US Route 30 intersection. The 20-inch drain tile is located just south of the BNSF RR and runs west to east. See **Exhibit 5, Existing Drainage Plan**.

Other Existing Conditions

There are no pedestrian or bicycle facilities along any of the roadways within the project improvement limits. There are no bridges or structures within the project limits. There is no street lighting along the roadways within the project improvement limits, however, there is one beacon light at the intersection of US Route 30 and Dauberman Road. Maintenance of the beacon light was verified with the Village of Big Rock but they were unable to determine if the beacon light was their jurisdiction. Typically beacon lights are owned by the power company and the electric utility is paid for by the local agency, i.e. Village of Big

Rock. There are no fixed transit routes along the project roadways.³ Transit service in the area is provided through Pace Dial-a-Ride Service by Ride in Kane transportation program for registered riders. Although there are no signs prohibiting parking, there are no parking accommodations on any of the project roadways, within the project study area.

c. Traffic Data

Roadway	Current ADT	% trucks
Dauberman Road	1,200	8%
US Route 30	6,300 (east of Dauberman) 6,800 (west of Dauberman)	8%
Granart Road	5,550	2%

Will 80,000 trucks be legally permitted on this route? ☒ Yes ☐ No

Roadway	2040 ADT	2040 DHV	% trucks
Dauberman Road	7,800	1,035	8%
US Route 30	15,000 (east of Dauberman) 9,050 (west of Dauberman)	2,250 (east of Dauberman) 1,360 (west of Dauberman)	8%
Granart Road	4,250	640	2%

See **Exhibit 6, ADT Map**. CMAP concurrence on the 2040 ADT projections was received May 17, 2017. See **Appendix E** for the correspondence.

- d. **Structures** – Identify location within the proposed improvement of all structures on attached location map. Attach a copy of the Structure Master Report for all structures within the project limits. Attach a copy of the Bridge Condition Report or the Bridge Deck Resurfacing approval letter for structures to be replaced, rehabilitated, or resurfaced.

There are no existing structures or bridges within the project study area. Two bridges are proposed with the subject improvement and are described in more detail in Section 2c below.

- e. **Railroads** – Identify location of all railroad crossings on attached location map and complete the following:

Railroad Name	No. and Type of Tracks (Main or Switching)	Type of Warning Devices*	No. of Trains Per Day	Railroad Width of Crossing at Rt. Angles
BNSF Railroad	1 Main track	N/A	32**	N/A

*Include a sketch showing location of railroad protective devices from the edge of roadway and to the nearest track.

See **Exhibit 7, ICC Rhodes Street and Dugan Road Train Counts.

The BNSF Railroad (BNSF RR) runs east-west and parallels US Route 30 approximately 530 feet south of the roadway. There are no existing railroad crossings of the BNSF RR within the project study area.

³ Source: http://www.pacebus.com/pdf/RTA_System_map.pdf. Accessed November 4, 2017.

- f. **Contiguous Sections** – Describe the existing typical sections at each end of the proposed improvement including number of travel lanes, turning lanes and parking lanes, lane widths and roadway width (f-f of curbs or e-e of shoulders), and sidewalk widths.

The existing contiguous sections of the project roadways are the same as the existing roadway sections.

Dauberman Road has a rural cross section with one 12-foot lane in each direction and 3-foot wide aggregate shoulders, with a total width of 30 feet edge-to-edge (e-e) of shoulder. The right-of-way (ROW) width is 60 feet.

US Route 30 has a rural cross section with one 12-foot lane in each direction and 10-foot wide aggregate shoulders, with a total width of 44 feet e-e of shoulder. The ROW width is 80 feet.

Granart Road has a rural cross section with one 11.5-foot lane in each direction and 3 to 5-foot wide aggregate shoulders, with a total width of 29 to 33 feet e-e of shoulder. The ROW width is 66 feet.

There are no existing sidewalks or bicycle facilities on any of these roadway. See **Exhibit 2, Existing Typical Cross Sections**.

2. Proposed Improvement

- a. Discuss the purpose and need of the project:

The purpose of this project is to provide a roadway connection to improve access across the Burlington Northern Santa Fe (BNSF) Railroad tracks between the Village of Big Rock and the Village of Sugar Grove as well as address future transportation needs in accordance with the Kane County 2040 Transportation Plan and other local land use plans. The needs for this project include improving and enhancing the safety of the users, improving mobility in the region, and enhancing system linkage and connectivity of the transportation network in the County. See **Appendix C, Final Purpose and Need Document** for additional information.

- b. What design guidelines will be used for the proposed improvement? (Check One)

- ☒ Rural (BLRS Manual Chapter 32) **for all project roadways**
- ☐ Urban (BLRS Manual Chapter 32)
- ☐ Suburban (BLRS Manual Chapter 32)
- ☐ 3R Guidelines (BLRS Manual Chapter 33)
- ☐ Bicycle Guidelines (BLRS Manual Chapter 42)
- ☐ Pedestrian Guidelines
- ☐ Other:

Project Roadway	Functional Classification			
	Arterial	Collector	Local Road	Other:
Dauberman Road	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
US Route 30	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Granart Road	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Terrain: ☒ Level **for all project roadways** ☐ Rolling

Project Roadway	Regulatory or Posted Speed Limit	Design Speed
Dauberman Road	55 mph	60 mph
US Route 30	55 mph	60 mph
Granart Road	55 mph	60 mph

- c. Describe type of work to be accomplished by the improvement. Discussion should include width of proposed travel, parking, bicycle and turning lanes, sidewalks, shared-use paths, guardrail, traffic control devices, drainage items (including storm sewer outfalls), alignment changes, railroad work, utility adjustments, intersection improvements, side slopes and clear zones. Specify the e_{max} for horizontal curves. Attach typical sections, plan and profile sheets, and intersection design studies when applicable.

General Introduction

The proposed improvements will extend Dauberman Road south approximately 0.8 miles from its current terminus at US Route 30 to Granart Road. The proposed roadway extension will tie into existing Granart Road by implementing a southwesterly horizontal curve to provide a new continuous through movement along Dauberman Road. Two structures are proposed along the extension to carry Dauberman Road over US Route 30 and the BNSF Railroad. The existing Dauberman Road/US Route 30 T-intersection will be replaced with a proposed jughandle type intersection, also referred to as the Connector Road. Three T-intersections are proposed: US Route 30/Connector Road, Dauberman/Connector Road, and Dauberman Road Extension/Granart Road. A multi-use path will be constructed as part of the project. The following discussions provide additional details of each aspect of the proposed project improvements. See **Exhibit 8, Proposed Typical Cross Sections** and **Exhibit 9, Proposed Plan and Profile Sheets**.

Project Roadways

Dauberman Road will have a rural cross section with one 12-foot travel lane in each direction and 8-foot shoulders (4-foot paved and 4-foot aggregate). A 10-foot wide paved multi-use path will be constructed along the east side of the roadway. On each side beyond the shoulders, a front slope of 1:4 (V:H) and a back slope of 1:3 will be used along the drainage and detention ditches. The width of the ditch bottom will vary in width with a minimum width of 2 feet. The proposed ROW (PR ROW) will be a minimum of 100 feet to accommodate the proposed cross section. At its widest, the PR ROW is 375 feet when Dauberman Road is on fill in the section between the two proposed bridges. The horizontal design parameters for Dauberman Road uses a design speed of 60 mph. The alignment is mostly on tangent except at the southern end where it approaches the proposed Dauberman Road Extension/Granart Road intersection. At this location there is a long sweeping curve to tie into existing Dauberman Road to the west. The curve design applies an e_{max} of 4 percent, with a minimum radius of 1,715 feet, and a superelevation rate of 4 percent. The clear zone will be a minimum of 30 feet from the edge of traveled way. The vertical profile at the north end of the project will replicate the existing profile, until south of the private air strip. This avoids any possible impacts to the air strip's use and operations. Once past the air strip, the profile rises over US Route 30 and the BNSF RR with a maximum grade of 4 percent at the bridge approaches. The remainder of the vertical profile applies a minimum grade of 0.3 percent to provide positive drainage. The proposed pavement will be HMA and the pavement design will be completed during the Phase 2 Design Engineering.

US Route 30 will have a rural cross section with one 12-foot travel lane in each direction and 8-foot paved shoulders. A shelf for a future multi-use path will be graded along the north side of the roadway. On each side beyond the shoulders, there will be a 2-foot flat graded area, and then a front slope of 1:4 (V:H) and a back slope of 1:3 will be used along the drainage and detention ditches. The width of the ditch bottom will vary in width with a minimum width of 2 feet. The PR ROW will be a minimum of 130 feet to

accommodate the proposed cross section. At its widest, the PR ROW will be 180 feet along the east approach to the US Route 30/Connector Road intersection. The horizontal design parameters for US Route 30 uses a design speed of 60 mph. Most of the proposed horizontal alignment is on tangent but there is one curve at the east project limits. This proposed horizontal curve mimics the existing curve. The curve length is approximately 440 feet, with a radius of approximately 5,730 feet and an e_{\max} of 2.8 percent. The clear zone will be a minimum of 30 feet from the edge of traveled way. The proposed vertical profile follows the existing profile very closely except at the approaches to the Connector Road intersection. At the Connector Road intersection, the profile will be raised approximately one foot to allow for improved drainage at the Connector Road. The vertical curve length along US Route 30 at this location is 1,688 feet. The proposed profile design applies a minimum grade of 0.1 percent to provide positive drainage. The proposed pavement will be HMA. The pavement design will comply with IDOT requirements and the design will be completed during Phase 2 Design Engineering.

Granart Road will have a rural cross section with one 12-foot travel lane in each direction and 8-foot shoulders (4-foot paved and 4-foot aggregate). A 10-foot wide paved multi-use path will be constructed along the south side of the roadway. On each side beyond the shoulders, a front slope of 1:4 (V:H) and a back slope of 1:3 will be used along the shallow swales. The width of the swales will vary in width from 2 to 18 feet. The horizontal design parameters for Granart Road will use a design speed of 60 mph. Granart Road, from the east, will sweep northward from the existing road location to form a proposed T-intersection with the Dauberman Road extension. The horizontal curve will apply an e_{\max} of 4 percent, with a minimum radius of 1,715 feet, and a superelevation rate of 4 percent. The clear zone will be a minimum of 30 feet from the edge of traveled way. From the east, the proposed design will raise the vertical profile of Granart Road approximately 0 to 6.5 feet above existing ground level, for a distance of approximately 890 feet until it matches into the proposed Dauberman Road extension. The profile design applies a minimum grade of 0.3 percent to provide positive drainage. The ROW will remain at 66 feet at the eastern terminus of Granart Road. Within the area of realignment, the south ROW line will be retained, but the north ROW line extends between approximately 43 to 185 feet north of the proposed realigned Granart Road centerline. The triangular area bounded by the existing Granart Road, realigned Granart Road and the Dauberman Road extension will incorporate a proposed detention pond (see drainage details below). The proposed pavement will be HMA and the pavement design will be completed during Phase 2 Design Engineering.

A proposed jughandle, also referred to as the Connector Road, will be constructed in the northeast quadrant of the existing Dauberman Road/US Route 30 intersection. It will have a rural cross section with 12 to 14-foot wide lanes, 8-foot shoulders (4-foot paved and 4-foot aggregate), and a 4-foot wide striped center median. At the approach to the Dauberman Road intersection, the Connector Road will have two westbound lanes (a left and right turn lane) and one eastbound lane. At the approach to the US Route 30 intersection, it will widen out to two lanes in each direction, providing southbound left and right turn lanes, and two northbound receiving lanes. See intersections description details below. A 10-foot wide paved multi-use path will be constructed along the north and east sides of the Connector Road. On each side beyond the shoulders, a front slope of 1:4 (V:H) and a back slope of 1:3 will be used along the ditches. The width of the ditch bottom will vary in width with a minimum width of 2 feet. The horizontal design parameters for the jughandle connector, using a design speed of 30 mph, will apply an e_{\max} of 2 percent, with a minimum radius of 275 feet, and a superelevation rate of 2.0 percent. The clear zone will be a minimum of 10 feet from the edge of traveled way. The vertical profile will be approximately 1.5 to 4.5 feet above existing ground for a distance of approximately 1,000 feet. There is a slight peak in the middle of the profile (at Sta. 805+02) where there is a 90-foot long vertical curve, its approach grades are 0.3 and 0.45 percent, south to north respectively. Proposed vertical grades will range from 0.3 to 2.0 percent along its length. The PR ROW will encompass the full jughandle Connector Road and its infield. The infield will incorporate a proposed drainage detention pond (see drainage details below).

The proposed pavement will be HMA and the pavement design will be completed during Phase 2 Design Engineering.

Intersections

The existing Dauberman Road and US Route 30 intersection will be replaced with a jughandle/Connector Road type intersection. The proposed Connector Road will form two T-intersections; one with Dauberman Road and one with US Route 30.

The proposed US Route 30/Connector Road T-intersection is proposed to be signalized. The new traffic signal for the intersection meets Warrant 3B: Peak Hour Vehicular for 2020 opening day traffic. The intersection design vehicle will be a WB-65, as US Route 30 is a Class II truck route. All lanes along US Route 30 will be 12 feet wide with 8-foot paved shoulders. In the eastbound direction there will be one through lane and a left turn lane adjacent to a 2-foot wide flush painted median. In the westbound direction there will be a right turn lane and one through lane adjacent to a 14-foot wide flush center median. The north leg of the intersection, the jughandle/Connector Road, will tie into US Route 30 at a 90 degree angle. The jughandle/Connector Road will have two 12-foot wide southbound lanes (providing a right turn and a left turn lane) and two 12-foot wide northbound receiving lanes, at the intersection. The Connector Road will have 8-foot shoulders (4-foot paved and 4-foot aggregate) and a 4-foot wide center painted median. The pavement on the Connector Road will accommodate 14-foot lane widths, however 2 feet of striping will be provided along the outside edge of pavement to incorporate separation striping. This provides additional pavement width to accommodate off-tracking by the WB-65 design vehicle. The 2-foot lane edge strip will transition to the edge of shoulder stripe as it approaches the Dauberman/Connector Road intersection. The proposed multi-use path along the north and east sides of the Connector Road will terminate at US Route 30. Stubs to the path will be constructed in both northern corners of the intersection and pedestrian crosswalk marking will be provided across the north leg of the intersection. See **Exhibit 9, Plan and Profile Sheets**, and **Exhibit 10, US Route 30 IDS**. **Appendix B** contains the **IDOT Approvals** of the warrants and new signal, the geometrics, and the IDS. **Appendix E** contains the **Signal Warrants Summary**.

The proposed Dauberman Road/Connector Road T-intersection is proposed to be stopped controlled at the Connector Road leg. The intersection was reviewed for signal warrants incorporating 2020 opening day traffic; however, signals were not warranted. The intersection design vehicle will be a WB-55. All lanes along Dauberman Road will be 12 feet wide with 8-foot shoulders (4-foot paved and 4-foot aggregate). In the southbound direction there will be one through lane and a left turn lane adjacent to a 2-foot flush painted median. In the northbound direction there will be a right turn lane and one through lane adjacent to a 14-foot wide flush center median. The east leg of the intersection will tie into Dauberman Road at a 90-degree angle. The jughandle/Connector Road will have a 4-foot wide flush center painted median, two 12-foot wide westbound lanes (providing a right turn and a left turn lane) and one 12-foot wide eastbound receiving lane at the intersection. The Connector Road will have 8-foot shoulders (4-foot paved and 4-foot aggregate). From the north side of the Connector Road, the proposed multi-use path will cross the Connector Road and continue south along the east side of Dauberman Road. Striping to incorporate a pedestrian crosswalk will be provided across the east leg. See **Exhibit 9, Plan and Profile Sheets** and **Appendix E** for the **Signal Warrants Summary**.

The proposed Dauberman Road extension will ultimately provide a continuous through movement by connecting into the existing Granart Road to the west. As a result, a T-intersection is proposed to be stop controlled at Granart Road to provide a connection back to the eastern section of Granart Road. This will provide a north to southwesterly through movement along the extension of Dauberman Road. The intersection was reviewed for signal warrants incorporating 2020 opening day traffic; however, signals were not warranted. The intersection design vehicle will be a WB-55. Dauberman Road will have a

12-foot travel lane in each direction and 8-foot shoulders (4-foot paved and 4-foot aggregate). There will be a 12-foot southbound left turn lane adjacent to a 2-foot flush painted median. The left turn lane and median will be shadowed in the northbound direction with a 14-foot wide flush painted center median. There will also be a 12-foot northbound right turn lane on Dauberman Road. On the east leg of the intersection, Granart Road will have two 12-foot wide westbound lanes (providing a right turn and a left turn lane) and a 12-foot wide eastbound receiving lane. There will be a 2-foot wide flush painted center median adjacent to the westbound left turn lane. As part of the Dauberman Road and Granart Road improvements, approximately 1,200 feet of Granart Road will be abandoned in between the points of tangency from the proposed re-alignment of the intersection. The multi-use path which runs along the east side of Dauberman Road will continue south and cross the east leg of the intersection. A striped pedestrian crosswalk will be incorporated across the east leg. The path will then continue east along the south side of Granart Road. See **Exhibit 9, Plan and Profile Sheets** and **Appendix E** for the **Signal Warrants Summary**.

Bridges

The proposed bridge over US Route 30 (Structure Number 045-3402) will be a single span bridge with PPC beams and MSE wall abutments. The bridge will carry one 12-foot lane of traffic in each direction with 8-foot paved shoulders and accommodate a 10-foot multi-use path on the east side of the structure. The multi-use path will be separated from the through lanes by a concrete barrier wall with a railing mounted on top of the wall for a minimum height of 4 feet-6 inches. The proposed span over US Route 30 will be long enough to allow for future widening of US Route 30 and a multi-use path along the north side of the roadway. The clear zone from the edge of the future US Route 30 roadway is 30 feet. The proposed minimum vertical clearance over US Route 30 is 16 feet-6 inches. The bridge over US Route 30 is on a crest vertical curve as it goes south over the road. The vertical clearance is measured from the north edge-of-pavement for a future five-lane roadway, which is the lowest point of the bridge profile over the future roadway.

The proposed bridge over the BNSF RR (Structure Number 045-3401) will be a single span bridge with PPC beams and MSE wall abutments. The bridge will carry one 12-foot lane of traffic in each direction with 8-foot paved shoulders and accommodate a 10-foot multi-use path on the east side of the structure. The multi-use path will be separated from the through lanes by a concrete barrier wall with a railing mounted on top of the wall for a minimum height of 4 feet-6 inches. The proposed bridge over the BNSF RR will span the 100-foot railroad ROW. No work is anticipated on the railroad tracks or within the railroad ROW. The proposed minimum vertical clearance over the railroad ROW is 23 feet-6 inches from the existing top-of-rail.

See **Exhibit 11, Proposed Bridge Typical Cross Sections** and **Exhibit 12, Bridge Type, Size and Location (TS&L)** for both bridges. Refer to **Appendix B** for **IDOT Approval of the TS&L's**.

Bridge Approach Roadways and Roadway Adjacent to the Air Strip

For the roadway approaches to the bridges and the section of road between the two proposed bridges (which will be on fill) the cross section will include a 12-foot travel lane, 8-foot paved shoulder with M-4.24 curb and gutter, and guardrail in each direction. On the east side of the roadway behind the guardrail will be the 10-foot wide multi-use path with 2-foot graded grass shoulders. In the areas on fill, a maximum 1:2 (V:H) side slope will be provided. The purpose of the M-4.24 curb is to collect the roadway runoff and convey it to a lower point on the fill into the ditches at the base of the fill slopes. This would keep the runoff from potentially eroding the side slopes of the fill and would also keep the roadway runoff from flowing over the path.

To avoid potential impacts to the use and operation of the private air strip, it is proposed to provide a 4-foot wide paved shoulder with 1:2 (V:H) side slope along the west edge-of-pavement of Dauberman Road, instead of a full 8-foot wide proposed shoulder, from approx. Sta. 667+10 to Sta. 671+00. In addition, the proposed vertical profile of the roadway at the landing strip will replicate the existing profile in this section so as not to change any of the existing vertical profile conditions. Drainage will be conveyed from the curb to drainage structures and lateral storm sewers which will connect to the ditch along the east side of Dauberman Road.

See **Exhibit 8, Proposed Typical Cross Sections** and **Exhibit 9, Plan and Profile Sheets**.

Multi-Use Path (Bicycle and Pedestrian Facility)

A separate pedestrian facility, i.e. sidewalk, is not proposed with the project. However, the project includes a proposed multi-use path which will accommodate both pedestrians and bicycles. The path will be 10 feet wide and the proposed pavement will consist of HMA with 2-foot graded grass shoulders. A minimum 10-foot clear zone from the outside edge of the paved roadway shoulder to the edge of the multi-use path will be provided. In those sections where the clear zone is less than 10 feet, a barrier with a minimum height of 3 feet-6 inches will be provided. At intersections, a striped pedestrian crosswalk will be provided when the alignment of the path encroaches on the roadway. Tactile surfaces will be provided at the crossings. The multi-use path will comply with and meet ADA, KDOT, and IDOT requirements.

Within the project improvement limits, logical termini will be provided from US Route 30, at the north, to Big Rock Campground, at the south.

Along Dauberman Road, the proposed path will begin in the north at the northeast quadrant of the Collector Road intersection. It will continue south along the east side of Dauberman Road to the proposed Dauberman/Granart intersection. It will then cross the east leg of the intersection and continue east.

Along Granart Road, the proposed path will be along the south side of the road. It will continue east along Granart Road to the Big Rock Forest Preserve entrance (across from Mary Drive), terminating in the southwest quadrant of the entrance to Big Rock Campground.

Along the jughandle/Connector Road, the proposed path will continue from the northeast quadrant of the Dauberman/Connector Road intersection. The path will be on the “outside” of the Connector Road continuing to the US Route 30/Connector Road intersection. At this intersection the path will cross the north leg of the intersection and terminate in the northwest quadrant. The paved path will be stubbed out at both corners. However, continuing west and east from the Connector Road, along the north side of US Route 30, the PR ROW will be acquired and a shelf graded to accommodate a future path to be constructed by others.

BNSF Railroad

The railroad ROW is 100 feet wide. The proposed bridge will span the ROW. The allowable proposed minimum vertical clearance over the railroad ROW of 23 feet-6 inches, from the existing top-of-rail, has been accommodated. No work is proposed or anticipated on the tracks or within the railroad ROW. The project has been coordinated with the BNSF Railroad. Correspondence is included in **Appendix E**.

Drainage

There are three stormwater systems in the area – US Route 30, Dauberman Road, and regional farm and field tile drainage. The water from the three systems will be kept separate through the project area, with water from the project roads being detained separately prior to being released. The proposed drainage will maintain the existing eleven outlets and replicate the existing drainage patterns as closely as possible.

The proposed drainage system for Dauberman Road will consist of a combination of open and closed drainage system. Ditches will be used to convey drainage from approximately Sta. 619+00 to Sta. 645+00 and Sta. 673+00 to Sta. 679+00. In all other areas with high embankment, M-4.24 curb and gutter will be used to convey drainage to a storm sewer system. Several box/pipe culverts are proposed under Dauberman Road to convey overland sheet flow from one side of the road to the other (3-foot x 2-foot box culvert at Sta. 628+17, 4-foot x 2-foot box culvert at Sta. 636+52, 5-foot x 3-foot box culvert at Sta. 636+64, 24-inch culvert at Sta. 640+53, 38-inch x 24-inch culvert at Sta. 644+65, 4-foot x 2-foot box culvert at Sta. 649+50, 4-foot x 2-foot box culvert at Sta. 651+50, and 4-foot x 2-foot box culvert at Sta. 655+80). These box culverts will have depressed inverts below existing ground to minimize culvert backwater on upstream existing agricultural land. In addition, a combination of box/pipe culverts (3-foot x 3-foot box culvert, 5-foot x 2-foot box culvert and a 36-inch pipe culvert) is proposed at approximately Sta. 666+35 to replace multiple existing pipe culverts, which convey flow from an offsite drainage channel from the west side of Dauberman Road to the infield of the jughandle.

The proposed drainage system for US Route 30 will consist of ditches throughout the project limits. A single 7-foot x 3-foot box culvert is proposed at Sta. 354+70 to replace multiple existing box and pipe culverts, which convey flow from an offsite drainage channel from the infield of the jughandle to the existing channel south of US Route 30.

The proposed drainage system for Granart Road and the Connector Road will consist of ditches or swales throughout the project limits. No major box culverts are proposed under Granart Road from Sta. 700+00 to Sta. 712+00 or the Connector Road from Sta. 800+00 to Sta. 810+50.

All existing drain tile within the project limits will be replaced with reinforced concrete storm sewer pipe and a manhole will be placed at each end, within the roadway ROW. Where feasible, the existing drain tile will be replaced at its existing location. In areas where the existing drain tile crosses under proposed detention ponds or box culverts, the existing drain tile will be rerouted to avoid conflicts and allow for future maintenance.

Detention will be provided throughout the project limits for the increased impervious areas and to maintain existing release rates. Dauberman Road and Granart Road will utilize a combination of detention ponds and ditches to detain stormwater. The bottoms of these ditches and ponds will be depressed to retain a portion of the stormwater beneath the outlet. US Route 30 will utilize ditch detention to detain stormwater. In addition to the roadside ditches and roadside ponds, two large detention areas will be constructed within the infield of the jughandle and the triangular area bound by the proposed Dauberman/Granart intersection and the section of Granart Road which will be abandoned. In order to maintain water quality, the bottoms of the detention ponds and ditches will consist of native plantings. Depressional areas will be provided on each side of Dauberman Road between Sta. 624+00 and Sta. 644+00 to compensate for lost depressional storage volumes due to the proposed Dauberman Road extension embankment. See **Exhibit 13, Proposed Drainage Plan**. Refer to **Appendix B for IDOT Approval of the Location Drainage Study**. The Location Drainage Study is available under separate cover.

Utilities and Lighting

The proposed improvements along US Route 30, Dauberman Road and Granart Road will potentially require utility relocations of the following:

- ComEd: buried electrical cable, and aerial lines and poles
- Nicor: 2-inch main along project roadways
- AT&T: buried telecom lines along project roadways
- Local Utilities: 4-inch pressurized sanitary sewer along US Route 30

Street lighting is not proposed along the project roadways. IDOT confirmed that lighting is not required under the proposed bridge over US Route 30. See **Appendix B, IDOT Approval of No Lighting**.

Agricultural Field Access Drives

At the south end of the project, the proposed Dauberman Road extension will bisect one agricultural parcel. To maintain access to the west side of the parcel that is south of the railroad tracks, an agricultural field access drive will be provided at approximately Sta. 635+25. Access to the east side will remain the same by using the existing agricultural railroad crossing that is in the northeast area of the parcel. (Final locations of field access points will be coordinated with the property owners during the land acquisition process.) The proposed west side entrance will provide access to the west side of Dauberman Road for farming operations and machinery to pass through. The access drive will be 40 feet wide and have a 6 percent slope from the surrounding area up to Dauberman Road. The type of material will be coordinated with the land owner.

Along US Route 30, agricultural field access drives will be provided at three locations, at approximately Sta. 344+80 on the north and south sides of the road (with a 5 percent slope), at Sta. 352+20 on the south side of the road (with a 4.5 percent slope) and at Sta. 363+67 on the north side of the road (with a 2 percent slope). All access points will be 40 feet wide with 20-foot radii. The drives can be aggregate or graded grass areas. The type of material will be coordinated with each land owner.

- d. Discuss items affecting improvement such as hazardous mailbox supports, parking and truck restrictions, mail delivery from traffic lanes, justification (including warrants) for multi-way stop signs, traffic signals and other traffic control and railroad protective devices, stage construction, nearby airports, and additional lighting:

There are currently no provisions or accommodations for parking along any of the roadways within the project improvement limits and none are proposed within the subject improvements. However, the proposed improvements will provide 8-foot paved shoulders along US Route 30 and 4-foot paved with 4-foot aggregate shoulders on both Dauberman and Granart Roads. If necessary, the proposed shoulders could be used for temporary parking.

US Route 30 is a designated truck route. There are currently no posted truck restrictions along this route. Changes to these conditions are not proposed with the subject improvements.

Street lighting is not proposed along the project roadways. IDOT has confirmed that lighting is not required under the proposed bridge over US Route 30. See **Appendix B, IDOT Approval of No Lighting**.

Within the project study area, there is one roadside mailbox. This mail box is located on US Route 30 at approximately Sta. 361+60 on the south side of the roadway. A mailbox turnout is proposed. A 10-foot wide shoulder will be provided at the mailbox location following *IDOT Standard 406201-01, Mailbox Turnout*. Mail delivery from the roadside may continue. The proposed 8-foot paved shoulder plus the widened shoulder at the mailbox will provide a stable and smooth drivable surface for mail delivery. When the mailbox is relocated, any supports judged to be potentially hazardous will be replaced with breakaway supports meeting the safety criteria defined by the US Postal Service.

A proposed traffic signal will be installed at the US Route 30 and jughandle/Connector Road intersection. The intersection signal meets Warrant 3B: Peak Hour Vehicular for 2020 opening day traffic. See **Exhibit 10, US Route 30 IDS, Appendix B, IDOT Approval of Signal Warrant and New Signal, and Appendix E, Signal Warrants Summary**. There will not be any multi-way stop controlled intersections or railroad protective devices proposed with the subject improvement limits.

The J. Maddock Air Strip is along the west side of Dauberman Road, north of US Route 30. This is a privately owned air strip and there is one grass landing strip, typically used by small-size planes. The proposed project will not impact the landing strip use or operations. The proposed improvements will avoid any land acquisition from the landing strip and/or its frontage.

The Aurora Municipal Airport is approximately three miles east of the project limits. It is anticipated that the airport will not be affected by the project improvements. The *FAA Notice Criteria Tool* was completed for the proposed improvements. The tool indicated that the project is “in proximity to a navigation facility and may impact the assurance of navigation signal reception” and the project is requested to file a notice (prior to construction) for its location. Note that this criteria is more applicable to tall communication towers, cell phone towers, and utility towers carrying high-voltage lines. For the proposed project, the request is solely based on the project’s horizontal location from the airport; there are no vertical concerns and the bridges will not impact navigation signal reception. At the project’s highest point on the Dauberman Road bridge over the BNSF RR, there is more than 191 feet of vertical clearance between the extended airport approach surface elevation and the highest proposed project point. See **Appendix E for Clearance Diagram and FAA Part 77 Notice Criteria Tool**.

There are no privately-owned encroachments within the roadway ROW.

The maintenance of traffic (MOT) during construction will occur in stages to minimize the impacts to traffic. More details about the MOT can be found in Sections 14 and 16 of this report.

- e. Identify each aspect to be constructed at less than the design guidelines and provide a clear description of required design variances and appropriate justification. (BLRS Manual Section 27-7). If a design variance is required, include a copy of the approved BLR 22120 form as an attachment.

A Level One Design Variance is required for the 4-foot shoulder width along the west side of Dauberman Road from Sta. 667+10 to Sta. 671+00. The standard design requires an 8-foot shoulder (4-foot paved and 4-foot aggregate).

The J. Maddock Air Strip is a private grass landing strip located at Sta. 669+35 on the west side of Dauberman Road. To avoid impacts to the runway operation and use, while maintaining the tangent Dauberman Road alignment, a 4-foot wide paved shoulder with 1:2 (V:H) side slope is proposed along the west edge-of-pavement of Dauberman Road instead of the standard 8-foot wide shoulder (4-foot paved and 4-foot aggregate). This is proposed from Sta. 667+10 to Sta. 671+00, a distance of approximately 390 feet adjacent to the air strip property frontage. As a condition of this design exception approval, advanced warning signs will be installed to alert the traveling public to the narrow shoulder.

Shifting the alignment east with tapers or curves is not recommended because it would cause the Connector Road intersection to move farther north, potentially interfering with the air strip in the future if the intersection were to become signalized. Additionally, the location of the recently constructed Welch Creek Bridge to the north, limits other roadway realignment options.

A Level Two Design Variance is required for a short section of 1:2 slope is used near the J. Maddock Air Strip private grass landing strip to avoid property take from the runway. The steeper slope will allow development of a small ditch to drain offsite flow. The roadway fill height is only 2.2 feet, shallow enough that guardrail is not necessary (BDE Fig. 34-4.A).

See **Appendix B, IDOT Design Variance Approval**.

- f. Current estimated cost of proposed improvement? \$17,264,000 (in 2017 dollars)

See **Exhibit 14, Proposed Estimate of Cost.**

- g. Analyze the need for accommodating pedestrians, bicyclists and the handicapped. When applicable, describe the facilities to be provided for pedestrians and bicyclists. Discuss the ADA accessibility and maximum longitudinal grade of these facilities. (BLRS Manual Chapter 41)

A multi-use path will be constructed along the east side of Dauberman Road from US Route 30 to Granart Road. It will then continue east along the south side of Granart Road ending at the Big Rock Campground Forest Preserve entrance across from Mary Drive. The path will be paved, 10 feet wide with 2-foot graded grass shoulders, and located a minimum of 10 feet from the edge of travel way. Tactile surfaces will be provided at the intersection crossings. For additional details of the multi-use path, see **Section 2c, Multi-Use Path (Bicycle and Pedestrian Facility)** for an in depth discussion of the proposed path improvements.

The proposed vertical grades follow the roadway grades, 0.1 to 0.7 percent along the project roadways and the jughandle/Connector Road, and 4 percent on the bridge approaches. This is less than the 5 percent maximum allowed by ADA, without requiring flat areas. The project is fully compliant with Complete Streets and ADA requirements.

The proposed multi-use path is consistent with the 2015 Kane/Kendall County Bicycle Planning Map and will be part of a north-south regional trail. See **Exhibit 15, Bicycle Routes and Plans.**

Sidewalks/Shared-Use Paths:

Maximum 2% cross slope: ☒ Yes ☐ No ☐ Not Applicable

ADA ramps with detectable warnings at street intersections: ☒ Yes ☐ No ☐ Not Applicable

If no, provide justification.

- h. Discuss any proposed improvements being considered in adjacent segments including the anticipated construction startup date of these improvements.

IDOT is conducting an Environmental Assessment (EA) Phase 1 study along US Route 30 from just east of Dauberman Road to Illinois Route 47. IDOT's west project limit will match into this project's east limit along US Route 30. IDOT is considering potential improvements along the US Route 30 corridor to address future transportation needs. The EA is anticipated to be finalized in 2019, with construction to be programmed thereafter (as funding is identified). To address current safety and capacity issues at the US Route 30 and Dugan Road intersection, IDOT advanced an interim design and construction project for the intersection. Construction of the intersection improvements was completed in late 2016. This project has been coordinated with the IDOT Phase 1 study and will continue into the Phase 2 Design Engineering. See **Appendix E, Coordination Meeting with IDOT Programming.**

Similarly, the Village of Sugar Grove improved the intersection of Granart Road and Dugan Road as a roundabout. This intersection is located immediately south of the US Route 30 and Dugan Road intersection. The roundabout was completed in August 2015.

3. **Crash Analysis (BLRS Manual Section 22-2.11(b)(9))**

- a. Summarize crash data for the past five years, including a spot map or a location map showing crash locations when possible. Detail the types of crashes and include collision diagrams, if possible, especially at cluster sites. Give the source of this data.

Crash data was obtained from the Kane County Division of Transportation, the Kane County Sheriff's Department, and the IDOT Division of Traffic Safety for the period between January 1, 2010 and December 31, 2016. A total of 147 crashes occurred in the area studied during this seven-year period. See **Table 3.1** for a breakdown of the crashes for the entire study area by type of collision.

Table 3.1: Breakdown by Crash Types

Crash Type	Number of Crashes (Percentage of Total)
Animal	45 (30.6%)
Rear End	37 (25.2%)
Loss of Control	38 (25.8%)
Sideswipe	12 (8.2%)
Fixed Object	7 (4.8%)
Right Angle/Turning	8 (5.4%)
Total Crashes:	147 (100%)

There were 38 injuries and no fatalities during the seven-year study period. Injury crashes accounted for 25.9 percent of the total crashes. Of the 38 injuries, 9 (23.7 percent) were considered as a Type A (incapacitating) injury, and the remaining 29 (76.3 percent) were considered Type B (non-incapacitating) or Type C (reported, not evident) injuries.

See **Exhibit 16, Collision Diagrams and Crash Summary Tables** and, under separate cover, the **Traffic Crash Analysis** for additional details.

- b. Analyze available crash data including results of field check. Discussion should include high crash locations, critical wet weather sites, and other crash patterns. If the data is inconclusive, make a statement to that effect.

Of the 147 crashes, 65 (44.2 percent) occurred at intersections while the remaining 82 (55.8 percent) occurred at section locations between intersections. The analysis revealed that 112 (76.1 percent) crashes occurred during clear, dry conditions, 28 (19.1 percent) during snow or icy conditions, and 7 (4.8 percent) during rain or otherwise wet pavement conditions.

Seven-year night-to-day crash ratios were determined. Of the 147 crashes, 85 (57.8 percent) occurred during daylight conditions. This is less than the critical ratio of 2:1 which indicates additional roadway lighting within the project study area is not warranted based on the crash experience.

The predominant type of crash occurring within the study area is animal collisions. There are no known countermeasures to decrease the incidence of animal collisions, given the rural nature of the area.

One area of concern is along Granart Road, west of Mary Drive, where a tight superelevated existing curve has resulted in 17 crashes with drivers losing control of vehicles. This will be addressed with the realignment of Granart Road to connect with the Dauberman Road extension. Based on the existing speed limit of 55 mph and a proposed design speed of 60 mph, the existing superelevated curve radius of approximately 900 feet does not meet current IDOT design criteria. Additionally, the superelevation along the curve is approximately 3.5 percent which is substandard and should be designed to an e_{max} of

6 percent. This substandard curve will be addressed with the roadway realignment. The proposed horizontal curve will apply an e_{\max} of 4 percent, with a minimum radius of 1,715 feet, and a superelevation rate of 4 percent.

The intersection of Granart Road and Dugan Road was improved in 2015; a new roundabout (RAB) intersection replaced the existing T-intersection. (See inset photo to the right.) The RAB was constructed south of the existing intersection adjacent to Bucktail Lane. Granart Road was realigned approximately 0.1 mile to the south. As a result of the improvements, the crash data from 2010-2014 is no longer valid as the type of intersection control is vastly different. Since the opening of the RAB, there were two crashes reported in 2015, none in 2016. Both crashes were due to driver error – failing to yield the right-of-way in the RAB.



- c. Describe how the proposed project will address any crash issues.

The existing superelevated curve along Granart Road will be eliminated as part of this project. With the proposed Dauberman Road extension, eastbound Granart Road traffic will curve north to connect with the Dauberman Road extension. Vehicles continuing to travel east will turn right at the T-intersection with Granart Road. Westbound Granart Road traffic will curve northward and intersect the Dauberman Road extension at a stop-controlled T-intersection. Vehicles continuing west will make a left turn to continue along Granart Road. Auxiliary turn lanes, dedicating left and right turn lanes from Granart Road onto Dauberman Road, are also being added for turning movements.

4. Right-of-Way

- a. Describe the right-of-way taking, including the total acreage required for each of the following categories: ROW, permanent easements, temporary easements and temporary land use permits. Include the width of taking, number of property owners, acreage of right-of-way and/or easements, character of land, i.e., farm, residential, commercial or publicly owned properties, anticipated impacts to properties that remain, and location of any improvements with respect to required right-of-way. Discuss any impacts on setbacks required by zoning.

The project will require land acquisitions to construct the improvements. The proposed acquisitions will involve 25 properties; 24 are agricultural farmlands and 1 is residential; 23 are proposed ROW (43.44 acres), and 3 are a temporary easement (0.46 acres). Note that one property (PIN 13-23-300-002) has both proposed ROW and temporary easement. The total land acquisition needed is approximately 44.83 acres. See **Table 4.1, Summary of Proposed ROW**.

Table 4.1: Summary of Proposed ROW

Property	Property PIN	Property Owner	Character of Land	Category of Acquisition	Area Required (acres)
1	13-14-300-006	Cox, Edward A Jr Revoc. Trust	Agricultural	PR ROW	8.94
2	13-14-300-008	Cox, Edward A Jr Revoc. Trust	Agricultural	PR ROW	0.41
3	13-14-300-009	Cox, Edward A Jr Revoc. Trust	Agricultural	PR ROW	1.99
4	13-15-400-007	Jay Maddock	Agricultural	PR ROW	0.22
5	13-15-400-009	Joe Ludwig LLC	Agricultural	PR ROW	0.09
6	13-15-400-020	Jay Maddock	Agricultural	PR ROW	0.34
7	13-15-400-028	Old Kent Bank	Agricultural	PR ROW	0.04
8	13-15-400-029	Old Kent Bank	Agricultural	PR ROW	1.05
9	13-15-400-030	Joe Ludwig LLC	Agricultural	PR ROW	0.09
10	13-15-400-034	Joe Ludwig LLC	Agricultural	PR ROW	0.02
11	13-22-200-025	Raymond, Robert D & Barbara	Agricultural	PR ROW	0.14
12	13-22-200-030	Thompson, Bruce & Shirley	Agricultural	PR ROW	2.10
13	13-22-200-031	Thompson, Bruce & Shirley	Agricultural	PR ROW	6.93
14	13-22-400-001	Robert P II, Gwinn	Agricultural	PR ROW	3.95
15	13-23-100-006	Thompson, Bruce & Shirley	Agricultural	PR ROW	2.82
16	13-23-100-007	Thompson, Bruce & Shirley	Agricultural	PR ROW	8.19
17	13-23-100-008	Summers Family Trust	Agricultural	PR ROW	0.36
18	13-23-100-009	Forest Preserve District of Kane County	Agricultural	Temp Easement	0.71
19	13-23-127-019	David & Deborah Rosenwinkel	Agricultural	PR ROW	0.03
20	13-23-127-020	David & Deborah Rosenwinkel	Agricultural	PR ROW	0.03
21	13-23-127-021	David & Deborah Rosenwinkel	Agricultural	PR ROW	0.03
22	13-23-300-002	Robert P II, Gwinn	Agricultural	PR ROW Temp Easement	0.20 0.01
23	13-23-300-007	Summers Family Trust	Agricultural	PR ROW	5.35
24	13-23-300-008	Forest Preserve District of Kane County	Agricultural	Temp Easement	0.67
25	13-23-300-009	Anthony, Linda	Residential	PR ROW	0.12
				Total PR ROW	43.44
				Total Temp Easement	1.39
				TOTAL ACRES:	44.83

b. Are any residents, businesses or farms to be displaced?

☐ Yes ☒ No

If yes, describe the number and type of displacements anticipated and mitigation that will be taken to provide relief for this impact on an attached sheet

5. **Prime Farmland (BLRS Manual Section 20-10)**

- a. If the project requires more than 3 acres/mile (0.75 hectares/kilometers), 10 acres (4 hectares) for a non-linear improvement, or the project ROW is not contiguous to the existing ROW, contact the Illinois Department of Agriculture and attach results of the coordination and summarize the results below.
- b. ☒ The project requires consultation with the Natural Resource Conservation Service, Form AD-1006 has been completed and submitted to the local office of NRCS. The completed AD-1006 form is attached.
- ☐ The impact of this project on farmland conversion has been evaluated in accordance with the requirements of the US Natural Resources (NRCS). The project will cover 3 acres or less of farmland per mile (0.75 hectares or less of farmland per kilometer) and the conversion will not result in more than minor impacts. Accordingly, the project conforms to the general form AD-1006 prepared by NRCS. Therefore, further coordination with NRCS on this project will not be necessary.

See **Appendix B**, for **IDOA Approval Letter and NRCS AD-1006 form**.

6. **Floodplain Encroachment (BLRS Manual Section 20-7)**

Does the proposed work cross or encroach upon a 100-year floodplain, including a regulatory floodway?

☐ Yes ☒ No

If yes, summarize the location hydraulic study, regulatory floodway restrictions, the effect of any encroachment (including a comparison between existing and proposed conditions) and the effect of over-the-road flow on the proposed transportation facility. Attach any available floodplain maps.

No floodplain encroachment will occur from to the planned improvements. See **Exhibit 4, Floodplain Map FEMA Firmette**.

7. **Phase I & II NPDES Storm Water Permit Requirements (BLRS Manual Section 7-4.01)**

Will the project involve soil disturbance of 1 acre (0.4 hectare) or more?

☒ Yes ☐ No

If yes, the project must comply with the Phase II NPDES Storm Water Permit Requirements.

An NPDES permit will be completed and filed as part of the Phase 2 Design Engineering.

8. **"404" Permit (BLRS Manual Section 7-4.02)**

Does this project involve waters regulated by Section 404?

☐ Yes ☒ No

If yes, what type of 404 permit is required? ☐ Nationwide ☐ Individual ☐ Regional ☐ None

Attach a copy of any 404 permit authorization and/or coordination letters with the Corps of Engineers. If an individual Section 404 permit is required, please notify the Illinois Department of Transportation district office before submitting the application.

The wetlands within the project study limits were determined to be non-jurisdictional by US Army Corps of Engineers (USACE) on May 18, 2017. See **Appendix E**, for the **USACE Jurisdictional Determination Approval Letter**.

9. **Special Waste (BLRS Manual Section 20-12)**

- a. Following the special waste assessment screening criteria shown on Figure 20-12A of the BLRS Manual, is Preliminary Environmental Site Assessment (PESA) required?

☒ Yes ☐ No

A PESA Review (October 24, 2016) was provided by IDOT for US Route 30. See **Appendix B, IDOT PESA Review Summary and Recommendations**.

A PESA (July 2016) was prepared by Huff & Huff, Inc. for KDOT for Dauberman Road and Granart Road. See **Appendix B, Huff & Huff PESA Review, Executive Summary**.

- b. Is work being done on property in the name of the state or are contract plans being prepared by the state?

☒ Yes ☐ No

Work is proposed along US Route 30 which is under IDOT jurisdiction.

- c. If a PESA is required for either state or local ROW, did the PESA results determine that the project has Recognized Environmental Conditions (REC's) for special waste?

☒ Yes ☐ No

If the PESA results determined that the project contains REC's, describe how the special waste is proposed to be handled (including if a Preliminary Site Investigation (PSI) is required).

IDOT PESA

The IDOT PESA Review identified 11 sites determined to contain recognized environmental conditions (RECs) and 25 sites determined to contain *de minimis* conditions only. Based on the proposed geometry, the following sites will be impacted:

Table 9.1: Summary of Involvements along US Route 30

ISGS Site #	Property Name and Address	Involvement	Condition
28	Executive Affiliates, 47W247 US 30, Big Rock	Land acquisition for proposed improvements	<i>de minimis</i>
29	B&R Storage 47W247 US 30, Big Rock	excavation in IDOT existing ROW only for shoulder widening/ditch work, no proposed ROW required	<i>de minimis</i>
30	Ray-Vac Distributors 47W171 US 30, Big Rock	Land acquisition for proposed improvements	REC for chemical use; metals; transformers; potential ACM and lead paint
31	Agricultural Land	Land acquisition for proposed improvements	<i>de minimis</i>

ISGS Site #	Property Name and Address	Involvement	Condition
	46W700-47W800 blocks of US 30* Big Rock		
32	Farmstead 46W795 US 30 Big Rock	Land acquisition for proposed improvements	<i>de minimis</i>
33	Cyrus Ministries International 46W779 US 30, Big Rock	Land acquisition for proposed improvements	<i>de minimis</i>
34	Sugar Grove Self-Storage 46W657 US 30, Big Rock	Land acquisition for proposed improvements	<i>de minimis</i>

*Note: Combination of Cox, Old Kent Bank, Maddock & Thompson parcels.

Of all the properties along US Route 30 identified in the PESA, 1 REC parcel and 6 *de minimis* sites will be affected. The BNSF Railroad, designated with an ISGS Site #A, was listed as adjoining but not on the project. Since 1 REC was identified in the PESA, a PSI will be required. During the Phase 2 Design Engineering, a PESA Response will be submitted to IDOT to initiate the PSI. The mitigation measure for any special waste will be determined by the PSI.

Local Agency PESA (Non-State Routes)

The Huff & Huff PESA identified one potentially impacted property (PIP) within the project corridor along Dauberman Road and Granart Road, the BNSF RR. Based upon the preliminary engineering, the proposed Dauberman Road extension will be elevated over the BNSF RR, spanning the railroad ROW, and will avoid the PIP. In accordance with IDOT policy, a PSI is recommend only if soils associated with the BNSF RR ROW will be disturbed. The BNSF RR ROW will be avoided, therefore a PSI is not required.

10. Environmental Survey (BLRS Manual Section 20-2)

Whenever a project involves land acquisition (including easements), any in-stream work (including drainage structure run-around), is located within or adjacent to historic properties listed in (or eligible for) the National Register of Historic Places, a bridge on the historic list, is near wetlands, or known locations of threatened or endangered species, the Environmental Survey Request Form should be submitted early in the project development phase.

- a. Wild and Scenic Rivers – If this project crosses or affects a river on the National Wild and Scenic Rivers System or a river listed in the Nationwide Inventory of Rivers with Potential for inclusion on the system, include coordination between the National Park Service and the Bureau of Design and Environment (BDE).

☐ Involvement ☒ No Involvement

- b. Wetlands – Does the proposed work impact the use of regulatory wetlands?

☐ Yes ☒ No

A total of eight farmed wetlands were identified within the project limits. These wetlands were of low to poor quality with FQI values of 0.0 to 6.4, and determined to be isolated. Subsequently, the wetlands were determined to be non-jurisdictional by US Army Corps of Engineers (USACE) and provided a Jurisdictional Determination Approval Letter on May 18, 2017.

See **Appendix E, 5/18/17 USACE Jurisdictional Determination Approval Letter** and under separate cover the **Wetland Delineation Report**.

Seven of the eight wetlands will be impacted by the project. The project will comply with the Illinois Wetland Policy Act (IWPA). As such, the mitigation ratios were determined by IDOT and concurred upon by IDNR. A summary of the wetland impacts and the approved replacement ratios are shown in **Table 10.1**. A total of 3.52 acres of wetlands will be impacted requiring 11.72 acres of mitigation. Approval was provided by IDOT for the mitigation ratios and for use of wetland banking to mitigate the impacts.

See **Appendix B**, for **Wetland Resource Clearance** and **IDNR Wetland Clearance Concurrence**.

Table 10.1: Summary of Wetland Impacts

Site #	Wetland Type	Acreage within Project Limits	FQI	Acreage Impacted	Mitigation Ratio	Acreage of Mitigation Required
3	Farmed	0.001	1.4	0.01	2:1	0.02
4	Farmed	0.00	6.4	0.00	1:1	0.00
5	Farmed	1.21	1.4	1.30	4:1	5.20
6	Farmed	0.48	NA	0.48	2:1	0.96
7	Farmed	0.00	1.2	0.28	2:1	0.56
8	Farmed	0.00	5.3	0.36	2:1	0.72
9	Farmed	0.31	0.0	1.04	4:1	4.16
10	Farmed	0.00	0.0	0.05	2:1	0.10
Total:				3.52		11.72

If yes, indicate how the wetlands will be mitigated. ☒ Banking ☐ Accumulation ☐ On-Site ☐ Other

The wetland impacts will be mitigated by KDOT at the Sauer Family Forest Preserve Wetland Bank. The proposed bank is approximately four miles northeast of the project area, in the southwest quadrant of Lasher Road and Harter Road in Sugar Grove, Illinois. It is owned and operated by the Forest Preserve District of Kane County (FPDKC). It is not a commercial bank and is used only for KDOT and FPDKC projects. IDOT approved the use of the Sauer Family Wetland Bank.

See **Appendix B**, **IDOT Approval of Change in Wetland Bank**.

- c. Archaeological and Historical Preservation – Include results of coordination. Does the project impact an archaeological or historic preservation site?

☐ Yes ☒ No

See **Appendix B**, **Cultural Resources Clearance**.

If yes, describe any required documents.

- d. Threatened or Endangered Species – Does the project impact any endangered species or plants?

☐ Involvement ☒ No Involvement

Include copy of biological resources memorandum or signoff by BDE and/or IDNR.

See **Appendix B**, **Biological Resources Clearance**.

- e. Stream Modification and Wildlife Impacts – Include copies of any correspondence between BDE and IDNR or US Fish and Wildlife Service. Attach copies of any additional coordination between local agency and IDNR or US Fish and Wildlife Service whenever required as a result of biological review by BDE. Address any proposed mitigation measures.

☐ Involvement ☒ No Involvement

11. Section 4(f) Lands (BLRS Manual Section 20-3)

- a. Does this project require any right-of-way, including temporary construction easements, from a publicly owned park, recreational area, wildlife and waterfowl, or any historic site in or eligible for the National Register of Historic Places?

☒ Yes ☐ No

Construction of the multi-use path along the south side of Granart Road to the Big Rock Forest Preserve entrance will require use of lands owned by the Forest Preserve District of Kane County (FPDKC) and was purchased with OSLAD funds. The path will be on forest preserve property from approximately Sta. 707+90 to the entrance driveway, a distance of approximately 2,100 feet. KDOT has coordinated with the FPDKC and the IDNR and they are supportive of the project. The proposed path is consistent with the FPDKC Long Range Plans and their Trail Plan where it currently shows a mowed grass trail along its northwest lot line. See **Exhibit 15, Bicycle Routes and Plans**.

KDOT has also coordinated with IDNR with regard to the OSLAD funds used to purchase property for the Big Rock Forest Preserve. IDNR determined that the proposed trail is outdoor recreation and since the FPDKC is not giving up ownership of the land, they viewed this as additional outdoor recreational development on the OSLAD assisted site. Construction of the proposed path does not violate any OSLAD restrictions for use of the land. The FPDKC concurred with the IDNR determination. For the coordination documentation, see **Appendix B**, for the **Section 4(f) Temporary Occupancy Approval Document** and the **IDOT Approval of Section 4(f) Temporary Occupancy**.

- b. If yes, what type of the Section 4(f) involvement has been completed?

☐ Section 4(f) de minimis ☐ Standard Section 4(f) ☒ Temporary Occupancy ☐ None

A Section 4(f) Temporary Occupancy Approval Document was prepared for the project. Under 23 CFR 774.13(d) the temporary occupancy of the Big Rock Camp Ground, owned by the Forest Preserve District of Kane County, was approved by exception by IDOT. The County had satisfactorily documented the impacts to the Section 4(f) property are of a temporary nature, the scope of work is minor, there are no permanent adverse physical impacts, the land will be fully restored, and there is documented agreement with the Forest Preserve who is the Owner with Jurisdiction.

See **Appendix B**, for the **Section 4(f) Temporary Occupancy Approval Document** and the **IDOT Approval of Section 4(f) Temporary Occupancy**.

12. **Air Quality (BLRS Manual Section 20-11)** Check One:

a. ☐ This project is in an attainment area.

☒ Project within a portion of a nonattainment area for which the Chicago Metropolitan Agency for Planning (CMAP) is the MPO.

This project is included in the FYI 2014-2019 Transportation Improvement Plan (TIP) endorsed by the Chicago Metropolitan Agency for Planning (CMAP), the region's Metropolitan Planning Organization. The 2040 Regional Transportation Plan was found to conform by the Federal Highway Administration (FHWA) and the Federal Transit Administration (FTA) on October 9, 2014.

The TIP was found to conform by the FHWA on October 9, 2014 and the FTA on October 9, 2014.

The TIP number for this project is 09-99-0101.

☐ Project within a nonattainment area served by a Metropolitan Planning Organization other than CMAP.

This project is included in the Long-Range Transportation Plan and in the _____ Transportation Improvement Program (TIP) endorsed by _____, the Metropolitan Planning Organization for the region in which the project is located.

On _____ the Federal Highway Administration (FHWA) and the Federal Transit Administration (FTA) determined that the Long-Range Transportation Plan conforms with the transportation-related provisions of the Clean Air Act Amendments of 1990. The FHWA and the FTA determined on _____ that the TIP conforms with the Clean Air Act Amendments. These findings were in accordance with 40 CFR Part 93, "Criteria and Procedures for Determining Conformity to State or Federal Implementation Plans of Transportation Plans, Programs, and projects Funded or Approved Under Title 23 USC or the Federal Transit Act."

The project's design concept and scope are consistent with the project information used for the TIP conformity analysis. Therefore, this project conforms to the existing State Implementation Plan and the transportation-related requirements of the 1990 Clean Air Act Amendments.

b. **Mobile Source Air Toxics (See BDE PM 52-06)**

This project will not result in any meaningful changes in traffic volumes, vehicle mix, location of the existing facility, or any other factor that would cause an increase in emissions relative to the no-build alternative. As such, FHWA has determined that this project will generate minimal air quality impacts for Clean Air Act criteria pollutants and has not been linked with any special Mobile Source Air Toxic concerns. Consequently, this effort is exempt from analysis for MSATs.

Moreover, EPA regulations for vehicle engines and fuels will cause overall MSATs to decline significantly over the next 20 years. Even after accounting for a 64 percent increase in VMT, FHWA predicts MSATs will decline in the range of 57 to 87 percent, from 2000 to 2020, based on regulations now in effect, even with a projected 64 percent increase in VMT. This will both reduce the background level of MSATs as well as the possibility of even minor MSAT emissions from the project.

c. **Construction-related Particulate Matter**

Demolition and construction activities can result in short-term increases in fugitive dust and equipment-related particulate emissions in and around the project area. (Equipment-related particulate emissions

are usually insignificant when equipment is well maintained.) The potential air quality impacts will be short-term, occurring only when demolition and construction work is in progress and local conditions are appropriate.

The potential for fugitive dust emissions typically is associated with building demolition, ground clearing, site preparation, grading, stockpiling of materials, on-site movement of equipment, and transportation of materials. The potential is greatest during dry periods, periods of intense construction activity, and during high wind conditions.

The Department's *Standard Specifications for Road and Bridge Construction* include provisions on dust control. Under these provisions, dust and airborne dirt generated by construction activities will be controlled through dust control procedures or a specific dust control plan, when warranted. The contractor and the Department will meet to review the nature and extent of dust-generating activities and will cooperatively develop specific types of control techniques appropriate to the specific situation. Techniques that may warrant consideration include measures such as minimizing track-out of soil onto nearby publicly-traveled roads, reducing speed on unpaved roads, covering haul vehicles, and applying chemical dust suppressants or water to exposed surfaces, particularly those on which construction vehicles travel. With the application of appropriate measures to limit dust emissions during construction, this project will not cause any significant short-term particulate matter air quality impacts.

d. **Project-level Hot Spot Analysis. Check One:**

- ☐ This project is in an attainment area and does not require a hot spot analysis.
- ☒ This project does not meet the definition of a project of air quality concern as defined in 40 CFR 93.123(b)(1). Due to no significant increase in diesel traffic, the percent of truck volumes does not increase,
and low ADTs,
it has been determined that the project will not cause or contribute to any new localized PM_{2.5} or PM₁₀ violations or increase the frequency or severity of any PM_{2.5} or PM₁₀ violations. USEPA has determined that such projects meet the Clean Air Act's requirements without any further Hot-Spot analysis.
- ☐ This project is in a non-attainment or maintenance area and is a project of air quality concern. Therefore, a qualitative hot spot analysis is required. See Attachment _____

e. **COSIM**

Are through lanes or auxiliary turn lanes being added with this project?

☒ Yes ☐ No

If yes, has a COSIM pre-screen analysis been completed?

☒ Yes ☐ No

If yes, pre-screen analysis is attached as Attachment See Appendix B, COSIM Pre-screening Clearance.

If no, explain why an analysis has not been performed. _____

If yes, did the COSIM pre-screen analysis pass or fail? ☒ Pass ☐ Fail

If the COSIM pre-screen analysis failed, a full COSIM analysis would be required.

13. Noise (BLRS Manual Section 20-6)

- ☐ The referenced project meets the criteria for a Type III project established in 23 CFR Part 772. Therefore, the proposed project requires no traffic noise analysis or abatement evaluation. Type III projects do not involve added capacity, construction of new through lanes, changes in the horizontal or vertical alignment of the roadway, or exposure of noise sensitive land uses to a new or existing highway noise source.
- ☒ Based on the traffic noise analysis and noise abatement evaluation conducted, highway traffic noise abatement measures are likely to be implemented based on preliminary design. The noise barriers determined to meet the feasible and reasonable criteria are identified on the attachment. If it subsequently develops during final design that constraints not foreseen in the preliminary design or public input substantially change, the abatement measures may need to be modified or removed upon completion of the project's final design and the public involvement process.

If this project involves a new alignment, additional lanes, or involves a significant alignment change, attach a traffic noise analysis.

Because the project involves construction of a highway on a new alignment, the project meets the criteria for a Type I project as established in 23 CFR Part 772. As required for Type I projects, a traffic noise analysis and noise abatement evaluation was conducted. The result of this analysis and evaluation was that noise impacts were present but noise abatement was not feasible and reasonable according to the applicable standards. As such, highway traffic noise abatement measures are not likely to be implemented based on preliminary design. See **Appendix B, IDOT Approval of Noise Report**.

For a full discussion of traffic noise, see the **Dauberman Road Extension Noise Analysis Study, November 2017**, available under separate cover.

14. Work Zone Transportation Management Plans

Does the project intersect or follow a state route?

☒ Yes ☐ No

Is the state or local route considered a significant route?

☒ Yes ☐ No ☐ Not Applicable

If yes, describe how the Work Zone Transportation Management Plan is being implemented.

US Route 30 is currently a two-lane roadway and there are no major intersections present within the proposed improvement limits. The project along US Route 30 consists of widening and reconstruction. A traffic signal is proposed for the intersection of US Route 30 and the jughandle, also known as the Connector Road.

The projected traffic along US Route 30 is anticipated to be 15,000 vehicles per day in the design year 2040 according to the CMAP (MPO for the region). Although detour routes and runarounds were considered, they were excluded because of adverse lengths of feasible detour routes and the lack of existing ROW for runarounds.

Due to limited ROW and the lack of appropriate detour routes, staged construction is the only feasible construction alternative. The proposed maintenance of traffic plan will allow for two lanes of traffic (one in

each direction) throughout the duration of construction. This will be accomplished through the use of temporary pavement and temporary concrete barriers to protect the work zone. Since the number of lanes during construction is the same as the current number of lanes, and since there are no major intersections within the construction zone along US Route 30, no significant delays or backups are anticipated.

Temporary Traffic Control Plan measures shall utilize the use of temporary widening, use of night work, and temporary/restricted lane closures along with a speed limit reduction from the current posted 55 mph to 50 mph. Additionally, increased Work Zone violations and penalties will be implemented.

The Public Information Plan shall include a Media Press Release, Web Page through KDOT, and Changeable Message Signs along with Static Message Signs warning drivers of the construction occurring along US Route 30.

Temporary staging may include pre-stage construction of the culverts east of Dauberman Road which pass beneath US Route 30.

15. Complete Streets (BLRS Manual Chapter 10)

Does the project include the addition of a travel, turning, or bi-directional turn lane on a state highway?

☒ Yes ☐ No

If yes, describe how the Complete Streets Law requiring accommodating bicycles on a state route apply.

A multi-use path will be constructed along the east side of Dauberman Road from US Route 30 to Granart Road. It will then continue east along the south side of Granart Road ending at the Big Rock Campground Forest Preserve entrance across from Mary Drive. The path will be paved, 10 feet wide with 2-foot graded grass shoulders, and located a minimum of 10 feet from the edge of travel way. Tactile surfaces will be provided at the intersection crossings. For additional details of the multi-use path, see **Section 2c, Multi-Use Path (Bicycle and Pedestrian Facility)** for an in depth discussion of the proposed path improvements.

Specific to US Route 30, the path facility will continue from Dauberman Road, along the Connector Road, to the northeast quadrant of the US Route 30/Connector Road intersection. At this intersection the path will cross the north leg of the intersection and terminate in the northwest quadrant. The paved path will be stubbed out at both corners. However, continuing west and east from the Connector Road, along the north side of US Route 30, the PR ROW will be acquired and a shelf graded to accommodate a future path to be constructed by others.

The span of the proposed bridge over US Route 30 will be long enough to allow for future widening of US Route 30 and a multi-use path along the north side of the roadway. The clear zone from the edge of the future five-lane US Route 30 roadway is 30 feet.

The project is fully compliant with Complete Streets requirements.

16. Maintenance of Traffic (BLRS Manual Section 22-2.11(b)(9))

Discuss how vehicle traffic and pedestrians will be accommodated during construction, including the impacts of any road and/or sidewalk closure. If the road will be closed, include information concerning location of alternate routes, their ability to handle the additional traffic (street width, number of traffic lanes, structural adequacy, etc.), and the amount of adverse travel. When a marked detour route will be provided, include coordination with appropriate agencies, a description of the adverse travel, and include a map showing the alternate routes or marked detour in the report.

US Route 30 will be kept as two 11-foot wide travel lanes, one in each direction, during construction. Temporary pavement and/or temporary barrier wall will be utilized to maintain traffic while construction is occurring in the opposite lanes.

South of US Route 30, Dauberman Road will be extended on a new alignment to Granart Road. No traffic staging will be necessary within this area as there are no existing roadways to maintain. North of US Route 30, when bridge embankment work begins, Dauberman Road will be detoured using Scott Road and Davis Road back to US Route 30. This is the same detour route utilized by KDOT when the Dauberman Road over Welch Creek bridge reconstruction (just north of the project terminus) occurred during 2016. The existing condition of this detour route should adequately accommodate the diverted traffic, as was the case when the bridge reconstruction was ongoing. Scott Road and David Road are both two-lane rural County roads with 12-foot lanes and 2-foot paved shoulders; the speed limits are 55 mph.

Granart Road will be maintained as two lanes during construction except when the changeover to the new alignment will occur. This will necessitate a short-term (2 weeks, approximately) closure of Granart Road at the proposed changeover locations. Traffic to and from the east will continue to travel along Granart Road. Traffic to and from the west will be detoured to Jericho Road to Hinckley Road to E. County Line Road to US Route 30. The detour travels along County routes and should be able to accommodate the additional traffic for the short-term closure.

Due to the surrounding land use, there are no existing pedestrian accommodations present along Dauberman Road, Granart Road, or US Route 30; therefore no specific accommodations are proposed for non-motorized transportation during construction.

Access to all private properties will be maintained during construction via temporary aggregate driveways. However, some short-duration closures may be necessary during construction.

See **Exhibit 17, Detour Route Map**.

17. Public Involvement (BLRS Manual Chapter 21)

- a. Summarize public informational meetings, formal public hearings, property owner signoffs, council or board meetings, media coverage, and personal contact with public. Include copies of newspaper advertisements, letter to property owners, public comments, and documents showing all public comments have been addressed.

Public Meeting

An Open House Public Meeting was held February 9, 2017. The purpose of the meeting was to introduce the project and conduct a Scoping Meeting for the Environmental Assessment (EA). Since then, the project level of environmental processing has changed from an EA to a Categorical Exclusion. The public meeting was held at the Hinckley-Big Rock Middle School from 5:00 to 7:30 p.m. Approximately 100 people signed the meeting register. Eleven comments were received within the 2-week comment period. The comments covered a variety of topics including:

- | | |
|-------------------------|-----------------------------|
| ○ Air Quality | ○ Need for Project |
| ○ Benefits | ○ Neighborhood Impacts |
| ○ Capacity | ○ Opposition to Project |
| ○ Drainage Concerns | ○ Recreational Connectivity |
| ○ Environmental Impacts | ○ Safety |
| ○ Impacts on Wildlife | ○ Support for Project |

Of the 11 comments received, six supported the project, one was opposed, and four expressed concerns. Of the comments expressing concern, most were regarding the potential for increase in traffic generated by the Dauberman Road extension and regarding poor drainage in the area. The sole opposition comment was from a resident. She did not support the project because she felt that the wait at the railroad tracks was not too long; the project would incur environmental impacts; and would not address the regional drainage problems.

Public Hearing

An Open House Public Hearing was held February 15, 2018. The purpose of the hearing was to provide a project update, present the environmental studies, review the alternatives analysis, present the recommended alternative, discuss the Section 4(f) Temporary Occupancy document, and show the Detour Route Map.

The public hearing was held at the Hinckley-Big Rock Middle School from 5:00 to 7:30 p.m. A court reporter was present if attendees desired to dictate their comment or question. Two, 20-minute live presentations were made to the public at 6:00 P.M. and at 6:45 P.M. by the project consultant. The meeting was attended by 61 people. Twelve comments were received within the 2-week comment period. The comments covered a variety of topics including:

- | | |
|---------------------|------------------------|
| ○ Project Benefits | ○ Neighborhood Impacts |
| ○ Drainage Concerns | ○ Project Location |
| ○ Farm Access | ○ Safety |
| ○ Economic Impact | ○ Sight Distance |
| ○ Need for Project | ○ Support for Project |

Of the comments received, none opposed the project. Five comments were concerning drainage, largely focused on regional drainage problems and the desire to include additional improvements to solve flooding concerns to the west of the project.

See **Appendix D, Public Meeting and Public Hearing**, for documents and additional information on the public meeting and public hearing. The **KDOT Public Hearing Certification Letter** is also included in **Appendix D**.

b. Has any opposition been expressed toward the improvement?

☐ Yes ☒ No

There has not been any significant or organized opposition towards the project. At the Public Meeting, there was one resident who stated opposition to the project, however, by the Public Hearing, there were no comments received opposing the project.

If yes, briefly discuss the type and extent of opposition.

c. If yes, discuss how the opposition has been addressed with the property owners?

18. Coordination: LA-IDOT-FHWA (BLRS Manual Section 22-1.02)

Have there been any coordination meetings for the project? ☒ Yes ☐ No

If yes, list the date(s) of the coordination meeting(s) below and attach coordination meeting minutes in the report.

There have been five meetings held: 1/12/16, 3/14/17, 5/9/17, 7/11/17, and 12/5/17. See **Appendix C, FHWA/IDOT Coordination Meeting Minutes**.

19. Other Coordination

Attach results.

The following coordination and meetings were completed:

Meeting Date	Agency/Group Met With
8/25/16	Initial Meeting with Local Agencies (Big Rock and Sugar Grove)
2/21/17	Big Rock Drainage District Meeting
5/17/17	CMAQ ADT Concurrence Letter
5/18/17	USACE Jurisdictional Determination Approval Letter
10/25/17	Coordination Meeting with IDOT Programming
2/2/18	Coordination Meeting with IDOT BLRS
2/5/18	Coordination Meeting with IDOT BLRS
2/8/18	IDOT Approval to hold the Public Hearing
2/15/18	IDOT Response regarding the Prairie Parkway
5/10/18	Coordination with the BNSF Railroad - Received Comments (Calvin Nutt)
6/7/18	Coordination Meeting with IDOT Hydraulics Unit

See **Appendix E, Other Coordination and Meetings**.

20. Summary of Commitments

1. Design exception for the proposed 4-foot wide paved shoulder along the J. Maddock Air Strip frontage is approved contingent on KDOT installing advanced warning signs to alert the traveling public to the narrow shoulder. See **Appendix B, IDOT Design Variance Approval**.
2. FAA Part 77 Notice Criteria Tool request for a notice will be filed during Phase 2 Design Engineering. See **Appendix E, FAA Part 77 Notice Criteria Tool**.

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APPENDIX A

Exhibits

Exhibit 1, Project Limits Map

Exhibit 2, Existing Typical Cross Sections

- Dauberman
- US Route 30
- Granart Road

Exhibit 3, Environmental Resources Map

Exhibit 4, Floodplain Map FEMA Firmette

Exhibit 5, Existing Drainage Plan

Exhibit 6, ADT Map

Exhibit 7, ICC Train Counts

- Rhodes Street
- Dugan Road

Exhibit 8, Proposed Typical Cross Sections

- Dauberman Road
- US Route 30
- Granart Road
- Connector Road

Exhibit 9, Proposed Plan and Profile Sheets

Exhibit 10, US Route 30 IDS

Exhibit 11, Proposed Bridge Typical Cross Sections

- Bridge over US Route 30
- Bridge over BNSF RR

Exhibit 12, Bridge Type, Size and Location (TS&L)

- Bridge over US Route 30
- Bridge over BNSF RR

Exhibit 13, Proposed Drainage Plan

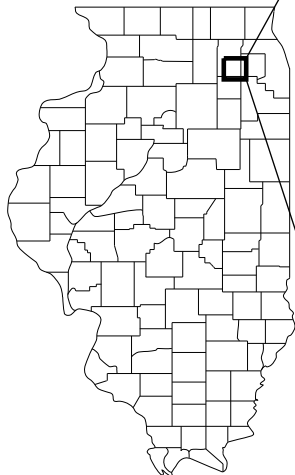
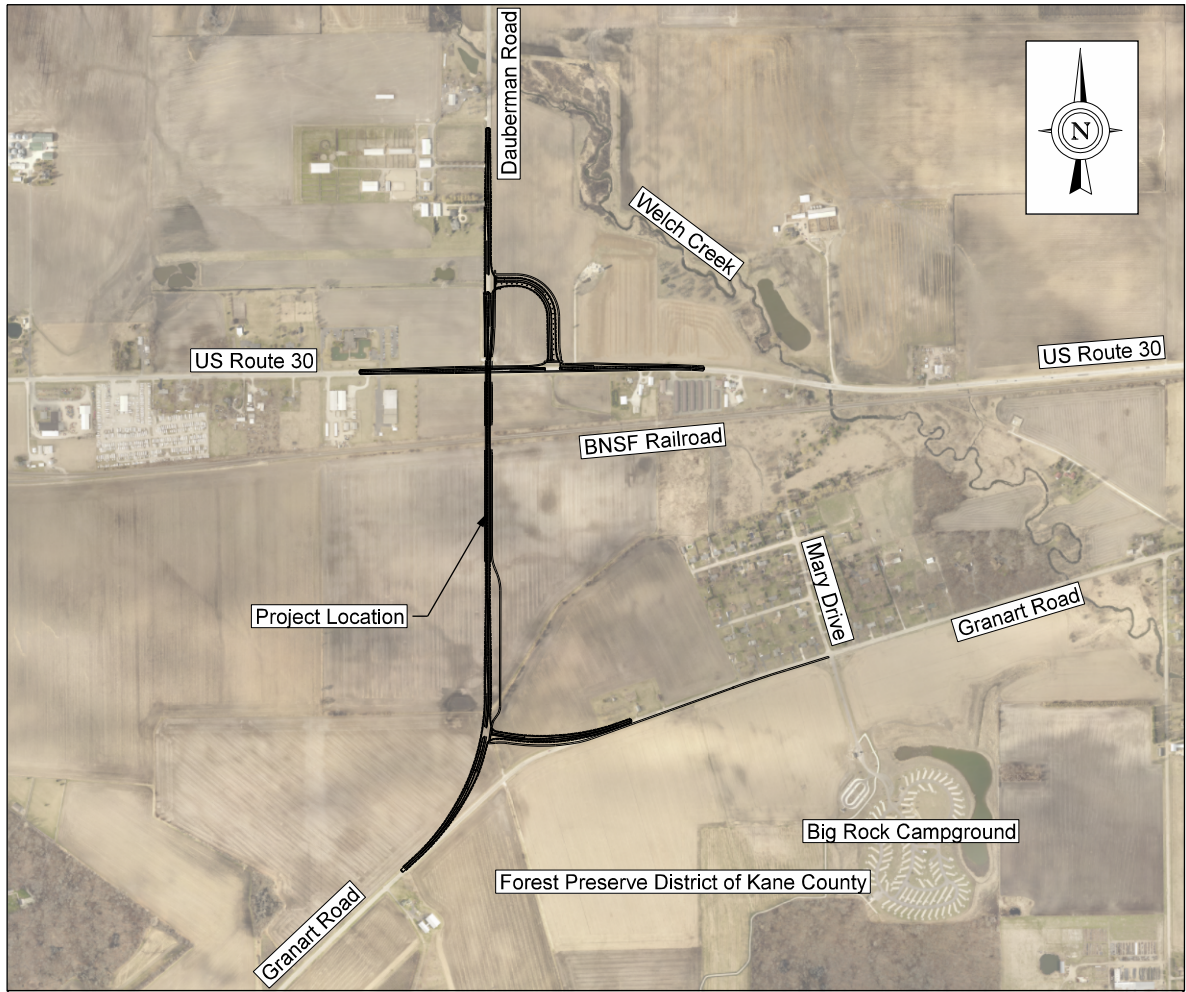
Exhibit 14, Proposed Estimate of Cost

Exhibit 15, Bicycle Routes and Plans

- Kane/Kendall County Bike Plan
- Big Rock Forest Preserve Bike Route Map

Exhibit 16, Collision Diagrams and Crash Summary Tables

Exhibit 17, Detour Route Map



FILE NAME = \$FILEL\$

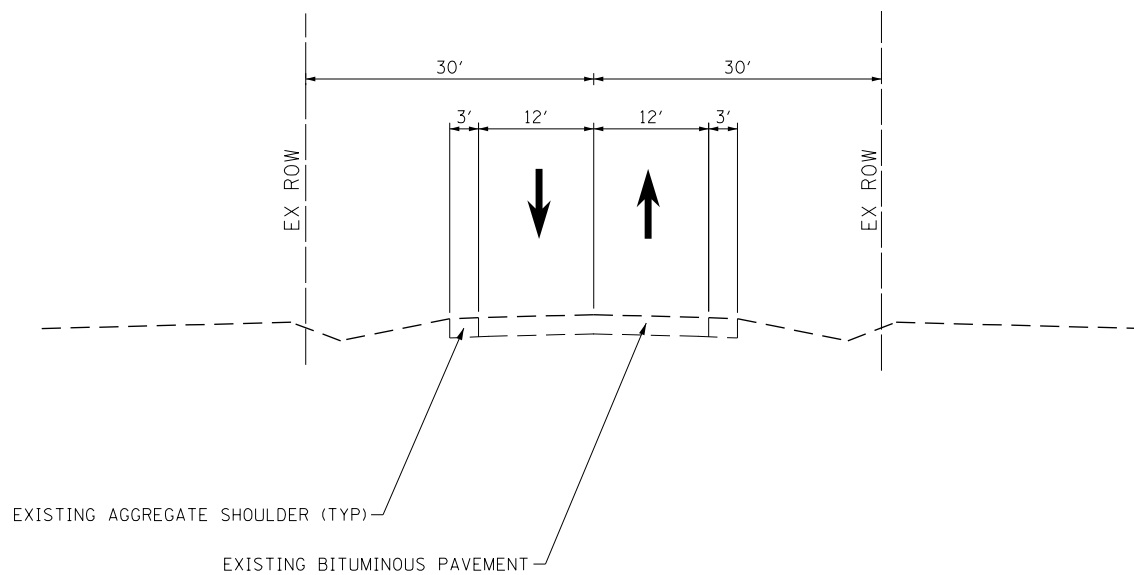


USER NAME = \$USER\$
PLOT SCALE = \$SCALE\$
PLOT DATE = \$DATE\$

Project Location and Limits Map

F.A.S RTE.	SECTION	COUNTY	APPX	EXHIBIT	SHEET
1107	15-00277-01-BR	KANE	A	1	1 OF 1
Dauberman Road Extension Project Development Report Project Location and Limits Map					

FILE NAME = 0086-PDR_Appx4.02_ex_Typ_Cross Sections



DAUBERMAN ROAD

EXISTING STATIONING:
STA. 560+00 TO STA. 583+00
PROPOSED STATIONING:
STA. 660+00 TO STA. 683+00



USER NAME = bjholman

PLOT SCALE = *SCALE*

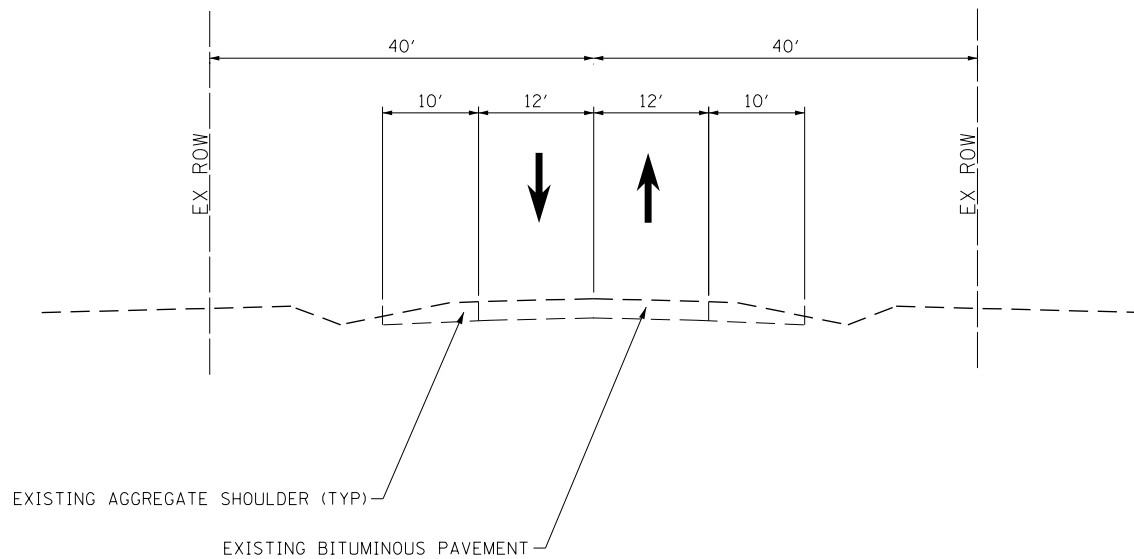
PLOT DATE = 8/2/2018

Kane County
Division of Transportation

EXISTING
TYPICAL SECTION

F.A.S RTE.	SECTION	COUNTY	APPX	EXHIBIT	SHEET
1107	15-00277-01-BR	KANE	A	2	1
Dauberman Road Extension Project Development Report					

FILE NAME = 0086-PDR_Appx4.02.ex_Typ. Cross Sections



US ROUTE 30
STA. 314+00 TO STA. 367+00



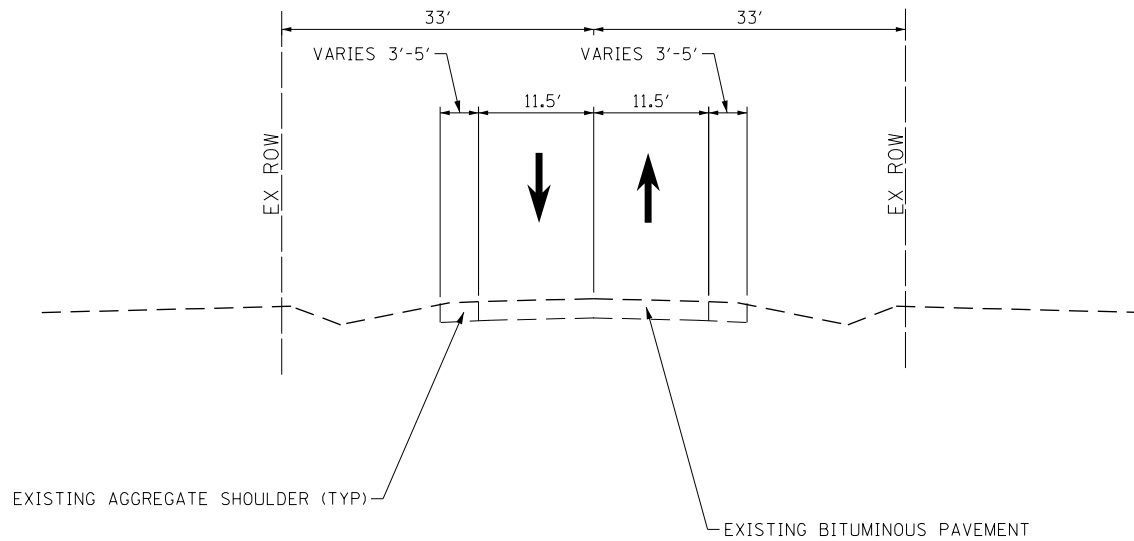
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PLOT DATE = 8/2/2018

Kane County
Division of Transportation

EXISTING
TYPICAL SECTION

F.A.S RTE.	SECTION	COUNTY	APPX	EXHIBIT	SHEET
1107	15-00277-01-BR	KANE	A	2	2
Dauberman Road Extension Project Development Report					

FILE NAME = 0086-PDR_Appx4.02.ex_Typ. Cross Sections



GRANART ROAD
STA. 103+00 TO 172+00



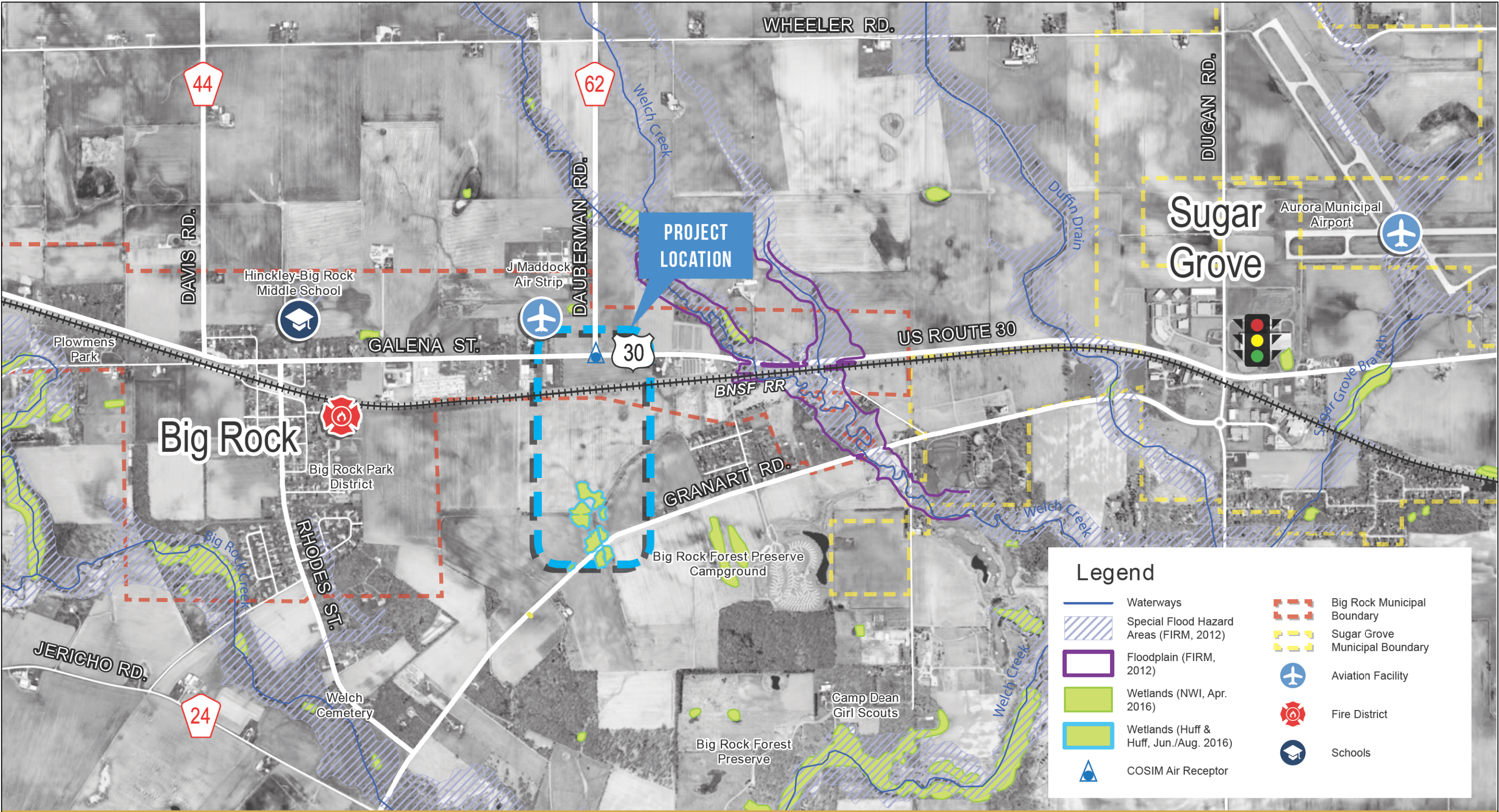
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PLOT DATE = 8/2/2018

Kane County
Division of Transportation

EXISTING
TYPICAL SECTION

F.A.S RTE.	SECTION	COUNTY	APPX	EXHIBIT	SHEET
1107	15-00277-01-BR	KANE	A	2	3
Dauberman Road Extension Project Development Report					

FILE NAME: p:\h\p\p\101\env\transcorp\p\Documents\Projects\CH401 - Chicago\p401150086 - Kane County - Dauberman Road Extension\303.00 - Road\303.02 - Exhibits\14 - PDR Exhibits\0086-PDR-AppA.03.ERM



DAUBERMAN ROAD EXTENSION | ENVIRONMENTAL RESOURCES

Kane County, Illinois



Not to Scale

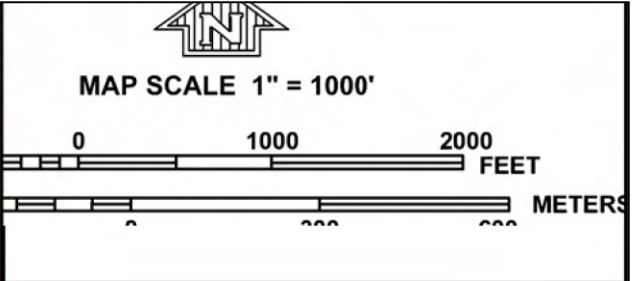
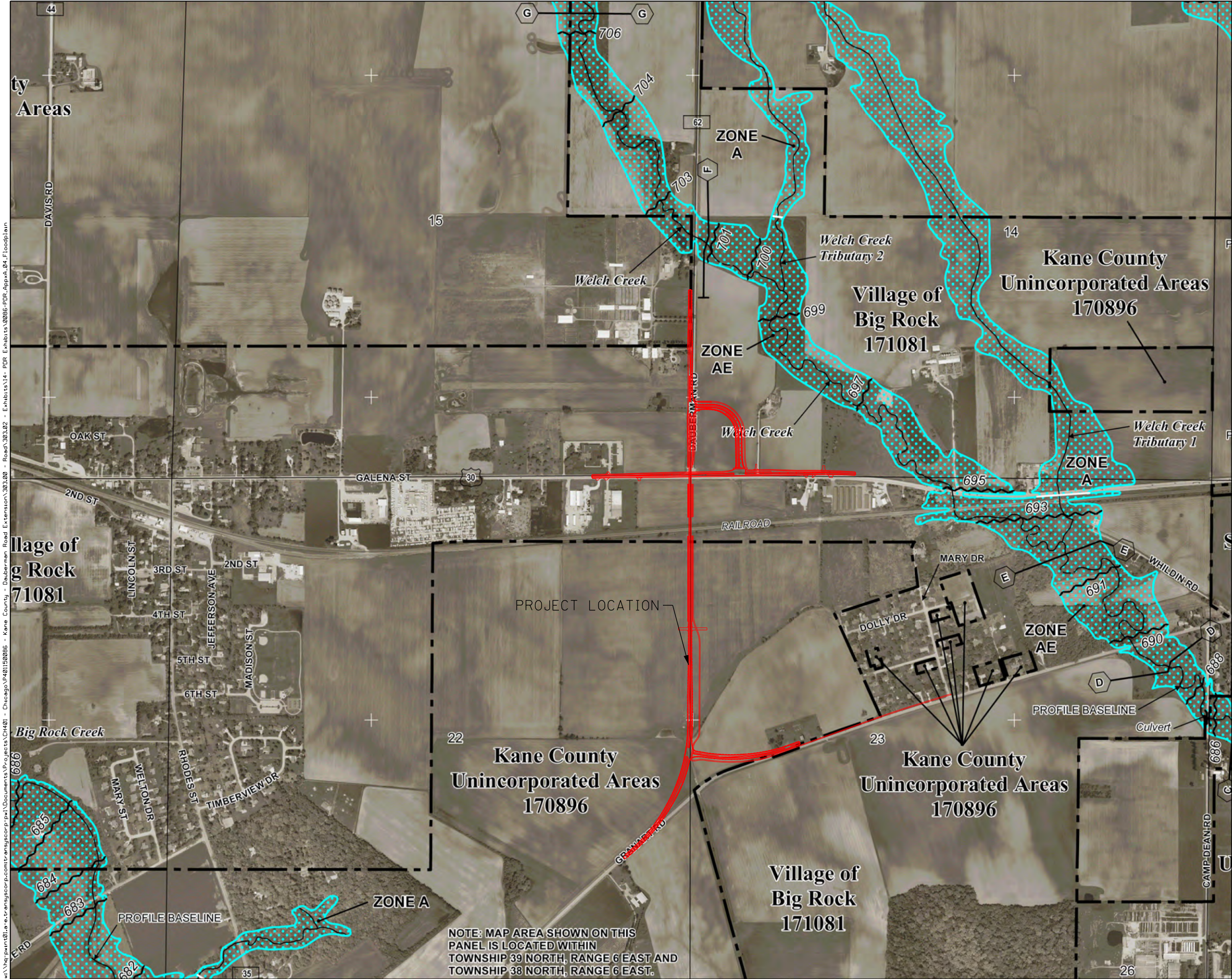


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	DRAWN -	
PLOT SCALE = 1600.0000' / in.	CHECKED -	
PLOT DATE = 11/3/2017	DATE -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL RESOURCES MAP

F.A.S. RTE.	SECTION	COUNTY	APPX	EXHIBIT	SHEET
1107	15-00277-01-BR	KANE	A	3	1 OF 1
Dauberman Road Extension Project Development Report Environmental Resources Map					



NFIP
NATIONAL FLOOD INSURANCE PROGRAM

PANEL 0295J

FIRM
FLOOD INSURANCE RATE MAP
KANE COUNTY,
ILLINOIS
AND INCORPORATED AREAS

PANEL 295 OF 410
(SEE MAP INDEX FOR FIRM PANEL LAYOUT)


CONTAINS:

COMMUNITY	NUMBER	PANEL	SUFFIX
BIG ROCK, VILLAGE OF	171081	0295	J
KANE COUNTY	170896	0295	J
SUGAR GROVE, VILLAGE OF	170333	0295	J


Notice to User: The Map Number shown below should be used when placing map orders; the Community Number shown above should be used on insurance applications for the subject community.

MAP NUMBER
17089C0295J

MAP REVISED
JULY 17, 2012


Federal Emergency Management Agency

NOTE: MAP AREA SHOWN ON THIS
PANEL IS LOCATED WITHIN
TOWNSHIP 39 NORTH, RANGE 6 EAST AND
TOWNSHIP 38 NORTH, RANGE 6 EAST.

	USER NAME = mbmwhorter	DESIGNED -	
		DRAWN -	
	PLOT SCALE = 1000.0000' / in.	CHECKED -	
	PLOT DATE = 11/10/2017	DATE -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FLOODPLAIN MAP
FEMA FIRMETTE

F.A.S RTE.	SECTION	COUNTY	APPX	EXHIBIT	SHEET
1107	15-00277-01-BR	KANE	A	4	1 OF 1
Dauberman Road Extension Project Development Report Floodplain Map					

This is an official copy of a portion of the above referenced flood map. It was extracted using F-MIT On-Line. This map does not reflect changes or amendments which may have been made subsequent to the date on the title block. For the latest product information about National Flood Insurance Program flood maps check the FEMA Flood Map Store at www.msc.fema.gov

FILE NAME = Projects\CH401 - Chicago\401150086 - Kane County - Dauberman Road Extension - Hydraulics - Location Drainage Study\LOS.Sheets\LOS.EXISTING

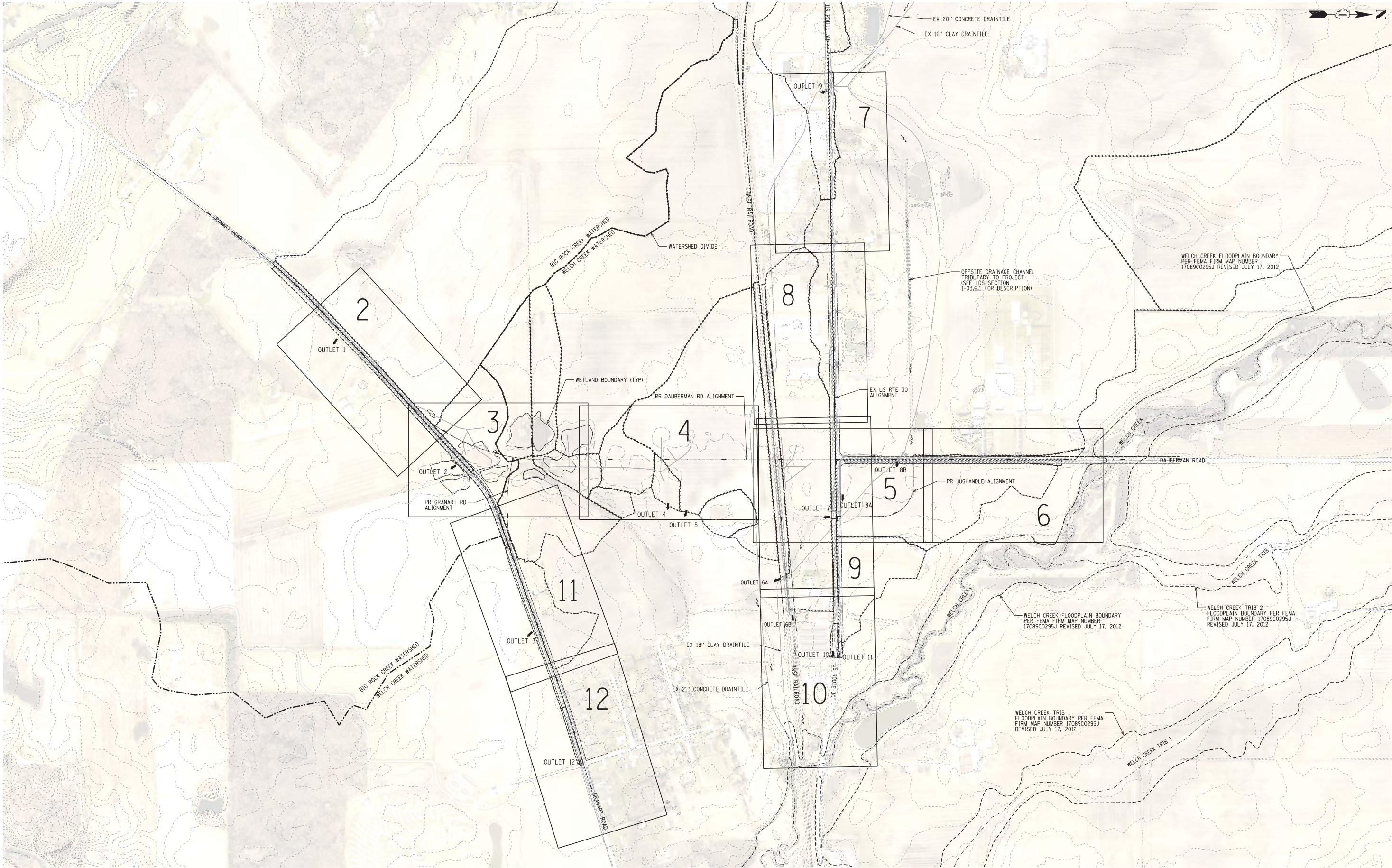


EXHIBIT 5



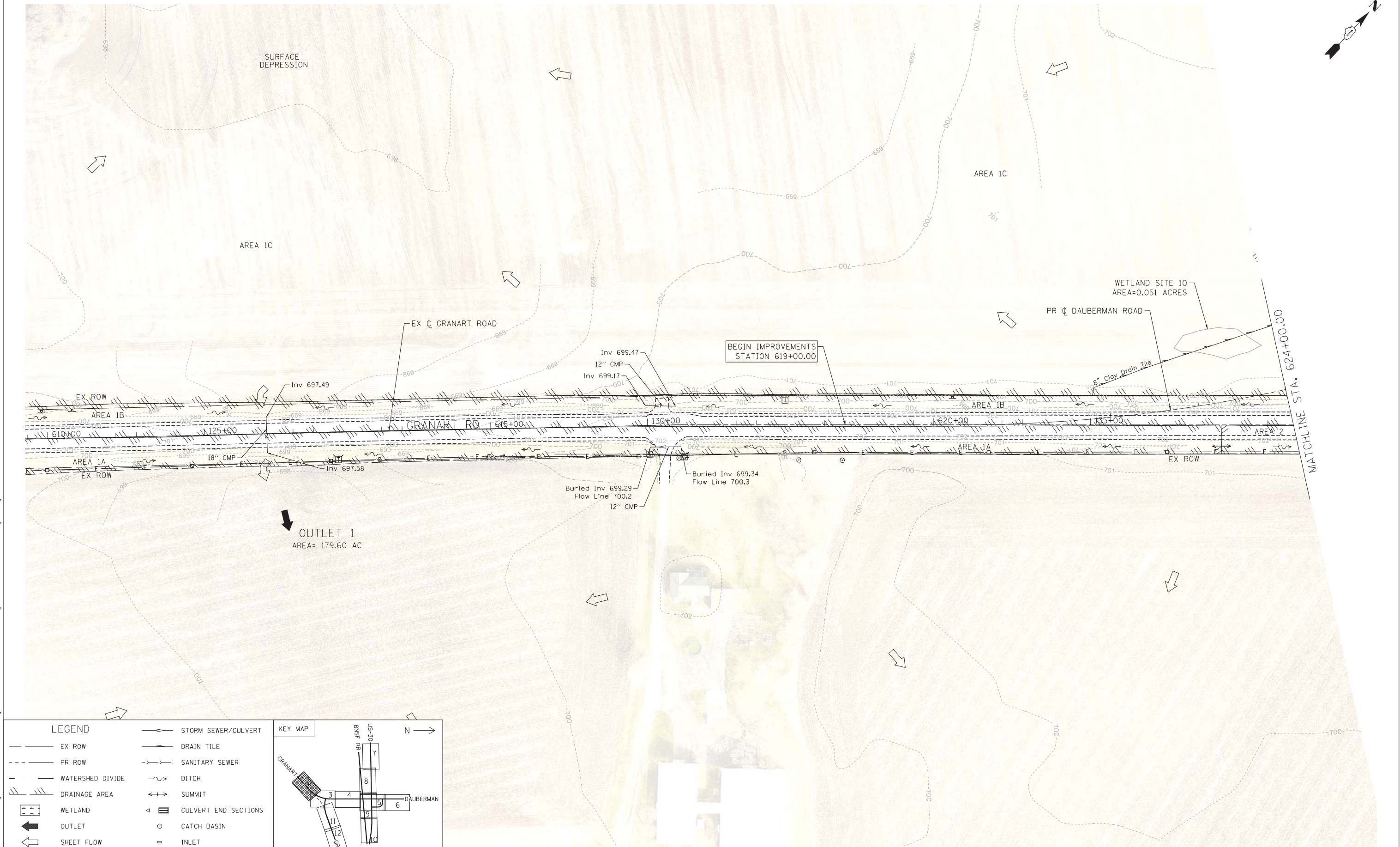
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	DRAWN - MK	REVISED - 11/09/2017
PLOT SCALE = 1"=400'	CHECKED - KEK	REVISED - 03/09/2018
PLOT DATE = 11/28/2017	DATE - 02/01/2017	REVISED -

KANE COUNTY
DIVISION OF TRANSPORTATION

EXISTING DRAINAGE PLAN KEY
DAUBERMAN ROAD EXTENSION

SCALE: 1"=400' SHEET 1 OF 12 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		KANE	12	1
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				


**Bollinger, Lach
& Associates, Inc.**
 ITASCA, ILLINOIS

**EXISTING DRAINAGE PLAN
DAUBERMAN ROAD EXTENSION**

EXHIBIT 5

FILE NAME = Projects\CH401 - Chicago\40150086 - Kane County - Dauberman Road Extension - Hydraulics - Location Drainage Study\LD5.Sheets\LD5.EXISTING

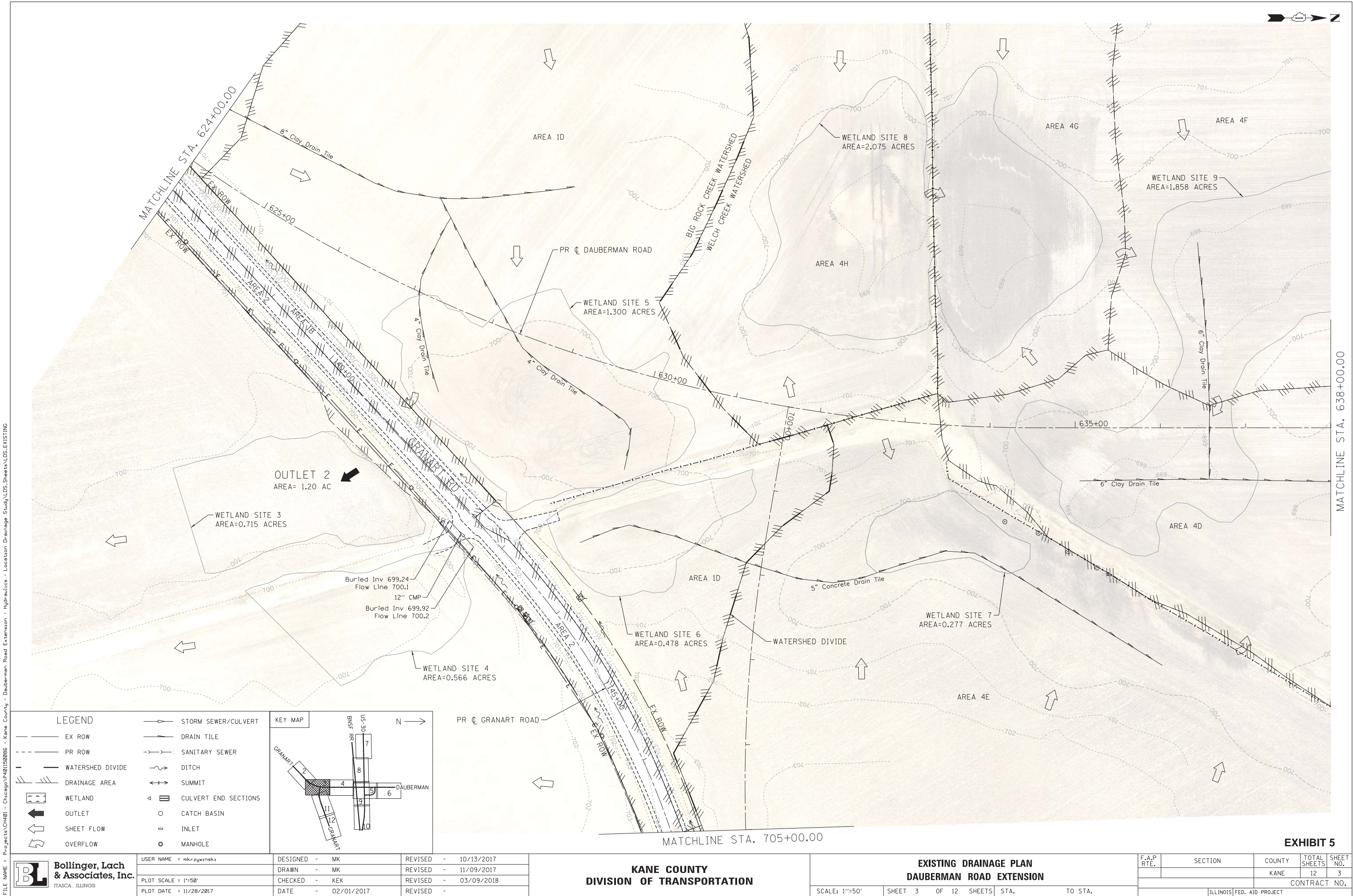


EXHIBIT 5

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		KANE	12	3
CONTRACT NO.				



USER NAME = mkrzynski
PLOT SCALE = 1"=50'
PLOT DATE = 11/28/2017

DESIGNED - MK
DRAWN - MK
CHECKED - KEK
DATE - 02/01/2017

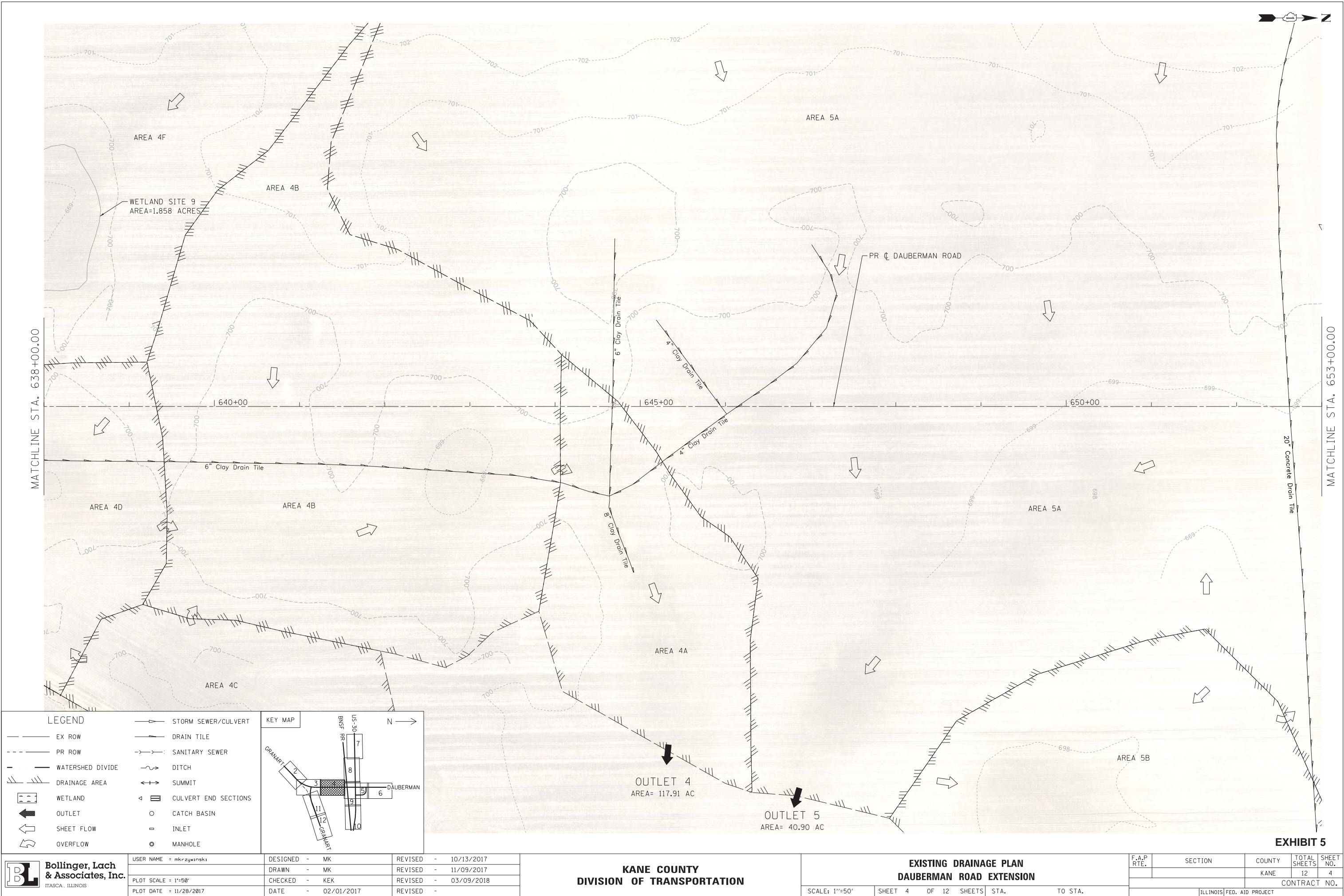
REVISED - 10/13/2017
REVISED - 11/09/2017
REVISED - 03/09/2018
REVISED -

KANE COUNTY
DIVISION OF TRANSPORTATION

EXISTING DRAINAGE PLAN
DAUBERMAN ROAD EXTENSION

SCALE: 1"=50' SHEET 3 OF 12 SHEETS STA. TO STA.

FILE NAME = Projects\CH401 - Chicago\40150086 - Kane County - Dauberman Road Extension - Hydraulics - Location Drainage Study\LD5.Sheets\LD5.EXISTING



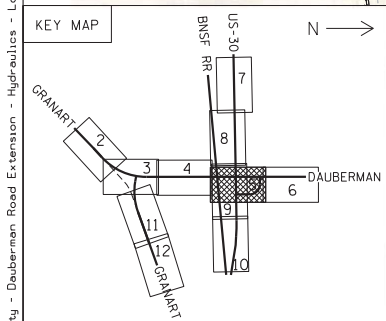
LEGEND

- EX ROW
- PR ROW
- WATERSHED DIVIDE
- DRAINAGE AREA
- WETLAND
- OUTLET
- SHEET FLOW
- OVERFLOW
- STORM SEWER/CULVERT
- DRAIN TILE
- SANITARY SEWER
- DITCH
- SUMMIT
- CULVERT END SECTIONS
- CATCH BASIN
- INLET
- MANHOLE

KEY MAP

 <div>Bollinger, Lach & Associates, Inc. ITASCA, ILLINOIS</div>	USER NAME = mkrzywinski	DESIGNED - MK	REVISED - 10/13/2017	<div>KANE COUNTY DIVISION OF TRANSPORTATION</div>	EXISTING DRAINAGE PLAN DAUBERMAN ROAD EXTENSION					F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - MK	REVISED - 11/09/2017									KANE	12	4
	PLOT SCALE = 1"=50'	CHECKED - KEK	REVISED - 03/09/2018		CONTRACT NO.									
	PLOT DATE = 11/28/2017	DATE - 02/01/2017	REVISED -		SCALE: 1"=50'	SHEET 4	OF 12	SHEETS	STA.	TO STA.		ILLINOIS FED. AID PROJECT		

FILE NAME = Projects\CH401 - Chicago\40150086 - Kane County - Dauberman Road Extension - Hydraulics - Location Drainage Study\05.Sheets\LD5.EXISTING



LEGEND	
	EX ROW
	PR ROW
	WATERSHED DIVIDE
	DRAINAGE AREA
	WETLAND
	OUTLET
	SHEET FLOW
	OVERFLOW
	STORM SEWER/CULVERT
	DRAIN TILE
	SANITARY SEWER
	DITCH
	SUMMIT
	CULVERT END SECTIONS
	CATCH BASIN
	INLET
	MANHOLE

Bollinger, Lach & Associates, Inc.
ITASCA, ILLINOIS

USER NAME = mkrzynski
DESIGNED - MK
DRAWN - MK
PLOT SCALE = 1"=50'
PLOT DATE = 11/28/2017

REVISD - 10/13/2017
REVISD - 11/09/2017
REVISD - 03/09/2018
REVISD -
CHECKED - KEK
DATE - 02/01/2017

KANE COUNTY
DIVISION OF TRANSPORTATION

EXISTING DRAINAGE PLAN
DAUBERMAN ROAD EXTENSION

SCALE: 1"=50' SHEET 5 OF 12 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		KANE	12	5
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

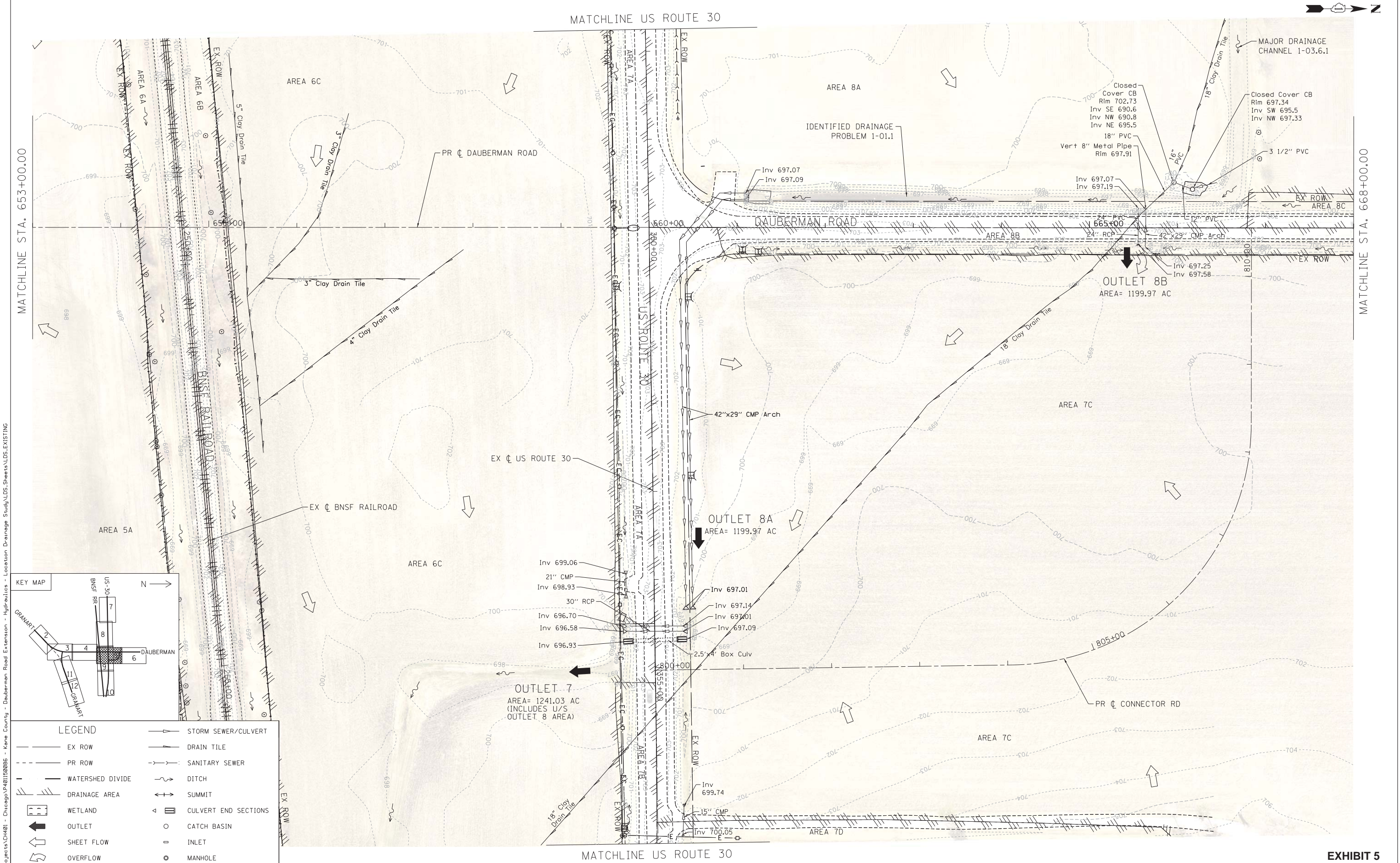
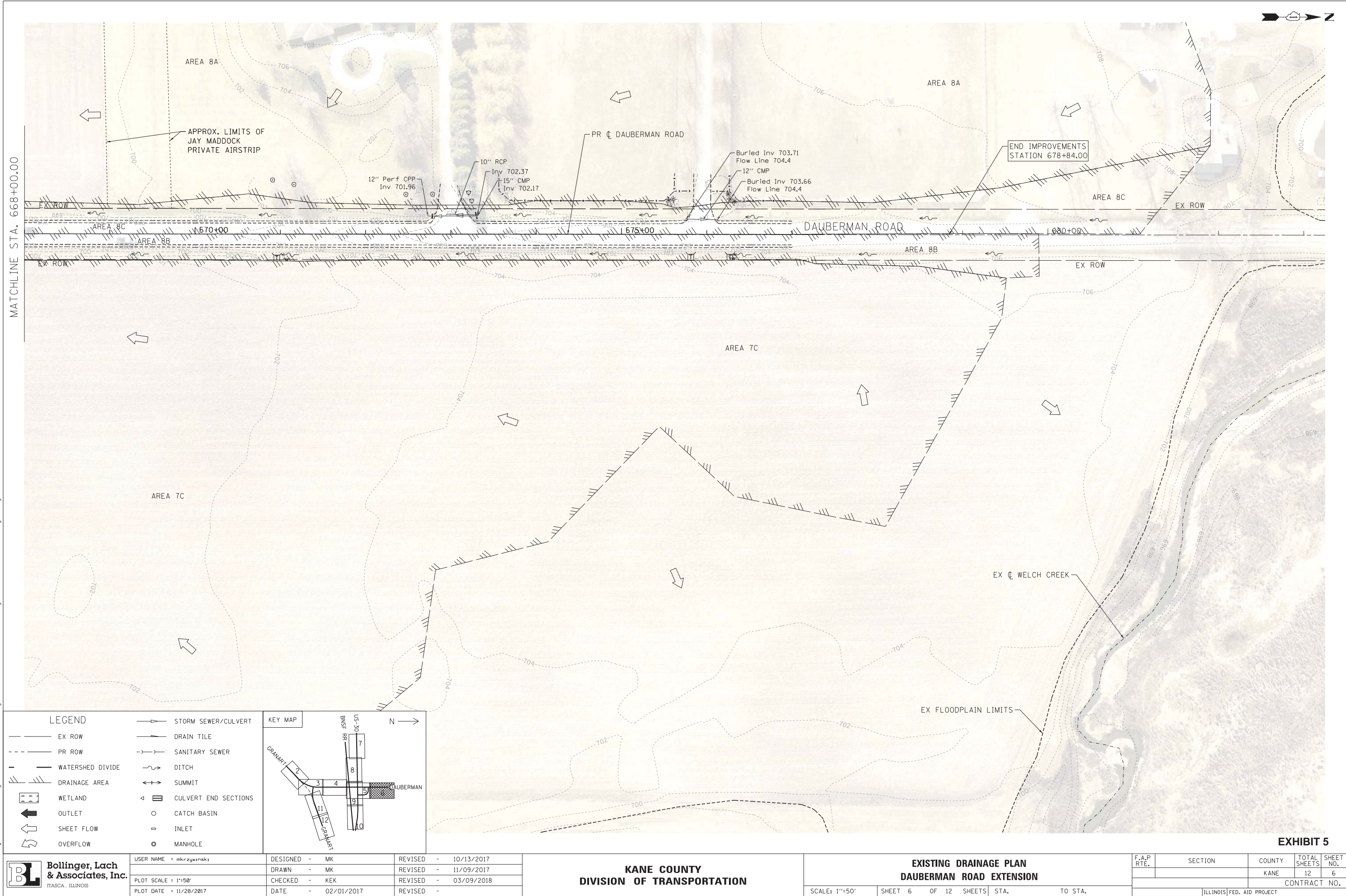


EXHIBIT 5

FILE NAME = Projects\CH401 - Chicago\40150086 - Kane County - Dauberman Road Extension - Hydraulics - Location Drainage Study\LD5.Sheets\LD5.EXISTING

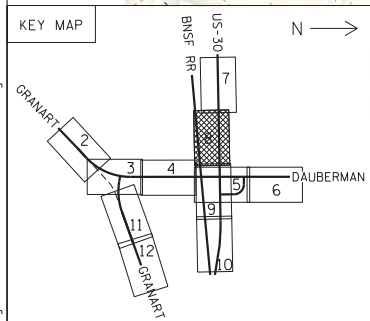


LEGEND

- EX ROW
- PR ROW
- WATERSHED DIVIDE
- DRAINAGE AREA
- WETLAND
- OUTLET
- SHEET FLOW
- OVERFLOW
- STORM SEWER/CULVERT
- DRAIN TILE
- SANITARY SEWER
- DITCH
- SUMMIT
- CULVERT END SECTIONS
- CATCH BASIN
- INLET
- MANHOLE

KEY MAP

FILE NAME = Projects\CH401 - Chicago\40150086 - Kane County - Dauberman Road Extension - Hydraulics - Location Drainage Study\LD5.Sheets\LD5.EXISTING



LEGEND	
	EX ROW
	PR ROW
	WATERSHED DIVIDE
	DRAINAGE AREA
	WETLAND
	OUTLET
	SHEET FLOW
	OVERFLOW
	STORM SEWER/CULVERT
	DRAIN TILE
	SANITARY SEWER
	DITCH
	SUMMIT
	CULVERT END SECTIONS
	CATCH BASIN
	INLET
	MANHOLE

USER NAME = mkrzynski	DESIGNED - MK	REVISED - 10/13/2017
PLOT SCALE = 1"=50'	DRAWN - MK	REVISED - 11/09/2017
PLOT DATE = 11/28/2017	CHECKED - KEK	REVISED - 03/09/2018
	DATE - 02/01/2017	REVISED -

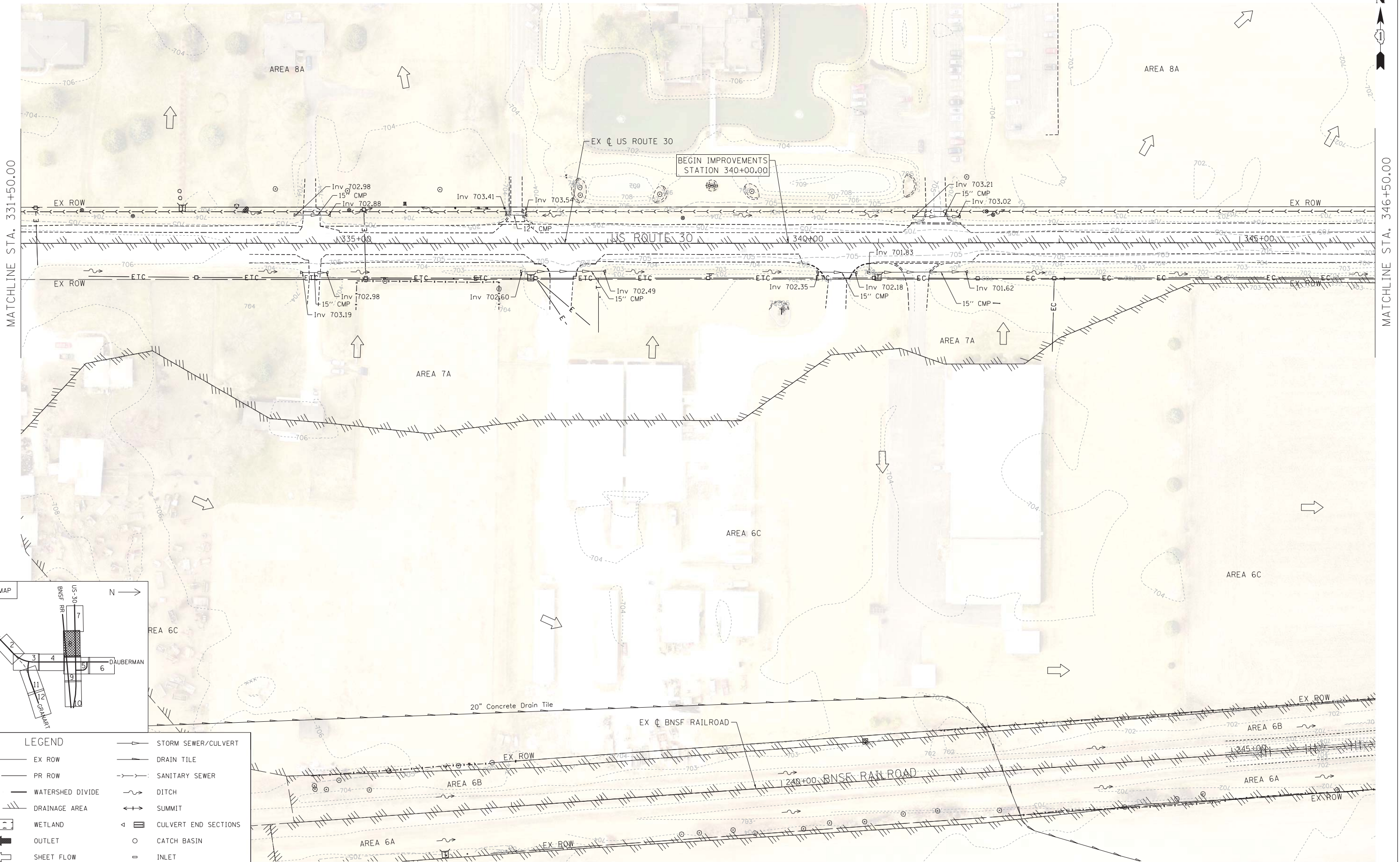
KANE COUNTY
DIVISION OF TRANSPORTATION

EXISTING DRAINAGE PLAN
DAUBERMAN ROAD EXTENSION

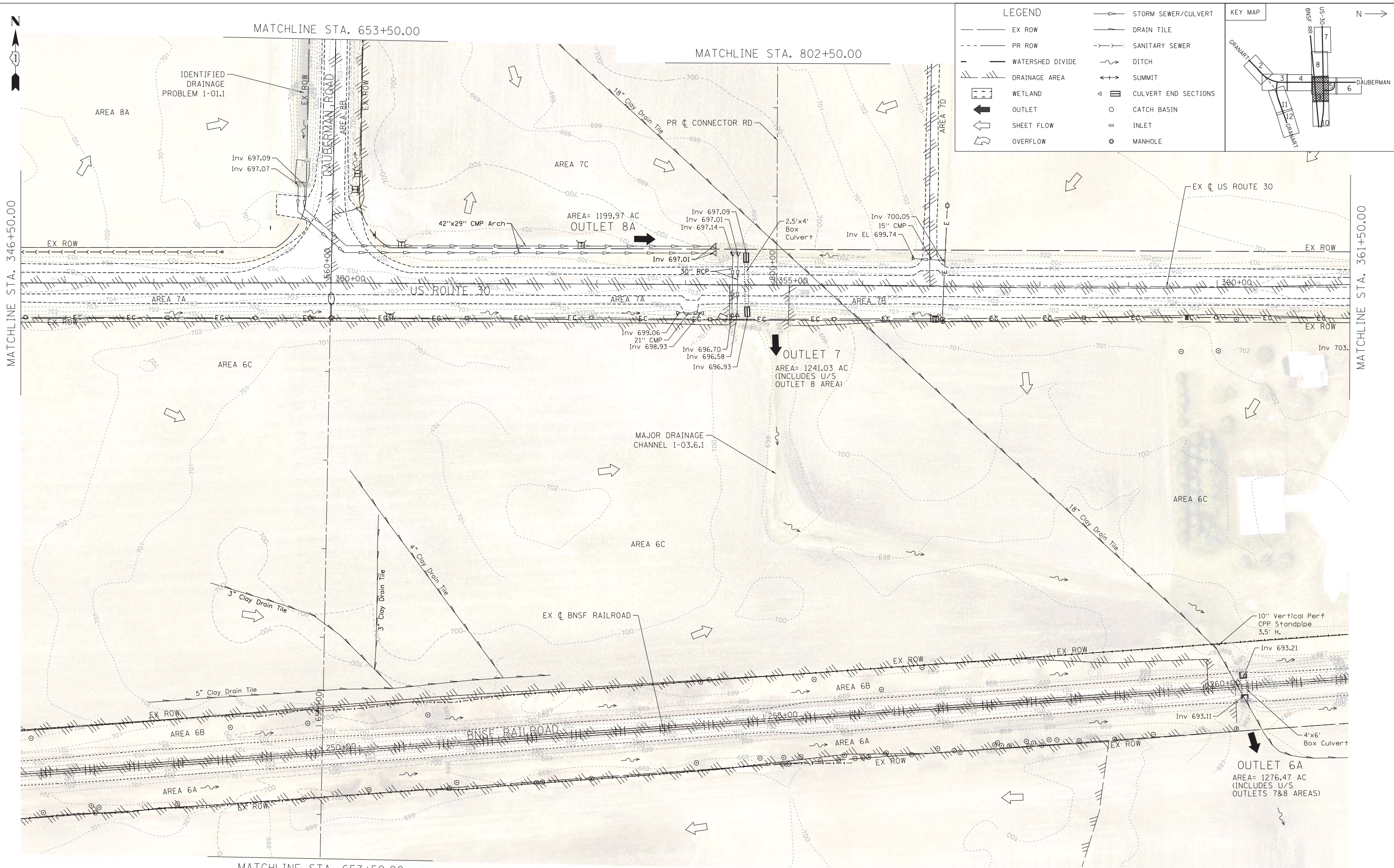
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		KANE	12	8
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

EXHIBIT 5



FILE NAME = Projects\CH401 - Chicago\40150086 - Kane County - Dauberman Road Extension - Hydraulics - Location Drainage Study\LD5.Sheets\LD5.EXISTING



B

L

Bollinger, Lach & Associates, Inc.

ITASCA, ILLINOIS

USER NAME = mkrzynski	DESIGNED - MK	REVISED - 10/13/2017
	DRAWN - MK	REVISED - 11/09/2017
PLOT SCALE = 1"=50'	CHECKED - KEK	REVISED - 03/09/2018
PLOT DATE = 11/28/2017	DATE - 02/01/2017	REVISED -

**KANE COUNTY
DIVISION OF TRANSPORTATION**

**EXISTING DRAINAGE PLAN
DAUBERMAN ROAD EXTENSION**

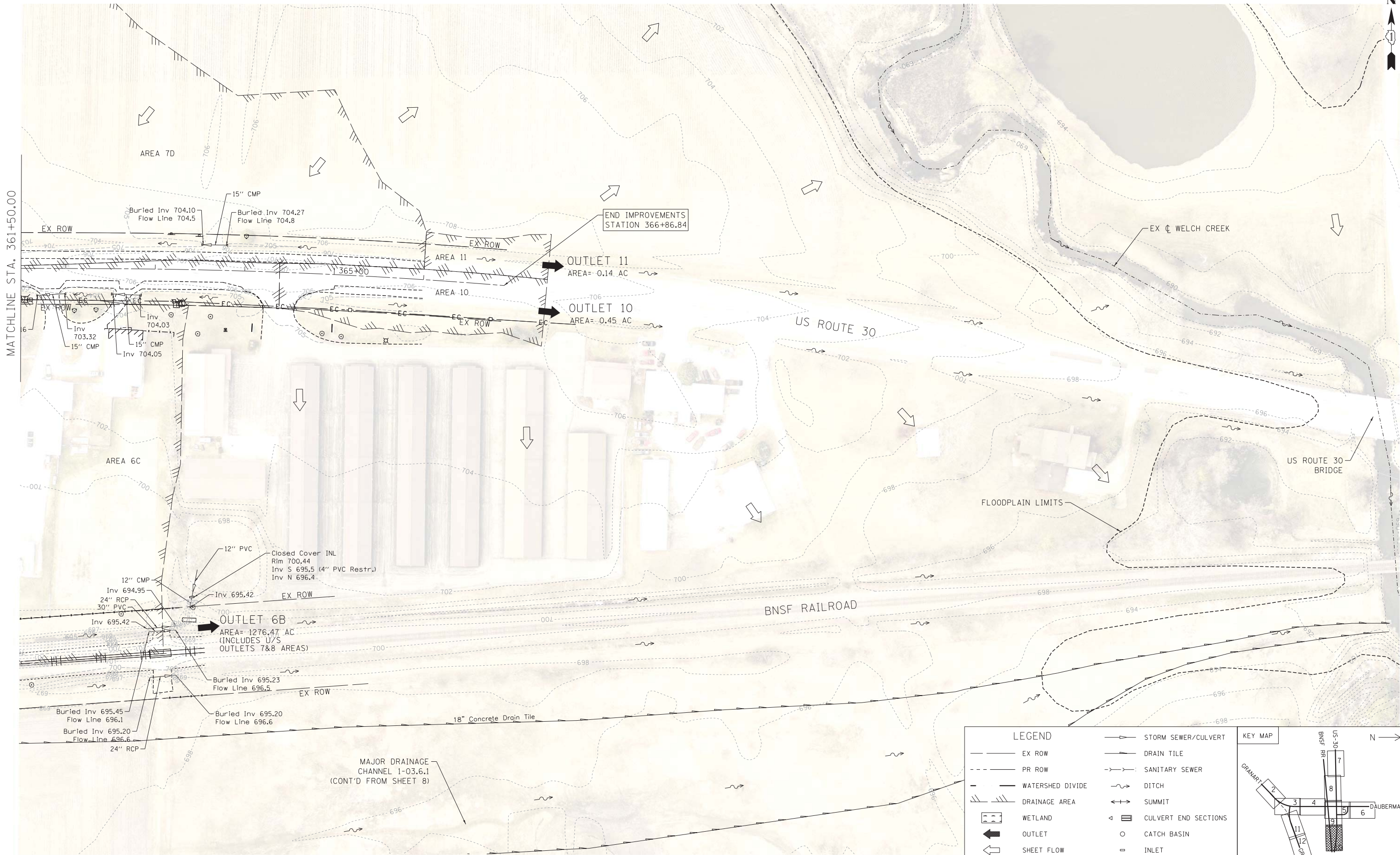
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F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		KANE	12	9
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

EXHIBIT 5

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MATCHLINE STA. 361+50.00

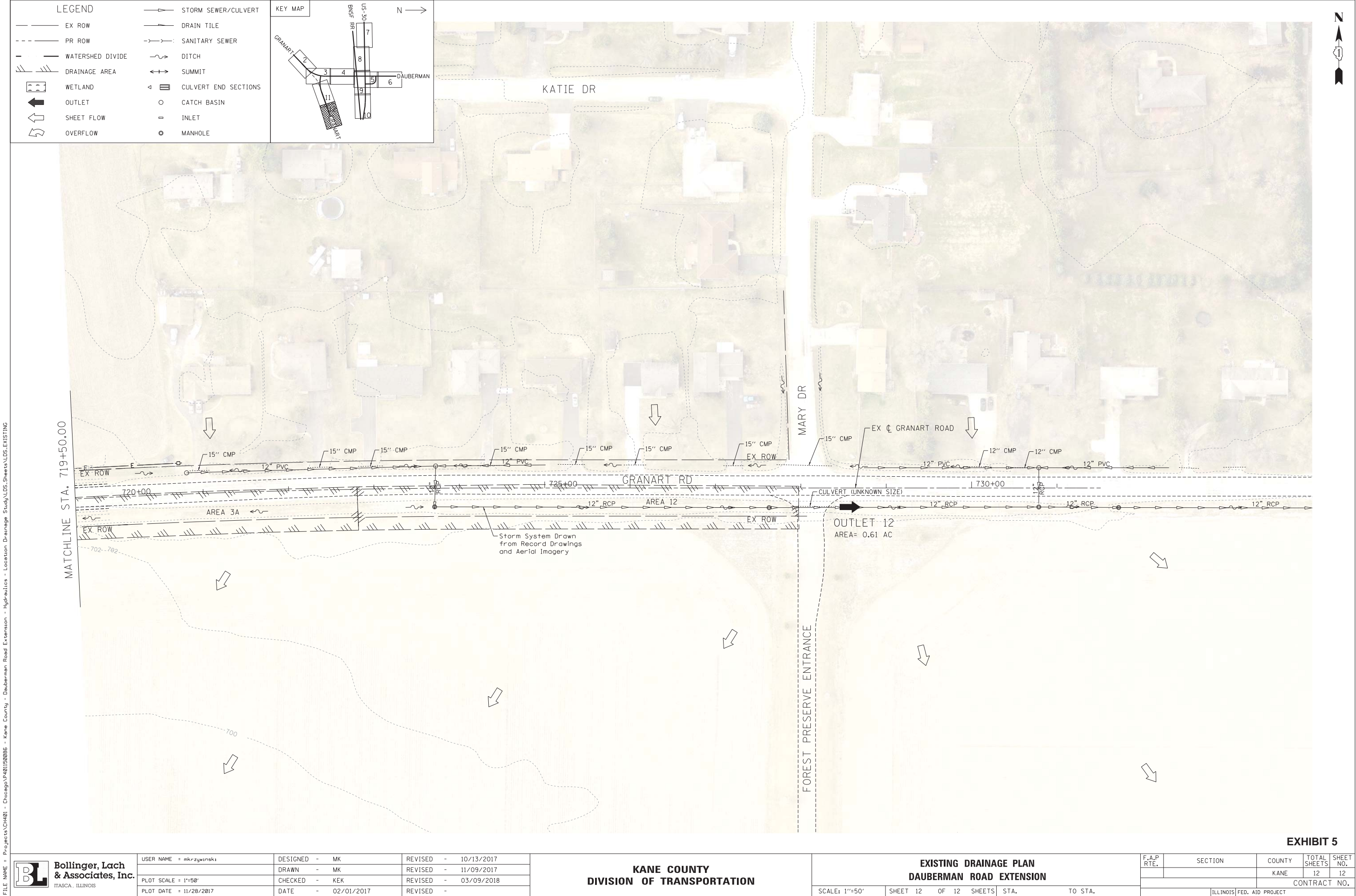


LEGEND

EX ROW	STORM SEWER/CULVERT
PR ROW	DRAIN TILE
WATERSHED DIVIDE	SANITARY SEWER
DRAINAGE AREA	DITCH
WETLAND	SUMMIT
OUTLET	CULVERT END SECTIONS
SHEET FLOW	CATCH BASIN
OVERFLOW	INLET
	MANHOLE

KEY MAP

EXHIBIT 5



FILE NAME = Projects\CH401 - Chicago\40150086 - Kane County - Dauberman Road Extension - Hydraulics - Location Drainage Study\05.Sheets\05.EXISTING

B

Bollinger, Lach & Associates, Inc.

ITASCA, ILLINOIS

USER NAME = mkrzynski

DESIGNED - MK

DRAWN - MK

PLOT SCALE = 1"=50'

PLOT DATE = 11/28/2017

DESIGNED - MK

DRAWN - MK

CHECKED - KEK

DATE - 02/01/2017

REVISED - 10/13/2017

REVISED - 11/09/2017

REVISED - 03/09/2018

REVISED -

KANE COUNTY

DIVISION OF TRANSPORTATION

EXISTING DRAINAGE PLAN

DAUBERMAN ROAD EXTENSION

SCALE: 1"=50'

SHEET 12 OF 12 SHEETS

STA. TO STA.

F.A.P. RTE.

SECTION

COUNTY

TOTAL SHEETS

SHEET NO.

KANE

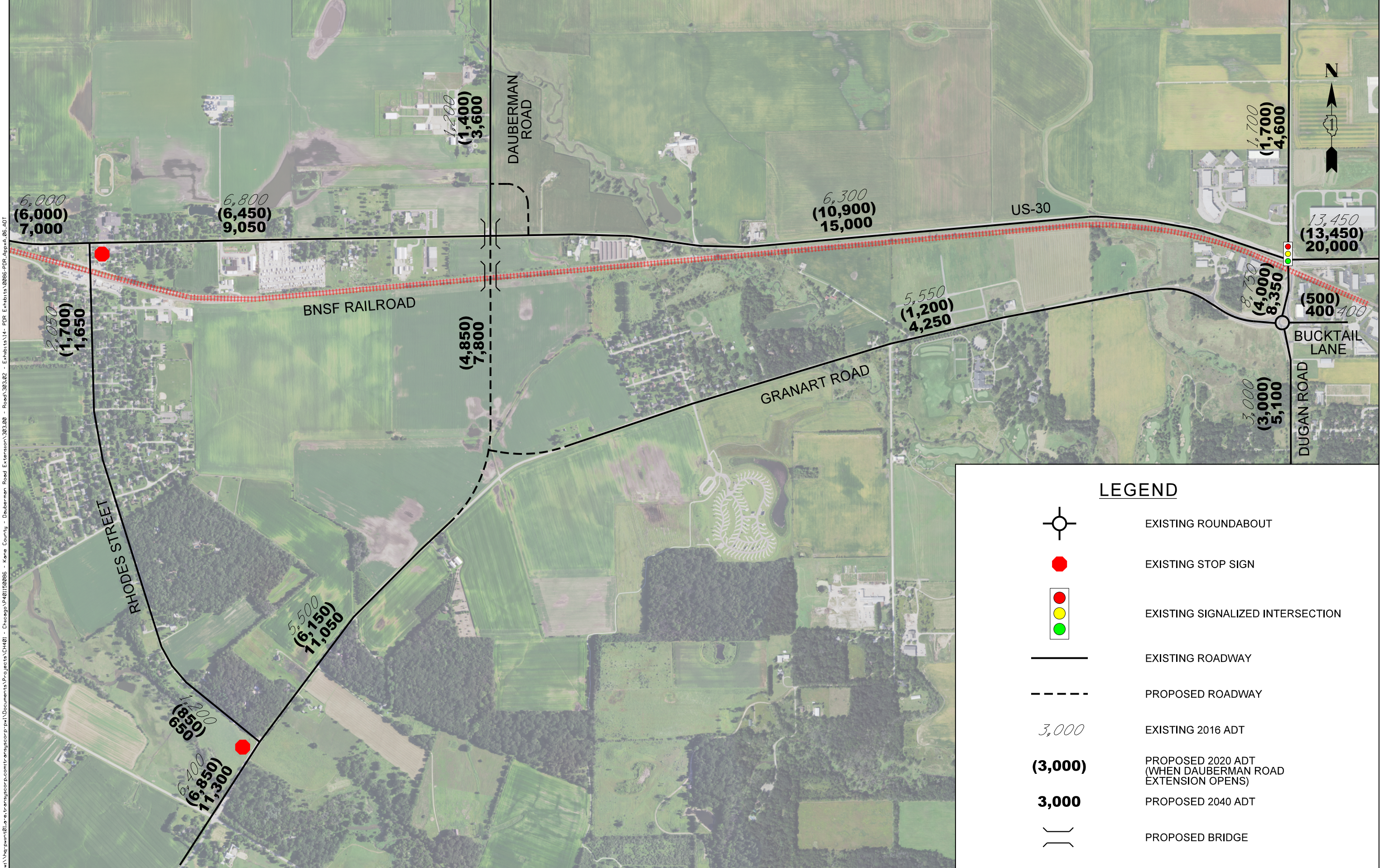
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12

ILLINOIS

FED. AID PROJECT

EXHIBIT 5



FILE NAME : p:\net\p101\ave-trans\scor-pcm\Documents\Projects\CH401 - Chicago\p40150086 - Kane County - Dauberman Road Extension\303.00 - Road\303.02 - Exhibits\0086-PDR-Appx4_06.ADT



USER NAME = mbmwhorter
PLOT SCALE = 1150.0000' / in.
PLOT DATE = 11/10/2017

DESIGNED -
DRAWN -
CHECKED -
DATE -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

AVERAGE DAILY TRAFFIC (ADT) MAP

F.A.S RTE.	SECTION	COUNTY	APPX	EXHIBIT	SHEET
1107	15-00277-01-BR	KANE	A	6	1 OF 1
Dauberman Road Extension Project Development Report Average Daily Traffic Map					

Crossing Information

DOT/AAR Number:	069734V
Status:	Open
Railroad:	BNSF Railway Company
Milepost:	50.23
ICC Line Code:	B
Crossing Type:	Public
If Private Crossing, Type:	Unk
County Name:	Kane
City Name:	BIG ROCK
In Or Near:	In
Street Name:	RHODES STREET
Highway Number:	TR35
Maximum Warning Devices:	Gates
Railroad Division:	CHICAGO
Railroad Subdivision:	AURORA
Number of Main Tracks:	2
Number of Other Tracks:	0
Crossing Surface Type:	Concrete
Average Number of Daily Trains:	32
Average Number of Daily Passenger Trains:	0
Passenger Train Type:	
Train Speed - Timetable:	60
Train Speed - Minimum Likely:	1
Train Speed - Maximum Likely:	60
AADT:	1050
Traffic Lanes:	2
Truck Percent:	6
Angle of Intersection - Road to Rail:	60° to 90°
FHWA Road Class:	Urban / ColMin
On State Highway:	
Pavement Markings:	Both-Line/Symbl
Advanced Warning Signs:	Yes
Road Intersection Near:	Yes
DOT Structure ID 1:	
FRA Quiet Zone:	1
Latitude:	41.76308600
Longitude:	-88.54728600
LAT LONG Source:	A
ENS Sign:	
EMERGENCY Phone Number:	8008325452
Posted Highway Speed:	30
Narrative Railroad:	
Narrative State 1:	NOE AS OF 1/4/2016
Data Last Updated:	05/09/2017

Pictures



(/railroad/images/o/069734V/069734v Hanson 1999 001.pdf)



(/railroad/images/o/069734V/069734V-1292004-1.jpg)



(/railroad/images/o/069734V/069734V-1292004-2.jpg)



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(/railroad/images/o/069734V/069734V.pdf)



(/railroad/images/o/069734V/069734v.tif)

Crossing Information

DOT/AAR Number:	069728S
Status:	Open
Railroad:	BNSF Railway Company
Milepost:	47.19
ICC Line Code:	B
Crossing Type:	Public
If Private Crossing, Type:	Unk
County Name:	Kane
City Name:	SUGAR GROVE
In Or Near:	In
Street Name:	DUGAN ROAD
Highway Number:	FAU2312
Maximum Warning Devices:	AFLS-Gates-Cant-over
Railroad Division:	CHICAGO
Railroad Subdivision:	AURORA
Number of Main Tracks:	1
Number of Other Tracks:	0
Crossing Surface Type:	Concrete
Average Number of Daily Trains:	32
Average Number of Daily Passenger Trains:	0
Passenger Train Type:	
Train Speed - Timetable:	60
Train Speed - Minimum Likely:	1
Train Speed - Maximum Likely:	60
AADT:	3000
Traffic Lanes:	3
Truck Percent:	4
Angle of Intersection - Road to Rail:	30° to 59°
FHWA Road Class:	Urban / ArtMin
On State Highway:	
Pavement Markings:	Both-Line/Symbl
Advanced Warning Signs:	Yes
Road Intersection Near:	Yes
DOT Structure ID 1:	
FRA Quiet Zone:	1
Latitude:	41.76335700
Longitude:	-88.48910600
LAT LONG Source:	A
ENS Sign:	
EMERGENCY Phone Number:	8008325452
Posted Highway Speed:	40
Narrative Railroad:	
Narrative State 1:	NOE AS OF 8/4/2008
Data Last Updated:	05/09/2017

Pictures



(/railroad/images /o/069728S/069728s Hanson 1999 001.pdf)



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7409 SW Tech Center Dr, Ste B150
Tigard, OR 97223
971-223-0003



Site Code: 14252101
Date: 3/15/2017
Rhodes & Train
Location: Crossing

www.qualitycounts.net

Appendix A - Exhibit 7 - Page 3/4

Time In (When RR Crossing Arms Lower)	Time Out (When Arms are Raised)	Number of Trains Passing	Total Time Train in Crossing	Approximate Train Length (ft)	Track (N or S)	Notes
12:02:48 AM	12:04:58 AM	1	0:02:10	6800	N	WB
12:32:18 AM	12:34:13 AM	1	0:01:55	5200	N	EB
1:33:47 AM	1:35:07 AM	1	0:01:20	2400	N	EB
5:22:04 AM	5:24:13 AM	1	0:02:09	5600	N	WB
5:50:56 AM	5:53:06 AM	1	0:02:10	7000	N	EB
8:26:45 AM	8:28:45 AM	1	0:02:00	5800	N	EB
9:58:37 AM	10:01:40 AM	1	0:03:03	7400	N	EB
10:16:40 AM	10:16:54 AM	0	0:00:14	N/A	N	WB; truck (hi-rail)
10:17:53 AM	10:18:08 AM	0	0:00:15	N/A	N	WB; truck (hi-rail)
11:10:00 AM	11:11:27 AM	1	0:01:27	4500	N	EB
12:49:45 PM	12:51:49 PM	1	0:02:04	6800	N	WB
2:08:28 PM	2:09:50 PM	1	0:01:22	3500	N	WB
2:25:08 PM	2:25:52 PM	1	0:00:44	200	N	EB
3:54:32 PM	3:56:28 PM	1	0:01:56	5800	N	WB
5:09:08 PM	5:10:57 PM	1	0:01:49	4200	N	EB
5:23:54 PM	5:25:34 PM	1	0:01:40	4200	N	EB
7:21:00 PM	7:22:22 PM	1	0:01:22	1350	N	WB
7:25:20 PM	7:46:36 PM	2	0:21:16	5500	S	EB train stopped to allow WB train through; 7:39:20 to 7:40:59
7:39:20 PM	7:40:58 PM	1	0:01:38	4000	N	WB
9:07:40 PM	9:10:06 PM	1	0:02:26	6200	N	EB
9:46:05 PM	10:02:33 PM	2	0:16:28	5500	S	EB train stopped to allow WB train through; 9:56:41 to 9:57:55
9:56:42 PM	9:57:56 PM	1	0:01:14	3000	N	WB
11:08:05 PM	11:08:54 PM	1	0:00:49	300	N	EB
11:19:20 PM	11:21:09 PM	1	0:01:49	4500	N	EB
Totals		24	1:13:20	110,750		

8407 Laurel Fair Cir
Tampa, FL 33610
954-944-2363

www.qualitycounts.net



Site Code: 13956701

Date: 12/6/2016

Location: Dugan Rd & US 30

Appendix A - Exhibit 7 - Page 4/4

Time In (When RR Crossing Arms Lower)	Time Out (When RR Crossing Arms Raise)	Number of Trains Passing	Total Time Train in Crossing	Approximate Train Length (ft)	Notes
01:23:18 AM	01:24:35 AM	1	0:01:17	3300	
01:56:54 AM	01:59:10 AM	1	0:02:16	5100	
02:16:49 AM	02:19:13 AM	1	0:02:24	5500	
02:53:21 AM	02:55:15 AM	1	0:01:54	5000	
03:21:34 AM	03:23:46 AM	1	0:02:12	6000	
03:32:49 AM	03:34:33 AM	1	0:01:44	5200	
04:11:12 AM	04:12:49 AM	1	0:01:37	5000	
06:48:13 AM	06:51:02 AM	1	0:02:49	6500	
07:24:40 AM	07:26:32 AM	1	0:01:52	5100	
09:50:31 AM	09:51:42 AM	1	0:01:11	2900	
12:17:45 PM	12:20:16 PM	1	0:02:31	6200	
12:46:03 PM	12:48:52 PM	1	0:02:49	6500	
01:25:20 PM	01:26:42 PM	1	0:01:22	2600	
03:02:40 PM	03:04:36 PM	1	0:01:56	4200	
03:16:41 PM	03:19:40 PM	1	0:02:59	6800	
04:00:32 PM	04:02:32 PM	1	0:02:00	5400	
04:19:59 PM	04:22:20 PM	1	0:02:21	5800	
04:33:34 PM	04:35:01 PM	1	0:01:27	3200	
05:04:58 PM	05:07:01 PM	1	0:02:03	5300	
05:18:06 PM	05:20:00 PM	1	0:01:54	5000	
06:33:25 PM	06:35:29 PM	1	0:02:04	5200	
07:41:31 PM	07:42:20 PM	1	0:00:49	2100	
09:32:35 PM	09:34:37 PM	1	0:02:02	5600	
09:51:20 PM	09:53:26 PM	1	0:02:06	5400	
11:25:32 PM	11:27:21 PM	1	0:01:49	4900	
Totals		25	0:49:28	123,800	



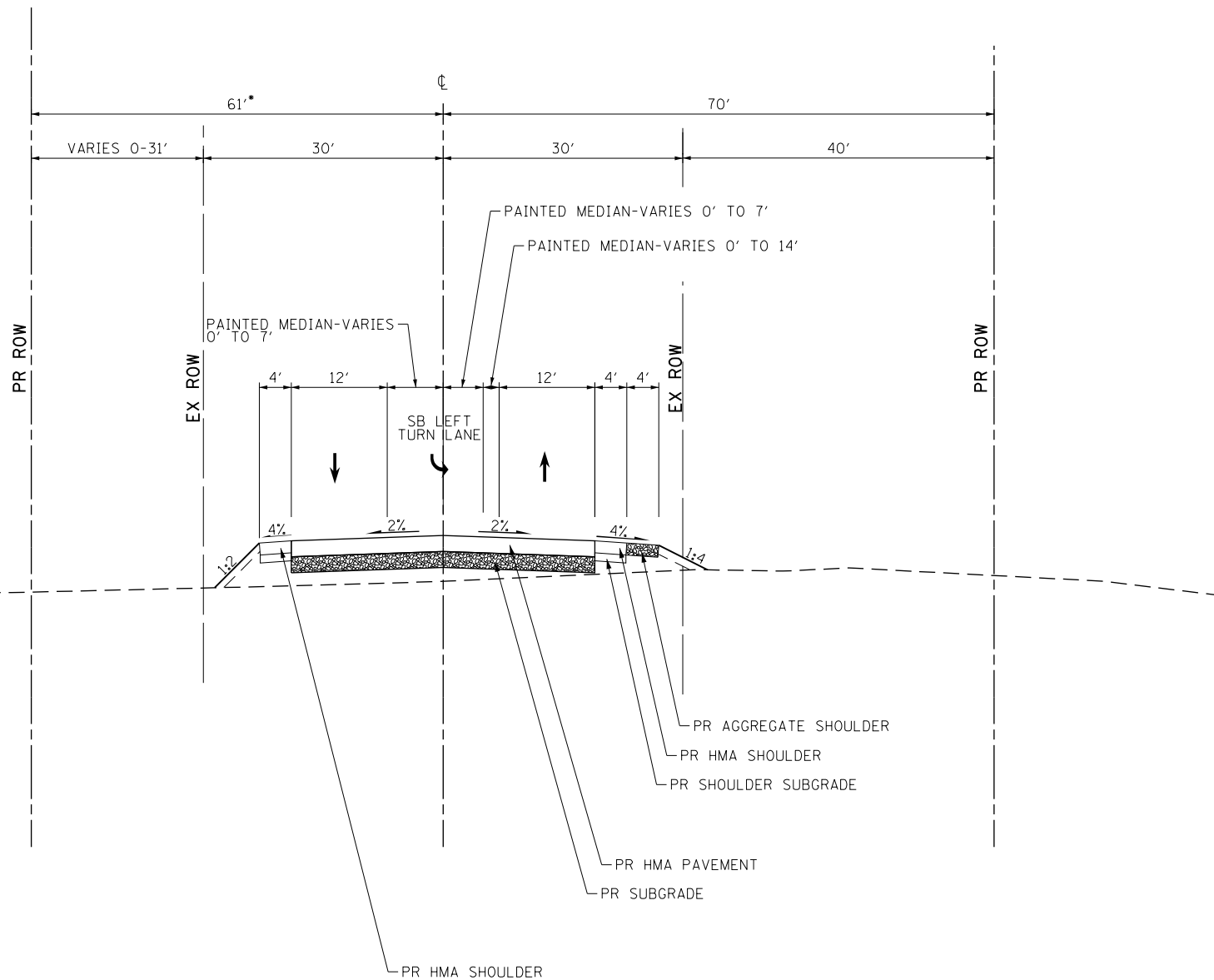
TranSystems

PLOT DATE	= 8/6/2018
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PROPOSED
TYPICAL SECTION

F.A.S RTE.	SECTION	COUNTY	APPX	EXHIBIT	SHEET
1107	15-00277-01-BR	KANE	A	8	2
Dauberman Road Extension Project Development Report					

FILE NAME = 0086-PDR_Appx4.0B_1.jpg Cross Sections



*PR ROW IS FROM STA. 667+50 TO 668+50, AND 670+50 TO 672+70.

DAUBERMAN ROAD
STA. 667+50 TO 672+70.



USER NAME = bjholm

PLOT SCALE = *SCALE*

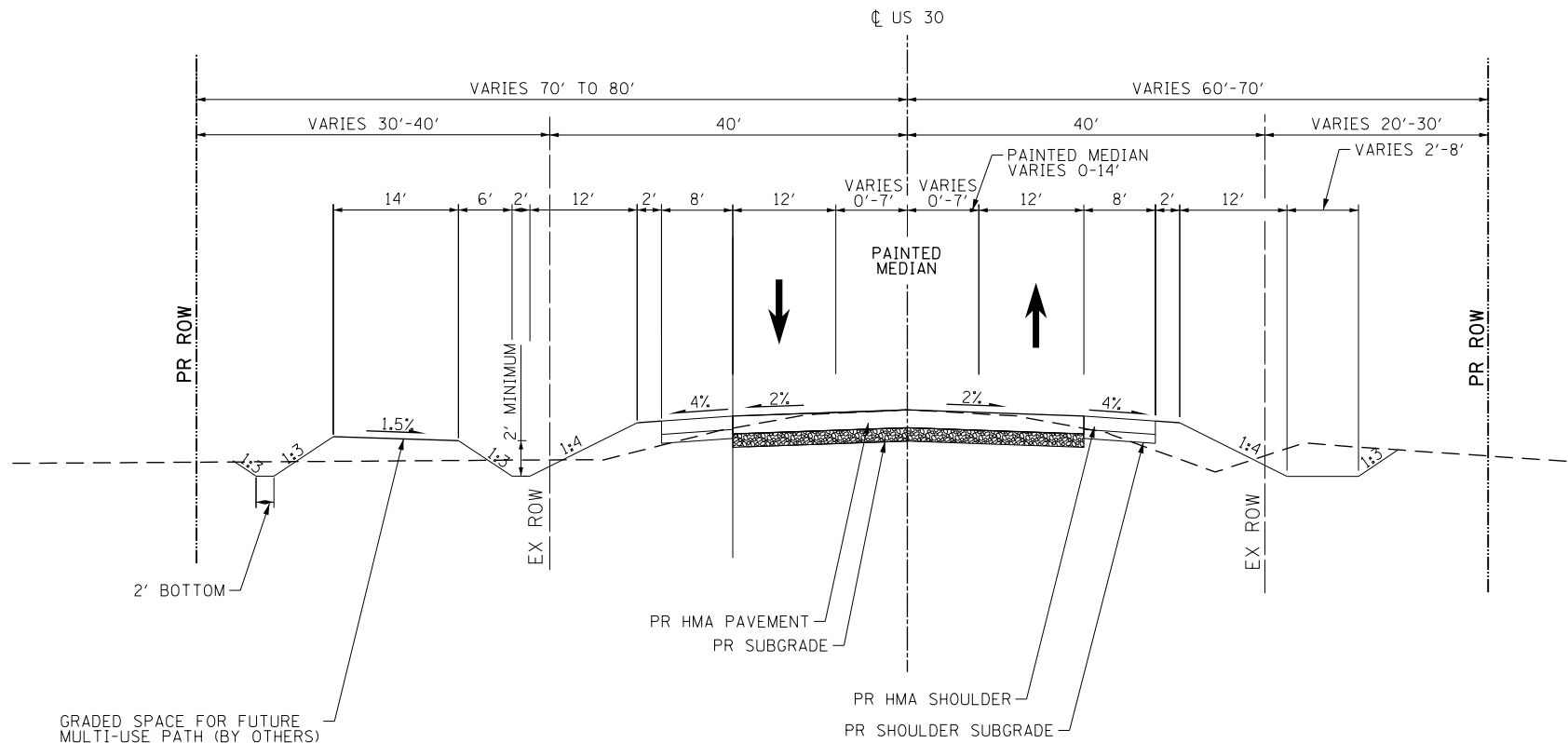
PLOT DATE = 8/2/2018

ILLINOIS DEPARTMENT
OF TRANSPORTATION

PROPOSED
TYPICAL SECTION

F.A.S RTE.	SECTION	COUNTY	APPX	EXHIBIT	SHEET
1107	15-00277-01-BR	KANE	A	8	3

Dauberman Road Extension Project Development Report



US ROUTE 30
STA. 340+00 TO 348+88.83

0-14' MEDIAN (VARIES)
342+38.50 TO 347+38.50
14' MEDIAN
347+38.50 TO 348+88.83

FILE NAME = 0086-PDR_Appx4.08_1.jpg Cross Sections

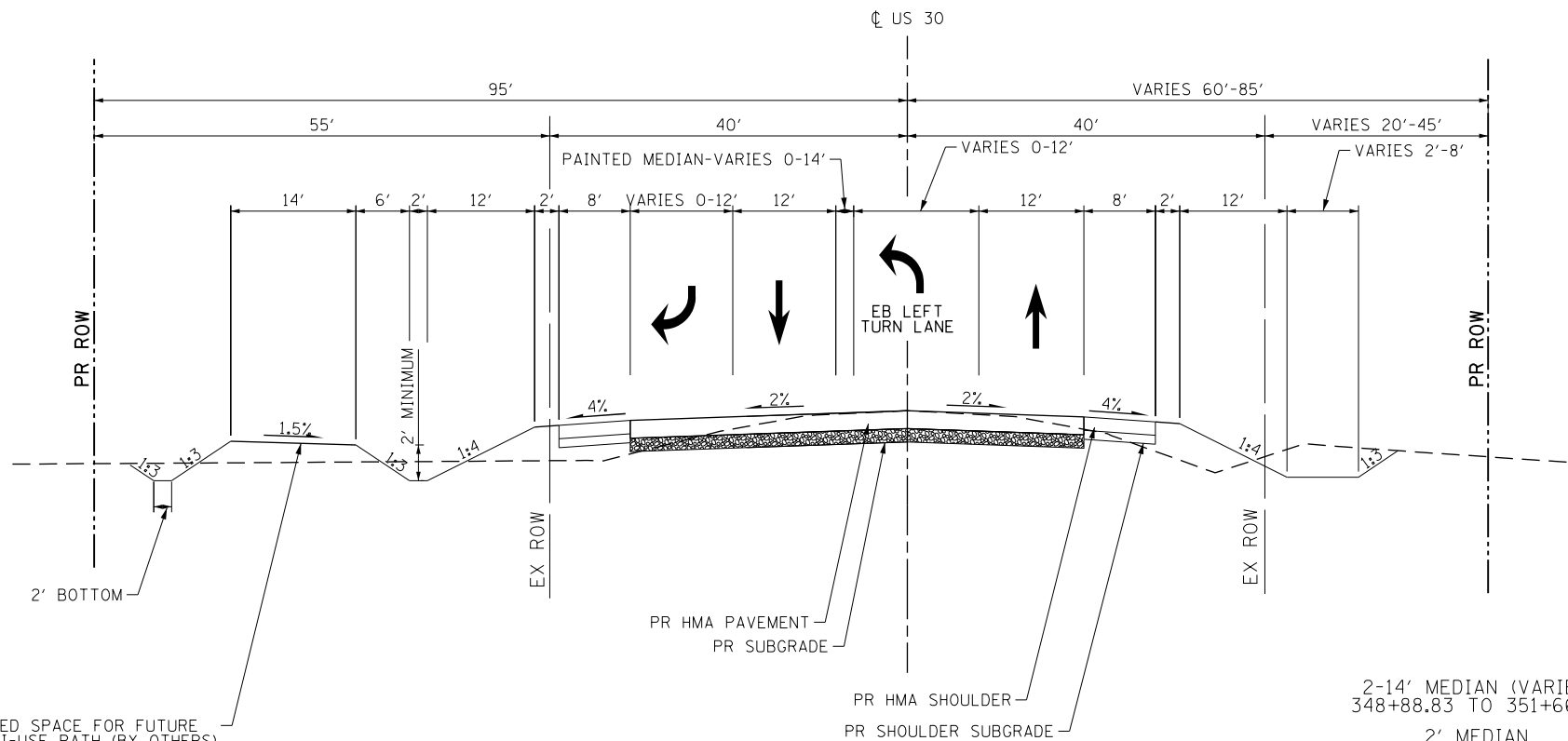


USER NAME = bjholman
PLOT SCALE = *SCALE*
PLOT DATE = 8/6/2018

ILLINOIS DEPARTMENT
OF TRANSPORTATION

PROPOSED
TYPICAL SECTION

F.A.S RTE.	SECTION	COUNTY	APPX	EXHIBIT	SHEET
1107	15-00277-01-BR	KANE	A	8	4
Dauberman Road Extension Project Development Report					



2-14' MEDIAN (VARIES)
348+88.83 TO 351+66.83
2' MEDIAN
351+68.83 TO 354+48.83
LEFT TURN LANE
351+68.83 TO 354+48.83
RIGHT TURN LANE
355+66.84 TO 358+31.84
14' MEDIAN
355+66.84 TO 360+96.84

US ROUTE 30
STA. 348+88.83 TO 360+96.84

ILLINOIS DEPARTMENT
OF TRANSPORTATION

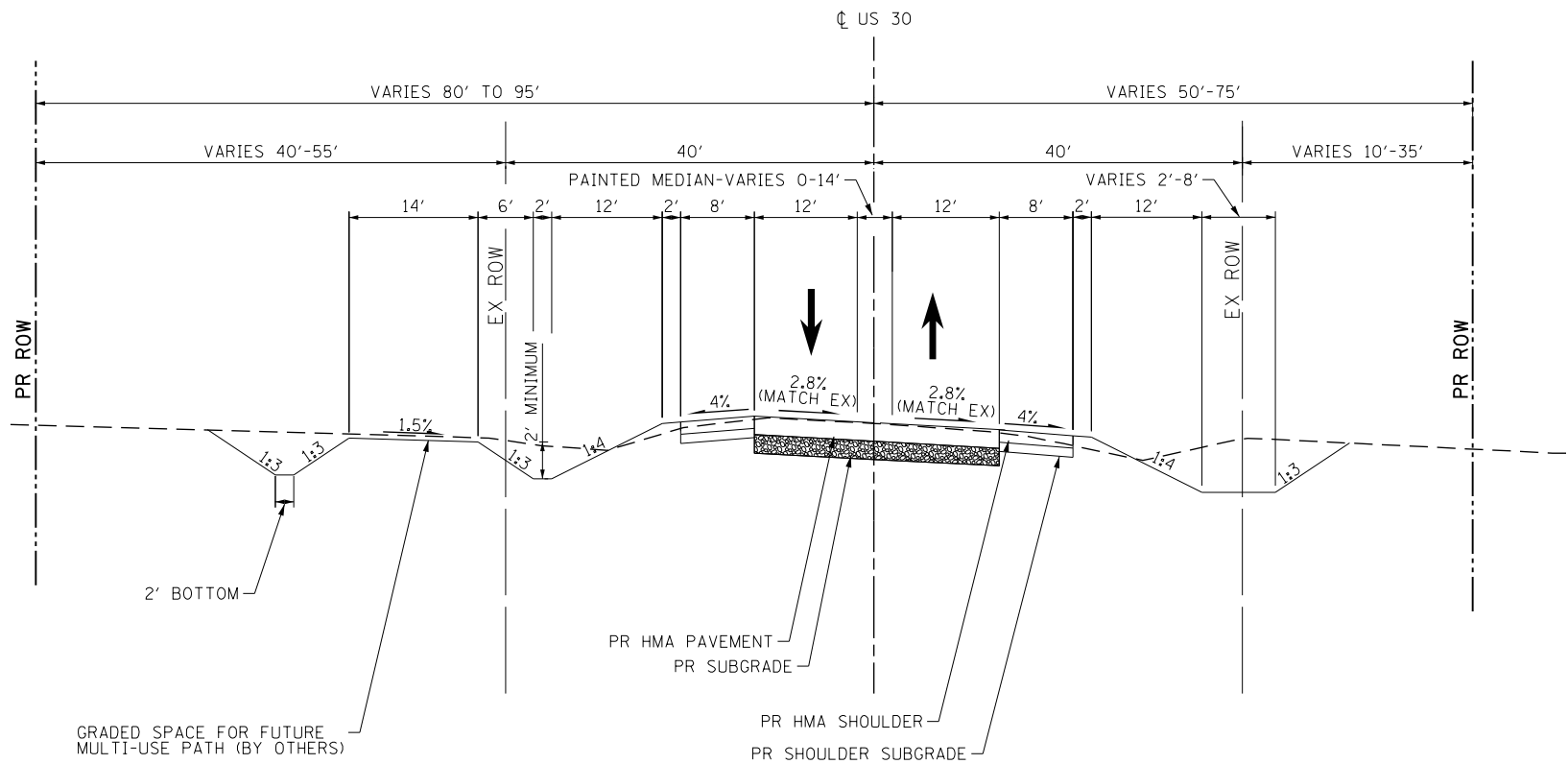
PROPOSED
TYPICAL SECTION

F.A.S RTE.	SECTION	COUNTY	APPX	EXHIBIT	SHEET
1107	15-00277-01-BR	KANE	A	8	5
Dauberman Road Extension Project Development Report					

USER NAME = bjholman
PLOT SCALE = *SCALE*
PLOT DATE = 8/2/2018



FILE NAME = 0086-PDR_Appx4.08_1typ_Cross Sections



US ROUTE 30
LOOKING EAST THROUGH EXISTING CURVE
SUPERELEVATION AT EAST LIMIT OF
PROJECT

STA. 360+96.84 TO 366+86.84

0-14' MEDIAN (VARIES)
360+96.84 TO 365+91.88



USER NAME = b.j.holman
PLOT SCALE = *SCALE*
PLOT DATE = 8/2/2018

ILLINOIS DEPARTMENT
OF TRANSPORTATION

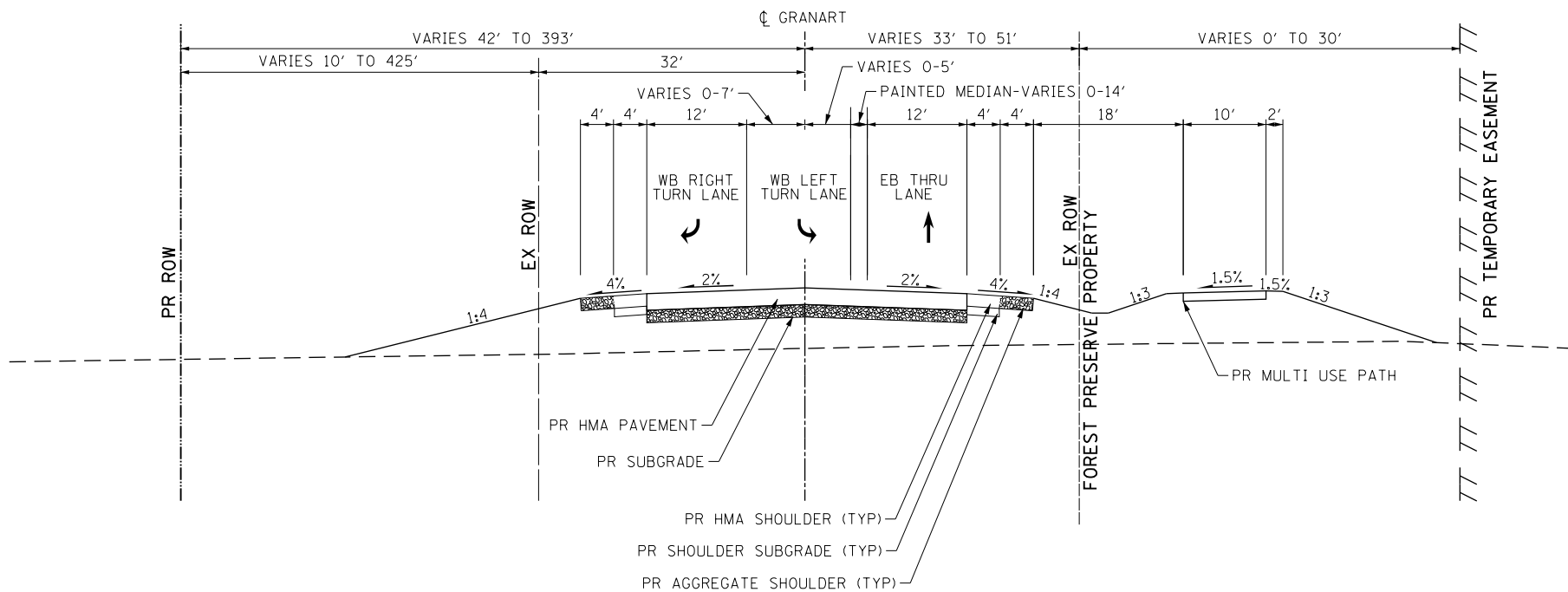
PROPOSED
TYPICAL SECTION

F.A.S RTE.	SECTION	COUNTY	APPX	EXHIBIT	SHEET
1107	15-00277-01-BR	KANE	A	8	6
Dauberman Road Extension Project Development Report					



F.A.S RTE.	SECTION	COUNTY	APPX	EXHIBIT	SHEET
1107	15-00277-01-BR	KANE	A	8	7
Dauberman Road Extension Project Development Report					

FILE NAME = 0086-PDR_Appx4.08_1.jp. Cross Sections



GRANART ROAD
STA. 707+90 TO STA. 711+70



USER NAME = bjholman

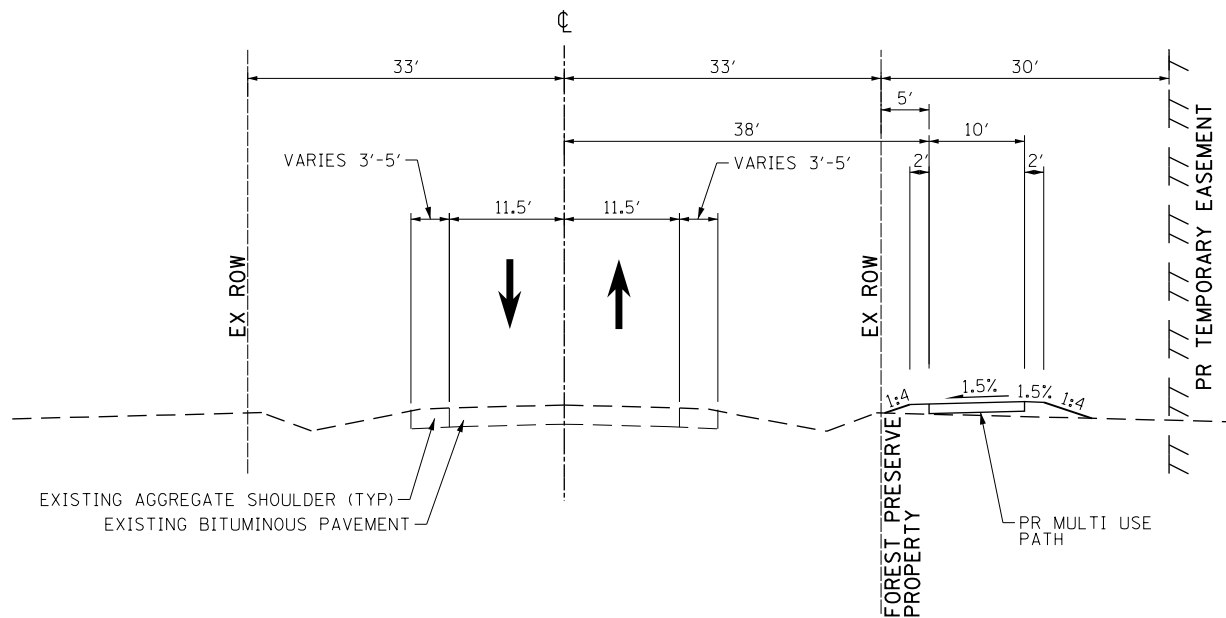
PLOT SCALE = *SCALE*

PLOT DATE = 8/6/2018

ILLINOIS DEPARTMENT
OF TRANSPORTATION

PROPOSED
TYPICAL SECTION

F.A.S RTE.	SECTION	COUNTY	APPX	EXHIBIT	SHEET
1107	15-00277-01-BR	KANE	A	8	8
Dauberman Road Extension Project Development Report					



GRANART ROAD-MULTI USE PATH
STA. 711+70 TO 727+93



USER NAME = bjholman
PLOT SCALE = *SCALE*
PLOT DATE = 8/6/2018

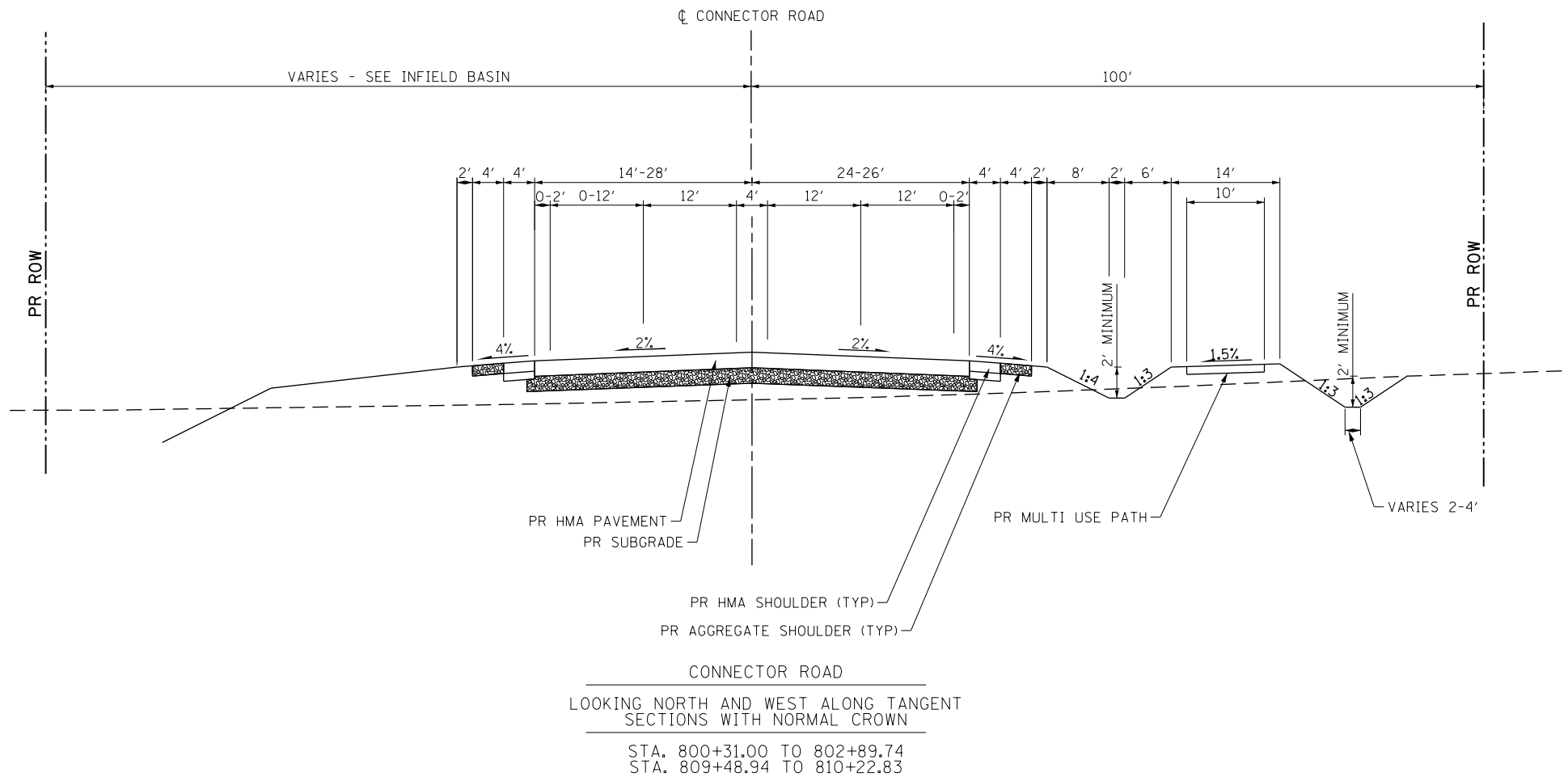
ILLINOIS DEPARTMENT
OF TRANSPORTATION

PROPOSED
TYPICAL SECTION

F.A.S RTE.	SECTION	COUNTY	APPX	EXHIBIT	SHEET
1107	15-00277-01-BR	KANE	A	8	9

Dauberman Road Extension Project Development Report

FILE NAME = 0086-PDR_Appx4.08_1.jpg, Cross Sections



USER NAME = bjholman

PLOT SCALE = *SCALE*

PLOT DATE = 8/2/2018

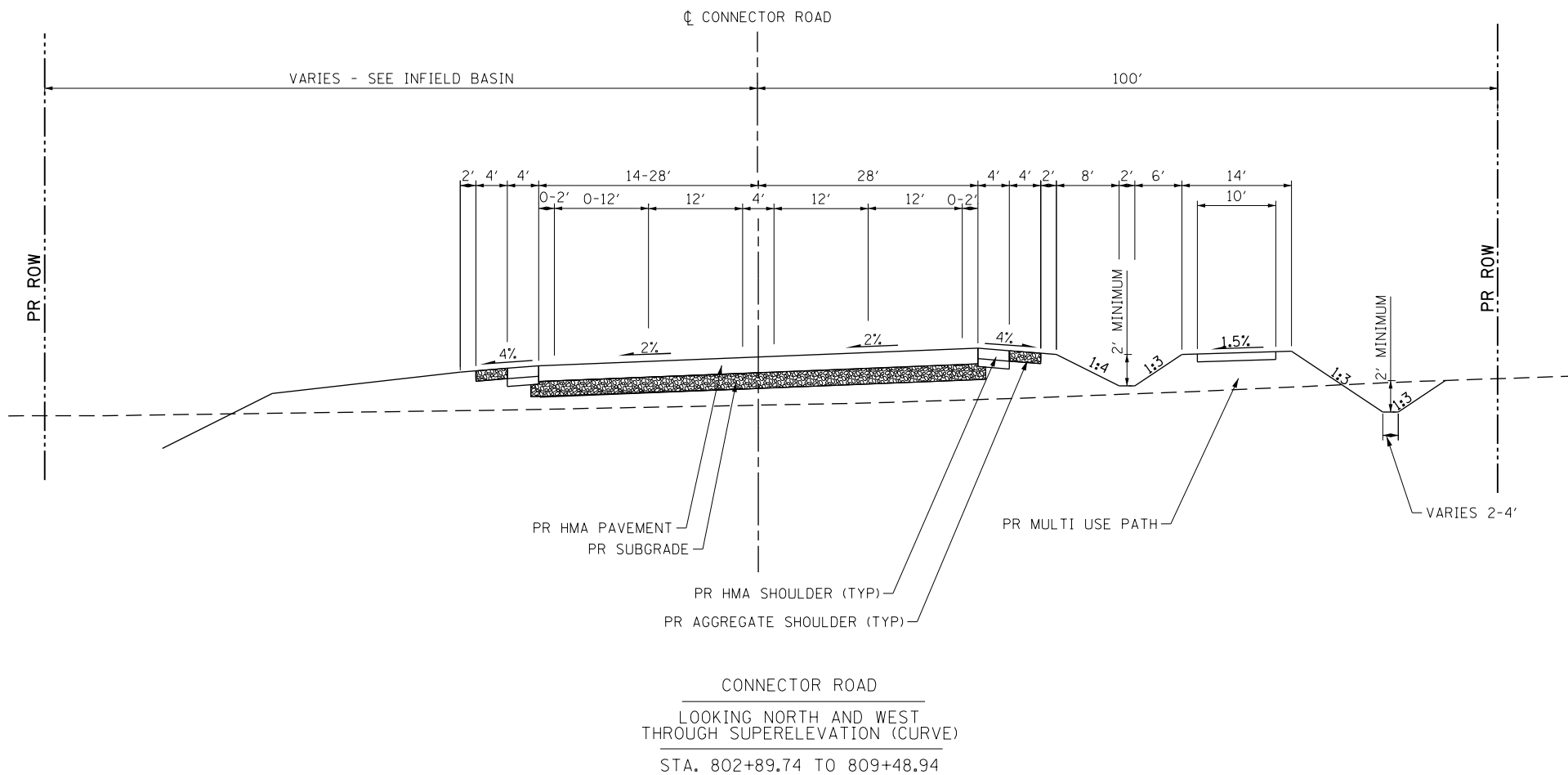
ILLINOIS DEPARTMENT
OF TRANSPORTATION

PROPOSED
TYPICAL SECTION

F.A.S RTE.	SECTION	COUNTY	APPX	EXHIBIT	SHEET
1107	15-00277-01-BR	KANE	A	8	10

Dauberman Road Extension Project Development Report

FILE NAME = 0086-PDR_Appx4.08_1.jpg, Cross Sections



USER NAME = bjholman

PLOT SCALE = *SCALE*

PLOT DATE = 8/2/2018

ILLINOIS DEPARTMENT
OF TRANSPORTATION

PROPOSED
TYPICAL SECTION

F.A.S RTE.	SECTION	COUNTY	APPX	EXHIBIT	SHEET
1107	15-00277-01-BR	KANE	A	8	11

Dauberman Road Extension Project Development Report

PROFILE	SURVEYED _____	BY _____	DATE _____
	PLOTTED _____		
NOTE BOOK	GRADES CHECKED _____		
	B.M. NOTED _____		
NO. _____	STRUCTURE NOTATIONS CH'KD _____		



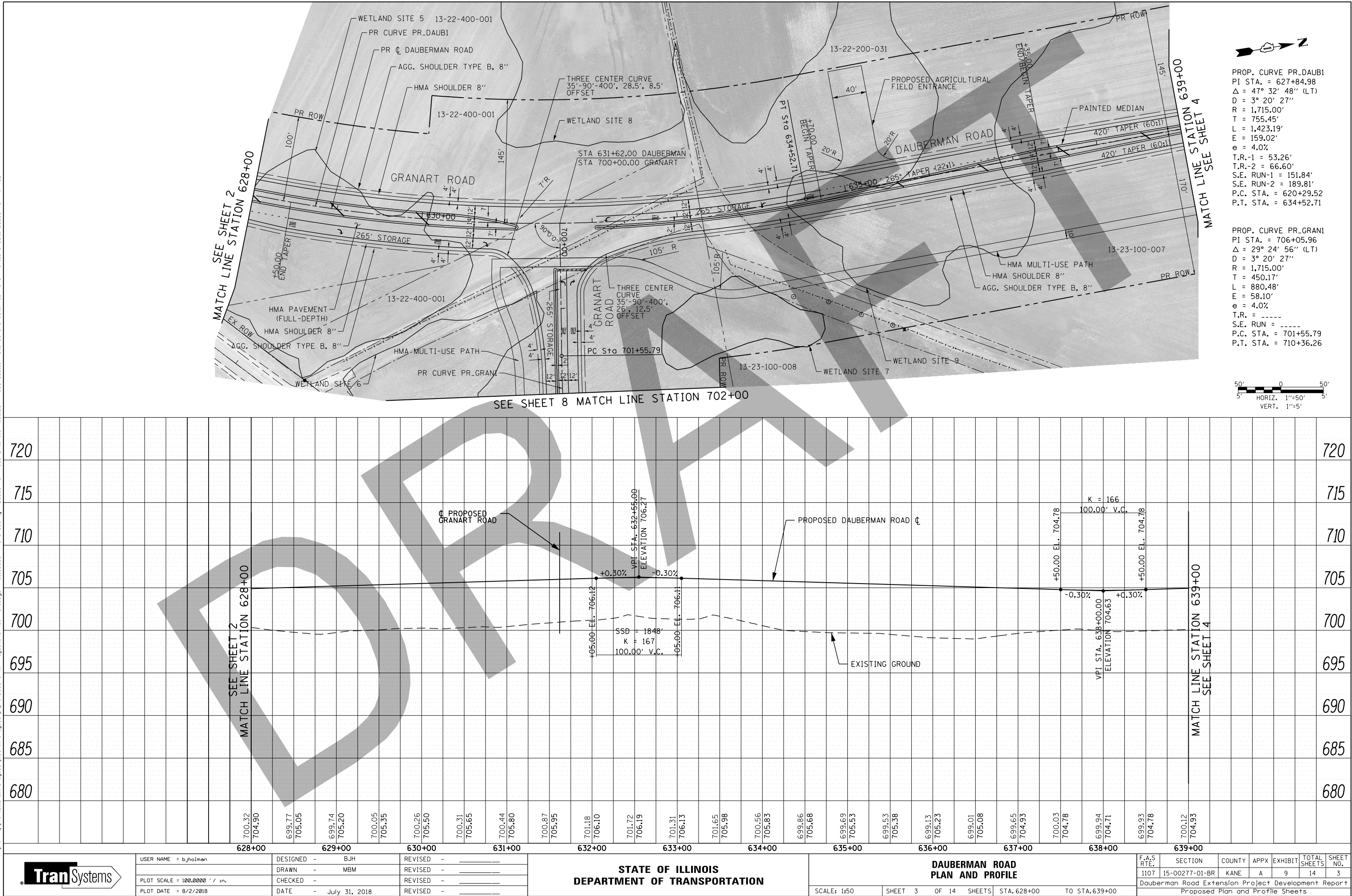
TranSystems

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

F.A.S RTE.	SECTION	COUNTY	APPX	EXHIBIT	TOTAL SHEETS	SHEET NO.
1107	15-00277-01-BR	KANE	A	9	14	1
Dauberman Road Extension Project Development Report Proposed Plan and Profile Sheets						

PROFILE	SURVEYED _____	BY _____	DATE _____
	PLOTTED _____		
NOTE BOOK _____	GRADES CHECKED _____		
	B.M. NOTED _____		
NO. _____	STRUCTURE NOTAT'NS CHKD _____		

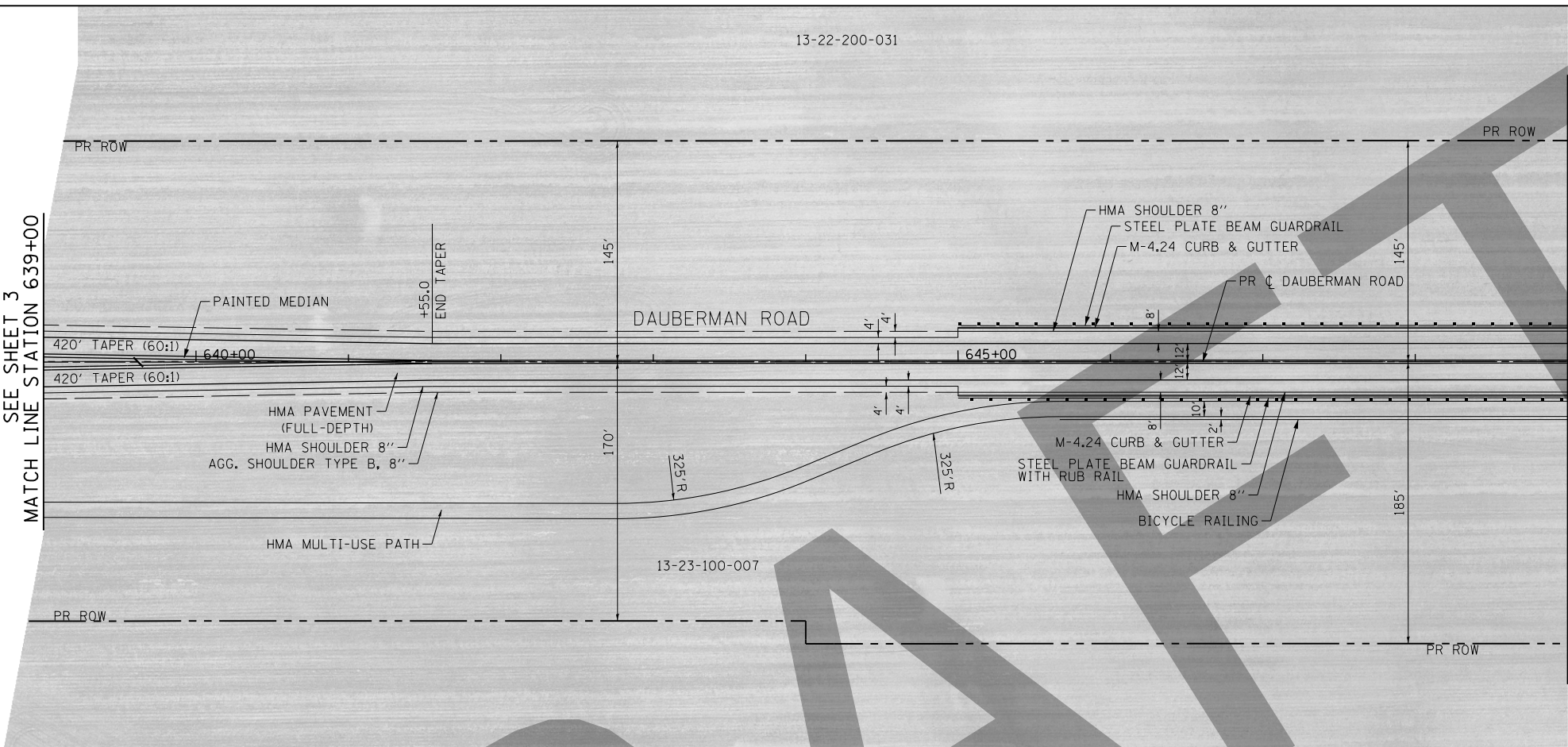
FILE NAME =



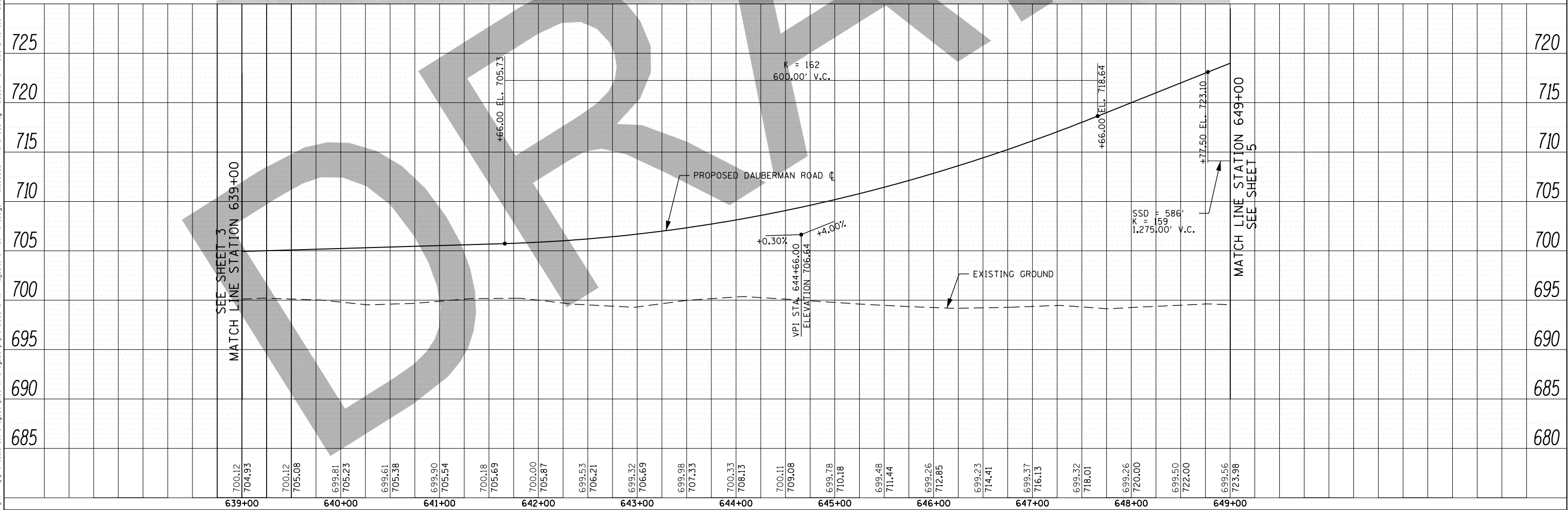
PLAN	BY		DATE
NOTE BOOK	SURVEYED		
NO. _____	PLOTTED		
	ALIGNMENT CHECKED		
	RT. OF WAY CHECKED		
	CADD FILE NAME		

PROFILE	SURVEYED _____	BY _____	DATE _____
	PLOTTED _____		
NOTE BOOK _____	GRADES CHECKED _____		
N.O. _____	B.M. NOTED _____		
	STRUCTURE NOTAT'NS CHK'D _____		

SEE SHEET 3
MATCH LINE STATION 639+00



MATCH LINE STATION 649+00
SEE SHEET 5



MATCH LINE	STATION 649+00
SEE SHEET 5	

USER NAME = bjholman	DESIGNED - BJH	REVISED - _____
	DRAWN - MBM	REVISED - _____
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - _____
PLOT DATE = 8/2/2018	DATE - July 31, 2018	REVISED - _____

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

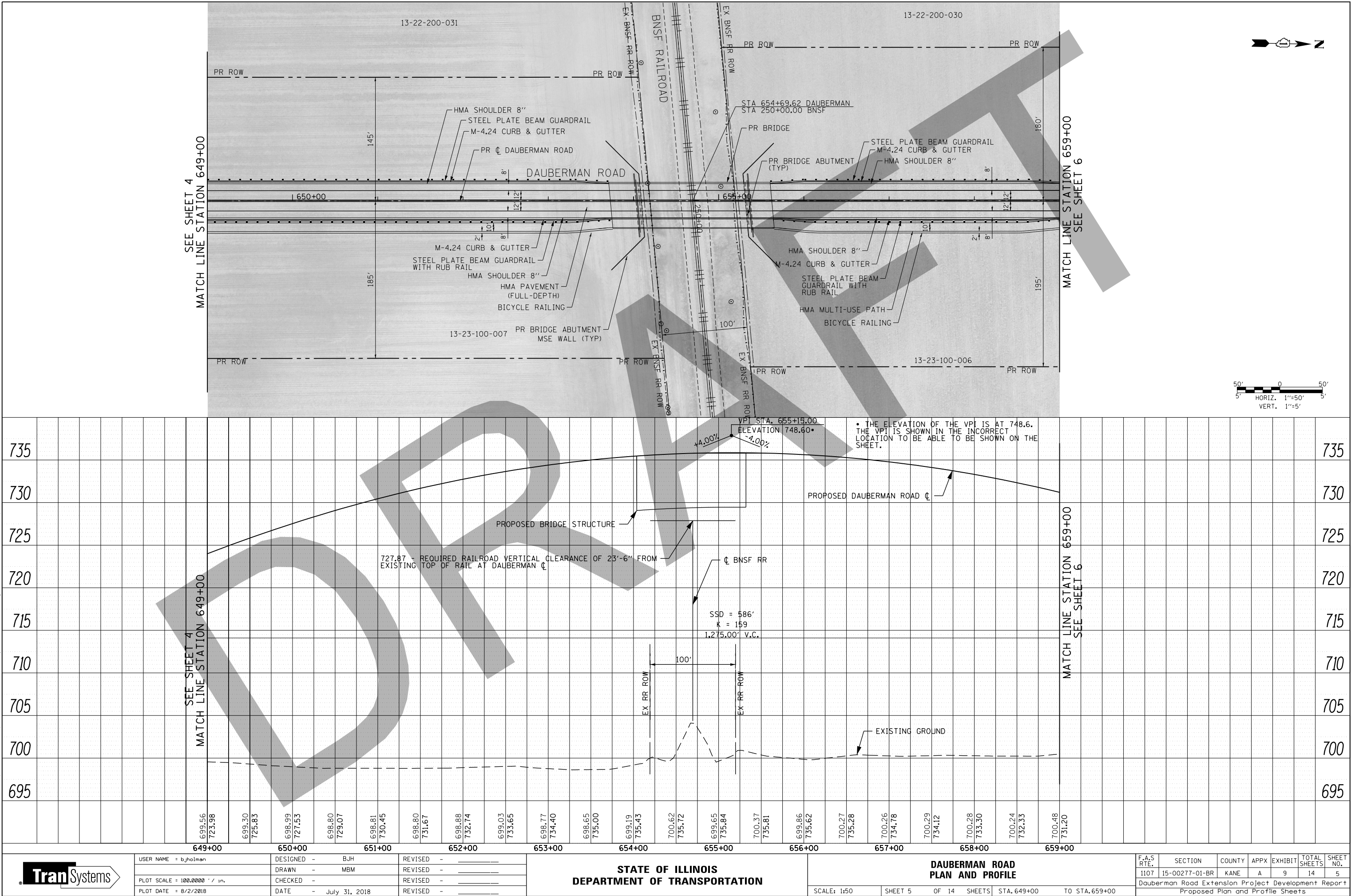
DAUBERMAN ROAD PLAN AND PROFILE

SCALE: 1:50	SHEET 4 OF 14 SHEETS	STA. 639+00 TO STA. 649+00
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F.A.S RTE.	SECTION	COUNTY	APPX	EXHIBIT	TOTAL SHEET S	SHEET NO.
1107	15-00277-01-BR	KANE	A	9	14	4
Dauberman Road Extension Project Development Report						
Proposed Plan and Profile Sheets						

PROFILE	SURVEYED _____	BY _____	DATE _____
	PLOTTED _____		
NOTE BOOK _____	GRADES CHECKED _____		
	B.M. NOTED _____		
NO. _____	STRUCTURE NOTAT'NS CHKD _____		

FILE NAME =



PROFILE	SURVEYED _____	BY _____	DATE _____
	PLOTTED _____		
NOTE BOOK	GRADES CHECKED _____		
	B.M. NOTED _____		
NO. _____	STRUCTURE NOTATIONS CH'KD _____		



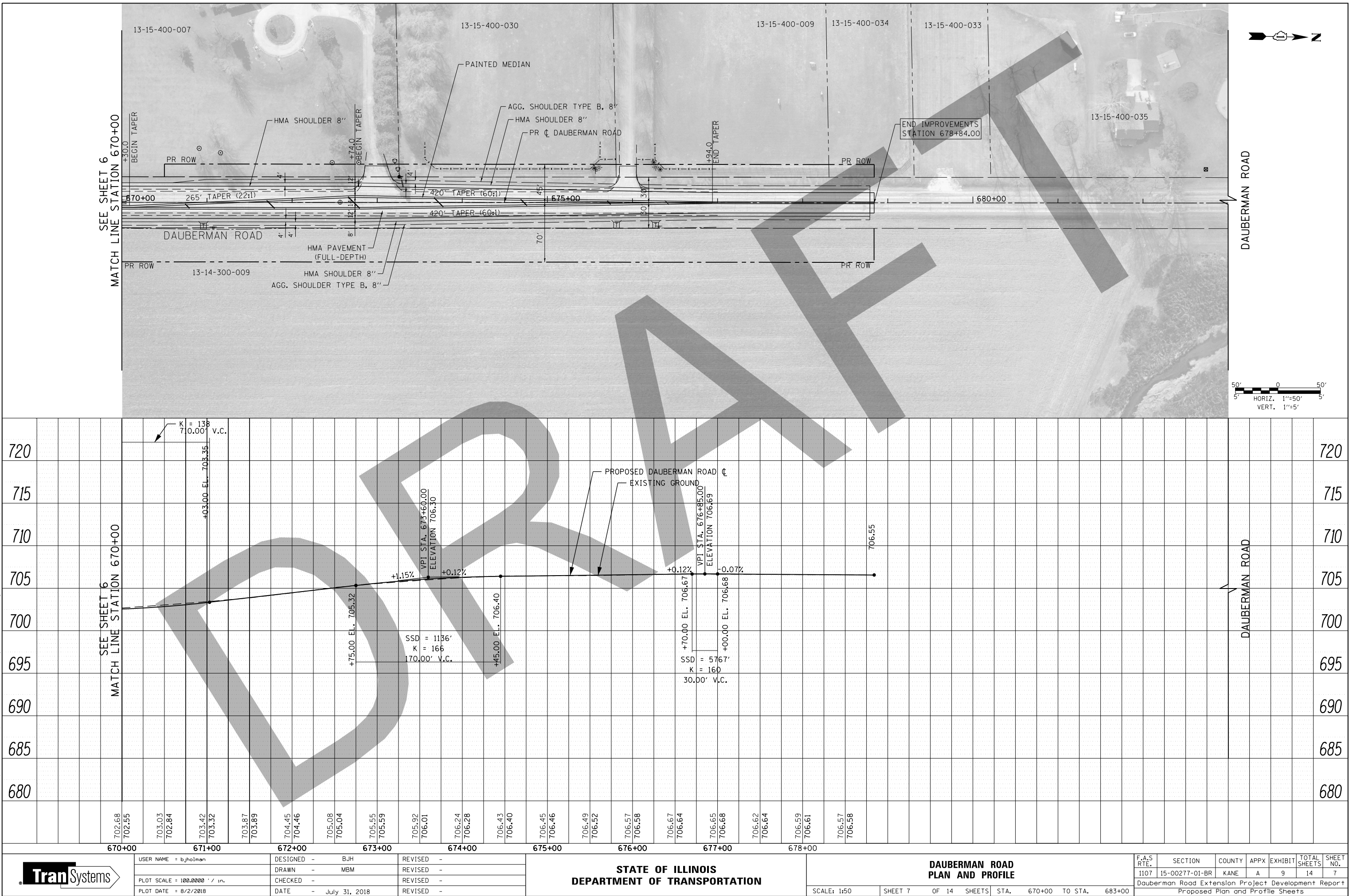
TranSystems

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.S RTE.	SECTION	COUNTY	APPX	EXHIBIT	TOTAL SHEETS	SHEET NO.
1107	15-00277-01-BR	KANE	A	9	14	6
Dauberman Road Extension Project Development Report						
Proposed Plan and Profile Sheets						

PROFILE	SURVEYED _____	BY _____	DATE _____
	PLOTTED _____		
NOTE BOOK _____	GRADES CHECKED _____		
	B.M. NOTED _____		
NO. _____	STRUCTURE NOTAT'NS CHKD _____		

FILE NAME =

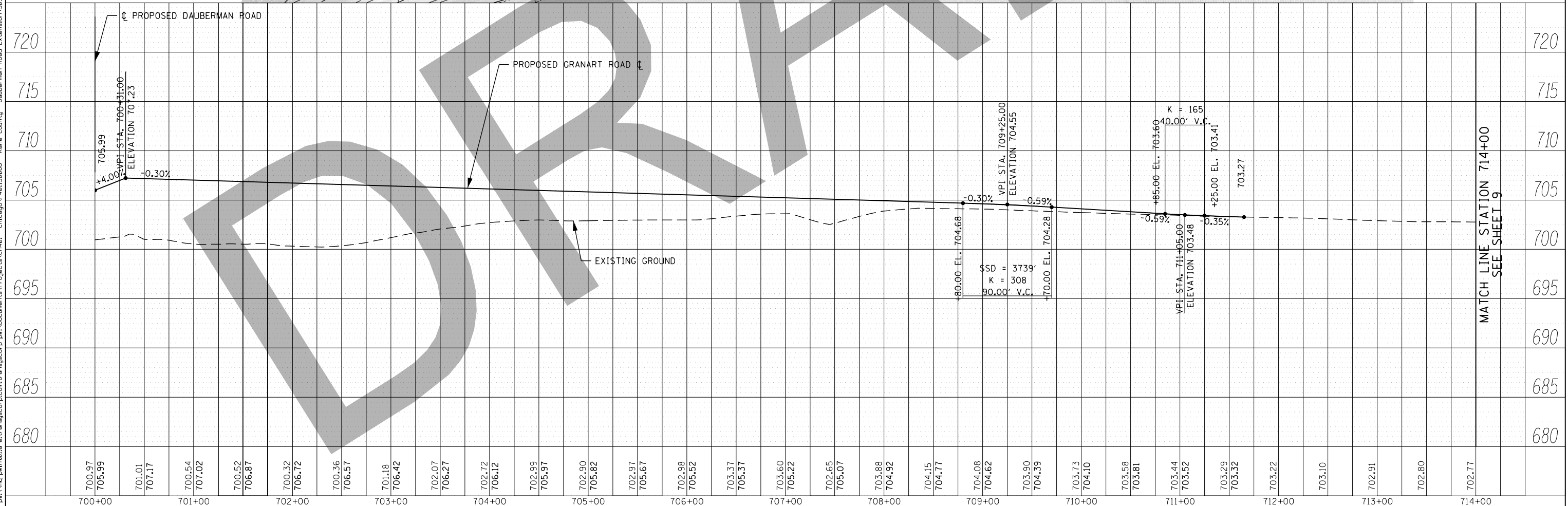
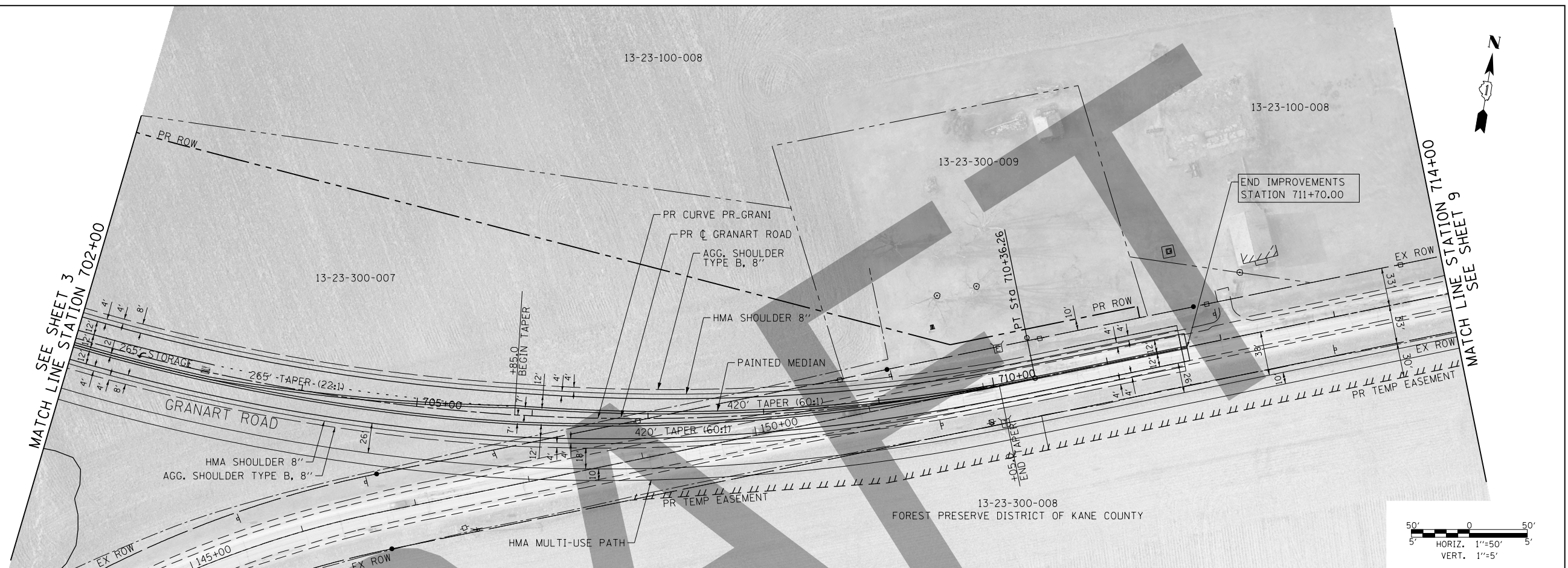


PLAN	SURVEYED _____		BY _____	DATE _____
	NOTE BOOK NO. _____	PLOTTED _____ ALIGNMENT CHECKED _____ RT. OF WAY CHECKED _____ CADD FILE NAME _____		

```

PROP. CURVE PR.GRANI
PI STA. = 706+05.96
Δ = 29° 24' 56" (LT)
D = 3° 20' 27"
R = 1,715.00'
T = 450.17'
L = 880.48'
E = 58.10'
e = 4.0%
T.R. = ----
S.E. RUN = ----
P.C. STA. = 701+55.79
P.T. STA. = 710+36.26

```



USER NAME = bjholmam	DESIGNED - BJH	REVISED -
	DRAWN - MBM	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 8/2/2018	DATE - July 31, 2018	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

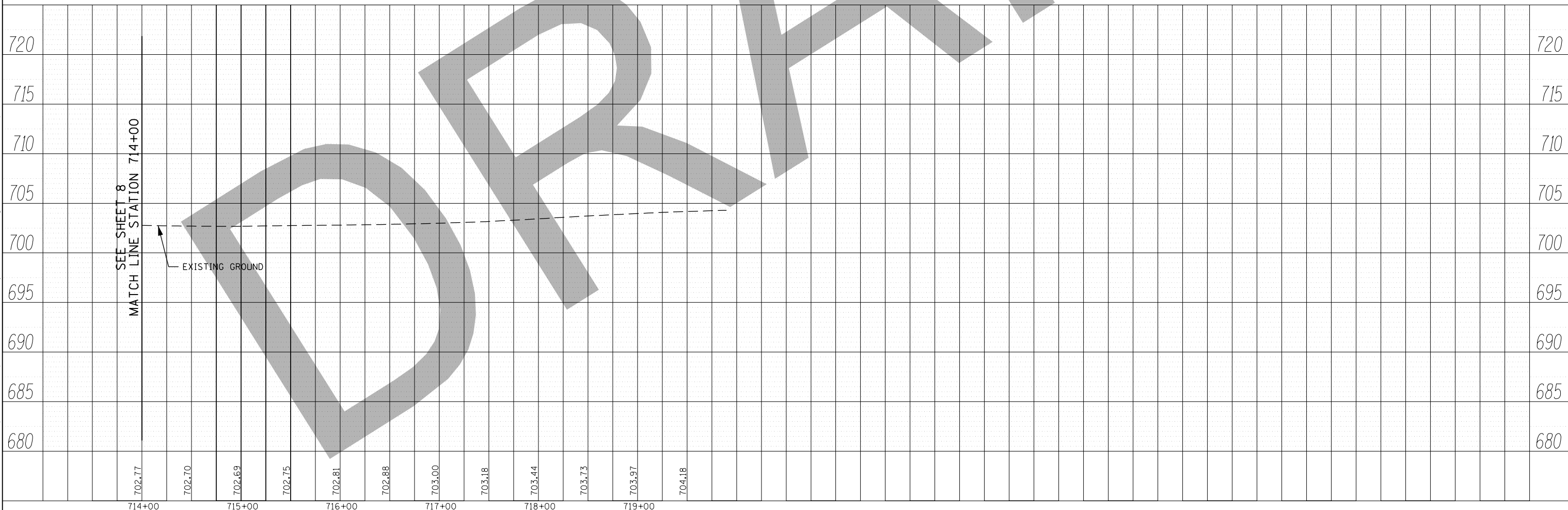
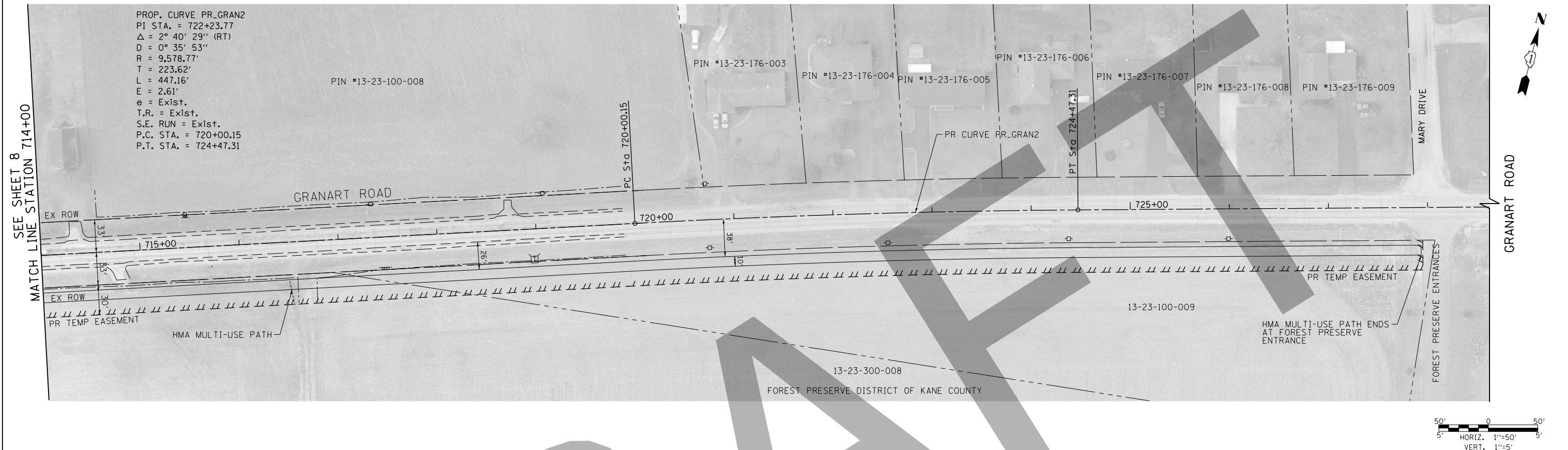
GRANART ROAD PLAN AND PROFILE				
SCALE: 1:50	SHEET 8	OF 14 SHEETS	STA. 702+00	TO STA. 714+00

F.A.S RTE.	SECTION	COUNTY	APPX	EXHIBIT	TOTAL SHEETS	SHEET NO.
1107	15-00277-01-BR	KANE	A	9	14	8
Dauberman Road Extension Project Development Report						
Proposed Plan and Profile Sheets						

PLAN	SURVEYED _____	BY _____	DATE _____
NOTE BOOK	PLOTTED _____		
NO. _____	ALIGNMENT CHECKED _____		
	RT. OF WAY CHECKED _____		
	CADD FILE NAME _____		

PROFILE	SURVEYED _____	BY _____	DATE _____
NOTE BOOK	PLOTTED _____		
	GRADES CHECKED _____		
	B.M. NOTED _____		
NO. _____	STRUCTURE NOTATIONS CHK'D _____		

FILE NAME = pw:\hq\pwint01a-e\transcorp\pw\Documents\Projects\CH401 - Chicago\40150086 - Kane County - Duberman Road Extension\303.00 - Road\303.01 - Standard Sheets\Plan and Profile Sheets\Granart P&P 08



USER NAME = bjholman	DESIGNED - BJH	REVISED -
	DRAWN - MBM	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 8/2/2018	DATE - July 31, 2018	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

GRANART ROAD PLAN AND PROFILE

SCALE: 1:50	SHEET 9	OF 14 SHEETS	STA. 714+00	TO STA. 728+62
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F.A.S RTE.	SECTION	COUNTY	APPX	EXHIBIT	TOTAL SHEETS	SHEET NO.
1107	15-00277-01-BR	KANE	A	9	14	9
Dauberman Road Extension Project Development Report						
Proposed Plan and Profile Sheets						

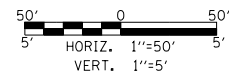
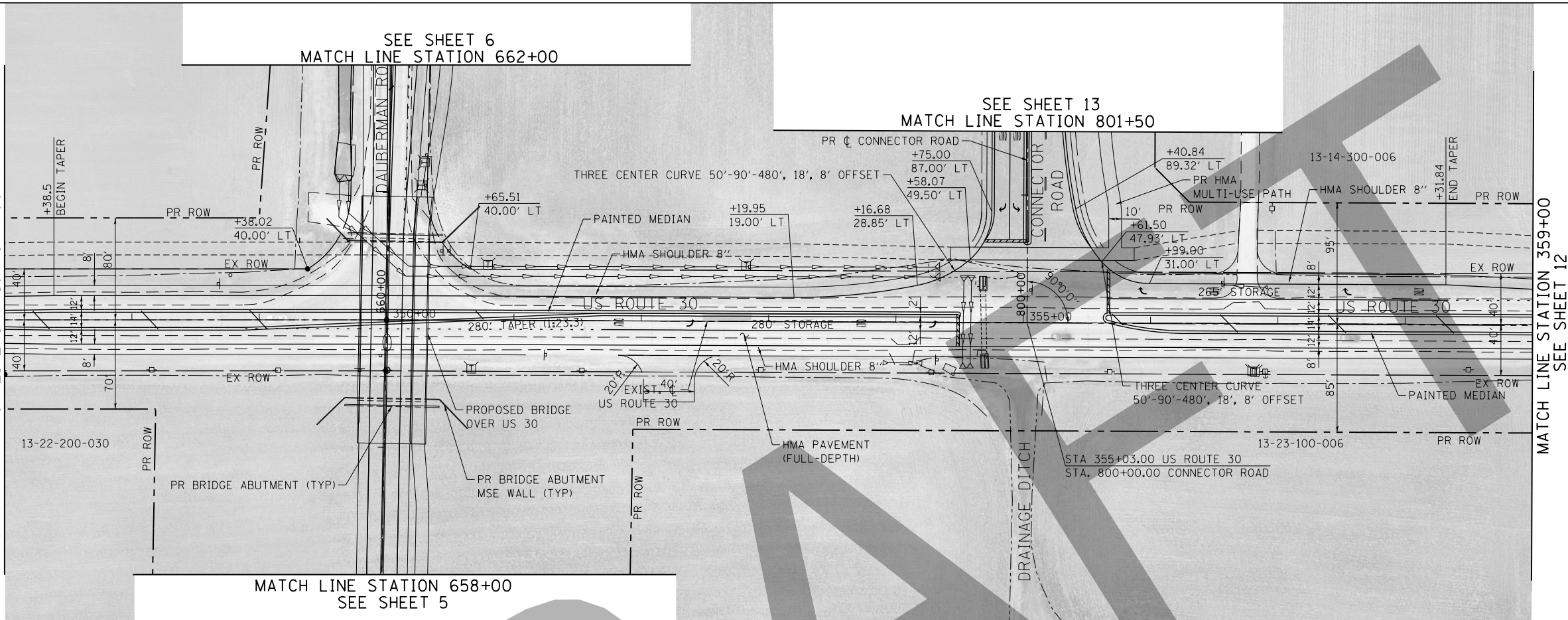
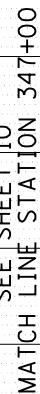
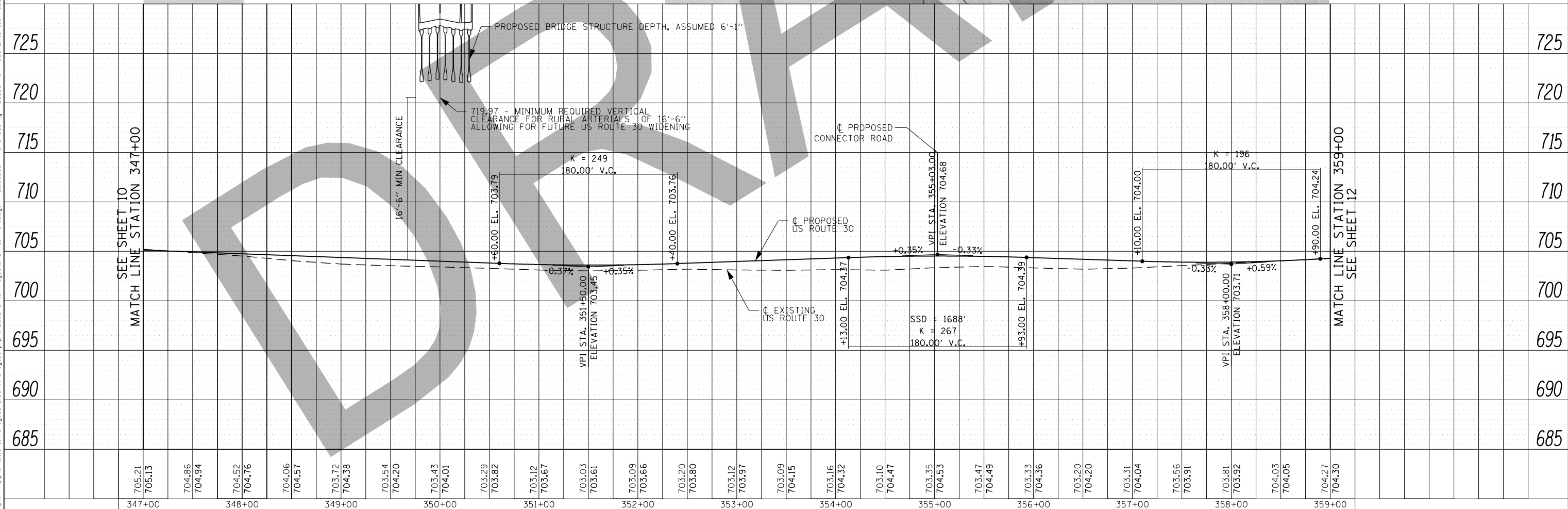
PROFILE	SURVEYED _____	BY _____	DATE _____
	PLOTTED _____		
NOTE BOOK	GRADES CHECKED _____		
	B.M. NOTED _____		
NO. _____	STRUCTURE NOTATIONS CH'KD _____		



TranSystems

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.S RTE.	SECTION	COUNTY	APPX	EXHIBIT	TOTAL SHEETS	SHEET NO.
1107	15-00277-01-BR	KANE	A	9	14	11
Dauberman Road Extension Project Development Report						
Proposed Plan and Profile Sheets						



PROFILE	SURVEYED _____	BY _____	DATE _____
	PLOTTED _____		
NOTE BOOK	GRADES CHECKED _____		
	B.M. NOTED _____		
NO. _____	STRUCTURE NOTATIONS CH'KD _____		



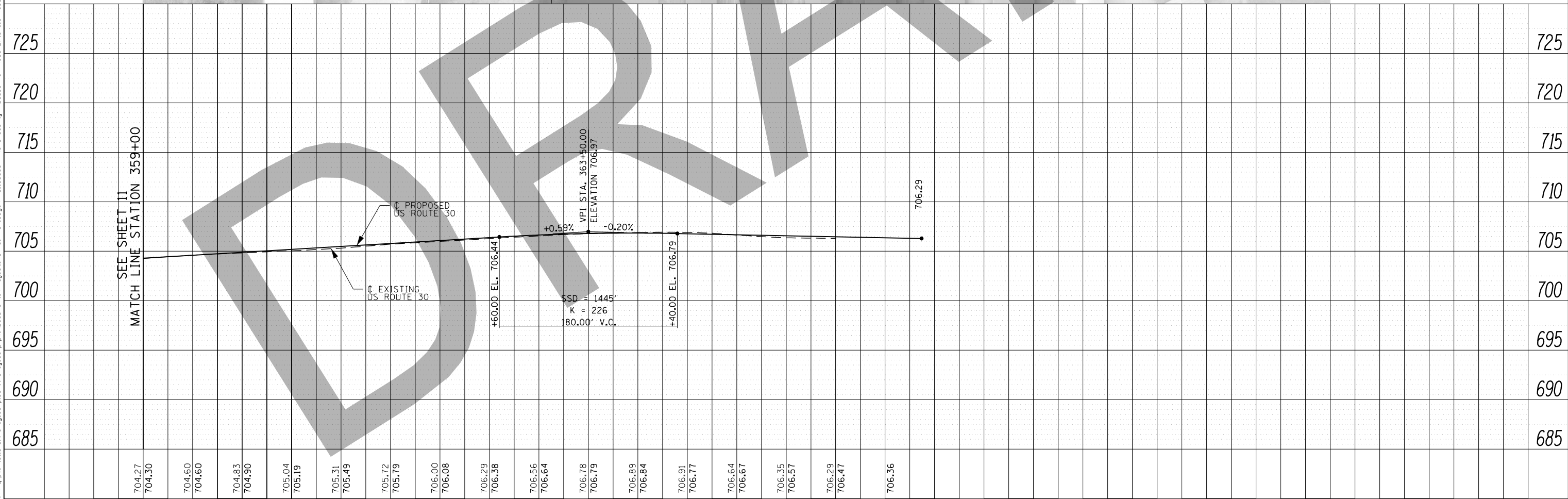
TranSystems

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE: 1:50	SHEET 12	OF 14	SHEETS	STA. 359+00	TO STA. 366+86.84
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F.A.S RTE.	SECTION	COUNTY	APPX	EXHIBIT	TOTAL SHEETS	SHEET NO.
1107	15-00277-01-BR	KANE	A	9	14	12

Dauberman Road Extension Project Development Report
Proposed Plan and Profile Sheets

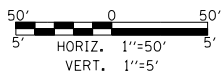


US ROUTE 30

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EXIST. CURVE EX_US301
PI STA. = 364+69.58
Δ = 4° 24' 43" (RT)
D = 1° 00' 00"
R = 5,729.36'
T = 220.70'
L = 441.18'
E = 4.25'
e = 2.8%
T.R. = -----
S.E. RUN = -----
P.C. STA. = 362+48.88
P.T. STA. = 366+90.06

```



PROFILE	SURVEYED _____	BY _____	DATE _____
	PLOTTED _____		
NOTE BOOK	GRADES CHECKED _____		
	B.M. NOTED _____		
NO. _____	STRUCTURE NOTATIONS CH'KD _____		



TranSystems

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.S RTE.	SECTION	COUNTY	APPX	EXHIBIT	TOTAL SHEETS	SHEET NO.
1107	15-00277-01-BR	KANE	A	9	14	14
Dauberman Road Extension Project Development Report						
Proposed Plan and Profile Sheets						

EXHIBIT 10
US ROUTE 30 IDS

PLOT DATE = 5/15/2018
FILE NAME = N:\070-0086 TrnsSystems\DOT Dauberman Road Phase 1\CADD\Exhibits\IDS-US 30 at Jug Handle\01-IDS.dgn
PLOT SCALE = 100.0000' / in.
USER NAME = jthede

SIGNALIZED CAPACITY DESIGN ANALYSIS

PROGRAM USED: HCS 2010, VERSION: 6.9 SIGNAL TYPE: ACTUATED AREA TYPE: OTHER
NUMBER OF PHASES: (A.M.) 3 (P.M.) 2 CYCLE LENGTH: (A.M.) 100 SEC.(P.M.) 100 SEC. PEAK HOUR FACTOR: 0.95
INTERSECTION DELAY/LEVEL-OF-SERVICE A.M. 26.8 SECONDS LOS C P.M. 15.2 SECONDS LOS B

APPROACH	EASTBOUND (C)				WESTBOUND (D)				N/A				SOUTHBOUND (A)			
LANE GROUP	L	T			T	R							L	R		
NUMBER OF LANES	1	1			1	1							1	1		
2040 30TH MAX. HOUR TRAFFIC (veh/h)	A.M.	85	485		125	130							575	45		
	P.M.	45	190		445	460							235	170		
BASE SATURATION FLOW RATE (veh/h)	1900	2000			2000	1900							1900	1900		
LANE WIDTH (FT)	12	12			12	12							14	14		
VOLUME OF RIGHT TURN ON RED (veh/h)	A.M.	N/A	P.M.	N/A	A.M.	0	P.M.	0	A.M.		P.M.		A.M.	0	P.M.	0
PEDESTRIANS/HOUR (ped/h)	A.M.	0	P.M.	5	A.M.	0	P.M.	0	A.M.		P.M.		A.M.	0	P.M.	0
ARRIVAL TYPE	3				3								3			
LANE UTILIZATION ADJ. FACTOR	1.000	1.000			1.000	1.000							1.000	1.000		
GREEN TIME (SECONDS)	A.M.	5.0	51.1		42.6	42.6							36.9	36.9		
	P.M.	3.0	71.5		65.0	65.0							16.6	16.6		
GREEN RATIO (g/C)	A.M.	0.50	0.51		0.43	0.79							0.37	0.42		
	P.M.	0.70	0.71		0.65	0.82							0.17	0.20		
CAPACITY (c)	A.M.	608	947		789	1185							643	649		
	P.M.	584	1323		1203	1215							288	303		
v/c RATIO (X)	A.M.	0.147	0.539		0.167	0.115							0.942	0.073		
	P.M.	0.081	0.151		0.389	0.398							0.858	0.590		
STORAGE QUEUE (FEET)	A.M.	65	360		110	45							600	40		
	P.M.	25	85		235	125							295	205		
LANE GROUP DELAY(SECONDS)	A.M.	13.7	18.7		18.2	2.5							43.8	17.4		
	P.M.	5.7	4.8		9.2	3.5							43.5	37.3		
LANE GROUP LEVEL-OF-SERVICE	A.M.	B	B		B	A							D	B		
	P.M.	A	A		A	A							D	D		
APPROACH DELAY (SECONDS/VEHICLE)	A.M.		18.0			10.2								41.9		
	P.M.		5.0			6.3								40.9		
APPROACH LEVEL-OF-SERVICE	A.M.		B			B								D		
	P.M.		A			A								D		

ELEMENTS CONTROLLING DESIGN

PREFERRED ROUTE: US ROUTE 30

F.A. ROUTE NUMBER: FAP 573. MARKED ROUTE NUMBER: US 30.
STREET NAME: US ROUTE 30. SRA ROUTE? Y/N N.
FUNCTIONAL CLASSIFICATION: MINOR ARTERIAL. OSOW DESIGN? Y/N Y.
EXISTING ADT: 10,700 VPD. (2020) DESIGN YEAR ADT: 15,000 VPD. (2040)
PROPOSED DESIGN SPEED: 60 MPH. PROPOSED POSTED SPEED: 55 MPH.

SECONDARY ROUTE: CONNECTOR ROAD

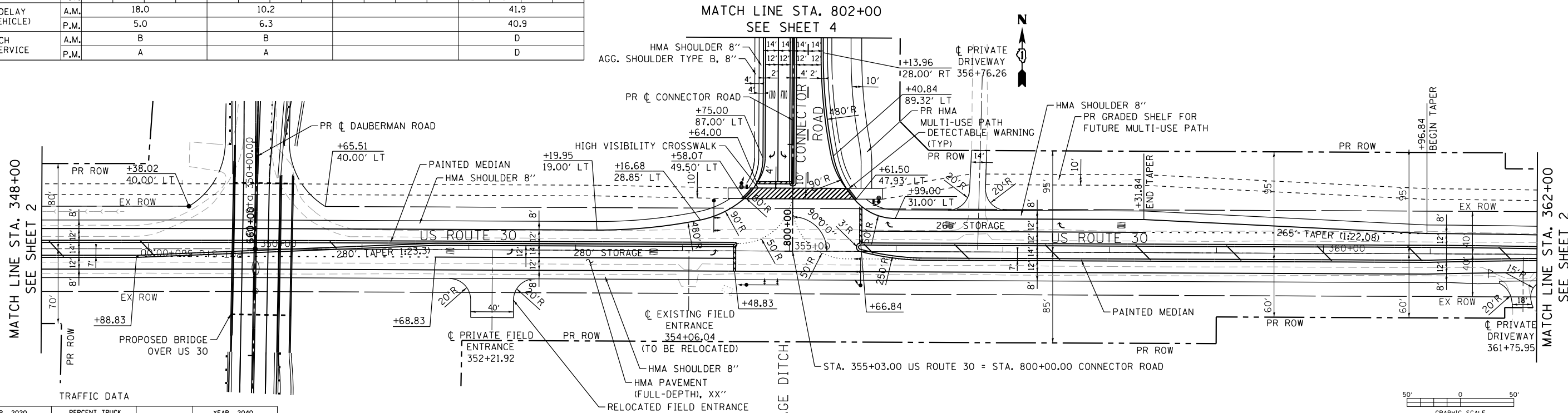
F.A. ROUTE NUMBER: . MARKED ROUTE NUMBER: .
STREET NAME: CONNECTOR ROAD. SRA ROUTE? Y/N N.
FUNCTIONAL CLASSIFICATION: MINOR COLLECTOR. OSOW DESIGN? Y/N N.
EXISTING ADT: 4,250 VPD. DESIGN YEAR ADT: 5,950 VPD.
PROPOSED DESIGN SPEED: 30 MPH. PROPOSED POSTED SPEED: 25 MPH.

IMPROVEMENT TYPE: NEW ROADWAY/INTERSECTION. ANTICIPATED YEAR OF CONSTRUCTION: 2020.
EXISTING METHOD OF TRAFFIC CONTROL: N/A. PROPOSED METHOD: SIGNAL.
SIGNAL WARRANT: 3B, 8A.
DESIGN VEHICLE: WB-65.
DESIGN YEAR: 2040 WHICH IS A 20 YEAR DESIGN.
TRUCK ROUTE DESIGNATION: PREFERRED ROADWAY: US ROUTE 30.
SECONDARY ROADWAY: DAUBERMAN ROAD (CONNECTOR ROAD).
DESIGN CRITERIA IDOT BDE CHAPTER 47.

GENERAL NOTES

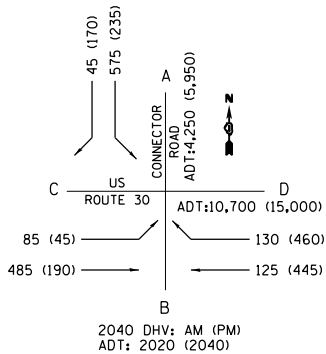
ARE PROFILES PROVIDED? YES. (CONNECTOR ROAD SINCE GRADE IS >1%)
TYPE N/A CURB AND GUTTER ON THE OUTSIDE OF THE ROADWAY/SHOULDERS.
TYPE N/A CURB AND GUTTER ON THE APPROACH MEDIAN.
TYPE N/A CURB AND GUTTER ON THE CORNER ISLANDS.
ALL DIMENSIONS ARE (E-E OR F-F) E-E, UNLESS OTHERWISE NOTED
THE RIGHT-OF-WAY LIMITS ARE PRELIMINARY.
DESIGN VEHICLE TURNING MOVEMENTS ARE ACCOMMODATED PER AUTOTURN SOFTWARE, VERSION9.
THE SCOPE OF WORK: NEW ROADWAY AND INTERSECTOIN, ROADWAY RECONSTRUCTION.
INTERSECTION DESIGN EXCEPTIONS:
N/A

ADDITIONAL NOTES: AS INTERSECTION DOES NOT EXIST TODAY, REGIONAL EXISTING TRAFFIC WAS REDISTRIBUTED TO CREATE 2020 OPENING DAY TRAFFIC.

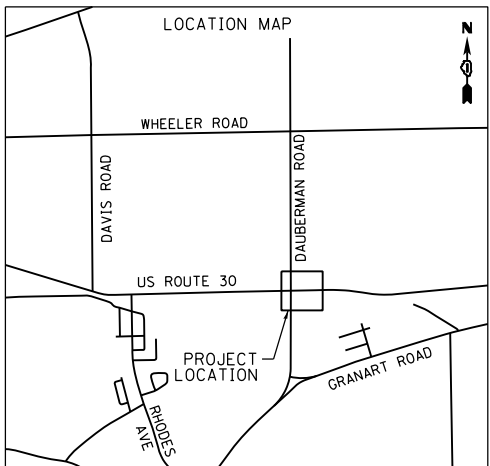


MOVEMENT	YEAR 2020 30TH MAXIMUM HOUR TRAFFIC		PERCENT TRUCK TRAFFIC IN 30TH MAX. HOUR		ESTIMATED PERCENT INCREASE BY 2040	YEAR 2040 30TH MAXIMUM HOUR TRAFFIC	
	A.M.	P.M.	A.M.	P.M.		A.M.	P.M.
AD (L)	320	105	8%	8%		575	235
AB (T)	N/A	N/A	N/A	N/A		N/A	N/A
AC (R)	25	60	8%	8%		45	170
BC (L)	N/A	N/A	N/A	N/A		N/A	N/A
BA (T)	N/A	N/A	N/A	N/A		N/A	N/A
BD (R)	N/A	N/A	N/A	N/A		N/A	N/A
CA (L)	55	35	8%	8%		85	45
CD (T)	350	165	8%	8%		485	190
CB (R)	N/A	N/A	N/A	N/A		N/A	N/A
DB (L)	N/A	N/A	N/A	N/A		N/A	N/A
DC (T)	95	340	8%	8%		125	445
DA (R)	95	350	8%	8%		130	460
TOTAL A	495	550				835	910
TOTAL B	N/A	N/A				N/A	N/A
TOTAL C	525	600				740	850
TOTAL D	860	960				1315	1330

T = THROUGH, L = LEFT, R = RIGHT



PREPARED BY: **BL** Bollinger, Lach & Associates, Inc.
ITASCA, ILLINOIS
PROJ. MGR. DBB PROJ. ENG. JLT



INTERSECTION DESIGN STUDY	
F.A.P. ROUTE 573	US ROUTE 30
ROUTE	WITH CONNECTOR ROAD
SEC. NO. 15-00277-01-BR	PROJ. NO.
SCALE 1"=50'	COUNTY KANE
SJN :	REV. NO.
DESIGNED BY BOLLINGER LACH & ASSOCIATES DATE 5/15/18	
SATISFACTORY	DISTRICT GEOMETRICS ENGINEER DATE
SATISFACTORY	DISTRICT PROGRAM DEVELOPMENT ENGINEER DATE
SATISFACTORY	DISTRICT OPERATIONS ENGINEER DATE
APPROVED	DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER DATE
CADD FILE NAME :	I.D.S. SHEET 1 OF 7



CURVE EX_US30						
SECTION	STATION	PROFILE ELEVATION	LEFT SIDE CROSS SLOPE %	LT E.P.	RIGHT SIDE CROSS SLOPE %	RT E.P.
A	360+85.74	705.41	-2.00%	705.02	2.00%	705.02
B	362+48.88	706.37	0.00%	706.37	2.00%	706.37
C	362+88.21	706.59	2.80%	707.05	2.80%	706.59
CURVE EX_US30 RUNOUT						
C	366+50.73	706.36	2.80%	706.68	2.80%	706.68
B	366+86.84 (MATCH EX.)	706.29 (MATCH EX.)	2.80% (MATCH EX.)	706.61 (MATCH EX.)	2.80% (MATCH EX.)	705.91 (MATCH EX.)

*RECONSTRUCTION AND PROJECT LIMITS TERMINATE AT STATION 366+86.84

F.A.P. ROUTE 573 WITH US ROUTE 30
ROUTE CONNECTOR ROAD

SCALE 1"=50' COUNTY KANE

DESIGNED BY BOLLINGER LACH & ASSOCIATES DATE 5/15/18

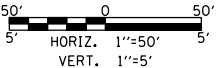
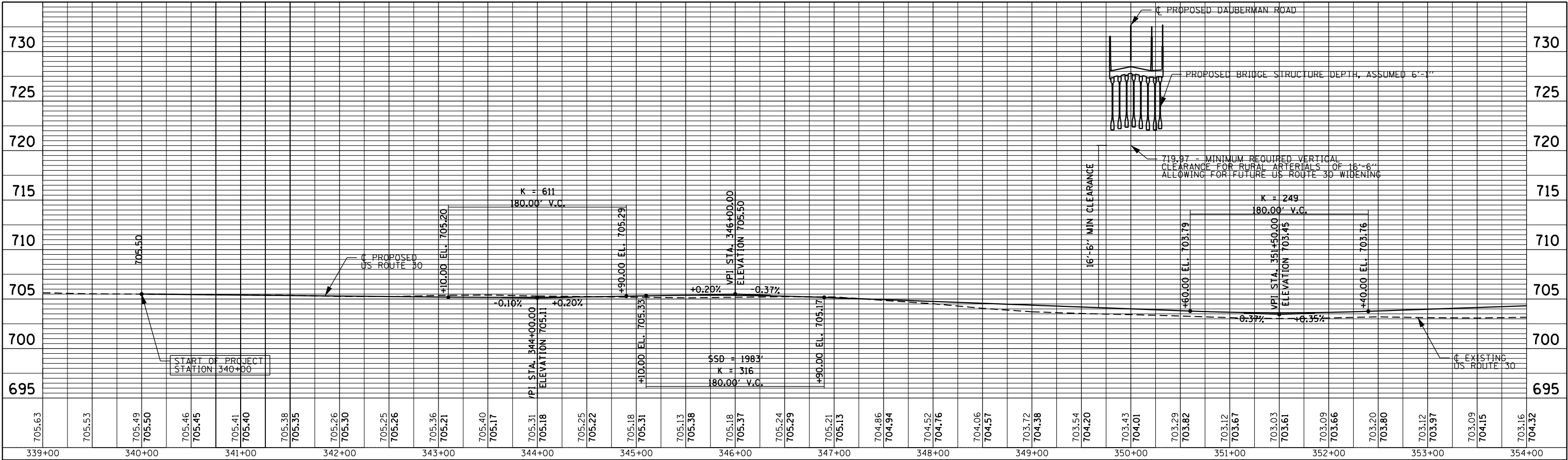
SATISFACTORY _____

APPROVED _____

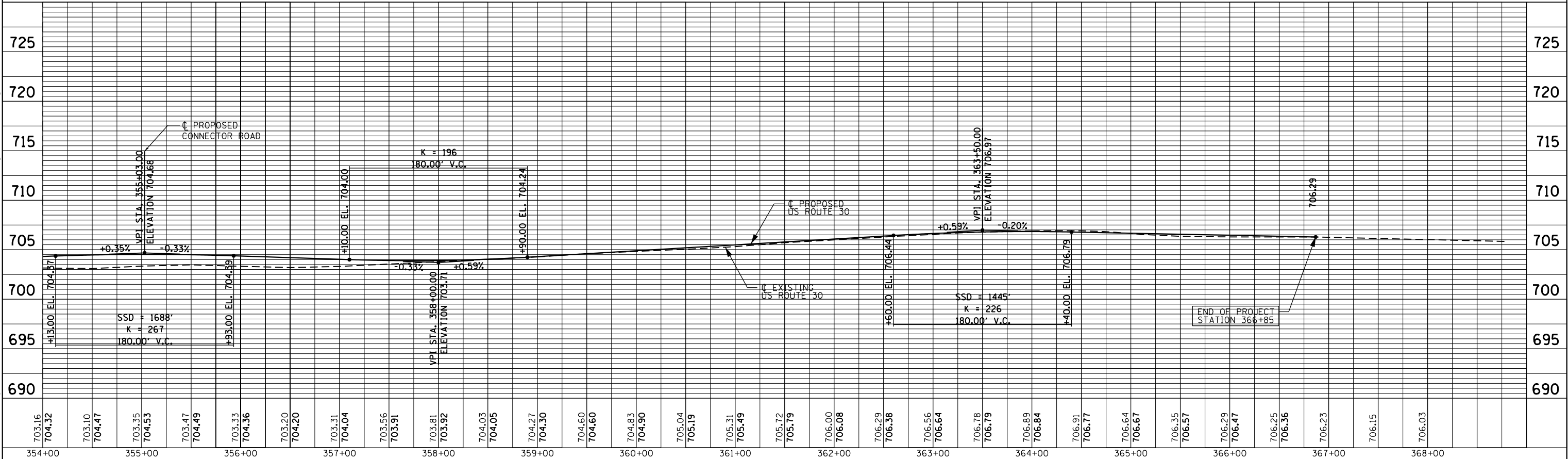
CADD FILE NAME : [] I.D.S. SHEET 2 OF 7

BDE-9

PLAN	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	ALIGNMENT CHECKED		
	STRUCTURE NOTED		
	CADD FILE NAME		



PROFILE	SURVEYED	BY	DATE
NOTE BOOK NO.	GRADES CHECKED		
	STRUCTURE NOTED		
	NOTATIS CHD		



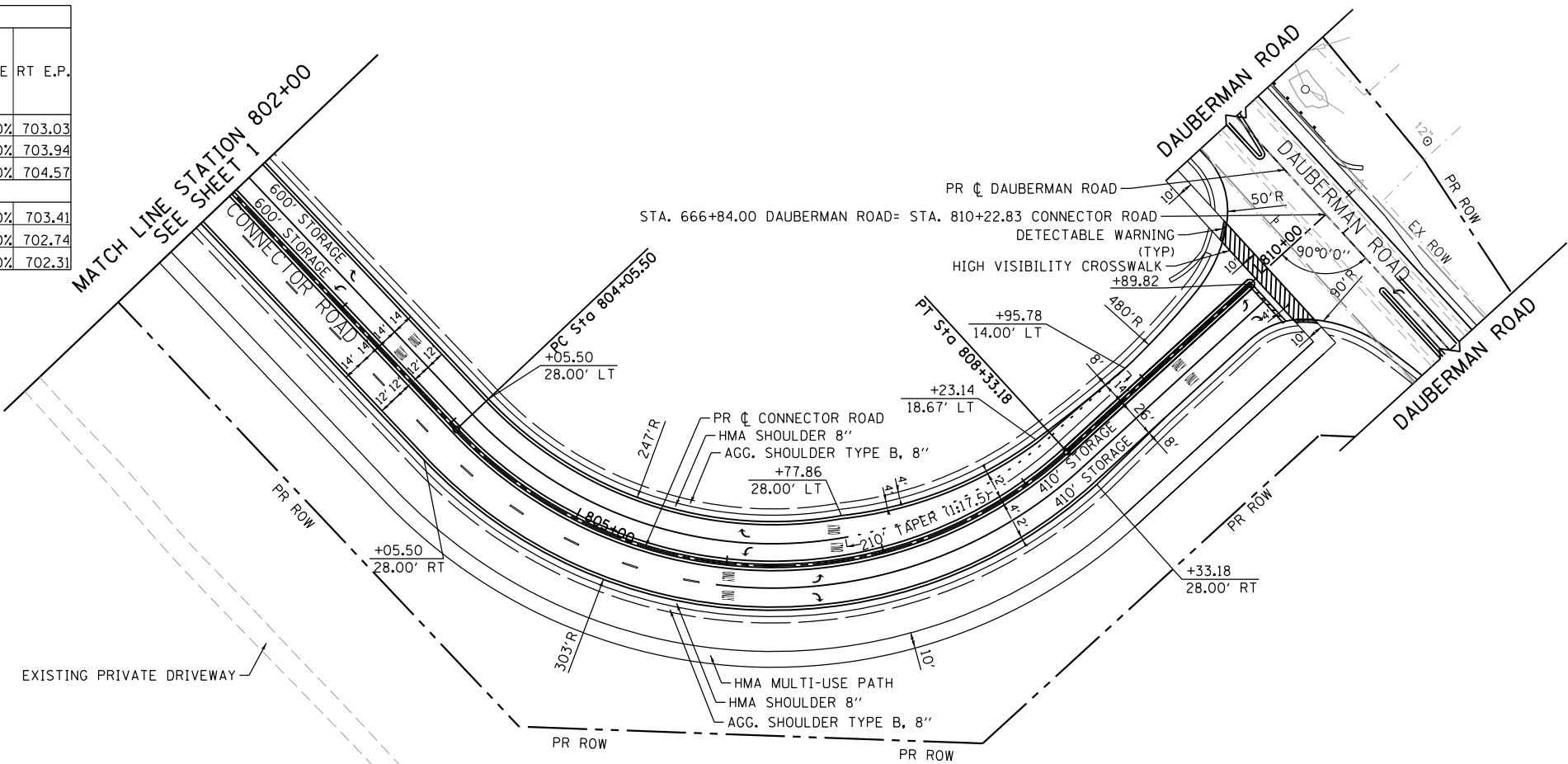
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	PLOT SCALE = 100.0000' / in.	DRAWN - JLT	REVISED -						15-00277-01-BR	KANE	7	3	
	PLOT DATE = 5/15/2018	CHECKED - DBB	REVISED -						CONTRACT NO.				
		DATE - 5/15/2018	REVISED -						ILLINOIS FED. AID PROJECT				
SCALE: 1:50		SHEET	OF	SHEETS	STA.	TO STA.							

PLAN	SURVEYED	DATE
NO.	BY	
NOTE BOOK		
CHECKED		
FILE NAME		

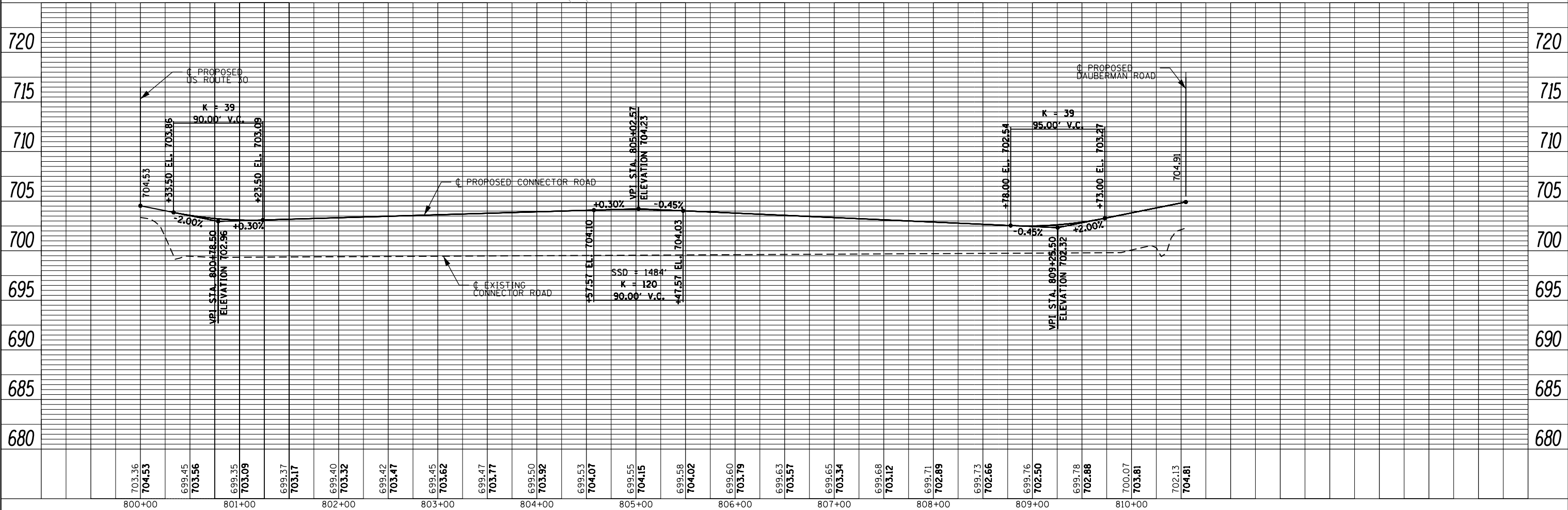
PROFILE	SURVEYED	DATE
NO.	BY	
NOTE BOOK		
CHECKED		
FILE NAME		

FILE NAME = W:\070-006 TrnsSystems KDOT Dauberman Road Phase 1\CAD\Exhibits\US-US 30 at Jug Handle\02-IDS Connector Road P&P.dgn

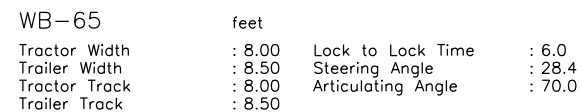
CURVE PR_JUG-1						
SECTION	STATION	PROFILE ELEVATION	LEFT SIDE CROSS SLOPE %	LT E.P.	RIGHT SIDE CROSS SLOPE %	RT E.P.
A	802+89.74	703.59	-2.00%	703.03	2.00%	703.03
B	804+05.50	703.94	-2.00%	703.38	0.00%	703.94
C	804+28.62	704.01	-2.00%	703.45	-2.00%	704.57
CURVE PR_JUG-1 RUNOUT						
C	808+10.06	702.85	-2.00%	702.29	-2.00%	703.41
B	808+33.18	702.74	-2.00%	702.18	0.00%	702.74
A	809+48.94	702.87	-2.00%	702.31	2.00%	702.31



PROP. CURVE PR_JUG-1
PI STA. = 806+76.24
 Δ = 89° 06' 25" (LT)
D = 20° 50' 05"
R = 275.00'
T = 270.75'
L = 427.68'
E = 110.91'
e = 2.0%
T.R. = -----
S.E. RUN = -----
P.C. STA. = 804+05.50
P.T. STA. = 808+33.18



Bollinger, Lach & Associates, Inc. ITASCA, ILLINOIS				USER NAME = jthede				DESIGNED - JLT DRAWN - JLT CHECKED - DBB DATE - 5/15/2018				REVISED - REVISED - REVISED - REVISED -				STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				INTERSECTION DESIGN STUDY CONNECTOR ROAD PLAN AND PROFILE				F.A.U. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO. 15-00277-01-BR KANE 7 4 CONTRACT NO. ILLINOIS FED. AID PROJECT			
PLOT SCALE = 100.0000' / in.				PLOT DATE = 5/15/2018				SCALE: 1"=50'				SHEET OF SHEETS				STA. 801+50 TO STA. 809+50											



**Bollinger, Lach
& Associates, Inc.**



F.A. ROUTE 573 WITH US ROUTE 30
ROUTE CONNECTOR ROAD

SEC. NO. 15-00277-01-BR PROJ. NO. _____

SCALE 1"=20' COUNTY KANE

SJN : _____ REV. NO. _____

DESIGNED BY BOLLINGER LACH & ASSOCIATES DATE 5/15/18

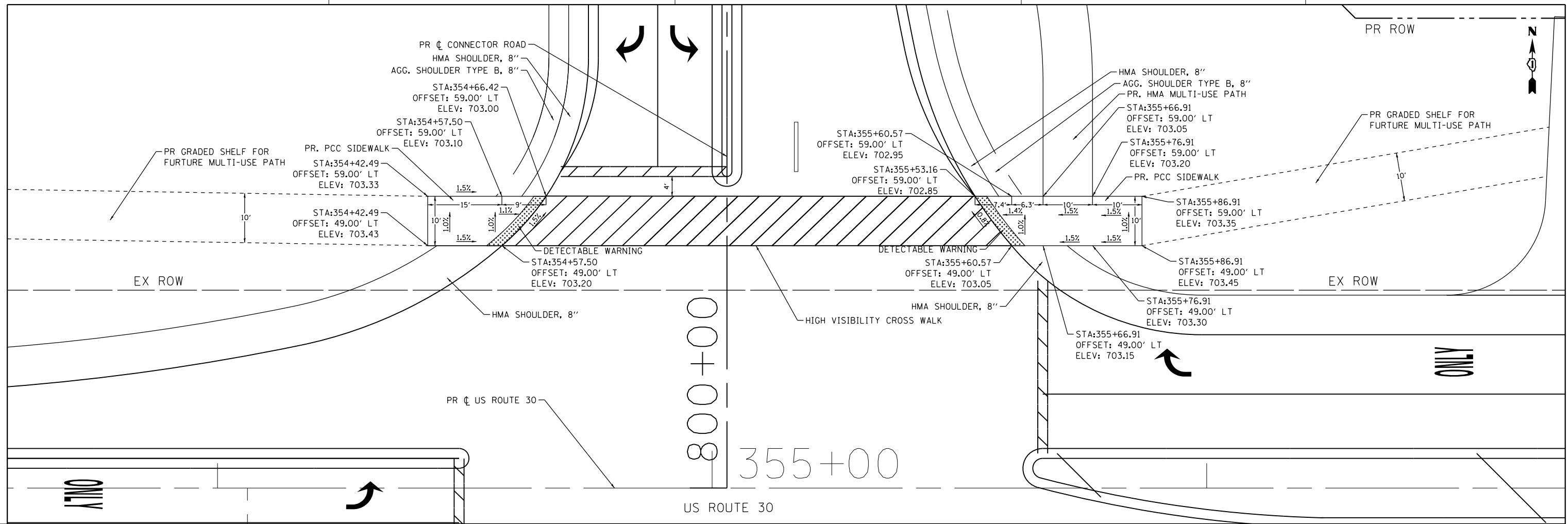
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DISTRICT GEOMETRICS ENGINEER DATE _____

SATISFACTORY _____
DISTRICT PROGRAM DEVELOPMENT ENGINEER DATE _____

SATISFACTORY _____
DISTRICT OPERATIONS ENGINEER DATE

APPROVED _____
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER DATE _____

CADD FILE NAME : [] I.D.S. SHEET 6 OF 7



INTERSECTION DESIGN STUDY

F.A.P. ROUTE 573 WITH US ROUTE 30
ROUTE WITH CONNECTOR ROAD

SEC. NO. 15-00277-01-BR PROJ. NO. _____

SCALE 1"=10' COUNTY KANE

SJN : _____ REV. NO. _____

DESIGNED BY BOLLINGER LACH & ASSOCIATES DATE 5/15/18

SATISFACTORY _____
DISTRICT GEOMETRICS ENGINEER DATE

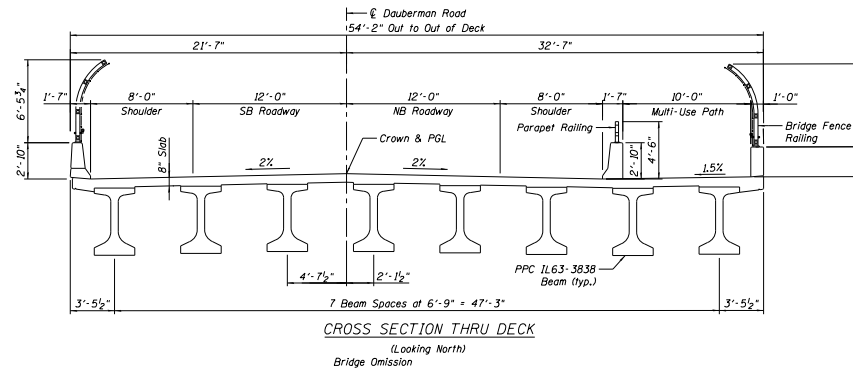
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DISTRICT PROGRAM DEVELOPMENT ENGINEER DATE _____

SATISFACTORY _____
DISTRICT OPERATIONS ENGINEER DATE _____

APPROVED _____
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER DATE _____

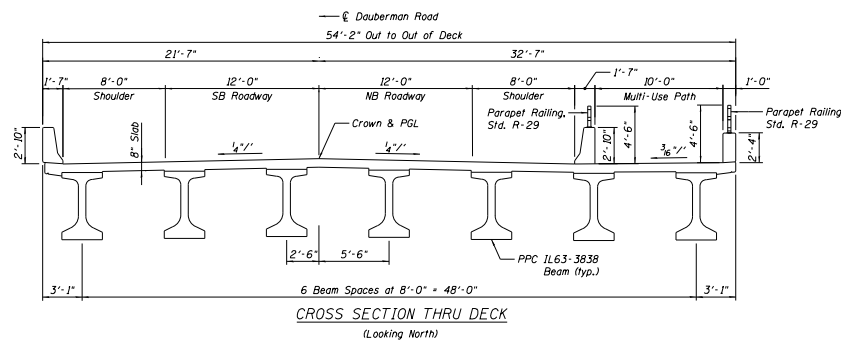
CADD FILE NAME : [] I.D.S. SHEET 7 OF 7

BNSF RAILROAD BRIDGE CROSSING



DAUBERMAN ROAD
STA. 654+04 TO 655+32

US ROUTE 30 RAILROAD BRIDGE CROSSING



DAUBERMAN ROAD
STA. 659+33 TO 660+67



USER NAME = b.j.holman
PLOT SCALE = *SCALE*
PLOT DATE = 8/2/2018

ILLINOIS DEPARTMENT
OF TRANSPORTATION

PROPOSED
TYPICAL SECTION

F.A.S RTE.	SECTION	COUNTY	APPX	EXHIBIT	SHEET
1107	15-00277-01-BR	KANE	A	11	1
Dauberman Road Extension Project Development Report					

EXHIBIT 12
PROPOSED BRIDGE TYPE, SIZE AND LOCATION

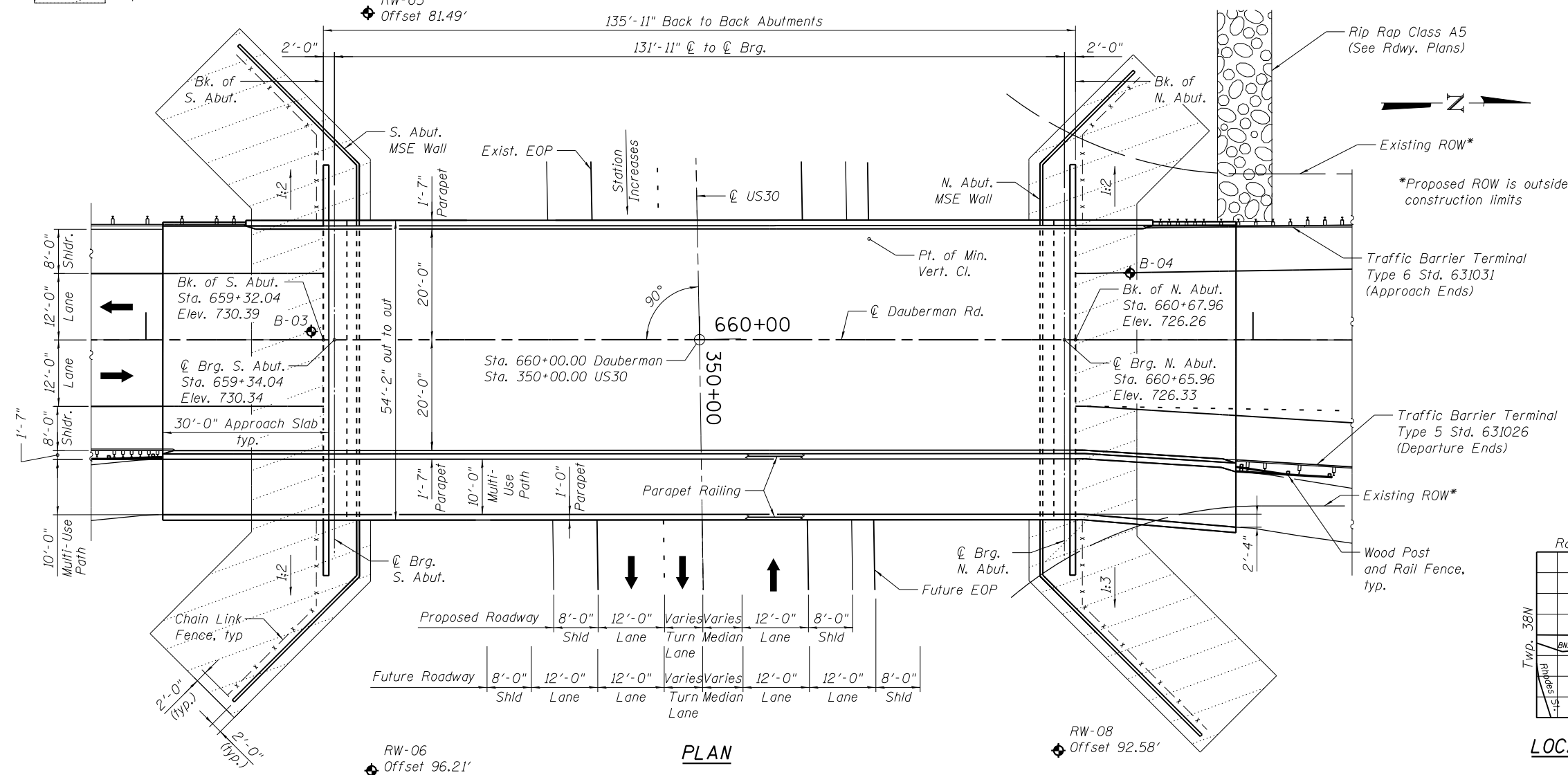
Existing Structure: None



Approximate Limits of Removal and Replacement of Unsuitable Soil

RW-05
Offset 81.49'

 RW-07
Offset 87.94'



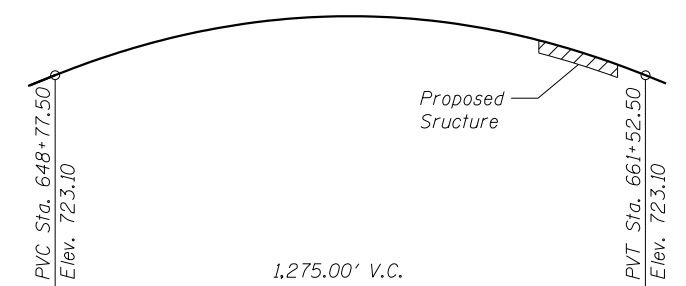
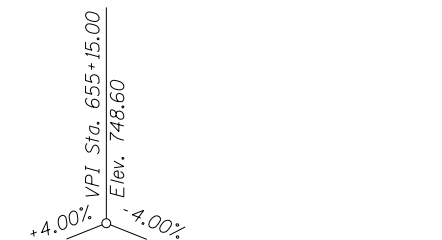
RW-06
Offset 96.21

PLAN

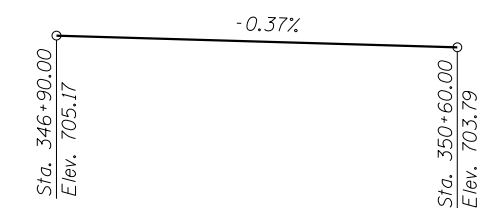
RW-08
Offset 92.58'

$f'_c = 8,500 \text{ psi}$
 $f'_{ci} = 7,000 \text{ psi}$
 $f_{pu} = 270,000 \text{ psi (0.6" } \phi \text{ low lax strands)}$
 $f_{pbt} = 202,500 \text{ psi (0.6" } \phi \text{ low lax strands)}$

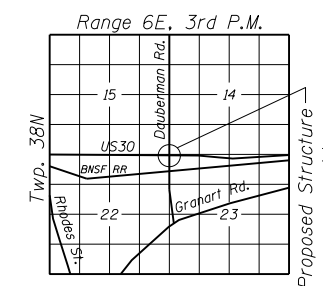
Seismic Performance Zone (SPZ) = 1
Design Spectral Acceleration at 1.0 sec. (S_{D1}) = 0.091
Design Spectral Acceleration at 0.2 sec. (S_{D5}) = 0.164
Soil Site Class = D



Along \mathbb{C} Dauberman Rd.



Along @ US30



GENERAL PLAN
DAUBERMAN ROAD (F.A.S. 1107)
OVER US ROUTE 30
SECTION 15-00277-01-BR
KANE COUNTY
STATION 660+00.00
STRUCTURE NO. 045-3402



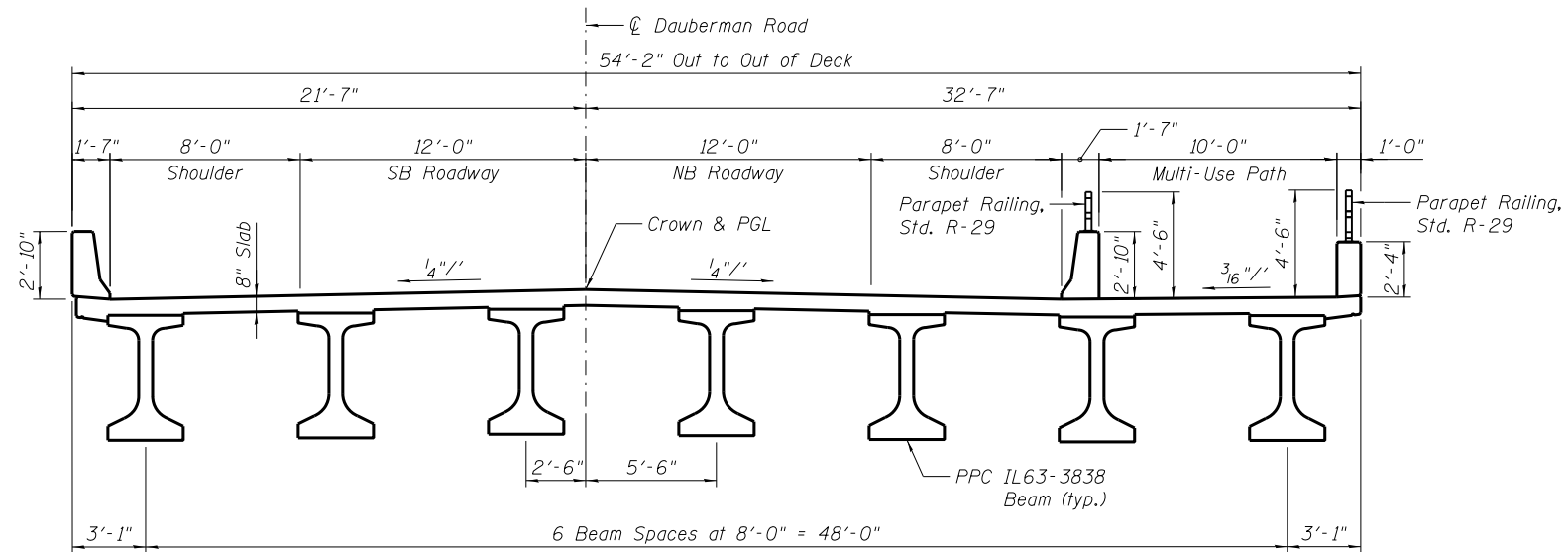
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	CHECKED JJI	REVISED -
PLOT SCALE =	DRAWN HB	REVISED -
PLOT DATE = 7/27/2018	CHECKED JJI	REVISED -

KANE COUNTY DIVISION OF TRANSPORTATION

SHEET NO. 1 OF 4 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1107	15-00277-01-BR	Kane	4	1
		CONTRACT NO.		
ILLINOIS FED. AID PROJECT				

ILLINOIS	FED. AID PROJECT
----------	------------------

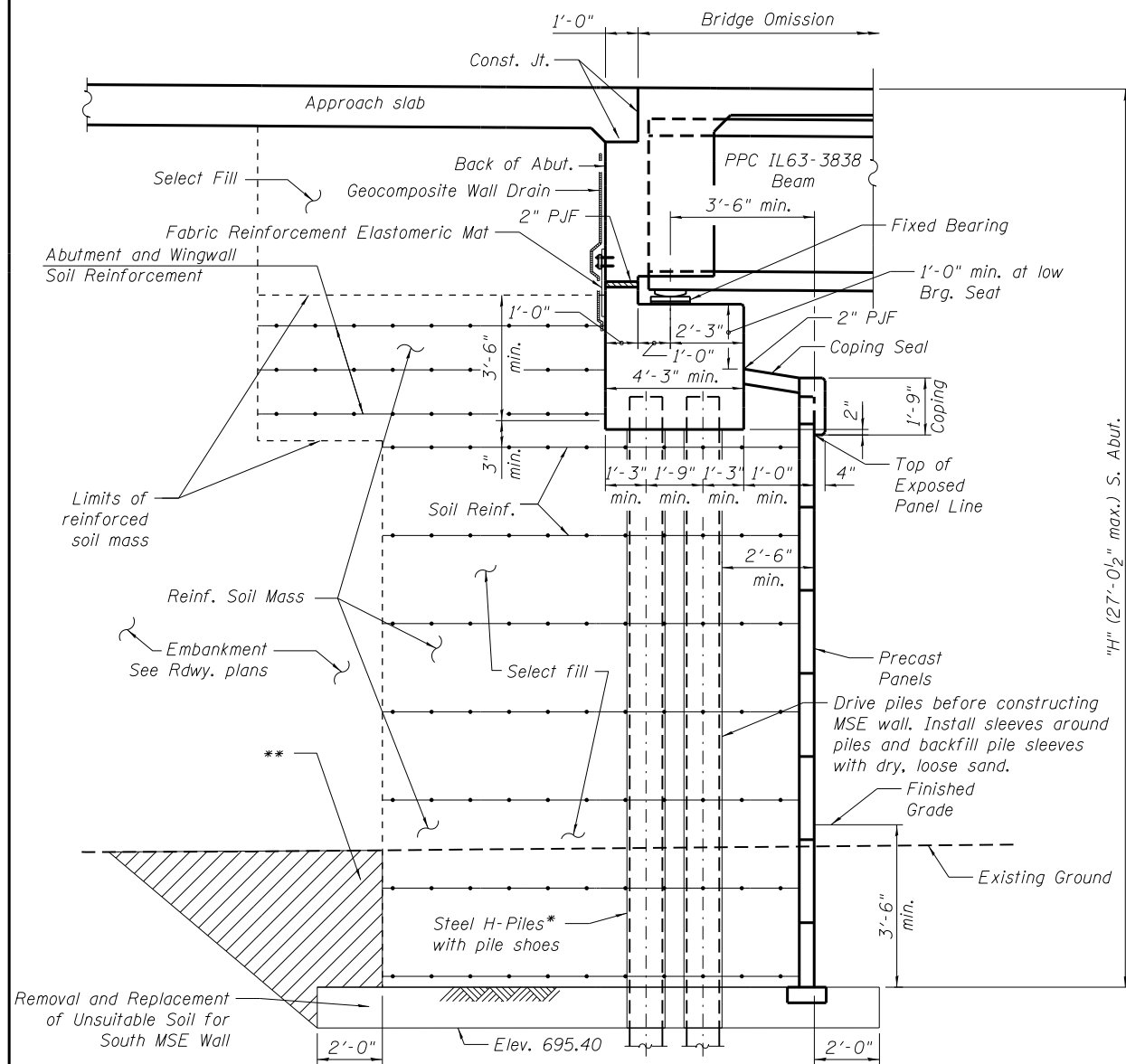


CROSS SECTION THRU DECK

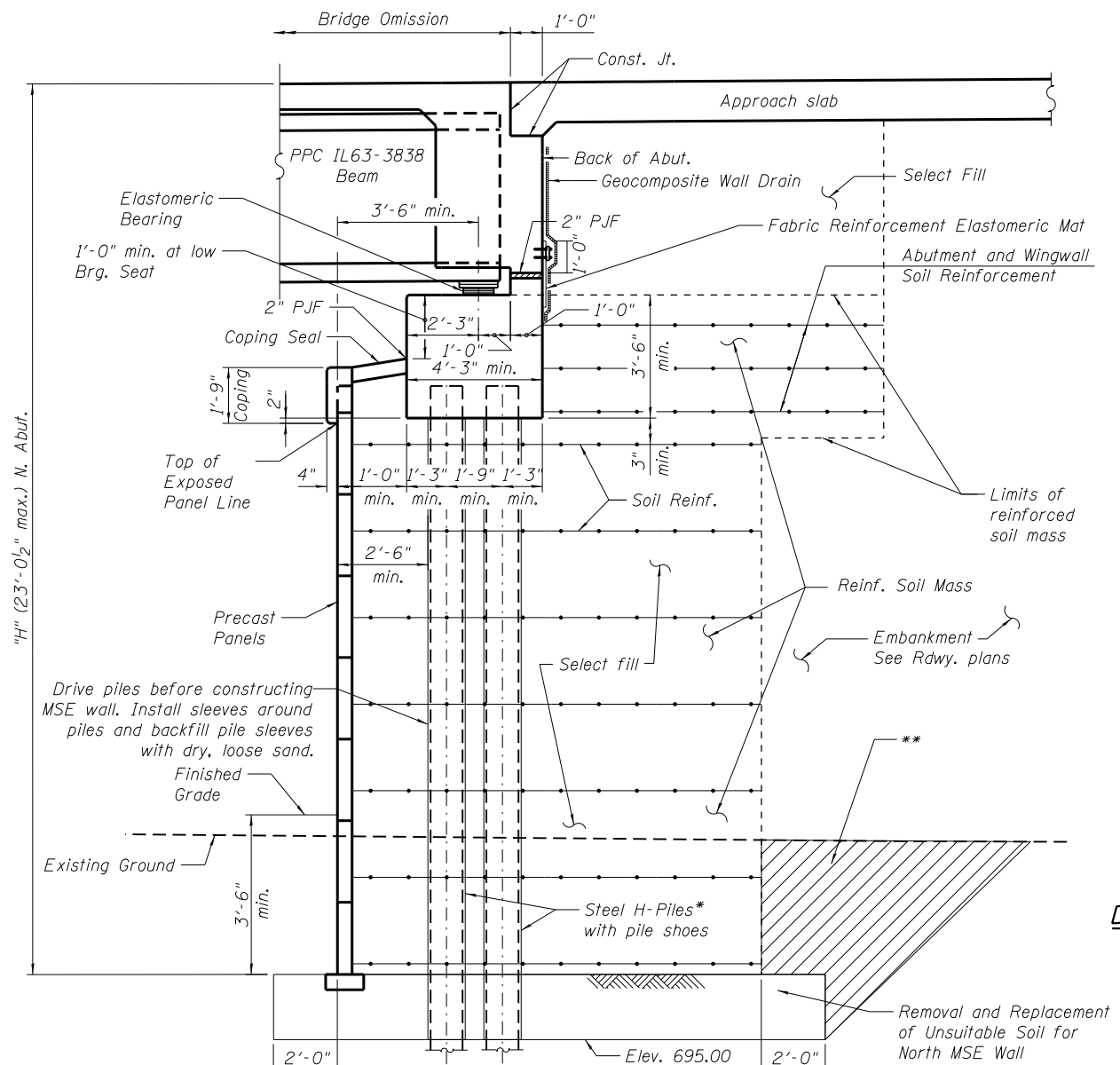
(Looking North)

* Piles shall be driven before the construction of MSE walls. 21" ϕ corrugated galvanized steel pipe, 10 gage min. shall be provided from the bottom of the abutment cap to the bottom of the excavation level. The cost of corrugated steel pipe and sand backfill shall be included in the cost of the furnishing piles.

** Overexcavation beyond the limits of Structure Excavation. This area not measured for payment. Backfill overexcavation with same material as used for select fill.



SECTION THRU SOUTH ABUTMENT



SECTION THRU NORTH ABUTMENT

DETAILS
DAUBERMAN ROAD (F.A.S 1107)
OVER US ROUTE 30
SECTION 15-00277-01-BR
KANE COUNTY
STATION 660+00.00
STRUCTURE NO. 045-3402

7/27/2018 9:59:39 AM

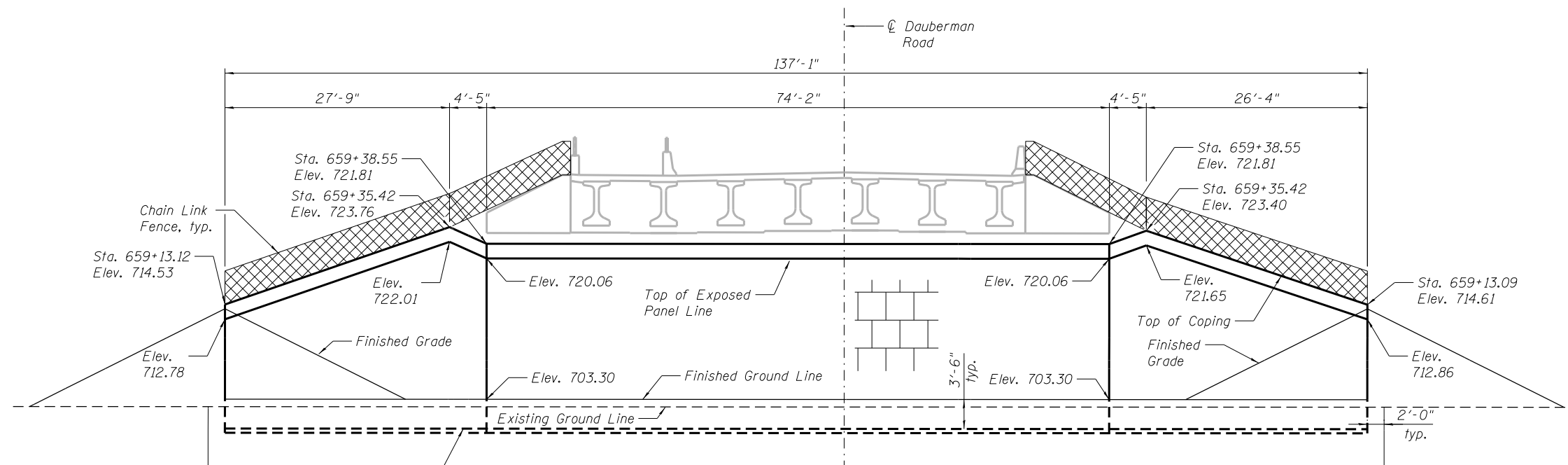
BLA, Inc.

USER NAME = HBrierton	DESIGNED HB	REVISED -
CHECKED JJI	REVISOR -	
PLOT SCALE =	DRAWN HB	REVISED -
PLOT DATE = 7/27/2018	CHECKED JJI	REVISED -

KANE COUNTY
DIVISION OF TRANSPORTATION

SHEET NO. 2 OF 4 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1107	15-00277-01-BR	Kane	4	2
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

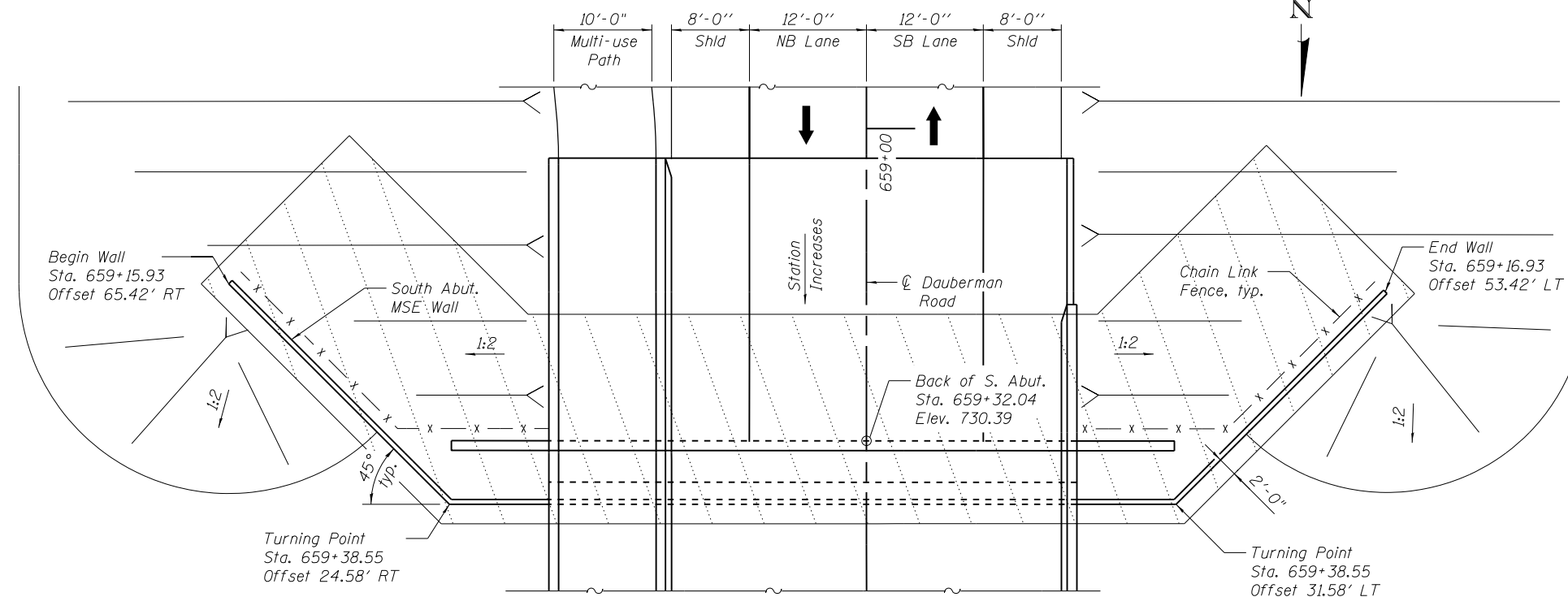


SOUTH ABUTMENT MSE WALL DEVELOPED ELEVATION

(Looking South)

Notes:

1. Wall stations offsets are given to the front face of wall and are measured from the CL of Dauberman Road.
2. Wall to be built in conjunction with new bridge.
3. Horizontal dimensions measured along front face of precast panels.
4. Existing and proposed ROW are outside of construction limits.



SOUTH ABUTMENT MSE WALL PLAN

LEGEND

Approximate Limits of Removal and Replacement of Unsuitable Soil

PLAN AND ELEVATION
SOUTH ABUTMENT MSE WALL
DAUBERMAN ROAD (F.A.S. 1107)
OVER US ROUTE 30
SECTION 15-00277-01-BR
KANE COUNTY
STATION 660+00.00
STRUCTURE NO. 045-3402

7/27/2018 9:20:25 AM

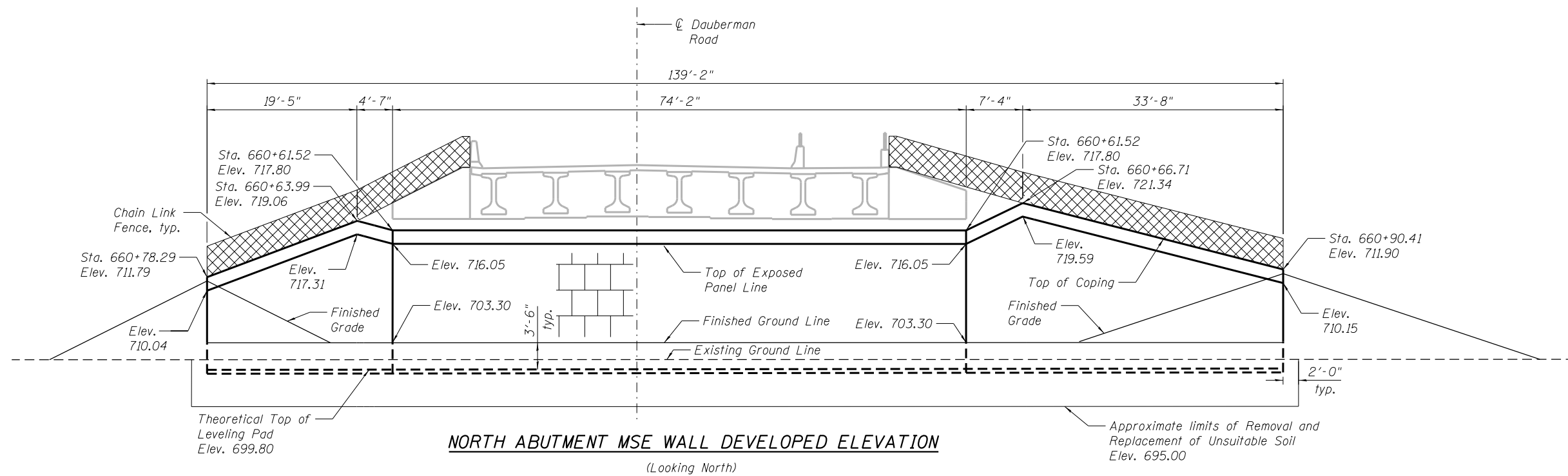
BLA, Inc.

USER NAME = HBrierton	DESIGNED HB	REVISED -
	CHECKED JJI	REVISED -
PLOT SCALE =	DRAWN HB	REVISED -
PLOT DATE = 7/27/2018	CHECKED JJI	REVISED -

KANE COUNTY
DIVISION OF TRANSPORTATION

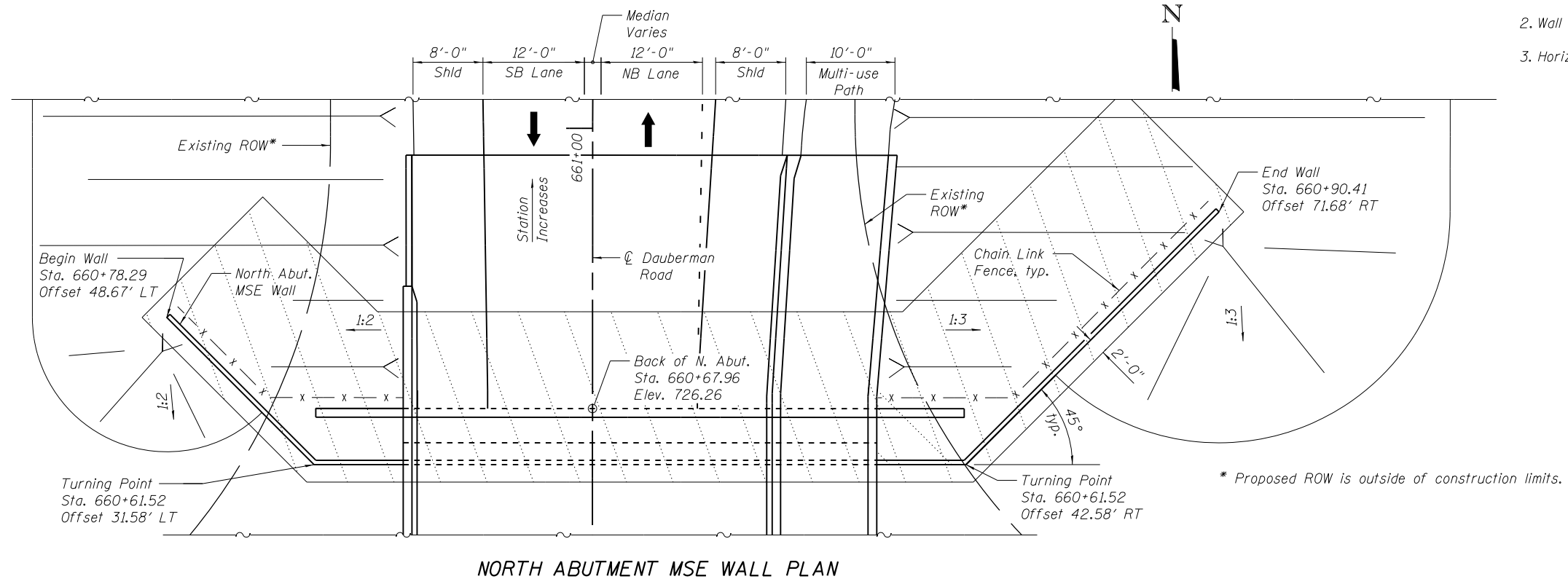
SHEET NO. 3 OF 4 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1107	15-00277-01-BR	Kane	4	3
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				



Notes:

1. Wall stations offsets are given to the front face of wall and are measured from the CL of Dauberman Road.
2. Wall to be built in conjunction with new bridge.
3. Horizontal dimensions measured along front face of precast panels.



LEGEND

Approximate Limits of Removal and Replacement of Unsuitable Soil

* Proposed ROW is outside of construction limits.

**PLAN AND ELEVATION
NORTH ABUTMENT MSE WALL
DAUBERMAN ROAD (F.A.S. 1107)
OVER US ROUTE 30
SECTION 15-00277-01-BR
KANE COUNTY
STATION 660+00.00
STRUCTURE NO. 045-3402**

7/27/2018 9:20:58 AM

BLA, Inc.

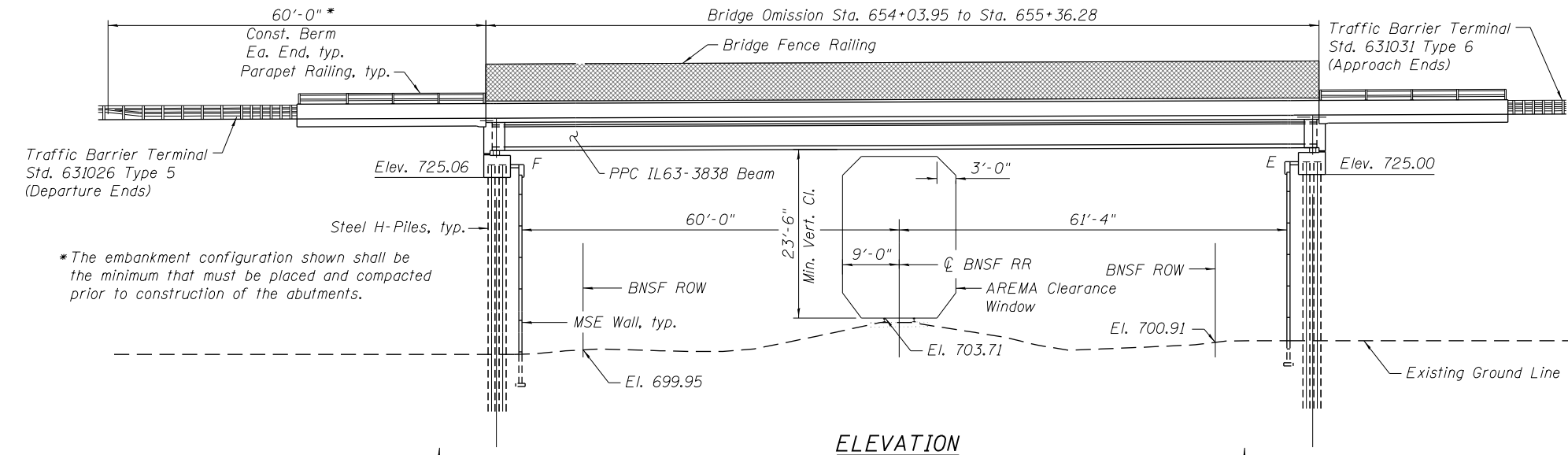
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	CHECKED JJI	REVISED -
PLOT SCALE =	DRAWN HB	REVISED -
PLOT DATE = 7/27/2018	CHECKED JJI	REVISED -

**KANE COUNTY
DIVISION OF TRANSPORTATION**

SHEET NO. 4 OF 4 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1107	15-00277-01-BR	Kane	4	4
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

Benchmark: Railroad spike set in power pole with light on south side of U.S. 30. Elev. 702.56
Existing Structure: None.



ELEVATION

DESIGN SPECIFICATIONS

2017 AASHTO LRFD Bridge Design
Specifications, 8th Edition

DESIGN STRESSES

FIELD UNITS

$f'_c = 4,000$ psi (Bridge Deck)
 $f'_c = 3,500$ psi (typ.)
 $f_y = 60,000$ psi (Reinforcement)

PRECAST UNITS

$f'_c = 4,500$ psi (Precast Panels)

PRECAST PRESTRESSED UNITS

$f'_c = 8,500$ psi
 $f'_{ci} = 7,000$ psi
 $f_{pu} = 270,000$ psi (0.6" ϕ low lax strands)
 $f_{pbt} = 202,500$ psi (0.6" ϕ low lax strands)

Notes:

1. Proposed ROW is outside of construction limits.
2. For North and South MSE Wall details, see Sheet 3 and 4 of 4.
3. No freefall deck drains will be permitted in the span over the tracks or within 10 ft. of cross arms of a railroad pole line.

HIGHWAY CLASSIFICATION

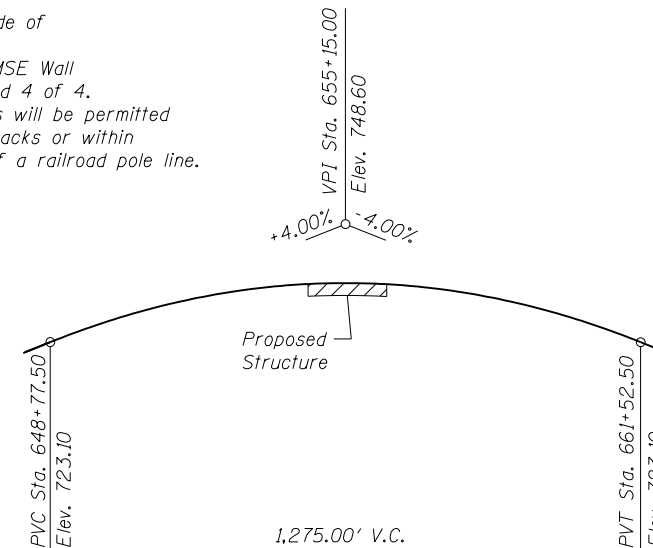
F.A.S. Rte 1107 - Dauberman Road
Functional Class: Major Collector
ADT: 1,200 (2015); 7,800 (2040)
ADTT: 96 (2015); 624 (2040)
DHW: 1,035
Design Speed: 60 m.p.h.
Posted Speed: 55 m.p.h.
Two-Way Traffic
Directional Distribution: 50/50

LOADING HL-93

Allow 50#/sq. ft. for future wearing surface.

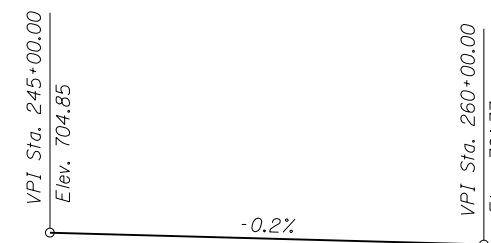
SEISMIC DATA

Seismic Performance Zone (SPZ) = 1
Design Spectral Acceleration at 1.0 sec. (S_{D1}) = 0.091
Design Spectral Acceleration at 0.2 sec. (S_{D5}) = 0.164
Soil Site Class = D



PROFILE GRADE DAUBERMAN

Along ϕ Dauberman Rd.

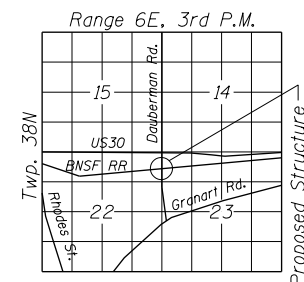


PROFILE GRADE BNSF

Along ϕ BNSF Railroad

LEGEND

Approximate Limits of Removal
of Unsuitable Material for Structures



LOCATION SKETCH

GENERAL PLAN
DAUBERMAN ROAD (F.A.S. 1107)
OVER BNSF RAILROAD
SECTION 15-00277-01-BR
KANE COUNTY
STATION 654+70.12
STRUCTURE NO. 045-3401

7/30/2018 11:36:31 AM



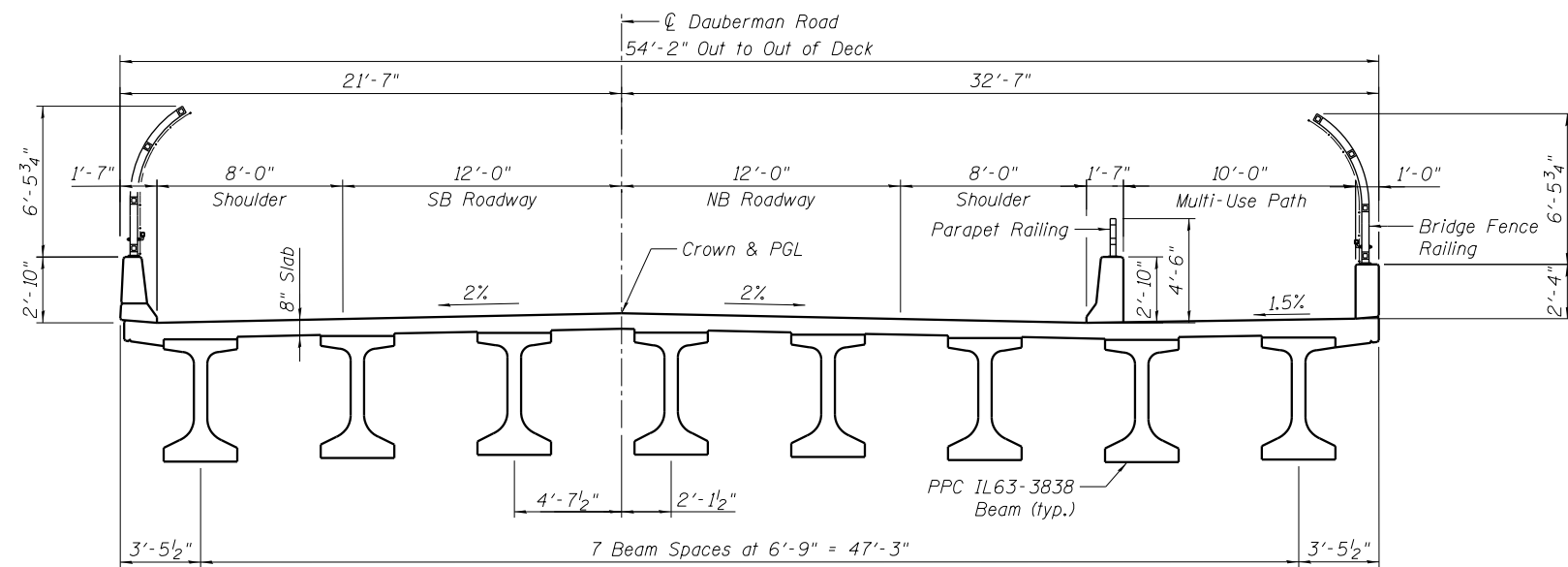
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	CHECKED - MDS	REVISED -
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PLOT DATE = 12-01-2017	CHECKED - MDS	REVISED -

KANE COUNTY
DIVISION OF TRANSPORTATION

GENERAL PLAN
DAUBERMAN ROAD OVER BNSF RAILROAD SN 045-3401

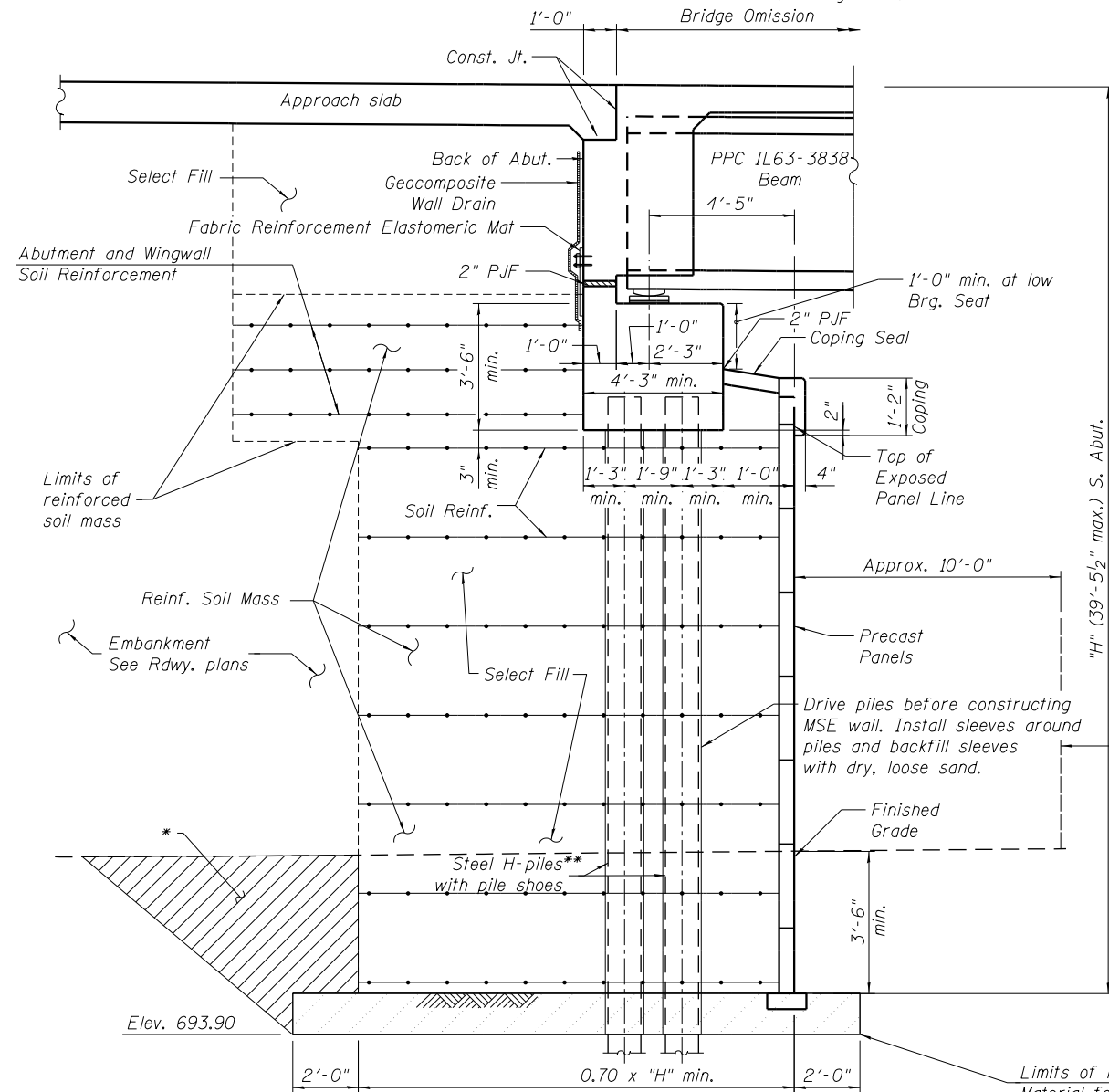
SHEET NO. 1 OF 4 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1107	15-00277-01-BR	Kane	4	1
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				



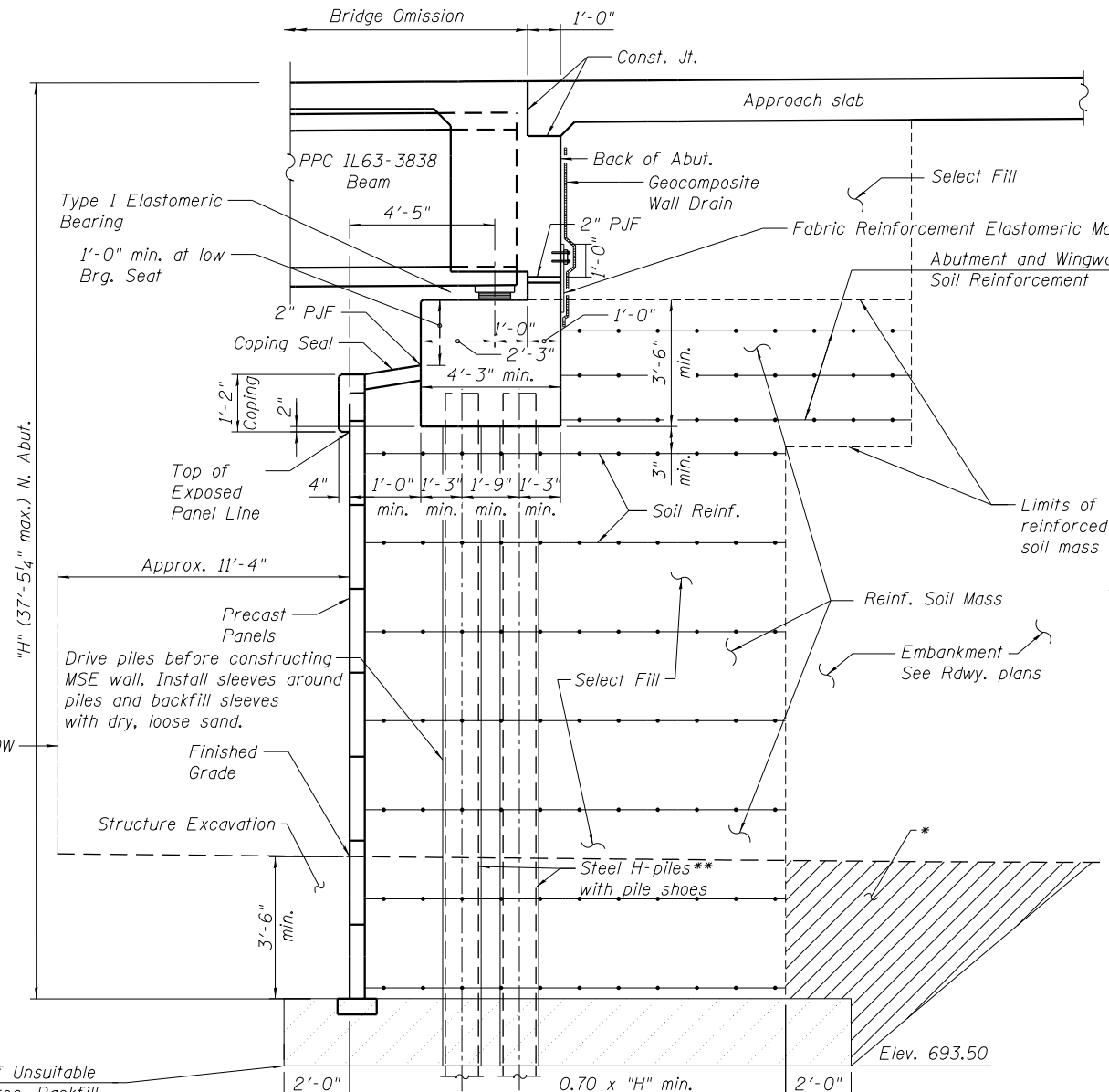
CROSS SECTION THRU DECK

(Looking North)



SECTION THRU SOUTH ABUTMENT

(Horiz. dim at Rt. L's)



SECTION THRU NORTH ABUTMENT

(Horiz. dim at Rt. L's)

** Piles shall be driven before the construction of MSE walls. 21" ϕ corrugated galvanized steel pipe, 10 gage min. shall be provided from the bottom of the abutment cap to the bottom of the excavation level.

* Overexcavation beyond structure excavation and removal of unsuitable material.

DETAILS
DAUBERMAN ROAD (F.A.S. 1107)
OVER BNSF RAILROAD
KANE COUNTY
SECTION 15-00277-01-BR
STATION 654+67.62
STRUCTURE NO. 045-3401

7/30/2018 10:47:53 AM

TranSystems

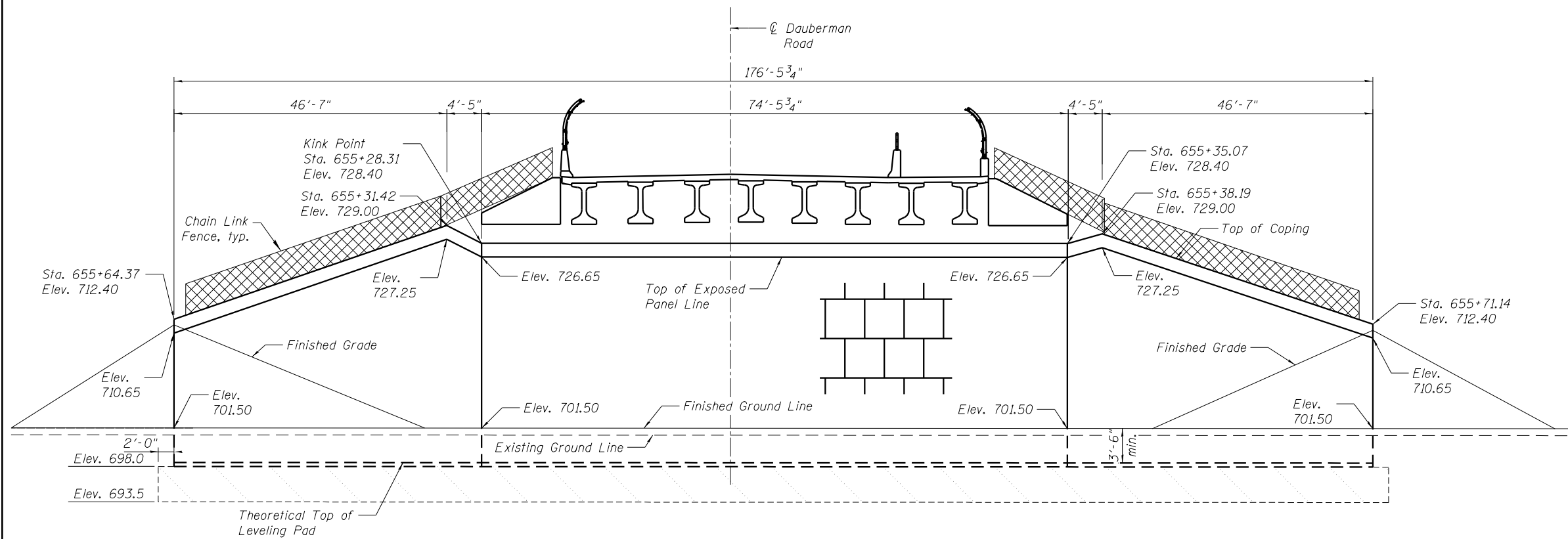
USER NAME = mdsantford	DESIGNED TLR	REVISED -
	CHECKED MDS	REVISED -
PLOT SCALE = 1/16" = 1'-0"	DRAWN MDS	REVISED -
PLOT DATE = 12-01-2017	CHECKED TLR	REVISED -

KANE COUNTY
DIVISION OF TRANSPORTATION

DETAILS
DAUBERMAN ROAD OVER BNSF RAILROAD SN 045-3401

SHEET NO. 2 OF 4 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1107	15-00277-01-BR	Kane	4	2
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

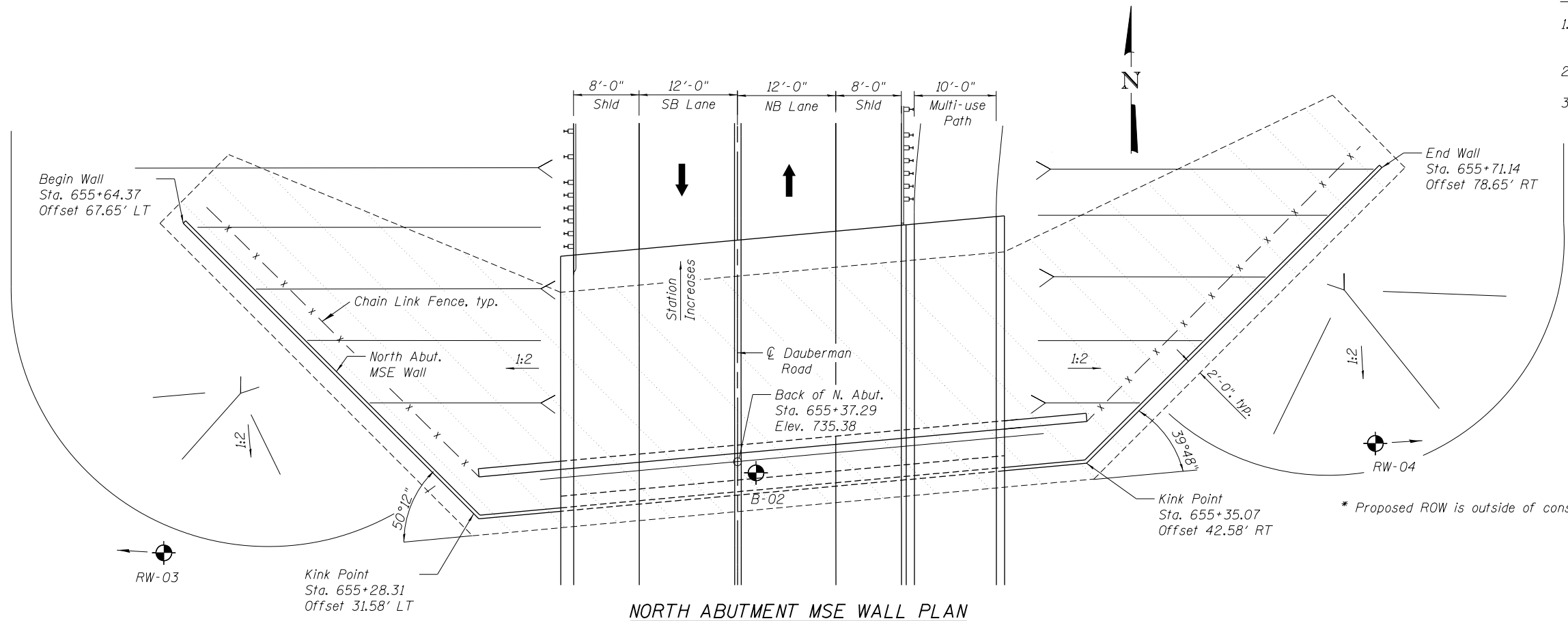


NORTH ABUTMENT MSE WALL DEVELOPED ELEVATION

(Looking North)

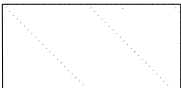
Notes:

1. Wall stations offsets are given to the front face of wall and are measured from the ϕ of Dauberman Road.
2. Wall to be built in conjunction with new bridge.
3. Horizontal dimensions measured along front face of precast panels.



NORTH ABUTMENT MSE WALL PLAN

LEGEND



Approximate Limits of Removal of Unsuitable Material for Structures

PLAN AND ELEVATION
NORTH ABUTMENT MSE WALL
DAUBERMAN ROAD (F.A.S. 1107)
OVER BNSF RAILROAD
KANE COUNTY
SECTION 15-00277-01-BR
STATION 654+67.62
STRUCTURE NO. 045-3401

* Proposed ROW is outside of construction limits.

7/30/2018 11:50:33 AM



USER NAME = mdsantford	DESIGNED TLR	REVISED -
	CHECKED MDS	REVISED -
PLOT SCALE = 1/16" = 1'-0"	DRAWN MDS	REVISED -
PLOT DATE = 12-01-2017	CHECKED TLR	REVISED -

KANE COUNTY
DIVISION OF TRANSPORTATION

PLAN AND ELEVATION - NORTH ABUTMENT MSE WALL
DAUBERMAN ROAD OVER BNSF RAILROAD SN 045-3401

SHEET NO. 4 OF 4 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1107	15-00277-01-BR	Kane	4	4
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

FILE NAME = Projects\CH401 - Chicago\401150086 - Kane County - Dauberman Road Extension - Hydraulics - Location Drainage Study\LOS.Existing

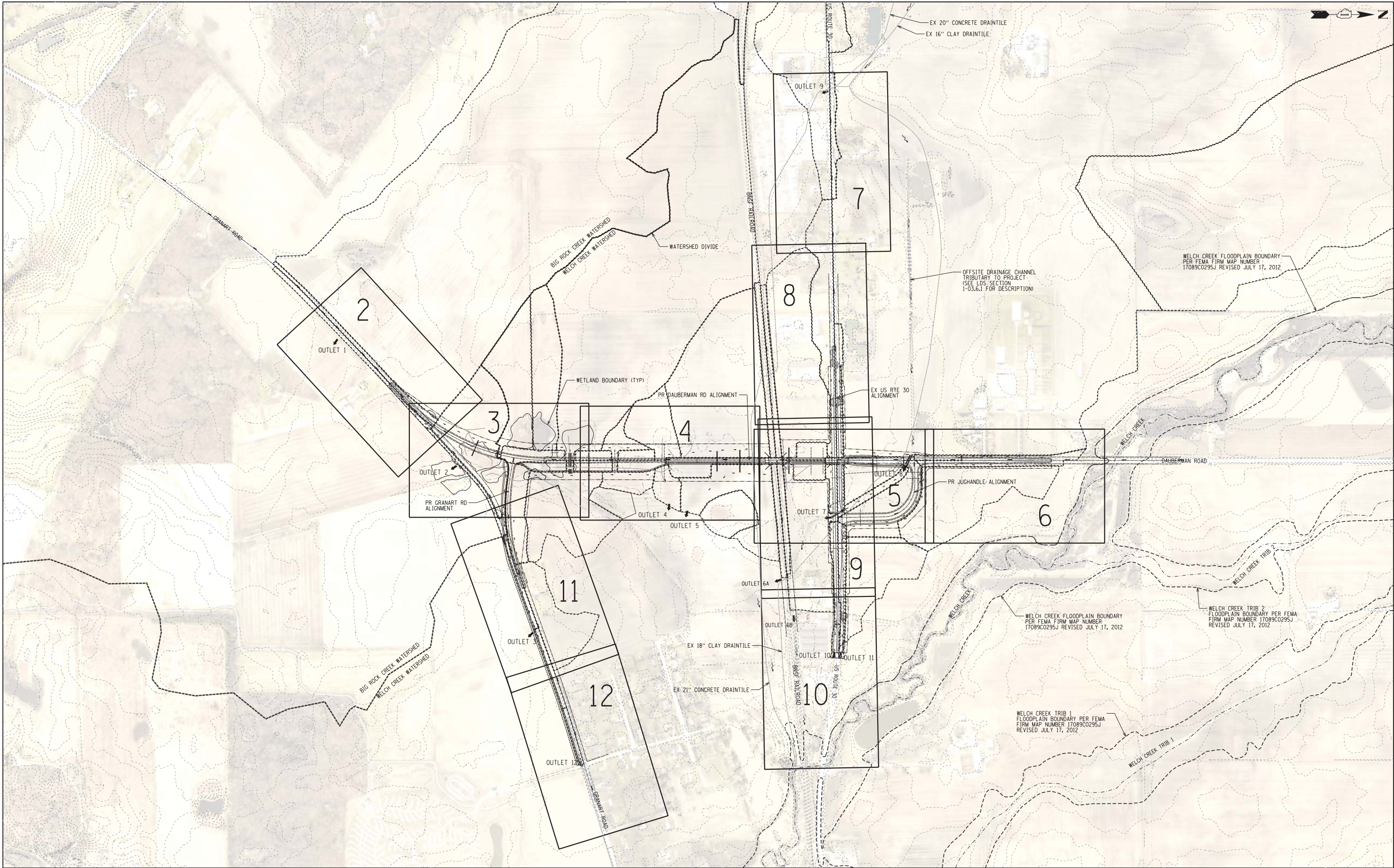


EXHIBIT 13



USER NAME = mkrzynski	DESIGNED - MK	REVISED - 10/13/2017
	DRAWN - MK	REVISED - 11/09/2017
PLOT SCALE = 1"=400'	CHECKED - KEK	REVISED - 03/08/2018
PLOT DATE = 11/28/2017	DATE - 02/01/2017	REVISED -

**KANE COUNTY
DIVISION OF TRANSPORTATION**

**PROPOSED DRAINAGE PLAN KEY
DAUBERMAN ROAD EXTENSION**

SCALE: 1"=400' SHEET 1 OF 24 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		KANE	24	1
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

FILE NAME = Projects\CH401 - Chicago\40150086 - Kane County - Dauberman Road Extension - Hydraulics - Location Drainage Study\05.Sheets\05.EXISTING

LEGEND

EX ROW

PR ROW

WATERSHED DIVIDE

DRAINAGE AREA

WETLAND

OUTLET

SHEET FLOW

OVERFLOW

STORM SEWER/CULVERT

DRAIN TILE

SANITARY SEWER

DITCH

SUMMIT

CULVERT END SECTIONS

CATCH BASIN

INLET

MANHOLE

KEY MAP

USER NAME = mkrzynski	DESIGNED - MK	REVISED - 10/13/2017
	DRAWN - MK	REVISED - 11/09/2017
PLOT SCALE = 1"=50'	CHECKED - KEK	REVISED - 03/08/2018
PLOT DATE = 11/28/2017	DATE - 02/01/2017	REVISED -

KANE COUNTY
DIVISION OF TRANSPORTATION

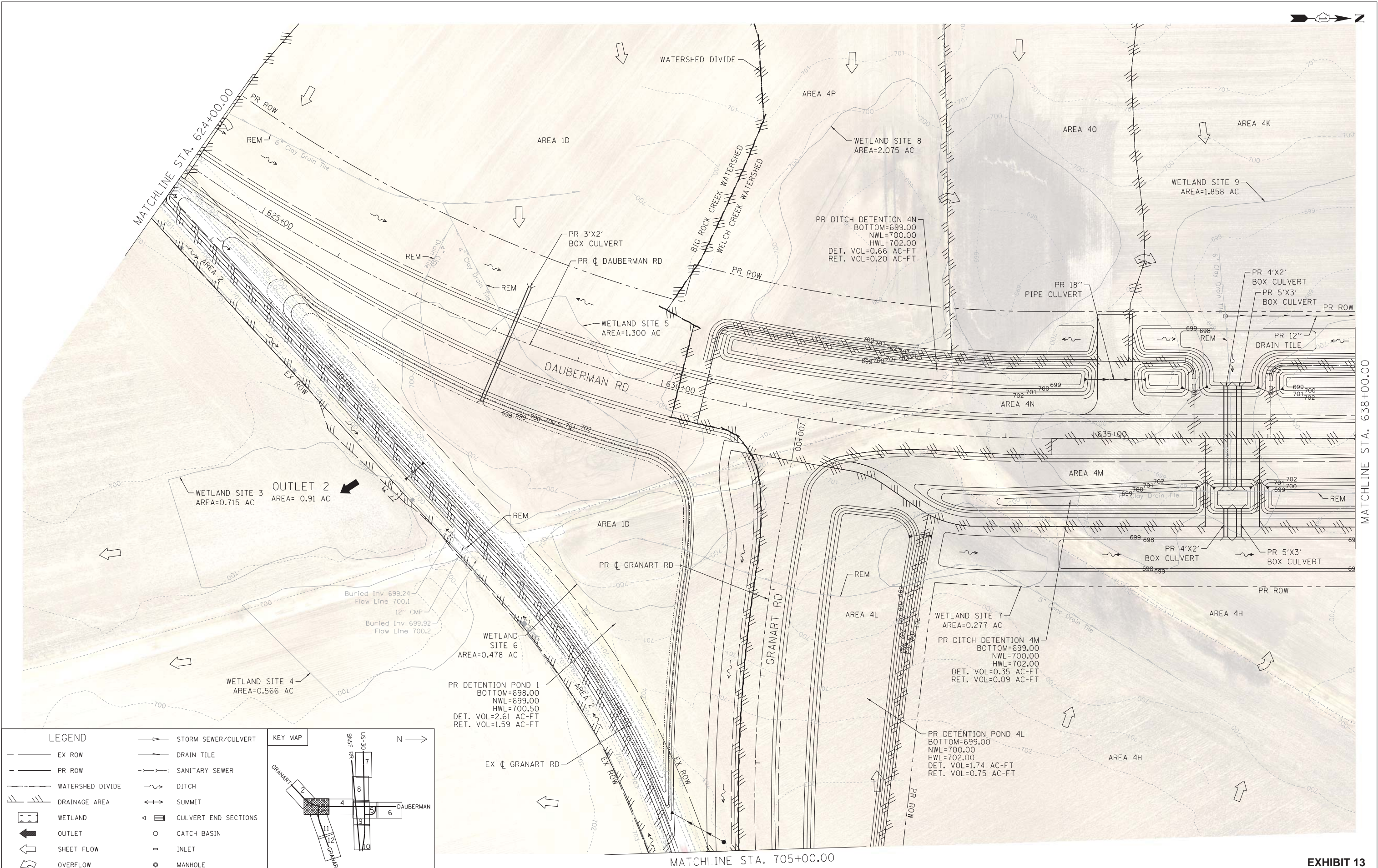
PROPOSED DRAINAGE PLAN
DAUBERMAN ROAD EXTENSION

SCALE: 1"=50' SHEET 2 OF 24 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		KANE	24	2
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

EXHIBIT 13

FILE NAME = Projects\CH401 - Chicago\40150086 - Kane County - Dauberman Road Extension - Hydraulics - Location Drainage Study\LOS.Sheets\LOS.EXISTING



LEGEND

- EX ROW
- PR ROW
- WATERSHED DIVIDE
- DRAINAGE AREA
- WETLAND
- OUTLET
- SHEET FLOW
- OVERFLOW
- STORM SEWER/CULVERT
- DRAIN TILE
- SANITARY SEWER
- DITCH
- SUMMIT
- CULVERT END SECTIONS
- CATCH BASIN
- INLET
- MANHOLE

KEY MAP

Bollinger, Lach & Associates, Inc.
ITASCA, ILLINOIS

USER NAME = mkrzynski	DESIGNED - MK	REVISED - 10/13/2017
	DRAWN - MK	REVISED - 11/09/2017
PLOT SCALE = 1"=50'	CHECKED - KEK	REVISED - 03/08/2018
PLOT DATE = 11/28/2017	DATE - 02/01/2017	REVISED -

**KANE COUNTY
DIVISION OF TRANSPORTATION**

**PROPOSED DRAINAGE PLAN
DAUBERMAN ROAD EXTENSION**

SCALE: 1"=50' SHEET 3 OF 24 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		KANE	24	3
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

EXHIBIT 13

FILE NAME = Projects\CH401 - Chicago\401\50086 - Kane County - Dauberman Road Extension - Hydraulics - Location Drainage Study\LD5.Sheets\LD5.EXISTING

LEGEND

EX ROW

PR ROW

WATERSHED DIVIDE

DRAINAGE AREA

WETLAND

OUTLET

SHEET FLOW

OVERFLOW

STORM SEWER/CULVERT

DRAIN TILE

SANITARY SEWER

DITCH

SUMMIT

CULVERT END SECTIONS

CATCH BASIN

INLET

MANHOLE

KEY MAP

USER NAME = mkrzynski	DESIGNED - MK	REVISED - 10/13/2017
PLOT SCALE = 1"=50'	DRAWN - MK	REVISED - 11/09/2017
PLOT DATE = 11/28/2017	CHECKED - KEK	REVISED - 03/08/2018
	DATE - 02/01/2017	REVISED -

KANE COUNTY
DIVISION OF TRANSPORTATION

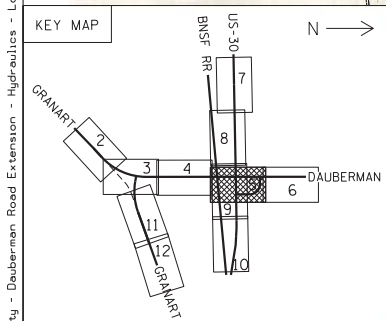
PROPOSED DRAINAGE PLAN
DAUBERMAN ROAD EXTENSION

SCALE: 1"=50' SHEET 4 OF 24 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		KANE	24	4
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

EXHIBIT 13

FILE NAME = Projects\CH401 - Chicago\401\50086 - Kane County - Dauberman Road Extension - Hydraulics - Location Drainage Study\LOS.Sheets\LOS.EXISTING



LEGEND	
	EX ROW
	PR ROW
	WATERSHED DIVIDE
	DRAINAGE AREA
	WETLAND
	OUTLET
	SHEET FLOW
	OVERFLOW
	STORM SEWER/CULVERT
	DRAIN TILE
	SANITARY SEWER
	DITCH
	SUMMIT
	CULVERT END SECTIONS
	CATCH BASIN
	INLET
	MANHOLE

B Bollinger, Lach & Associates, Inc.
ITASCA, ILLINOIS

USER NAME = mkrzynski
PLOT SCALE = 1"=50'
PLOT DATE = 11/28/2017

DESIGNED - MK
DRAWN - MK
CHECKED - KEK
DATE - 02/01/2017

REVISED - 10/13/2017
REVISED - 11/09/2017
REVISED - 03/08/2018
REVISED -

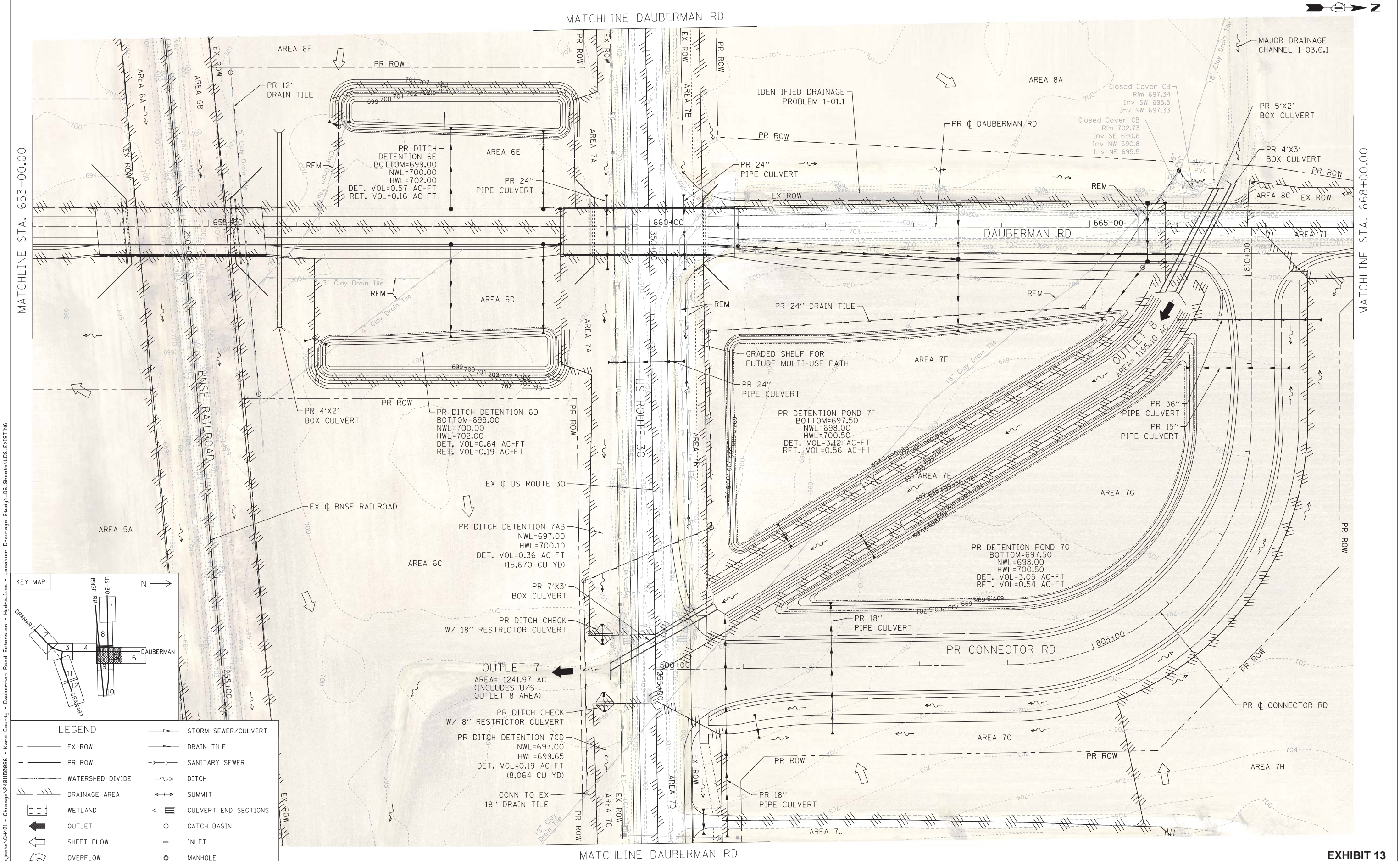
KANE COUNTY
DIVISION OF TRANSPORTATION

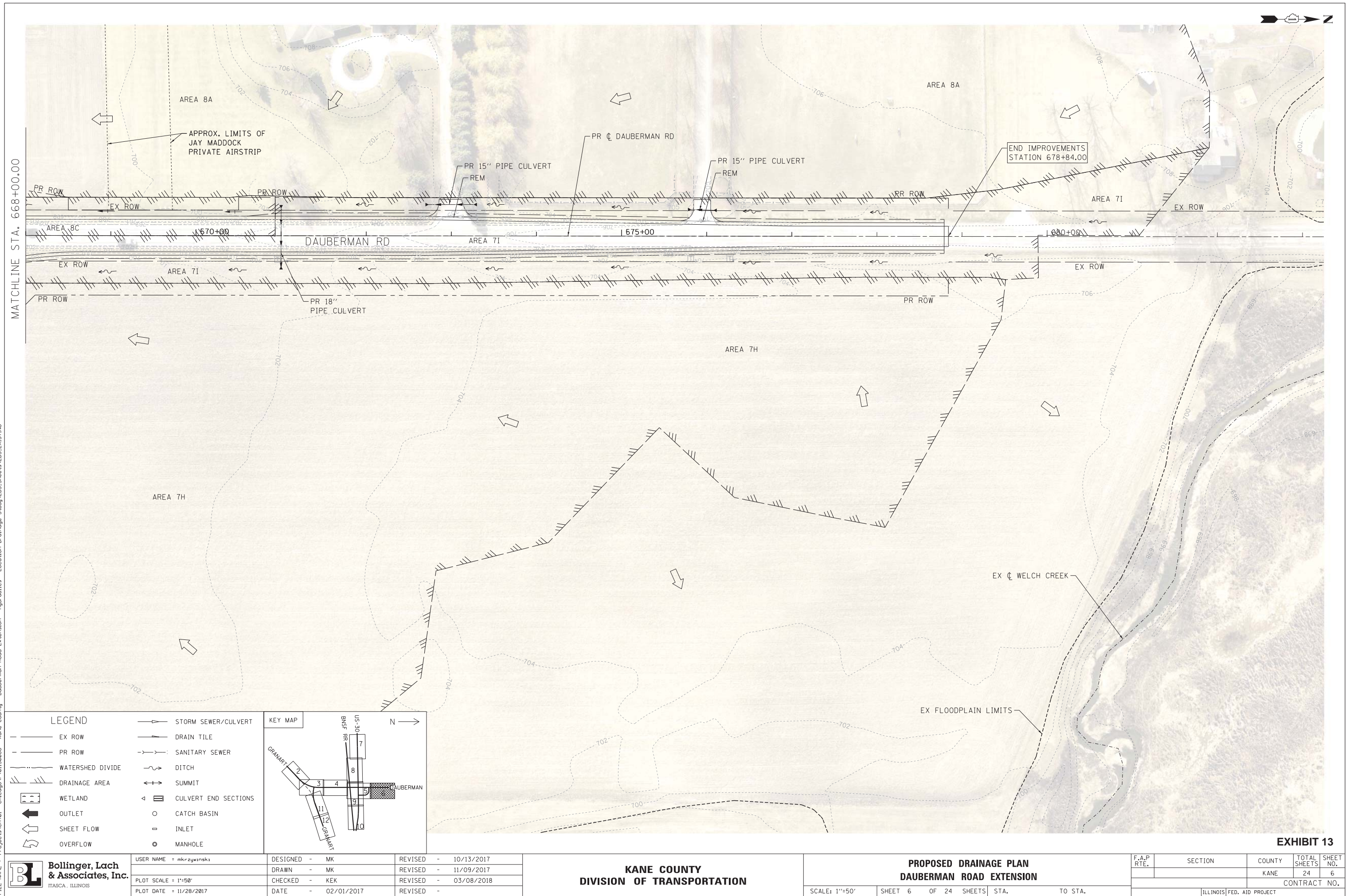
PROPOSED DRAINAGE PLAN
DAUBERMAN ROAD EXTENSION

SCALE: 1"=50' SHEET 5 OF 24 SHEETS STA. TO STA.

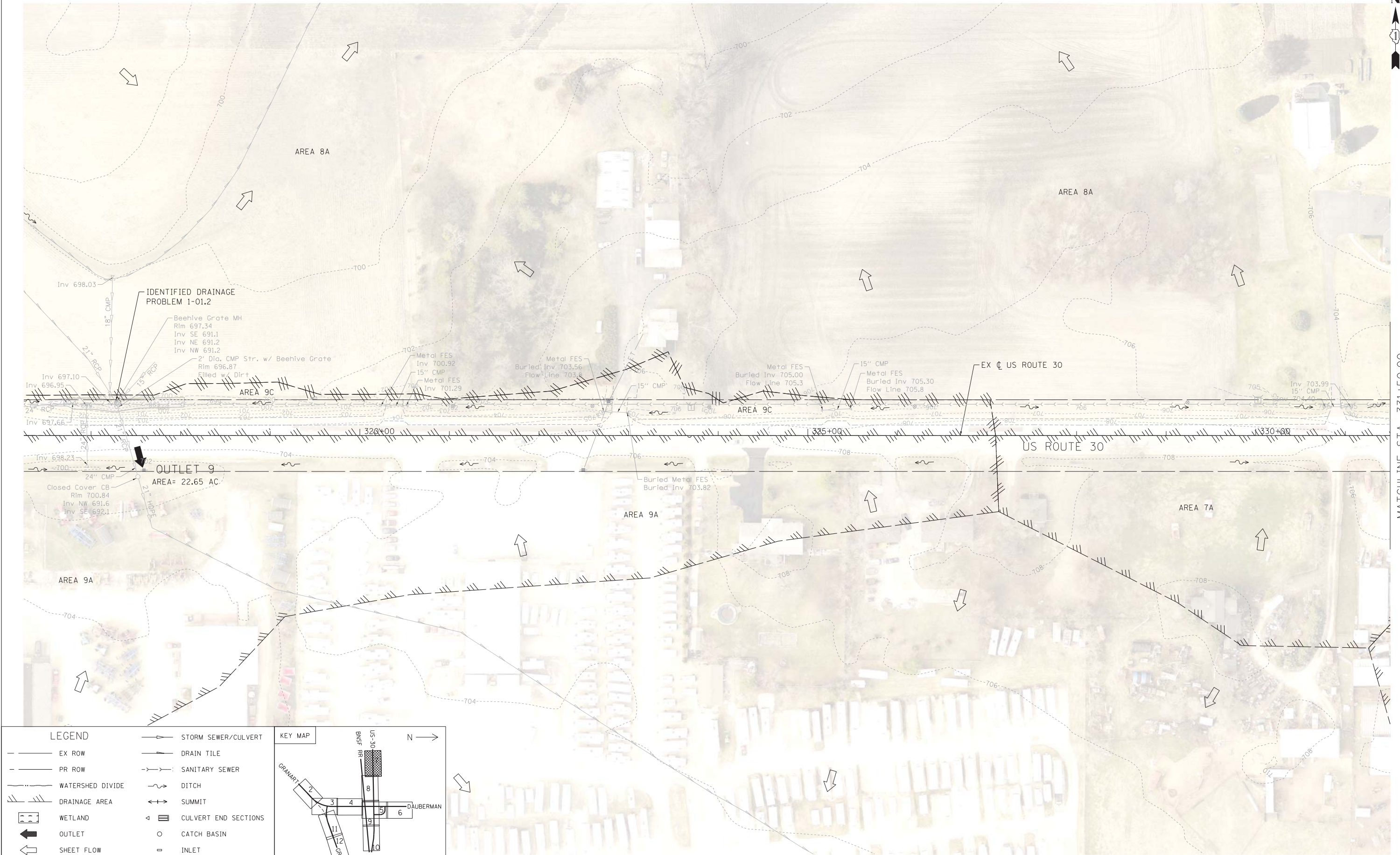
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		KANE	24	5
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

EXHIBIT 13





FILE NAME = Projects\CH401 - Chicago\40150086 - Kane County - Dauberman Road Extension - Hydraulics - Location Drainage Study\LD5-Sheets\LD5-EXISTING



LEGEND

- EX ROW
- PR ROW
- WATERSHED DIVIDE
- DRAINAGE AREA
- WETLAND
- OUTLET
- SHEET FLOW
- OVERFLOW

- STORM SEWER/CULVERT
- DRAIN TILE
- SANITARY SEWER
- DITCH
- SUMMIT
- CULVERT END SECTIONS
- CATCH BASIN
- INLET
- MANHOLE

KEY MAP

	USER NAME = mkrzynski		DESIGNED - MK	REVISED - 10/13/2017
	DRAWN - MK		REVISOR - MK	REVISED - 11/09/2017
	PLOT SCALE = 1"=50'		CHECKED - KEK	REVISED - 03/08/2018
	PLOT DATE = 11/28/2017		DATE - 02/01/2017	REVISED -

**KANE COUNTY
DIVISION OF TRANSPORTATION**

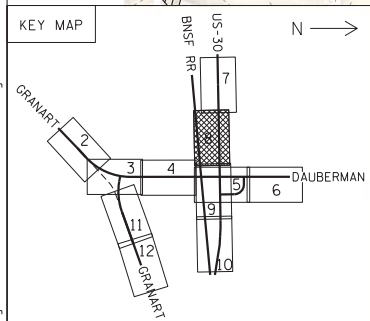
PROPOSED DRAINAGE PLAN DAUBERMAN ROAD EXTENSION			
SCALE: 1"=50'	SHEET 7	OF 24 SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		KANE	24	7
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

EXHIBIT 13

MATCHLINE STA. 331+50.00

FILE NAME = Projects\CH401 - Chicago\40150086 - Kane County - Dauberman Road Extension - Hydraulics - Location Drainage Study\LD5.Sheets\LD5.EXISTING



LEGEND	
	EX ROW
	PR ROW
	WATERSHED DIVIDE
	DRAINAGE AREA
	WETLAND
	OUTLET
	SHEET FLOW
	OVERFLOW
	STORM SEWER/CULVERT
	DRAIN TILE
	SANITARY SEWER
	DITCH
	SUMMIT
	CULVERT END SECTIONS
	CATCH BASIN
	INLET
	MANHOLE

Bollinger, Lach & Associates, Inc.
ITASCA, ILLINOIS

USER NAME = mkrzynski	DESIGNED - MK	REVISED - 10/13/2017
PLOT SCALE = 1"=50'	DRAWN - MK	REVISED - 11/09/2017
PLOT DATE = 11/28/2017	CHECKED - KEK	REVISED - 03/08/2018
	DATE - 02/01/2017	REVISED -

DESIGNED - MK	REVISED - 10/13/2017
DRAWN - MK	REVISED - 11/09/2017
CHECKED - KEK	REVISED - 03/08/2018
DATE - 02/01/2017	REVISED -

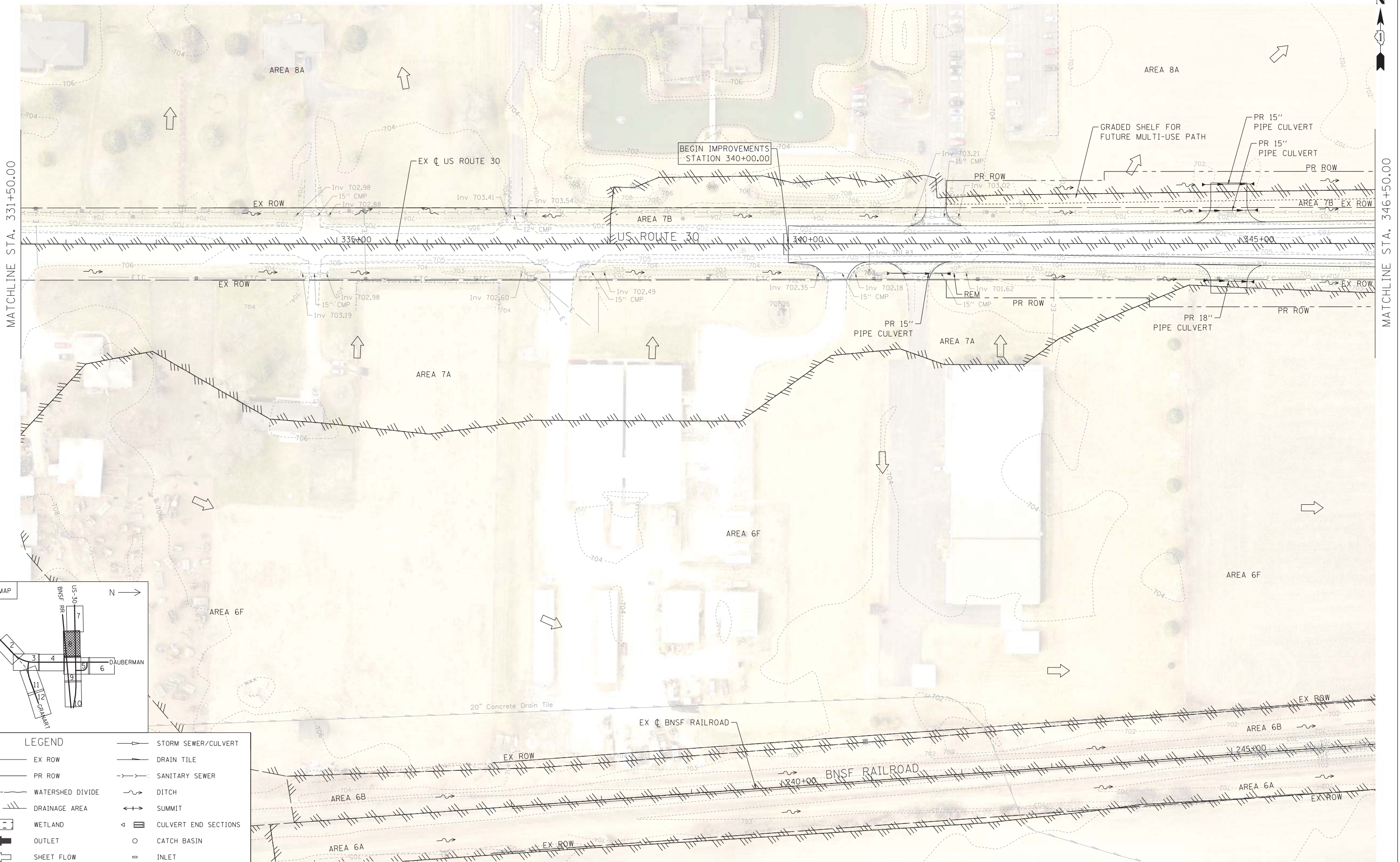
**KANE COUNTY
DIVISION OF TRANSPORTATION**

**PROPOSED DRAINAGE PLAN
DAUBERMAN ROAD EXTENSION**

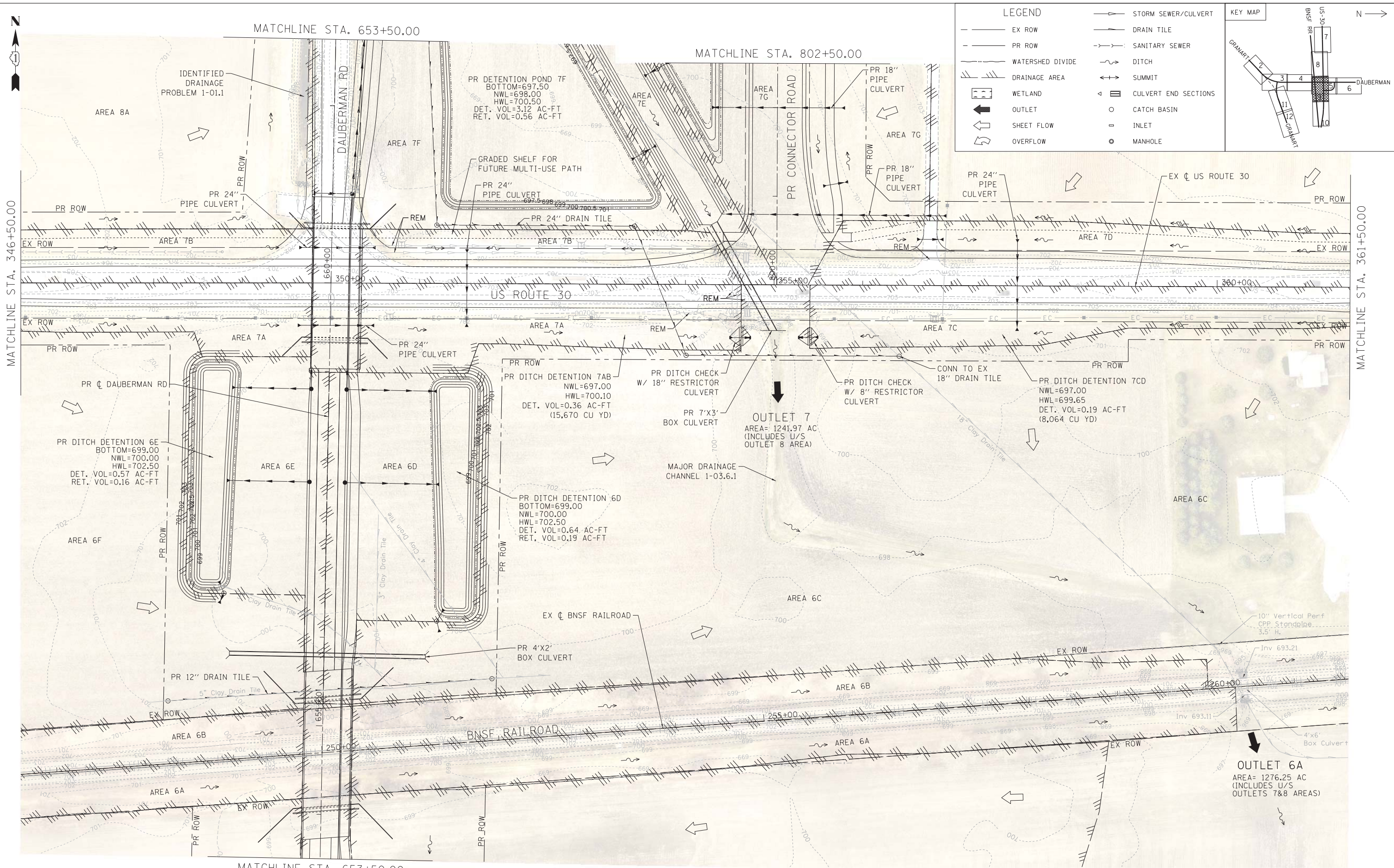
SCALE: 1"=50' SHEET 8 OF 24 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		KANE	24	8
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

EXHIBIT 13



FILE NAME = Projects\CH401 - Chicago\40150086 - Kane County - Dauberman Road Extension - Hydraulics - Location Drainage Study\05.Sheets\05.EXISTING



LEGEND

---	EX ROW	—>—	STORM SEWER/CULVERT
---	PR ROW	—>—	DRAIN TILE
---	WATERSHED DIVIDE	—>—	SANITARY SEWER
---	DRAINAGE AREA	—>—	DITCH
---	WETLAND	—>—	SUMMIT
---	OUTLET	—>—	CULVERT END SECTIONS
---	SHEET FLOW	○	CATCH BASIN
---	OVERFLOW	○	INLET
		○	MANHOLE

KEY MAP

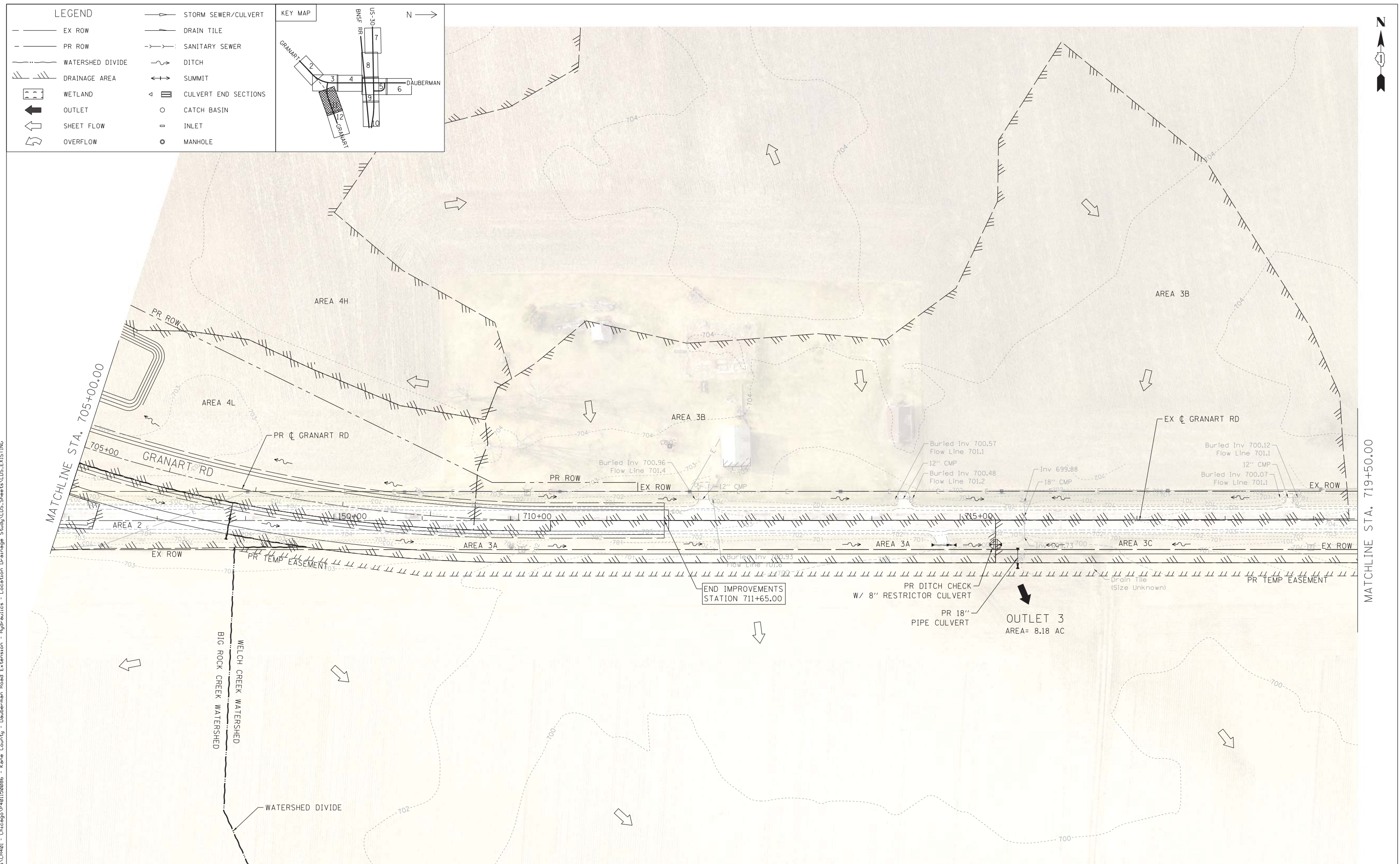


EXHIBIT 13



FILE NAME = Projects\CH401 - Chicago\40150086 - Kane County - Dauberman Road Extension - Hydraulics - Location Drainage Study\05.Sheets\05.EXISTING

B Bollinger, Lach & Associates, Inc.
ITASCA, ILLINOIS

USER NAME = mkrzynski	DESIGNED - MK	REVISED - 10/13/2017
	DRAWN - MK	REVISED - 11/09/2017
PLOT SCALE = 1"=50'	CHECKED - KEK	REVISED - 03/08/2018
PLOT DATE = 11/28/2017	DATE - 02/01/2017	REVISED -

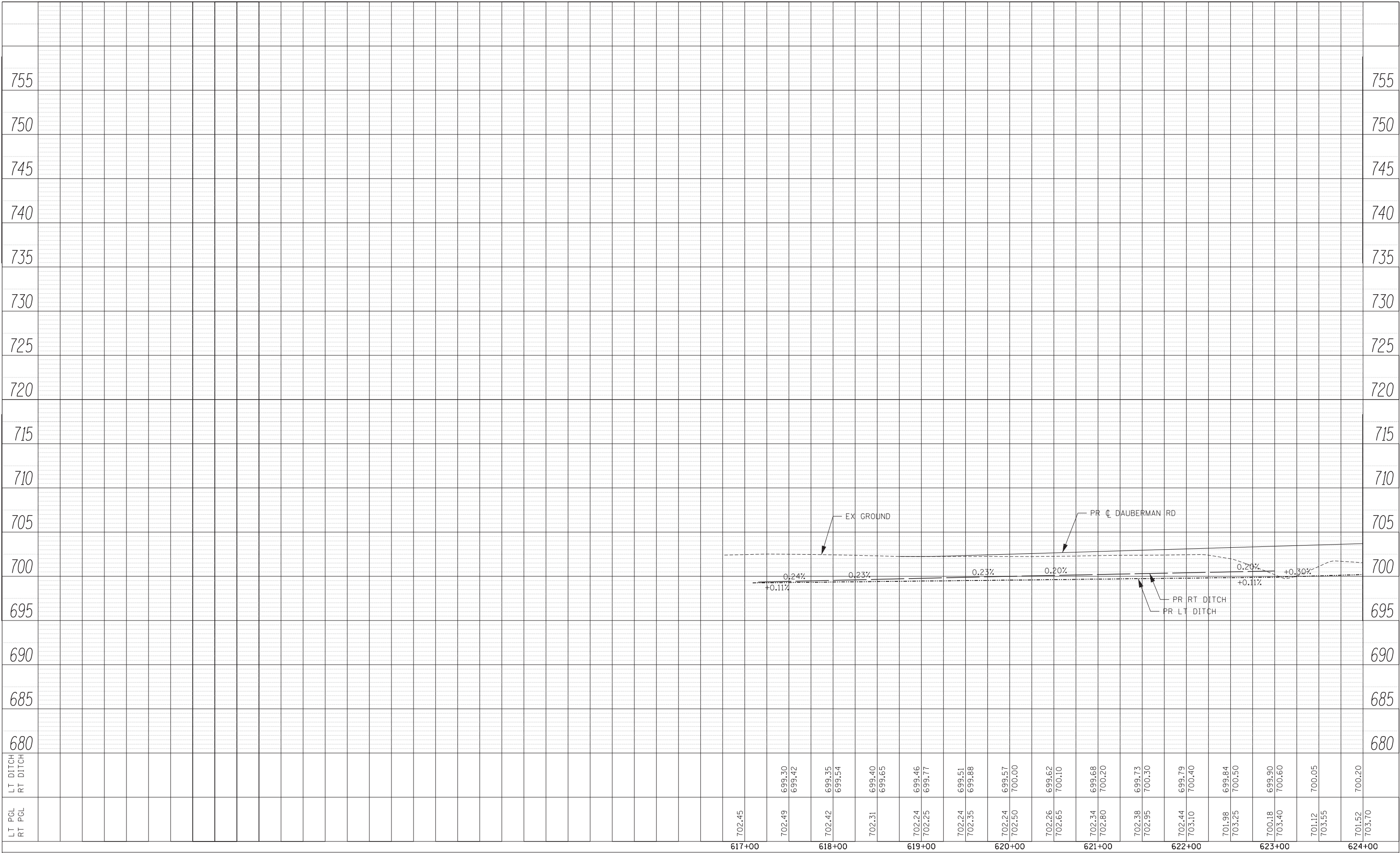
**KANE COUNTY
DIVISION OF TRANSPORTATION**

PROPOSED DRAINAGE PLAN DAUBERMAN ROAD EXTENSION			
SCALE: 1"=50'	SHEET 12 OF 24 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		KANE	24	12
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

EXHIBIT 13

FILE NAME = Projects\CH401 - Chicago\1401150886 - Kane County - Dauberman Road Extension - Hydraulics - Location Drainage Study\LOS-Sheets\LOS-EXISTING



Bollinger, Lach & Associates, Inc.
ITASCA, ILLINOIS

USER NAME = mkrzynski	DESIGNED - MK	REVISED - 03/09/2018
	DRAWN - MK	REVISED -
PLOT SCALE = 1"=50'	CHECKED - KEK	REVISED -
PLOT DATE = 11/28/2017	DATE - 11/07/2017	REVISED -

KANE COUNTY
DIVISION OF TRANSPORTATION

PROPOSED DRAINAGE PROFILE DAUBERMAN ROAD EXTENSION	
SCALE: 1"=50'	SHEET 13 OF 24 SHEETS
STA.	TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		KANE	24	13
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

ELEVATION		STATION	
755	750	745	740
735	730	725	720
715	710	705	700
695	690	685	680
LT PCL	RT PCL	LT DITCH	RT DITCH
701.52	703.70	700.20	700.20
701.15	700.05		
703.85			
700.83	699.90		
704.00			
700.60	699.75		
704.15			
700.54	699.60		
704.30			
700.58	699.45		
704.45			
700.38	699.30		
704.60			
700.28	699.10		
704.75			
700.32	698.90		
704.90			
699.77	699.10		
705.05			
699.74	699.60		
705.20			
700.05	700.03		
705.35			
700.26	700.45		
705.50			
700.31	705.65		
705.80			
700.87	705.95		
706.10			
701.72	706.19		
706.13			
701.65	705.98		
705.86			
700.56	705.83		
699.86	699.60		
705.68	698.20		
699.69	699.80		
705.53	698.00		
699.53	705.38		
699.13	698.00		
705.23	698.00		
699.01	698.00		
705.08	698.00		
699.65	698.00		
704.93	698.00		
700.03	698.00		
704.78	698.00		
699.94	698.35		
704.71	698.00		



**Bollinger, Lach
& Associates, Inc.**
ITASCA, ILLINOIS

USER NAME = mkrzywinski	DESIGNED - MK	REVISED - 03/09/2018
	DRAWN - MK	REVISED -
PLOT SCALE = 1"=50'	CHECKED - KEK	REVISED -
PLOT DATE = 11/28/2017	DATE - 11/07/2017	REVISED -

PROPOSED DRAINAGE PROFILE

DAUBERMAN ROAD EXTENSION

SCALE: 1"=50'	SHEET 14	OF 24 SHEETS	STA.	TO STA.
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F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		KANE	24	14
		CONTRACT NO.		
ILLINOIS		FED. AID PROJECT		

FILE NAME = Projects\CH40 - Chicago\40150086 - Kane County - Dauberman Road Extension - Hydraulics - Location Drainage Study\LOS-Sheets\LOS-EXISTING

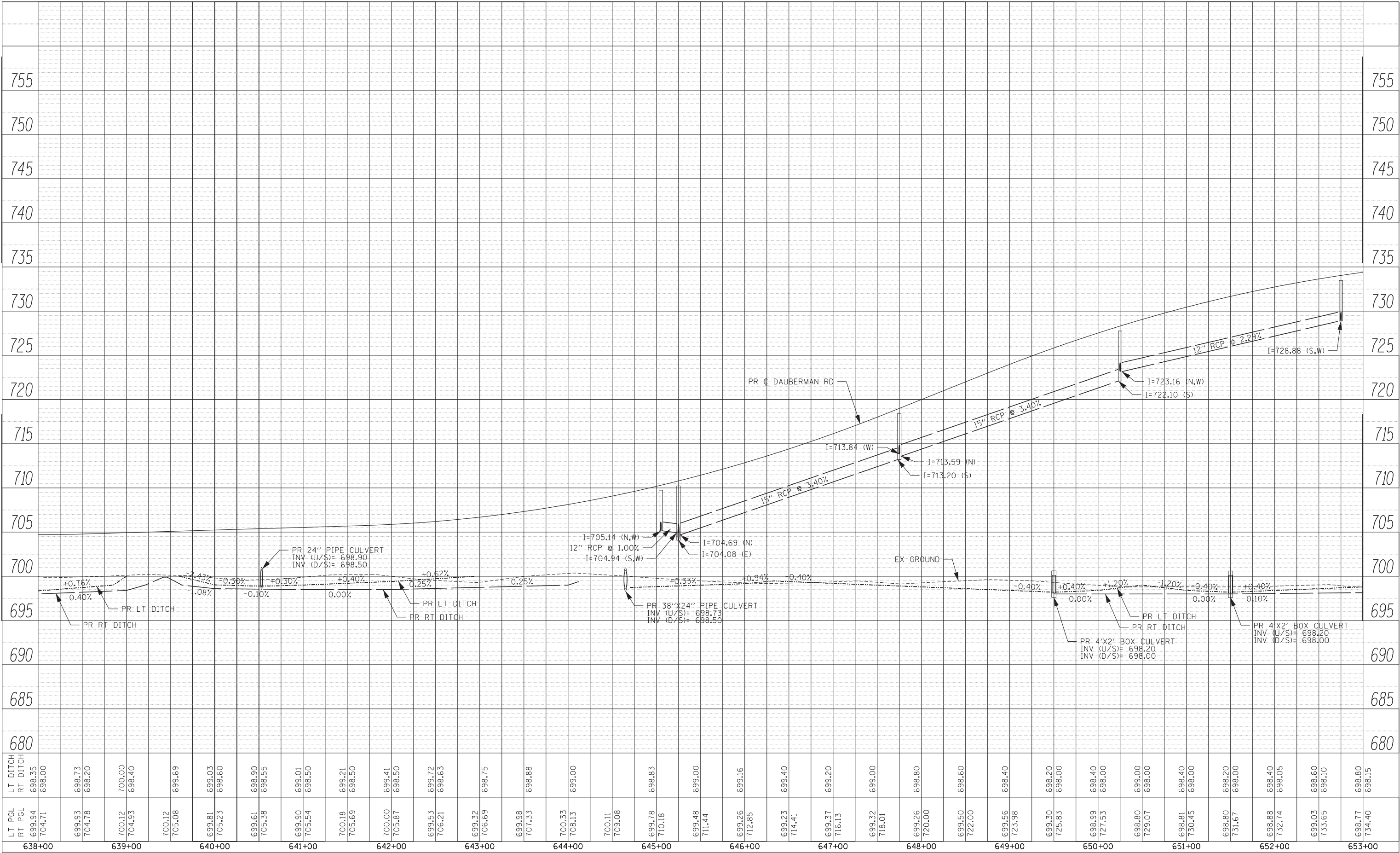


EXHIBIT 13

 Bollinger, Lach & Associates, Inc. ITASCA, ILLINOIS	USER NAME = mkrzynski	DESIGNED - MK	REVISED - 03/09/2018	KANE COUNTY DIVISION OF TRANSPORTATION	PROPOSED DRAINAGE PROFILE DAUBERMAN ROAD EXTENSION			F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 1"=50'	CHECKED - KEK	REVISED -							KANE	24	15
	PLOT DATE = 11/28/2017	DATE - 11/07/2017	REVISED -		SCALE: 1"=50'			SHEET 15 OF 24 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	
	CONTRACT NO.											

FILE NAME = Projects\CH40 - Chicago\40150886 - Kane County - Dauberman Road Extension - Hydraulics - Location Drainage Study\LOS-Sheets\LOS-EXISTING

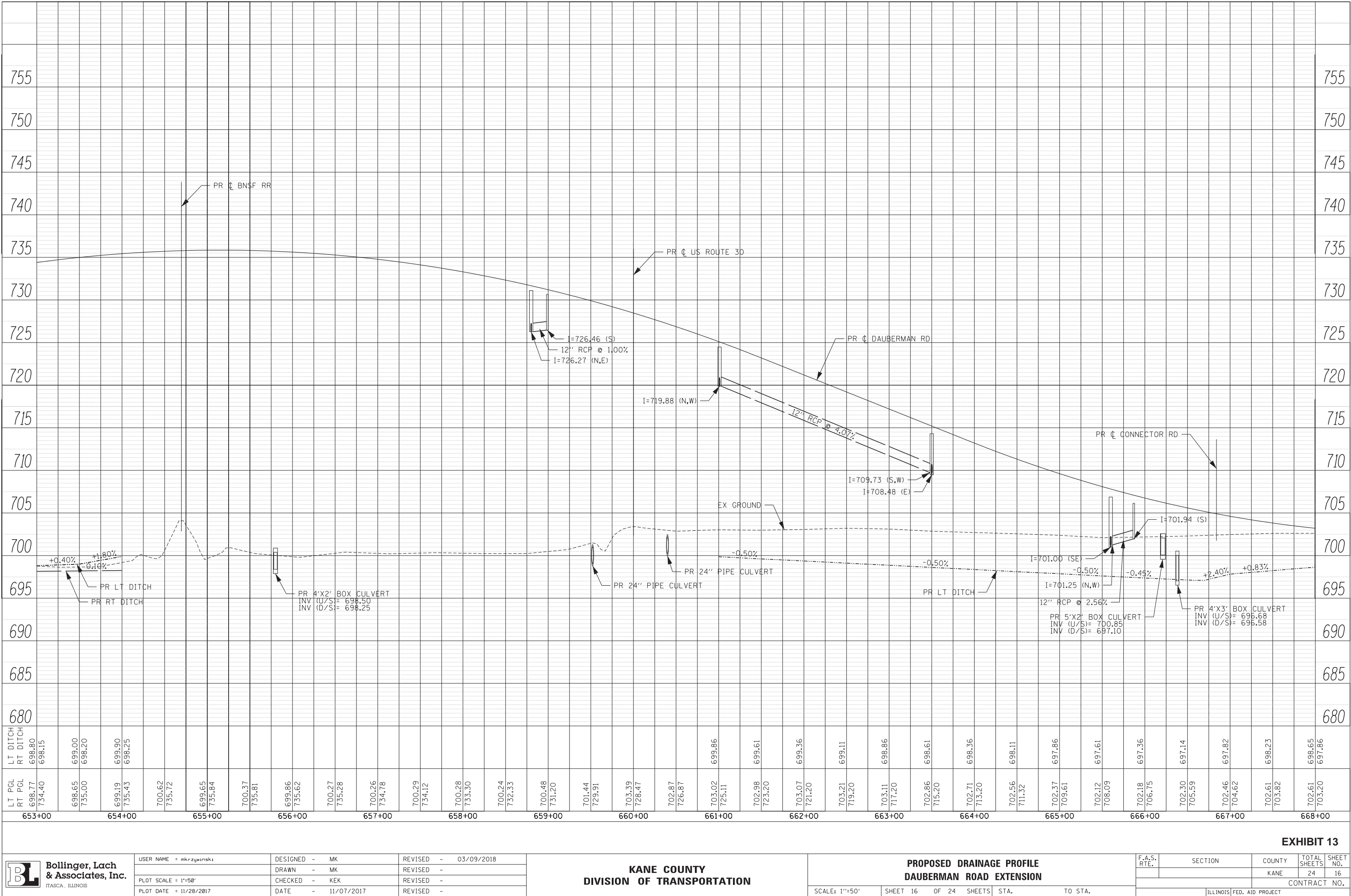


EXHIBIT 13

<div><div>B</div><div>Bollinger, Lach & Associates, Inc.</div><div>ITASCA, ILLINOIS</div></div>	USER NAME = mkrzynski		DESIGNED - MK	REVISED - 03/09/2018	KANE COUNTY DIVISION OF TRANSPORTATION				PROPOSED DRAINAGE PROFILE DAUBERMAN ROAD EXTENSION		F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 1"=50'		DRAWN - MK	REVISED -									KANE	24	16
	PLOT DATE = 11/28/2017		CHECKED - KEK	REVISED -										CONTRACT NO.	
			DATE - 11/07/2017	REVISED -							SCALE: 1"=50'	SHEET 16 OF 24 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT

FILE NAME = Projects\CH401 - Chicago\401\50086 - Kane County - Dauberman Road Extension - Hydraulics - Location Drainage Study\LOS_Sheets\LOS-EXISTING

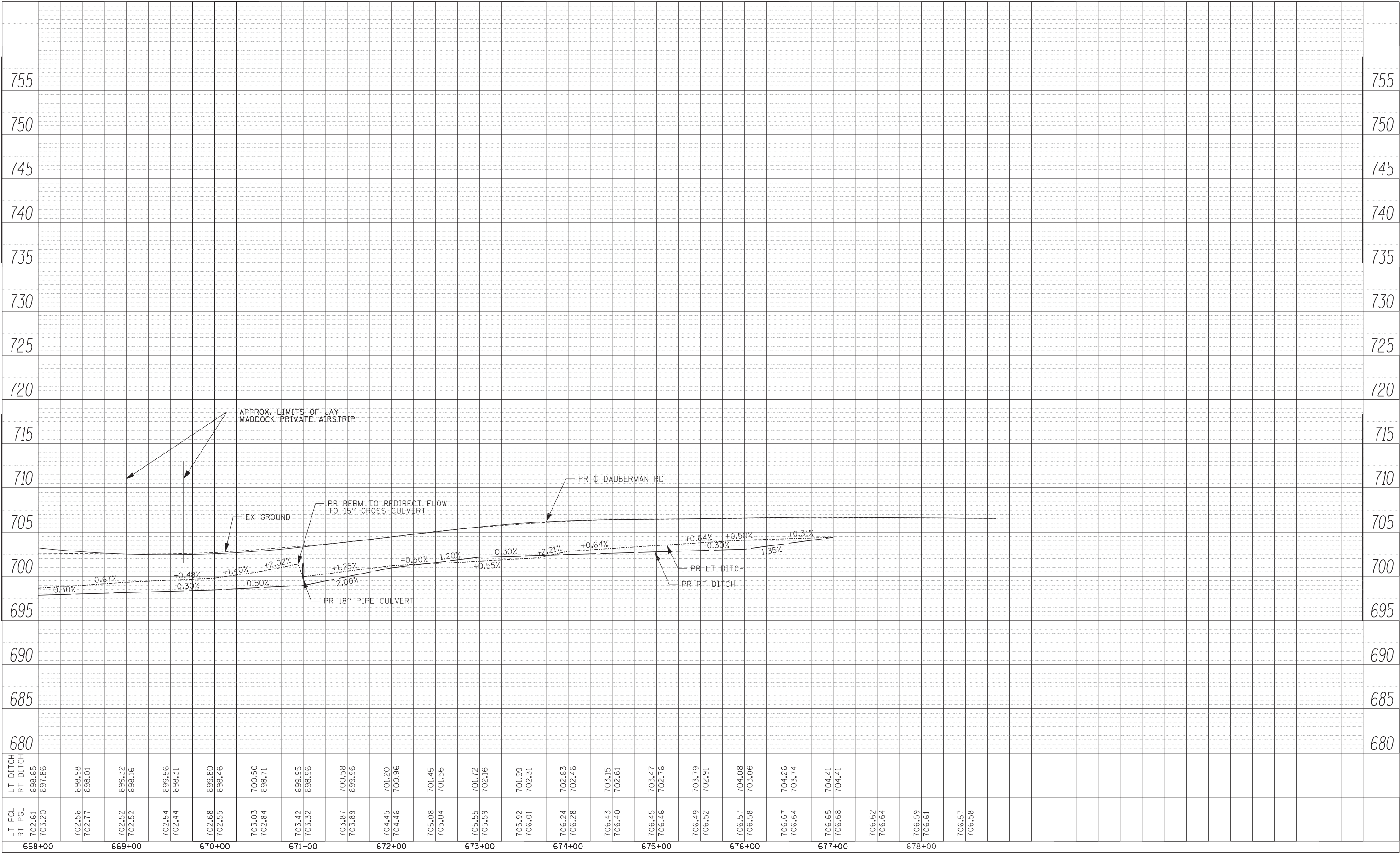


EXHIBIT 13

Bollinger, Lach & Associates, Inc.

ITASCA, ILLINOIS

USER NAME = mkrzynski

DESIGNED - MK

REVISD - 03/09/2018

DRAWN - MK

REVISD -

PLOT SCALE = 1"=50'

CHECKED - KEK

REVISD -

PLOT DATE = 11/28/2017

DATE - 11/07/2017

REVISD -

KANE COUNTY

DIVISION OF TRANSPORTATION

PROPOSED DRAINAGE PROFILE

DAUBERMAN ROAD EXTENSION

SCALE: 1"=50'

SHEET 17 OF 24 SHEETS

STA. TO STA.

F.A.S. RTE.

SECTION

COUNTY

TOTAL SHEETS

SHEET NO.

KANE

24

17

CONTRACT NO.

ILLINOIS

FED. AID PROJECT

LT PGL RT PGL		LT DITCH RT DITCH		ELEVATION	
				755	755
				750	750
				745	745
				740	740
				735	735
				730	730
				725	725
				720	720
				715	715
				710	710
				705	705
				700	700
				695	695
				690	690
				685	685
				680	680
705.91					
705.86					
705.80					
705.78					
705.75					
705.75					
705.73					
705.70					
705.64					
705.63					
705.53					
705.49					
705.50					
705.46					
705.45					
705.41					
705.40					
705.38					
705.35					
705.26	702.86				
705.30	701.70				
705.25	702.71				
705.26	701.55				
705.36	702.56				
705.21	701.40				
705.40	702.41				
705.17	701.25				
705.31	702.26				
705.18	701.10				
705.25	702.11				
705.22	700.95				
705.18	701.96				
705.31	700.80				
705.13	701.81				
705.38	700.63				
705.18	701.66				
705.37	700.45				
705.24	701.51				
705.29	700.28				

USER NAME = mkrzyżewski	DESIGNED - MK	REVISED - 03/09/2018
	DRAWN - MK	REVISED -
PLOT SCALE = 1"=50'	CHECKED - KEK	REVISED -
PLOT DATE = 11/28/2017	DATE - 11/07/2017	REVISED -

PROPOSED DRAINAGE PROFILE DAUBERMAN ROAD EXTENSION

SCALE: 1"=50'	SHEET 18 OF 24 SHEETS	STA. TO STA.
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F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		KANE	24	18
		CONTRACT NO.		
ILLINOIS		FED. AID PROJECT		

EXHIBIT 13

Station		347+00	348+00	349+00	350+00	351+00	352+00	353+00	354+00	355+00	356+00	357+00	358+00	359+00	360+00	361+00																																															
LT PGL	RT PGL	705.24	705.29	705.21	705.13	704.86	704.94	704.52	704.76	704.06	704.57	703.72	704.38	703.54	704.20	703.43	704.01	703.29	703.82	703.12	703.67	703.03	703.61	703.09	703.66	703.20	703.80	703.12	703.97	703.09	704.15	703.16	704.32	703.10	704.47	703.35	704.53	703.47	704.49	703.33	704.36	703.20	704.20	703.31	704.04	703.56	703.91	703.81	703.92	704.03	704.05	704.27	704.30	704.60	704.60	704.83	704.90	705.04	705.19	705.31	705.49	705.72	705.79
LT DITCH	RT DITCH	701.51	700.28	701.36	700.10	701.17	699.92	700.98	699.75	700.79	699.58	700.60	699.40	700.42	699.06	700.24	698.72	700.07	698.39	699.89	698.05	699.74	697.90	699.88	697.75	700.03	697.60	700.19	697.45	700.34	697.30	700.49	697.15	697.00	696.40	697.00	697.15	700.18	697.30	700.02	697.45	699.87	697.60	699.90	697.75	700.13	699.02	700.35	700.29	700.69	700.66	701.03	701.04	701.40	701.41	701.77	701.79	702.29	702.18				

USER NAME = mkrzyżewski	DESIGNED - MK	REVISED - 03/09/2018
	DRAWN - MK	REVISED -
PLOT SCALE = 1"=50'	CHECKED - KEK	REVISED -
PLOT DATE = 11/28/2017	DATE - 11/07/2017	REVISED -

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		KANE	24	19
		CONTRACT NO.		
ILLINOIS		FED. AID PROJECT		

EXHIBIT 13

FILE NAME = Projects\CH401 - Chicago\1401150886 - Kane County - Dauberman Road Extension - Hydraulics - Location Drainage Study\LOS-Sheets\LOS-EXISTING

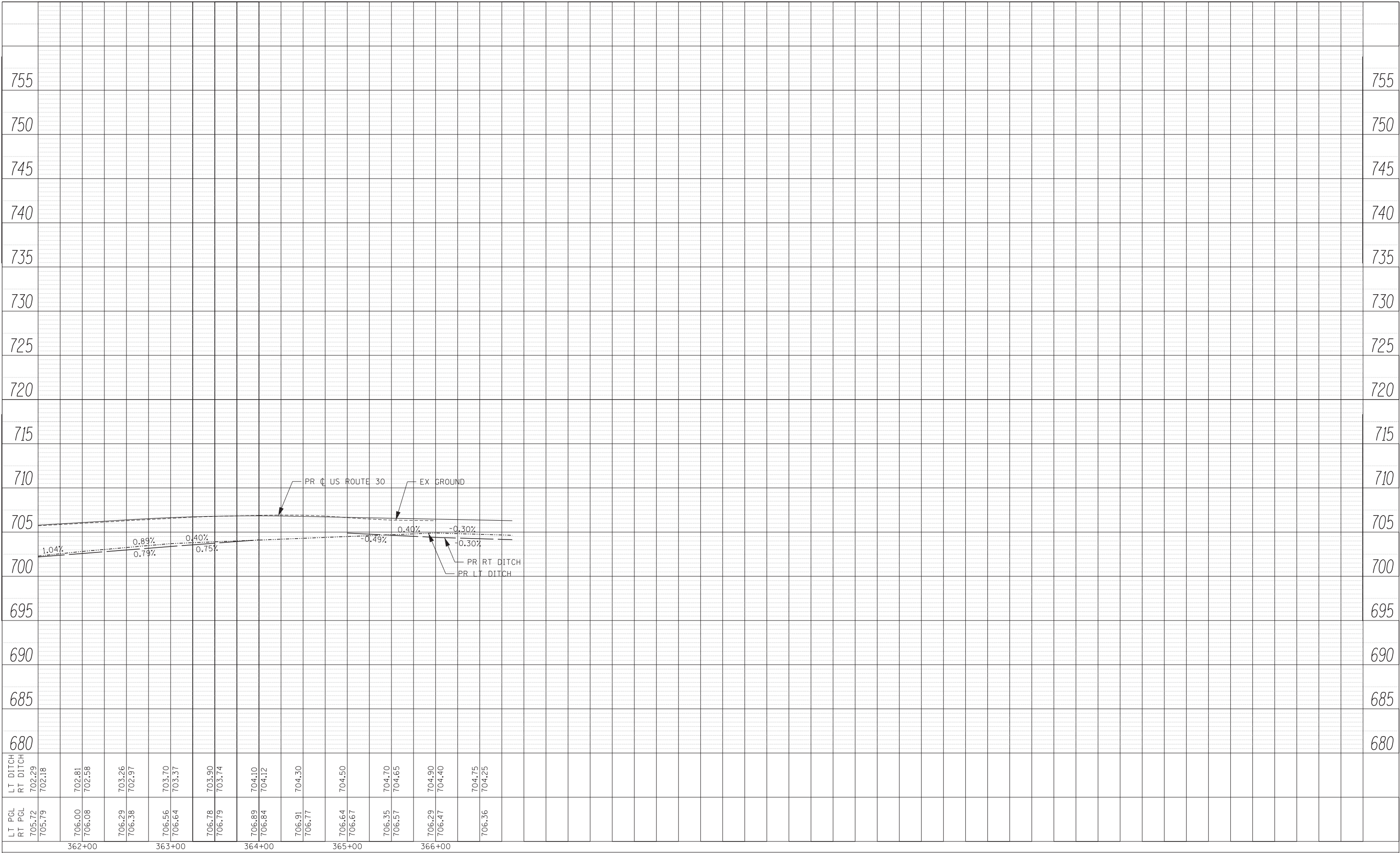


EXHIBIT 13

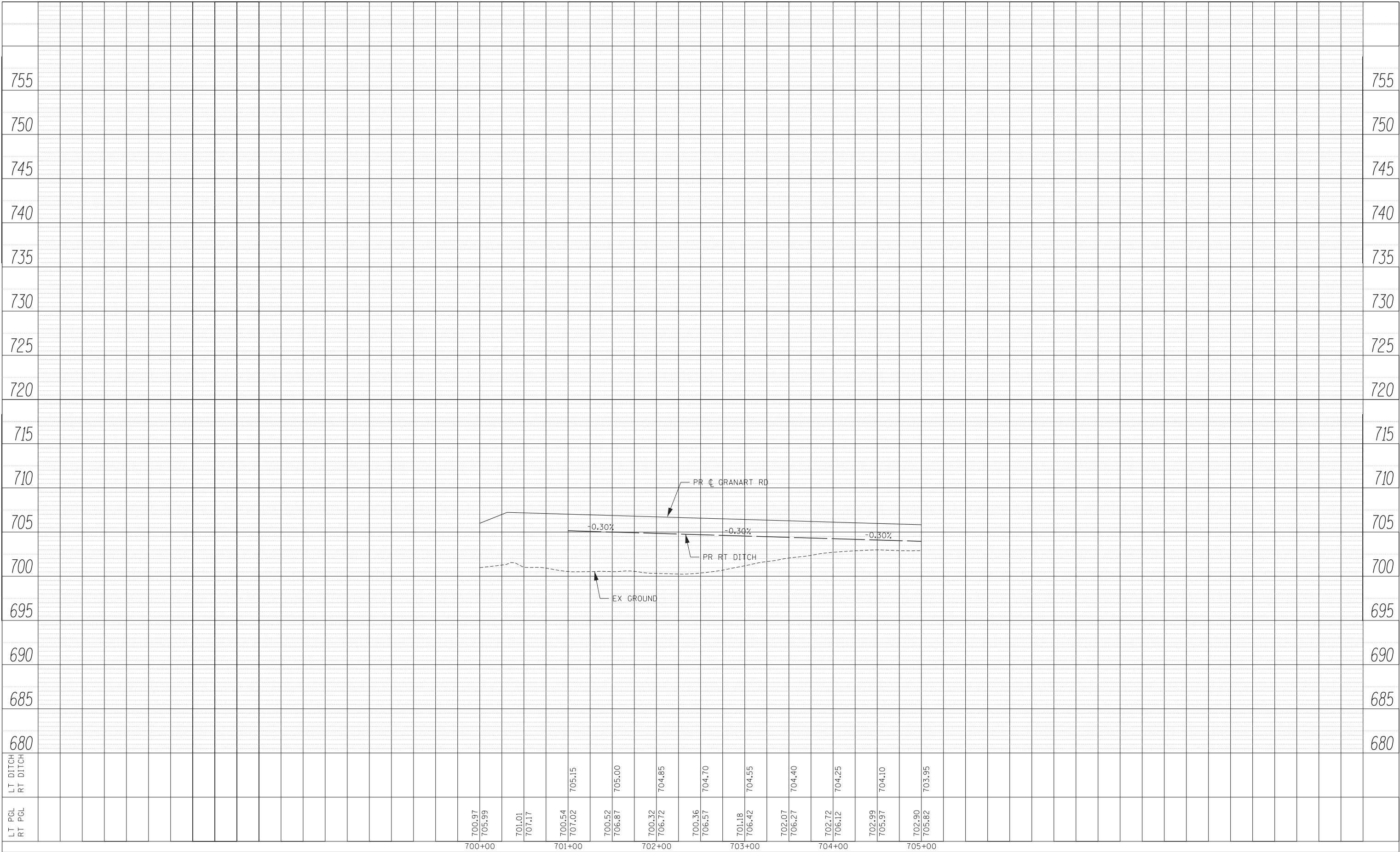
<div><div>B</div><div>Bollinger, Lach & Associates, Inc.</div><div>ITASCA, ILLINOIS</div></div>	USER NAME = mkrzynski		DESIGNED - MK	REVISED - 03/09/2018	KANE COUNTY				F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 1"=50'		DRAWN - MK	REVISED -							KANE	24	20
	PLOT DATE = 11/28/2017		CHECKED - KEK	REVISED -	DAUBERMAN ROAD EXTENSION				CONTRACT NO.				
			DATE - 11/07/2017	REVISED -	SCALE: 1"=50'				SHEET 20 OF 24 SHEETS				TO STA.

KANE COUNTY
DIVISION OF TRANSPORTATION

PROPOSED DRAINAGE PROFILE
DAUBERMAN ROAD EXTENSION

ILLINOIS FED. AID PROJECT

FILE NAME = Projects\CH401 - Chicago\401\50086 - Kane County - Dauberman Road Extension - Hydraulics - Location Drainage Study\LOS_Sheets\LOS-EXISTING





Bollinger, Lach & Associates, Inc.

ITASCA, ILLINOIS

USER NAME = mkrzynski

DESIGNED - MK

DRAWN - MK

REVISD - 03/09/2018

REVISD -

PLT SCALE = 1"=50'

CHECKED - KEK

REVISD -

PLT DATE = 11/28/2017

DATE - 11/07/2017

REVISD -

KANE COUNTY

DIVISION OF TRANSPORTATION

PROPOSED DRAINAGE PROFILE

DAUBERMAN ROAD EXTENSION

SCALE: 1"=50'

SHEET 21 OF 24 SHEETS

STA. TO STA.

F.A.S. RTE.

SECTION

COUNTY

TOTAL SHEETS

SHEET NO.

KANE

24

21


ILLINOIS

FED. AID PROJECT

EXHIBIT 13

CONTRACT NO.

[illegible]

 Bollinger, Lach & Associates, Inc. ITASCA, ILLINOIS	USER NAME = mikrzywinski	DESIGNED = MK	REVISED = 03/09/2018
		DRAWN = MK	REVISED =
	PLOT SCALE = 1"=50'	CHECKED = KEK	REVISED =
	PLOT DATE = 11/28/2017	DATE = 11/07/2017	REVISED =

<p align="center">PROPOSED DRAINAGE PROFILE DAUBERMAN ROAD EXTENSION</p>					
SCALE: 1"=50'	SHEET 22	OF 24	SHEETS	STA.	TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		KANE	24	22
		CONTRACT NO.		
ILLINOIS		FED. AID PROJECT		

[illegible]

USER NAME = mkrzyżewski	DESIGNED - MK	REVISED - 03/09/2018
	DRAWN - MK	REVISED -
PLOT SCALE = 1"=50'	CHECKED - KEK	REVISED -
PLOT DATE = 11/28/2017	DATE - 11/07/2017	REVISED -

PROPOSED DRAINAGE PROFILE

DAUBERMAN ROAD EXTENSION

SCALE: 1"=50'	SHEET 23	OF 24 SHEETS	STA.	TO STA.
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F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		KANE	24	23
		CONTRACT NO.		
ILLINOIS		FED. AID PROJECT		

EXHIBIT 13

[illegible]

USER NAME = mkrzyżewski	DESIGNED - MK	REVISED - 03/09/2018
	DRAWN - MK	REVISED -
PLOT SCALE = 1"=50'	CHECKED - KEK	REVISED -
PLOT DATE = 11/28/2017	DATE - 11/07/2017	REVISED -

PROPOSED DRAINAGE PROFILE DAUBERMAN ROAD EXTENSION

SCALE: 1"=50'	SHEET 24 OF 24 SHEETS	STA.	TO STA.
---------------	-----------------------	------	---------

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		KANE	24	24
		CONTRACT NO.		
ILLINOIS		FED. AID PROJECT		

EXHIBIT 13

Dauberman Road Extension
Granart Road to US Route 30
15-00277-01-BR
Kane County

PRELIMINARY ESTIMATE OF COST
August 1, 2018
Exhibit 14

Length (feet) = 10,998

	Items	<u>UNIT PRICE</u> ¹	<u>UNIT</u>	<u>QUANTITY</u>		<u>ITEM TOTAL</u>
1	Removal					
a	Pavement Removal	\$10	SQ YD	19,322	\$	194,000
b	Tree Removal	\$40	INCH	133	\$	6,000
c	Shoulder Removal	\$10	SQ YD	3,500	\$	35,000
d	Curb and Gutter Removal	\$8	FOOT	272	\$	3,000
e	Storm Sewer Removal, including TBF	\$30	FOOT	873	\$	27,000
f	Storm Sewer Structure Removal	\$750	EACH	24	\$	18,000
2	Earthwork					
a	Excavation	\$12	CU YD	34,773	\$	418,000
b	Furnish Excavation	\$12	CU YD	202,027	\$	2,425,000
c	Removal & Disposal of Unsuitable Materials (Topsoil)	\$20	CU YD	51,977	\$	1,040,000
d	Undercut and Aggregate Subgrade Improvement ²	\$65	CU YD	10,700	\$	696,000
3	Erosion Control					
a	Silt Fence	\$3	FOOT	19,571	\$	59,000
b	Ditch Checks	\$12	FOOT	4,695	\$	57,000
c	Erosion Control Blanket	\$2	SQ YD	57,993	\$	116,000
d	Riprap	\$85	SQ YD	800	\$	68,000
4	Drainage					
a	Storm Sewer	\$60	FOOT	2,943	\$	177,000
b	Trench Backfill	\$25	CU YD	2,398	\$	60,000
c	Storm Structures (Manholes, Catch Basins, Inlets, FES)	\$2,000	EACH	152	\$	304,000
d	Pipe Culverts	\$75	FOOT	1,861	\$	140,000
e	Drain Tile Replacements	\$85	FOOT	3,516	\$	299,000
f	Pipe Underdrains, 4" (including headwalls)	\$10	FOOT	23,000	\$	230,000
g	Box Culverts and Headwalls	\$500	FOOT	1,443	\$	722,000
5	Pavement					
a	HMA Pavement	\$45	SQ YD	43,984	\$	1,980,000
b	Multi-Use Path	\$28	SQ YD	4,851	\$	136,000
c	Driveways including Removal & Temporary Access	\$45	SQ YD	1,395	\$	63,000
d	HMA Shoulders (6")	\$25	SQ YD	12,937	\$	324,000
e	Aggregate Shoulders (6")	\$15	SQ YD	4,884	\$	74,000
f	Curb and Gutter	\$20	FOOT	3,600	\$	72,000
g	Aggregate Subgrade Improvement, 12"	\$12	SQ YD	46,183	\$	555,000
h	Subbase for Shoulders, 6"	\$10	SQ YD	16,877	\$	169,000
6	Landscaping					
a	Topsoil, Seeding, and Sodding	\$3	SQ YD	190,866	\$	573,000
7	Signing and Striping					
a	Permanent Marking, and Signing	0.5	%	OF CONSTRUCTION	\$	72,815
8	Traffic Control					
a	Traffic Control including Detour Signing	1.5	%	OF CONSTRUCTION	\$	218,445
9	Traffic Signals					
a	Traffic Signal at US Route 30 and Connector Road	300,000	Each	1	\$	300,000
10	Miscellaneous					
a	Field Office	\$2,500	MONTH	12	\$	30,000
b	Mobilization	3	%	OF CONSTRUCTION	\$	436,890
c	Construction Layout	2	%	OF CONSTRUCTION	\$	218,445
d	Railroad Flagging	\$900	DAY	70	\$	63,000
e	Railroad Liability Insurance	\$50,000	LSUM	1	\$	50,000
11	Incidentals					
a	Guardrail	\$30	FOOT	2,631	\$	79,000
12	Structures					
a	Bridge over US Route 30	\$1,660,000	LSUM	1	\$	1,660,000
b	Bridge over BNSF Railroad	\$1,630,000	LSUM	1	\$	1,630,000
13	Utility Adjustments ³					
a	Underdrain reconnection	\$750	EACH	12	\$	9,000
14	Contingency	10	%	OF CONSTRUCTION	\$	1,456,300

*TOTAL CONSTRUCTION COST (Rounded to nearest \$10,000): **\$ 17,264,000**

Dauberman Road Extension
Granart Road to US Route 30
15-00277-01-BR
Kane County

PRELIMINARY ESTIMATE OF COST
August 1, 2018
Exhibit 14

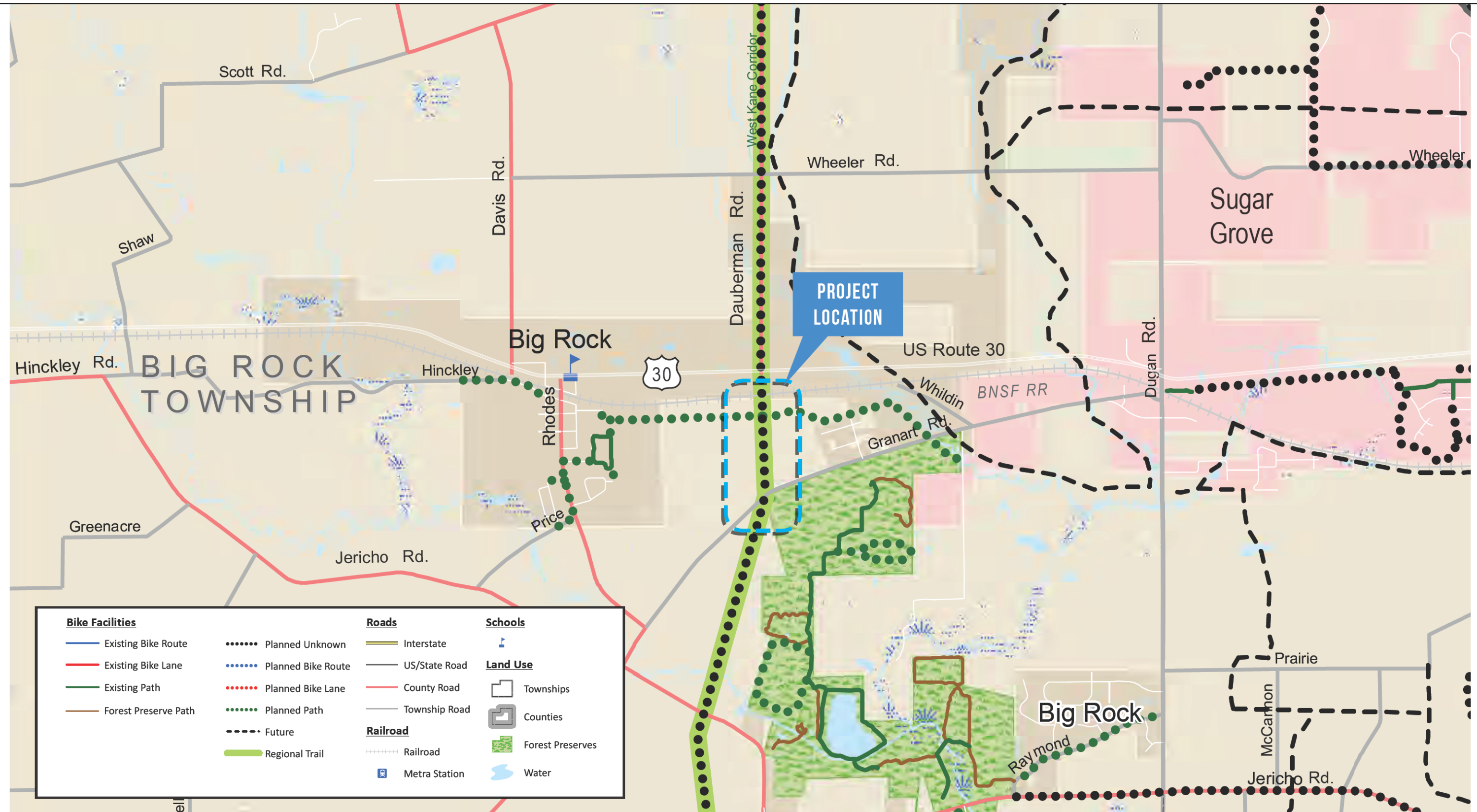
Length (feet) = 10,998

Items	<u>UNIT PRICE</u> ¹	<u>UNIT</u>	<u>QUANTITY</u>	<u>ITEM TOTAL</u>
Other Items				
15 Wetland Banking ⁴	\$0 ⁴	ACRE	11.72	\$ -
16 Right-of-Way ⁵				
a Plats and negotiations	\$15,000	PARCEL	26	\$ 390,000
b Right-of-Way	\$0.7	SQ FT	1,892,143	\$ 1,324,500
c Temporary Easements ⁶	\$5	SQ FT	300	\$ 1,500
TOTAL COST OF OTHER ITEMS:				<u>\$ 1,716,000</u>
TOTAL PROJECT COST :				<u>\$ 18,980,000</u>

NOTES:

1. The unit prices are in 2017 construction dollars
2. Proposed undercut and replacement quantities are based on preliminary assumptions that the subgrade material near the existing wetland areas will need to be replaced to a depth of 3 feet. This is subject to revision based on future geotechnical studies.
3. The estimate does not include any costs associated with relocation of power poles, buried electric, telephone, or fiber optic lines or gas main.
4. Wetland banking is anticipated to be done at the Sauer Family Forest Preserve Wetland Bank, which is owned by the Forest Preserve District of Kane County. It is used exclusively for KDOT and Forest Preserve projects. As such, no additional costs for wetland banking are anticipated at this time.
5. Right-of-way costs are based on 2017 assumptions and are subject to change.
6. Temporary Easements square footage does not include the easements on Kane County Forest Preserve property, as no cost is anticipated for that easement.

FILE NAME : p:\nrc\p101\transyscor\p101\Documents\Projects\CH401 - Chicago\p401150086 - Kane County - Dauberman Road Extension\303100 - Road\303102 - Exhibits\0086-PDR-AppA_15_BikeMap



Bike Facilities

- Existing Bike Route
- Existing Bike Lane
- Existing Path
- Forest Preserve Path
- Planned Unknown
- Planned Bike Route
- Planned Bike Lane
- Planned Path
- Future
- Regional Trail

Roads

- Interstate
- US/State Road
- County Road
- Township Road
- Railroad
- Metra Station

Schools

-

Land Use

- Townships
- Counties
- Forest Preserves
- Water



DAUBERMAN ROAD EXTENSION | BIKE LANES & PATHS

Kane County, Illinois

(2015 Kane/Kendall County Planning Map - DRAFT)



Not to Scale



USER NAME : mbmwhorner	DESIGNED : -	
	DRAWN : -	
PLOT SCALE : 2500.0000' / in.	CHECKED : -	
PLOT DATE : 11/3/2017	DATE : -	

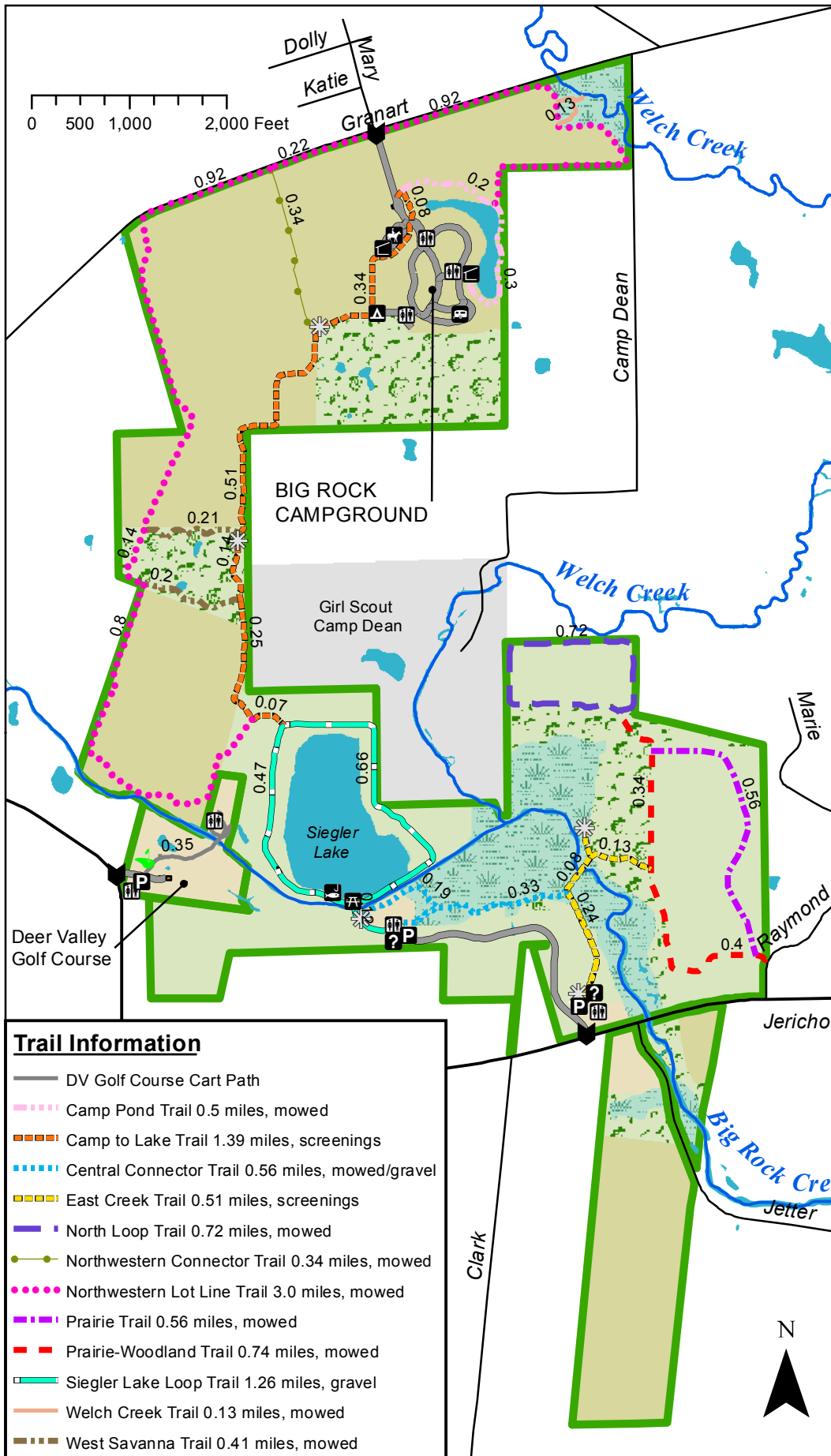
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXTRACT FROM KANE/KENDALL BIKE PLANNING MAP
PUBLISHED APRIL 2015

F.A.S. RTE.	SECTION	COUNTY	APPX	EXHIBIT	SHEET
1107	15-00277-01-BR	KANE	A	15	1 OF 1
Dauberman Road Extension Project Development Report Kane County Bike Plan & Big Rock Forest Preserve Map					



Big Rock Forest Preserve



Legend

- Main Entrance
- Information
- Parking
- Shelter
- Picnic Area
- Restroom
- RV Campground
- Tent Campground
- Equestrian Campground
- Fishing
- Golf
- Interpretive Sign
- Road
- Forest Preserve Road
- Creek
- Forest Preserve
- Natural Area -
 - Cropland
 - Grassland
 - Maintained
 - Open Water
 - Wetland
 - Woodland

Preserve Main Entrance:
46W072 Jericho Road,
Big Rock, IL 60511
Approx. 3.54 miles west
of IL Route 47

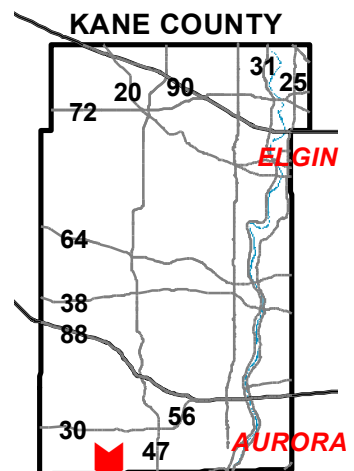
Campground:
46W499 Granart Road,
Big Rock, IL 60511

Acreage = 840
Total trail miles = 10.12

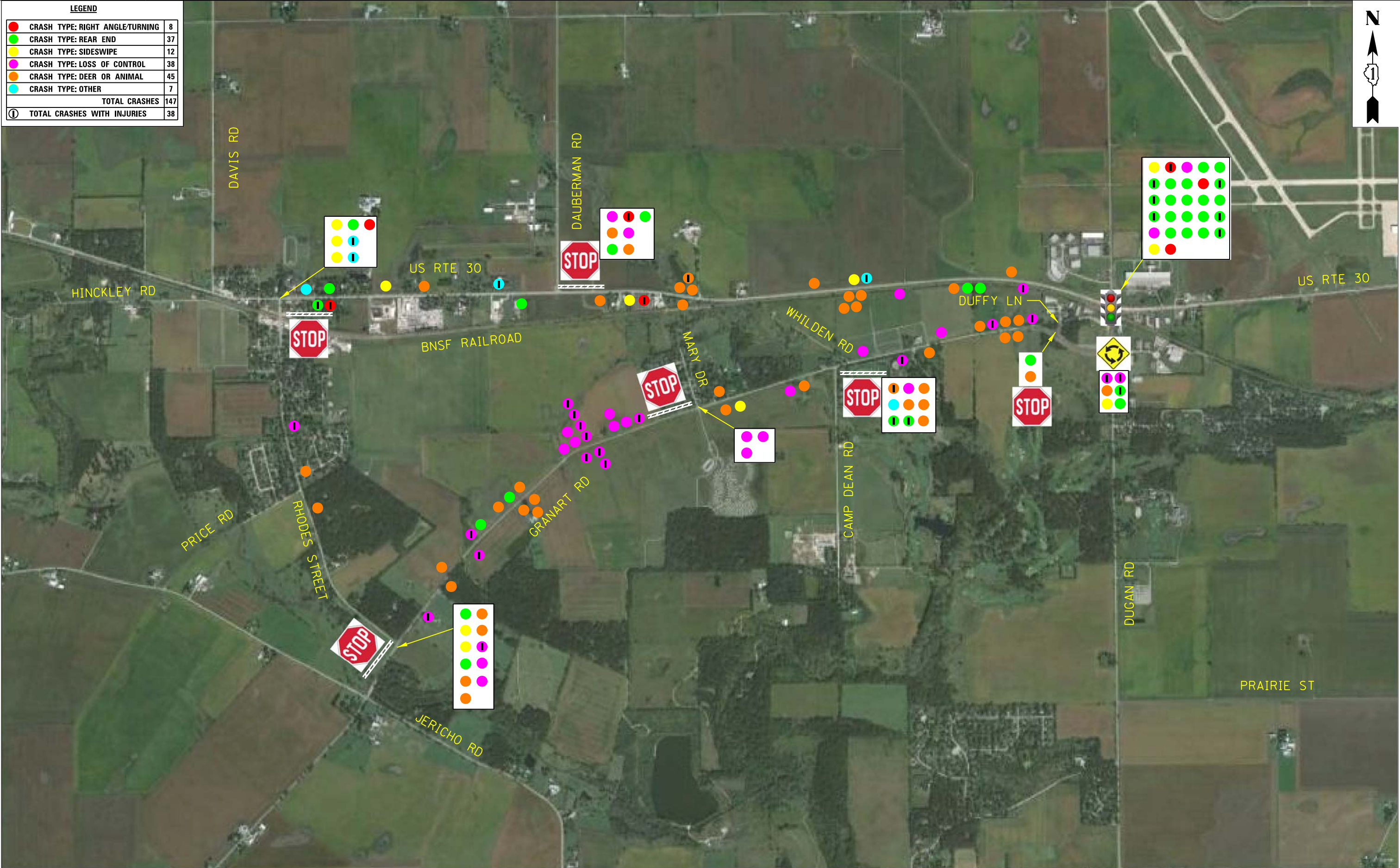
Information: (630) 232-5980
www.kaneforest.com
Hours: Daily, Sunrise-Sunset

Trail Information

- DV Golf Course Cart Path
- Camp Pond Trail 0.5 miles, mowed
- Camp to Lake Trail 1.39 miles, screenings
- Central Connector Trail 0.56 miles, mowed/gravel
- East Creek Trail 0.51 miles, screenings
- North Loop Trail 0.72 miles, mowed
- Northwestern Connector Trail 0.34 miles, mowed
- Northwestern Lot Line Trail 3.0 miles, mowed
- Prairie Trail 0.56 miles, mowed
- Prairie-Woodland Trail 0.74 miles, mowed
- Siegler Lake Loop Trail 1.26 miles, gravel
- Welch Creek Trail 0.13 miles, mowed
- West Savanna Trail 0.41 miles, mowed



LEGEND		
●	CRASH TYPE: RIGHT ANGLE/TURNING	8
●	CRASH TYPE: REAR END	37
●	CRASH TYPE: SIDESWIPE	12
●	CRASH TYPE: LOSS OF CONTROL	38
●	CRASH TYPE: DEER OR ANIMAL	45
●	CRASH TYPE: OTHER	7
TOTAL CRASHES		147
①	TOTAL CRASHES WITH INJURIES	38



KANE COUNTY
DIVISION OF TRANSPORTATION

DAUBERMAN ROAD EXTENSION PHASE I STUDY

BY: DBB DATE: 09/07/18 SCALE: N.T.S.

EXHIBIT 16-1
7-YEAR CRASH HISTORY MAP
2010-2016

DUGAN ROAD

CRASH SUMMARY TABLE (2010-2016)							
LOCATION:	US ROUTE 30 AT DUGAN ROAD						
AGENCY CRASH REPORT NO.	DATE	UNIT 1 ACTION	UNIT 2 ACTION	INCIDENT	CONDITIONS	LIGHTING	INJURY
10-02-0803	2/8/2010	RIGHT TURN	STOPPED	SIDESWIPE	CLEAR, WET	DAYLIGHT	PDO
10-02-0919	2/13/2010	LEFT TURN	DRIVING STRAIGHT	RIGHT ANGLE	CLEAR, DRY	DARKNESS, DUI	A (2)
10-02-1140	2/23/2010	LEFT TURN	-	LOSS OF CONTROL	CLEAR, DRY	DARKNESS	PDO
10-03-1401	3/6/2010	DRIVING STRAIGHT	STOPPED	REAR END	CLEAR, DRY	DAYLIGHT	PDO
10-10-6268	10/2/2010	DRIVING STRAIGHT	STOPPED	REAR END	CLEAR, DRY	DAYLIGHT	PDO
10-12-8699	12/26/2010	DRIVING STRAIGHT	STOPPED	REAR END	CLEAR, WET	DAYLIGHT	B (2)
11-01-0791	1/25/2011	LEFT TURN	STOPPED	REAR END	CLEAR, WET	DAYLIGHT	PDO
11-06-6316	6/20/2011	LEFT TURN	STOPPED	REAR END	CLEAR, DRY	DAYLIGHT	PDO
11-08-7921	8/11/2011	PASSING	DRIVING STRIGHT	RIGHT ANGLE	CLEAR, DRY	DAYLIGHT	PDO
11-11-11652	11/16/2011	LEFT TURN	STOPPED	REAR END	CLEAR, DRY	DARKNESS	A
12-03-2683	3/3/2012	LEFT TURN	STOPPED	REAR END	SNOW, ICE	DAYLIGHT	C
12-08-7436	8/6/2012	DRIVING STRAIGHT	STOPPED	REAR END	CLEAR, DRY	DAYLIGHT	PDO
12-08-7530	8/15/2012	DRIVING STRAIGHT	STOPPED	REAR END	CLEAR, DRY	DAYLIGHT	PDO
12-10-8766	10/15/2012	DRIVING STRAIGHT	STOPPED	REAR END	CLEAR, DRY	DAYLIGHT	PDO
12-10-8307	10/31/2012	DRIVING STRAIGHT	STOPPED	REAR END	CLEAR, DRY	DAYLIGHT	PDO
12-8776	12/29/2012	PASSING	STOPPED	REAR END	SNOW, ICE	DAYLIGHT	C
13-0799	4/4/2013	DRIVING STRAIGHT	STOPPED	REAR END	CLEAR, DRY	DAYLIGHT	PDO
13-2682	8/16/2013	DRIVING STRAIGHT	STOPPED	REAR END	CLEAR, DRY	DAYLIGHT	PDO
13-3174	9/24/2013	DRIVING STRAIGHT	STOPPED	REAR END	CLEAR, DRY	DAYLIGHT	PDO
13-4285	12/13/2013	DRIVING STRAIGHT	STOPPED	REAR END	CLEAR, DRY	DAYLIGHT	C
14-0667	1/27/2014	DRIVING STRAIGHT	-	LOSS OF CONTROL	SNOW, ICE	DAYLIGHT	PDO
14-0783	1/31/2014	LEFT TURN	LEFT TURN	REAR END	CLEAR, DRY	DAYLIGHT	PDO
14-1023	2/10/2014	DRIVING STRAIGHT	STOPPED	REAR END	CLEAR, DRY	DAYLIGHT	PDO
14-1056	2/11/2014	DRIVING STRAIGHT	STOPPED	REAR END	CLEAR, DRY	DAYLIGHT	PDO
14-1146	2/14/2014	STOPPED	LEFT TURN	REAR END	CLEAR, DRY	DU SK	C
14-3413	5/30/2014	U-TURN	DRIVING STRAIGHT	SIDESWIPE	CLEAR, DRY	DAYLIGHT	PDO
16-01199464	8/27/2016	U-TURN	DRIVING STRAIGHT	TURNING	CLEAR, DRY	DAYLIGHT	B

US ROUTE 30



DN

10-02-0919

N

10-02-1140

10-02-0803

10-03-1401

10-10-6268

10-12-8699

11-01-0791

11-06-6316

14-0783

12-10-8307

12-10-8766

12-08-7436

11-08-7921

11-11-11652

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11-11-11652

14-1146

BNSF RAILROAD

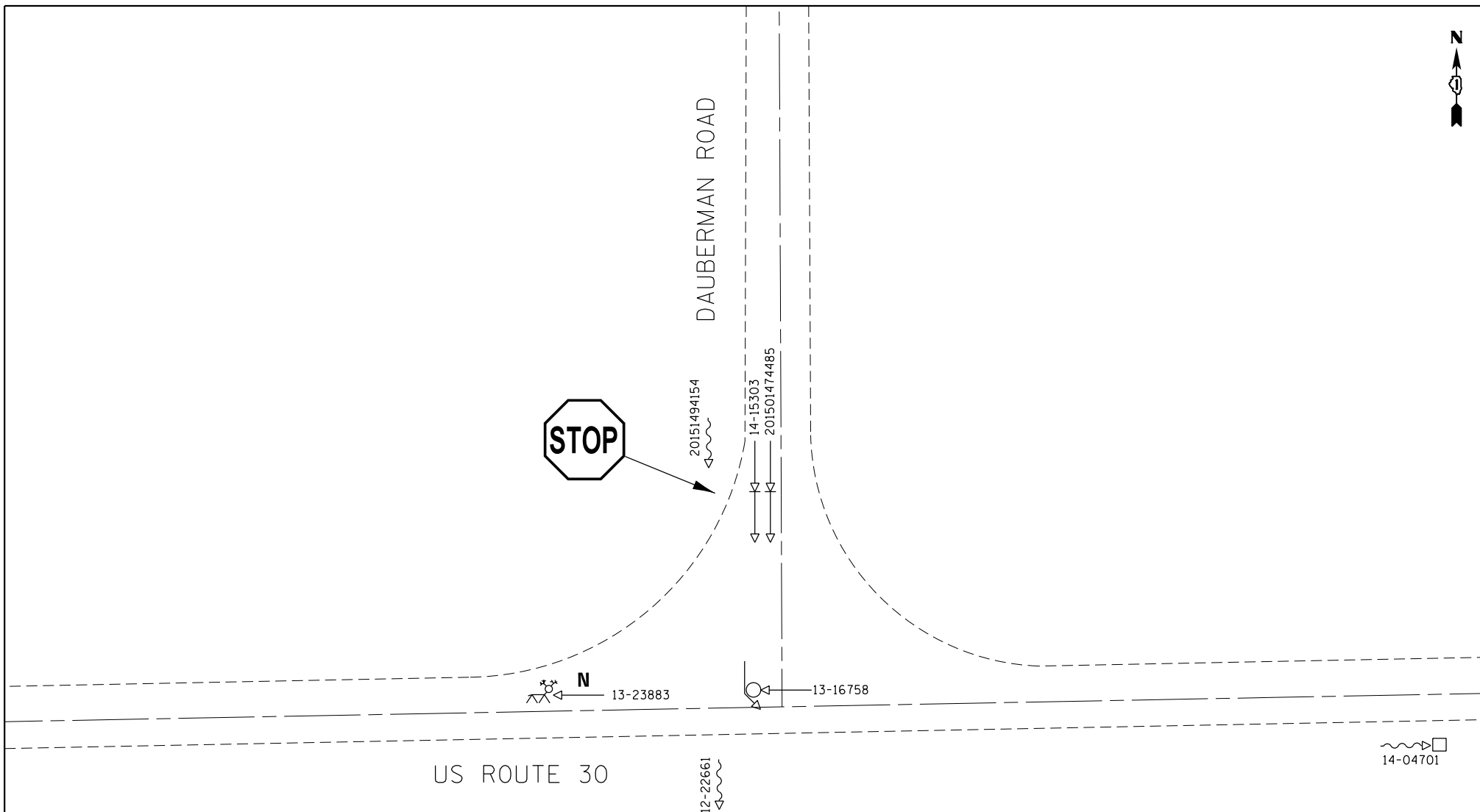


KANE COUNTY
DIVISION OF TRANSPORTATION

BY: DBB DATE: 09/07/18

DAUBERMAN ROAD EXTENSION
PHASE I STUDY

EXHIBIT 16-2
COLLISION DIAGRAM (2010-2016)
US ROUTE 30 AND DUGAN ROAD



CRASH SUMMARY TABLE (2010-2016)							
US ROUTE 30 AT DAUBERMAN ROAD							
AGENCY CRASH REPORT NO.	DATE	UNIT 1 ACTION	UNIT 2 ACTION	INCIDENT	CONDITIONS	LIGHTING	INJURY
12-22661	11/21/2012	DRIVING STRAIGHT	-	LOSS OF CONTROL INTO SIGN	RAIN, WET	DAYLIGHT	PDO
13-16758	8/3/2013	TURNING LEFT	DRIVING STRAIGHT	RIGHT ANGLE	CLEAR, DRY	DAYLIGHT	B
13-23883	11/8/2013	DRIVING STRAIGHT	-	DEER	CLEAR, DRY	DARKNESS	PDO
14-04701	3/2/2014	DRIVING STRAIGHT	-	LOSS OF CONTROL INTO SIGN	SNOW/ICE	DAYLIGHT	PDO
14-15303	7/22/2014	TURNING LEFT	STOPPED	REAR END	CLEAR, DRY	DAYLIGHT	PDO
201501474485	10/12/2015	DRIVING STRAIGHT	STOPPED	REAR END	CLEAR, DRY	DAYLIGHT	PDO
201501494154	12/30/2015	DRIVING STRAIGHT	-	LOSS OF CONTROL	SNOW/ICE	DAYLIGHT	PDO



KANE COUNTY
DIVISION OF TRANSPORTATION

BY: DBB DATE: 09/07/18

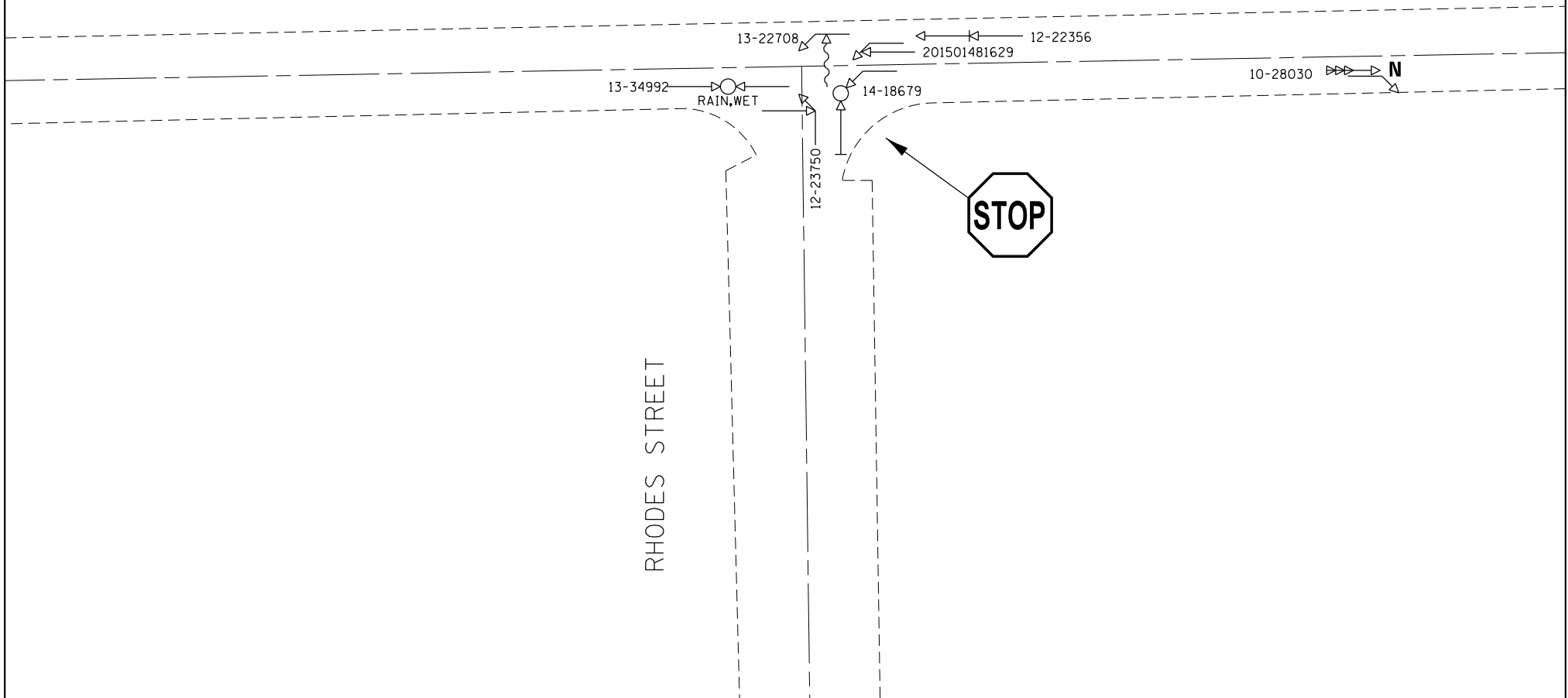
DAUBERMAN ROAD EXTENSION
PHASE I STUDY

EXHIBIT 16-3
COLLISION DIAGRAM (2010-2016)
US ROUTE 30 AND DAUBERMAN ROAD

CRASH SUMMARY TABLE (2010-2016)							
LOCATION:		US ROUTE 30 AT RHODES STREET					
AGENCY CRASH REPORT NO.	DATE	UNIT 1 ACTION	UNIT 2 ACTION	INCIDENT	CONDITIONS	LIGHTING	INJURY
10-28030	12/18/2010	TURNING RIGHT	DRIVING STRAIGHT	SIDESWIPE	CLEAR, DRY	DARKNESS	PDO
12-22356	11/16/2012	DRIVING STRAIGHT	STOPPED	REAR END	CLEAR, DRY	DAYLIGHT	PDO
12-23750	12/1/2012	TURNING LEFT	DRIVING STRAIGHT	SIDESWIPE	RAIN, WET	DAYLIGHT	PDO
13-34992	5/1/2013	DRIVING STRAIGHT	DRIVING STRAIGHT	HEAD ON	CLEAR, DRY	DAYLIGHT	C
13-22708	10/22/2013	TURNING LEFT	DRIVING STRAIGHT	SIDESWIPE	SNOW, WET	DAYLIGHT	PDO
14-18679	9/6/2014	TURNING LEFT	STOPPED	HEAD ON	CLEAR, DRY	DAYLIGHT	B
20150148629	9/9/2015	TURNING LEFT	DRIVING STRAIGHT	TURNING	CLEAR, DRY	DAYLIGHT	PDO



US ROUTE 30

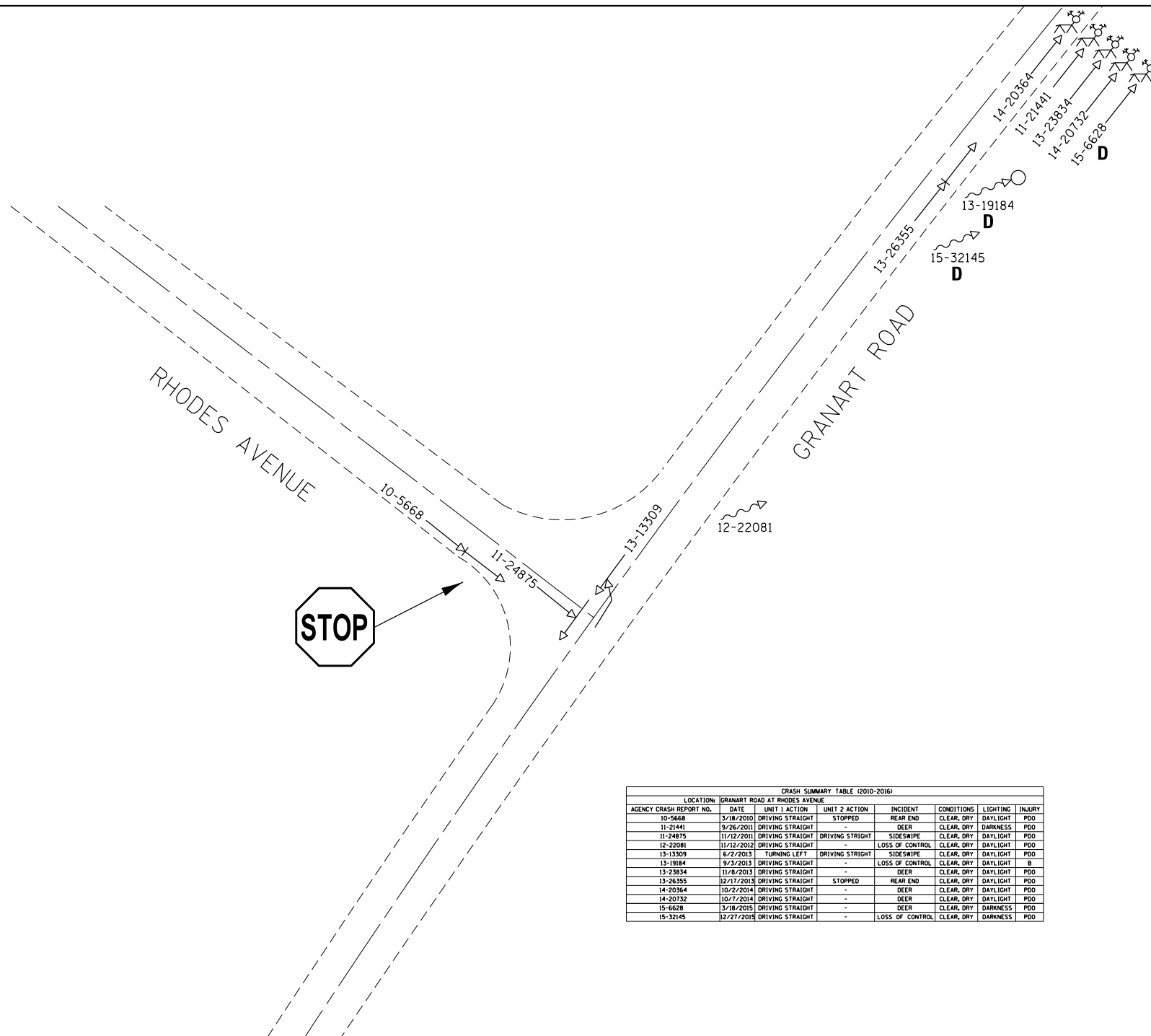


KANE COUNTY
DIVISION OF TRANSPORTATION

BY: DBB DATE: 09/07/18

DAUBERMAN ROAD EXTENSION
PHASE I STUDY

EXHIBIT 16-4
COLLISION DIAGRAM (2010-2016)
US ROUTE 30 AND RHODES STREET



CRASH SUMMARY TABLE (2010-2016)							
LOCATION: GRANART ROAD AT RHODES AVENUE							
AGENCY CRASH REPORT NO.	DATE	UNIT 1 ACTION	UNIT 2 ACTION	INCIDENT	CONDITIONS	LIGHTING	INJURY
10-5668	3/18/2010	DRIVING STRAIGHT	STOPPED	REAR END	CLEAR, DRY	DAYLIGHT	PDO
11-21441	9/26/2011	DRIVING STRAIGHT	-	DEER	CLEAR, DRY	DARKNESS	PDO
11-24875	11/12/2011	DRIVING STRAIGHT	DRIVING STRAIGHT	SIDESWIPE	CLEAR, DRY	DAYLIGHT	PDO
12-22081	11/12/2012	DRIVING STRAIGHT	-	LOSS OF CONTROL	CLEAR, DRY	DAYLIGHT	PDO
13-13309	6/2/2013	TURNING LEFT	DRIVING STRAIGHT	SIDESWIPE	CLEAR, DRY	DAYLIGHT	PDO
13-19184	9/3/2013	DRIVING STRAIGHT	-	LOSS OF CONTROL	CLEAR, DRY	DAYLIGHT	B
13-23834	11/8/2013	DRIVING STRAIGHT	-	DEER	CLEAR, DRY	DAYLIGHT	PDO
13-26355	12/17/2013	DRIVING STRAIGHT	STOPPED	REAR END	CLEAR, DRY	DAYLIGHT	PDO
14-20364	10/2/2014	DRIVING STRAIGHT	-	DEER	CLEAR, DRY	DAYLIGHT	PDO
14-20732	10/7/2014	DRIVING STRAIGHT	-	DEER	CLEAR, DRY	DAYLIGHT	PDO
15-6628	3/18/2015	DRIVING STRAIGHT	-	DEER	CLEAR, DRY	DARKNESS	PDO
15-32145	12/27/2015	DRIVING STRAIGHT	-	LOSS OF CONTROL	CLEAR, DRY	DARKNESS	PDO



KANE COUNTY
DIVISION OF TRANSPORTATION

BY: DBB DATE: 09/07/18

DAUBERMAN ROAD EXTENSION
PHASE I STUDY

EXHIBIT 16-5
COLLISION DIAGRAM (2010-2016)
GRANART ROAD AND RHODES AVENUE



N 16-2870

1009-5969
1212-8632
14-1846

DUFFY LANE

1110-10619

1208-7624

GRANART ROAD

2010-2014 T-INTERSECTION

1110-10158

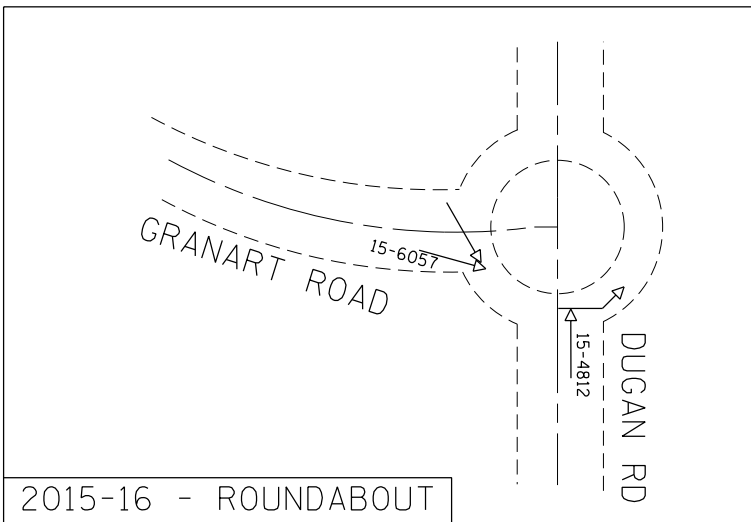
1207-7179

1201-0486
1302-0445

N

12-12-8687

DUGAN RD



2015-16 - ROUNDABOUT

CRASH SUMMARY TABLE (2010-2016)									
AGENCY CRASH REPORT NO.	LOCATION	DATE	UNIT 1 ACTION	UNIT 2 ACTION	INCIDENT	CONDITIONS	LIGHTING	INJURY	
1009-5969	GRANART ROAD AT DUFFY LANE, DUGAN ROAD	9/17/2010	DRIVING STRAIGHT	-	DEER	CLEAR, DRY	DUSK	PDO	
11101-0158		10/12/2011	DRIVING STRAIGHT	DID NOT STOP	LOSS OF CONTROL	CLEAR, DRY	DUSK	C	
1101-0619		10/23/2011	DRIVING STRAIGHT	-	DEER	CLEAR, DRY	DAWN	PDO	
1201-0486		1/12/2012	DRIVING STRAIGHT	DRIVING STRAIGHT	LOSS OF CONTROL	SNOW, ICE	DAYLIGHT	C	
1207-7179		7/22/2012	DRIVING STRAIGHT	-	LOSS OF CONTROL	CLEAR, DRY	DAYLIGHT	B	
208-7624		8/23/2012	DRIVING STRAIGHT	STOPPED	REAR END	CLEAR, DRY	DAYLIGHT	PDO	
1212-8632		12/13/2012	DRIVING STRAIGHT	-	DEER	CLEAR, DRY	DAWN	PDO	
1212-8687		12/18/2012	DRIVING STRAIGHT	STOPPED	REAR END	CLEAR, DRY	DAWN	C	
1302-0445		2/21/2013	DRIVING STRAIGHT	-	DEER	CLEAR, DRY	DARKNESS	PDO	
14-1846		3/17/2014	DRIVING STRAIGHT	-	DEER	CLEAR, DRY	DAWN	PDO	
15-4812		10/6/2015	TURNING	TURNING	REAR END	CLEAR, DRY	DAWN	PDO	
15-6057		12/13/2015	TURNING	DRIVING STRAIGHT	SIDESWIPE	CLEAR, DRY	DAWN	PDO	
16-2870		5/20/2016	DRIVING STRAIGHT	-	LOSS OF CONTROL (FELL ASLEEP)	CLEAR, DRY	DAWN	B	



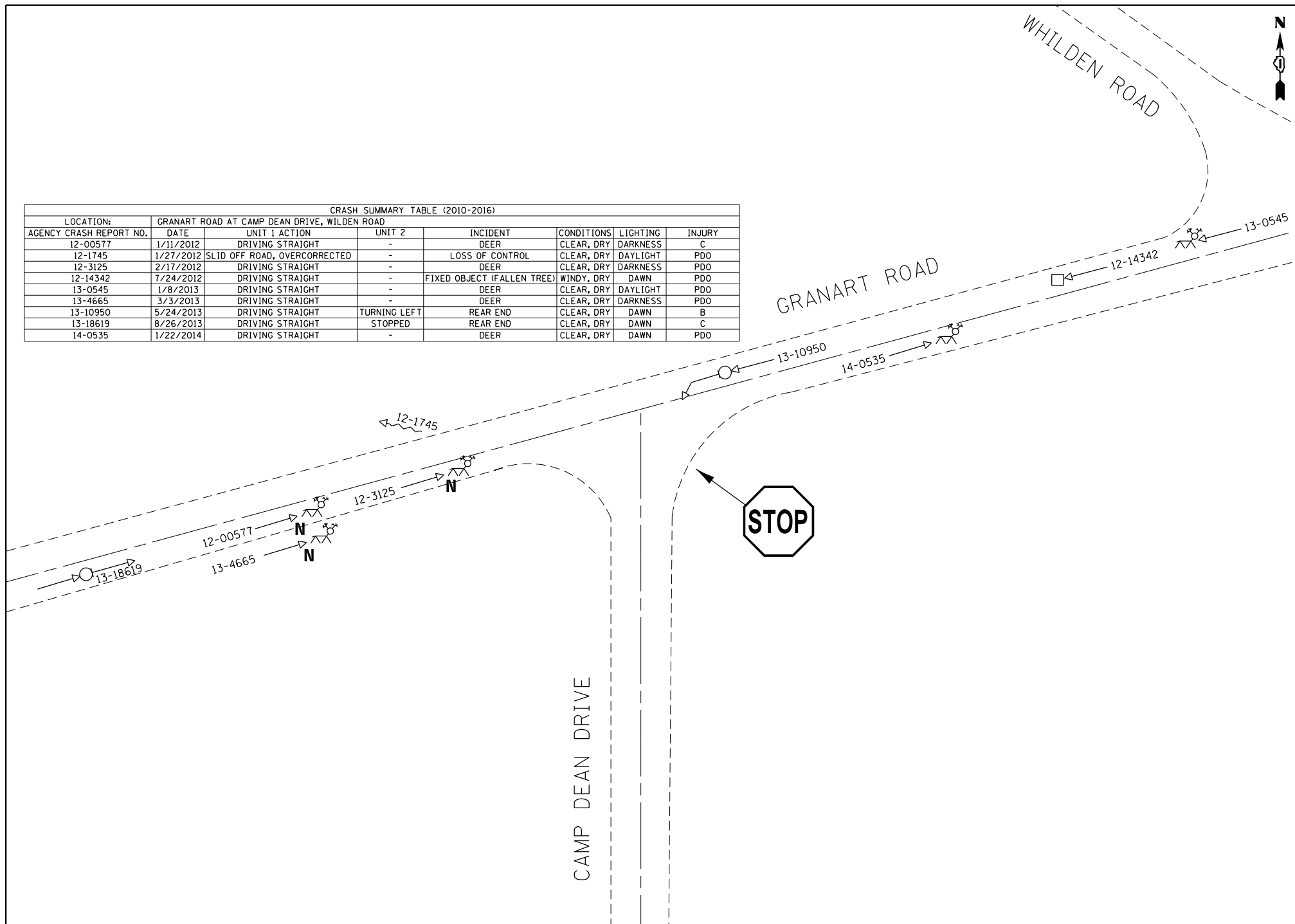
KANE COUNTY
DIVISION OF TRANSPORTATION

BY: DBB DATE: 09/07/18

DAUBERMAN ROAD EXTENSION
PHASE I STUDY

EXHIBIT 16-6
COLLISION DIAGRAM (2010-2016)
GRANART ROAD AT DUFFY LANE & DUGAN ROAD

CRASH SUMMARY TABLE (2010-2016)							
LOCATION:	GRANART ROAD AT CAMP DEAN DRIVE, WILDEN ROAD						
AGENCY CRASH REPORT NO.	DATE	UNIT 1 ACTION	UNIT 2	INCIDENT	CONDITIONS	LIGHTING	INJURY
12-00577	1/11/2012	DRIVING STRAIGHT	-	DEER	CLEAR, DRY	DARKNESS	C
12-1745	1/27/2012	SLID OFF ROAD, OVERCORRECTED	-	LOSS OF CONTROL	CLEAR, DRY	DAYLIGHT	PDO
12-3125	2/17/2012	DRIVING STRAIGHT	-	DEER	CLEAR, DRY	DARKNESS	PDO
12-14342	7/24/2012	DRIVING STRAIGHT	-	FIXED OBJECT (FALLEN TREE)	WINDY, DRY	DAWN	PDO
13-0545	1/8/2013	DRIVING STRAIGHT	-	DEER	CLEAR, DRY	DAYLIGHT	PDO
13-4665	3/3/2013	DRIVING STRAIGHT	-	DEER	CLEAR, DRY	DARKNESS	PDO
13-10950	5/24/2013	DRIVING STRAIGHT	TURNING LEFT	REAR END	CLEAR, DRY	DAWN	B
13-18619	8/26/2013	DRIVING STRAIGHT	STOPPED	REAR END	CLEAR, DRY	DAWN	C
14-0535	1/22/2014	DRIVING STRAIGHT	-	DEER	CLEAR, DRY	DAWN	PDO

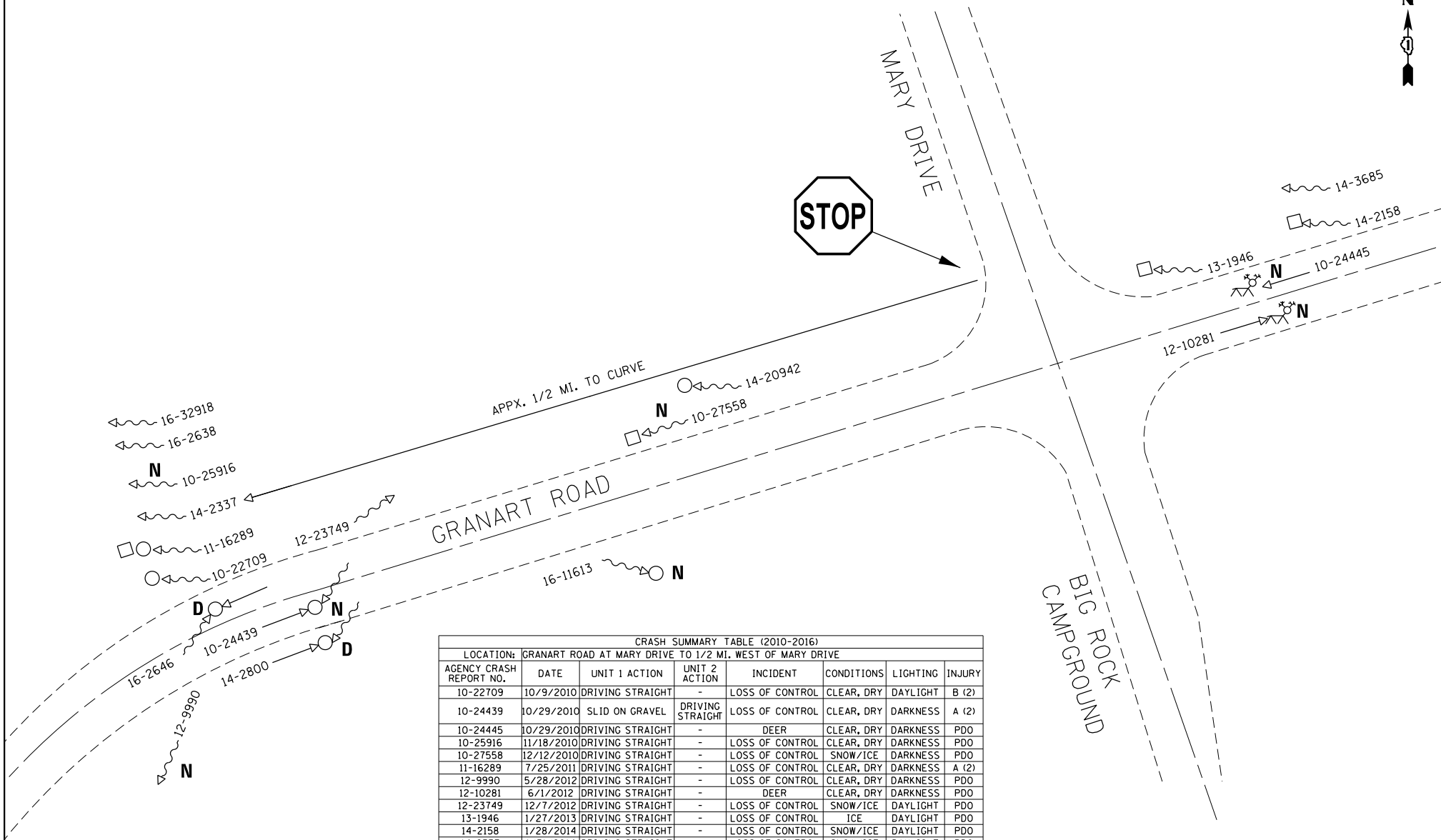


KANE COUNTY
DIVISION OF TRANSPORTATION

BY: DBB DATE: 09/07/18

DAUBERMAN ROAD EXTENSION
PHASE I STUDY

EXHIBIT 16-7
COLLISION DIAGRAM (2010-2016)
GRANART ROAD AT CAMP DEAN DRIVE, WILDEN ROAD



CRASH SUMMARY TABLE (2010-2016)							
LOCATION: GRANART ROAD AT MARY DRIVE TO 1/2 MI. WEST OF MARY DRIVE							
AGENCY CRASH REPORT NO.	DATE	UNIT 1 ACTION	UNIT 2 ACTION	INCIDENT	CONDITIONS	LIGHTING	INJURY
10-22709	10/9/2010	DRIVING STRAIGHT	-	LOSS OF CONTROL	CLEAR, DRY	DAYLIGHT	B (2)
10-24439	10/29/2010	SLID ON GRAVEL	DRIVING STRAIGHT	LOSS OF CONTROL	CLEAR, DRY	DARKNESS	A (2)
10-24445	10/29/2010	DRIVING STRAIGHT	-	DEER	CLEAR, DRY	DARKNESS	PDO
10-25916	11/18/2010	DRIVING STRAIGHT	-	LOSS OF CONTROL	CLEAR, DRY	DARKNESS	PDO
10-27558	12/12/2010	DRIVING STRAIGHT	-	LOSS OF CONTROL	SNOW/ICE	DARKNESS	PDO
11-16289	7/25/2011	DRIVING STRAIGHT	-	LOSS OF CONTROL	CLEAR, DRY	DARKNESS	A (2)
12-9990	5/28/2012	DRIVING STRAIGHT	-	LOSS OF CONTROL	CLEAR, DRY	DARKNESS	PDO
12-10281	6/1/2012	DRIVING STRAIGHT	-	DEER	CLEAR, DRY	DARKNESS	PDO
12-23749	12/7/2012	DRIVING STRAIGHT	-	LOSS OF CONTROL	SNOW/ICE	DAYLIGHT	PDO
13-1946	1/27/2013	DRIVING STRAIGHT	-	LOSS OF CONTROL	ICE	DAYLIGHT	PDO
14-2158	1/28/2014	DRIVING STRAIGHT	-	LOSS OF CONTROL	SNOW/ICE	DAYLIGHT	PDO
14-2337	1/31/2014	DRIVING STRAIGHT	-	LOSS OF CONTROL	SNOW/ICE	DAYLIGHT	PDO
14-2800	2/5/2014	DRIVING STRAIGHT	-	LOSS OF CONTROL	SNOW/ICE	DAYLIGHT	B
14-3685	2/7/2014	DRIVING STRAIGHT	-	LOSS OF CONTROL	SNOW/ICE	DAYLIGHT	PDO
14-20942	10/10/2014	DRIVING STRAIGHT	-	LOSS OF CONTROL	CLEAR, DRY	DAYLIGHT	PDO
16-2638	2/1/2016	DRIVING STRAIGHT	-	LOSS OF CONTROL	SNOW/ICE	DAYLIGHT	PDO
16-2646	2/1/2016	SLID OFF ROAD	DRIVING STRAIGHT	LOSS OF CONTROL	SNOW/ICE	DAYLIGHT	A
16-11613	5/1/2016	DRIVING STRAIGHT	-	LOSS OF CONTROL	RAIN/WET	DAYLIGHT	B
16-32918	12/10/2016	DRIVING STRAIGHT	-	LOSS OF CONTROL	SNOW/ICE	DARKNESS	PDO



KANE COUNTY
DIVISION OF TRANSPORTATION

DAUBERMAN ROAD EXTENSION
PHASE I STUDY

BY: DBB DATE: 09/07/18

EXHIBIT 16-8
COLLISION DIAGRAM (2010-2016)
GRANART ROAD AT MARY DRIVE TO 1/2 MI. WEST

INTERSECTION CRASH SUMMARY TABLE – 2010 to 2016						
LOCATION	CRASHES	% OF TOTAL CRASHES	PROPERTY DAMAGE ONLY	% OF TOTAL CRASHES	INJURIES	% OF TOTAL CRASHES
US 30 AT DUGAN ROAD	27	41.4%	20	74.1%	7	25.9%
US 30 AT DAUBERMAN ROAD	7	10.8%	6	85.7%	1	14.3%
US 30 AT RHODES STREET	7	10.8%	5	71.4%	2	28.6%
GRANART ROAD AT RHODES STREET	12	18.5%	11	91.7%	1	8.3%
GRANART ROAD AT BUCKTAIL LANE/DUGAN ROAD	12	18.5%	8	66.7%	4	33.3%
TOTAL (& PERCENTAGE OF TOTAL)	65	100.0%	50	76.9%	15	23.1%

SEGMENT CRASH SUMMARY TABLE – 2010 to 2016						
LOCATION	CRASHES	% OF TOTAL CRASHES	PROPERTY DAMAGE ONLY	% OF TOTAL CRASHES	INJURIES	% OF TOTAL CRASHES
GRANART ROAD: RHODES STREET TO DUGAN ROAD	49	59.7%	35	71.4%	14	28.6%
US 30: RHODES STREET TO DAUBERMAN ROAD	8	9.8%	5	62.5%	3	37.5%
US 30: DAUBERMAN ROAD TO DUGAN ROAD	20	24.4%	16	80.0%	4	20.0%
RHODES STREET: US 30 TO DUGAN ROAD	5	6.1%	3	60.0%	2	40.0%
TOTAL (& PERCENTAGE OF TOTAL)	82	100.0%	59	72.0%	23	28.0%

TOTAL CRASHES	147	109	74.1%	38	25.9%
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CRASH TYPE SUMMARY TABLE – 2010 to 2016						
CRASH TYPE	CRASHES	% OF TOTAL CRASHES	PROPERTY DAMAGE ONLY	% OF TOTAL CRASHES	INJURIES	% OF TOTAL CRASHES
REAR END	37	25.2%	27	73.0%	10	27.0%
DEER OR ANIMAL	45	30.6%	42	93.3%	3	6.7%
LOSS OF CONTROL	38	25.8%	23	60.5%	15	39.5%
SIDESWIPE	12	8.2%	12	100.0%	0	0.0%
OTHER (FIXED OBJECT, HEAD-ON)	7	4.8%	2	28.6%	5	71.4%
RIGHT ANGLE/TURNING	8	5.4%	3	37.5%	5	62.5%
TOTAL (& PERCENTAGE OF TOTAL)	147	100.0%	109	74.1%	38	25.9%

CRASH TYPE SUMMARY TABLE - BY ROAD CONDITION – 2010 to 2016		
WEATHER TYPE	CRASHES	%
CLEAR, DRY	112	76.1%
RAIN, WET	7	4.8%
SNOW, ICE	28	19.1%
TOTAL	147	100.0%

CRASH TYPE SUMMARY TABLE - BY LIGHTING - 2010 to 2016		
LIGHTING TYPE	CRASHES	%
DAWN	12	8.2%
DAYLIGHT	85	57.8%
DUSK	3	2.0%
DARKNESS	47	32.0%
TOTAL	147	100.0%

CRASH SEVERITY SUMMARY TABLE (2010 to 2016)								
YEAR	CRASHES	% OF CRASHES BY YEAR	PROPERTY DAMAGE ONLY	% OF TOTAL CRASHES	TYPE A INJURY	%OF TOTAL CRASHES	TYPE B OR C INJURY	% OF TOTAL CRASHES
2010	19	12.9%	14	73.7%	2	10.5%	3	15.8%
2011	13	8.8%	8	61.5%	4	30.8%	1	7.7%
2012	32	21.7%	23	71.9%	0	0.0%	9	28.1%
2013	23	15.6%	15	65.2%	0	0.0%	8	34.8%
2014	25	17.0%	21	84.0%	1	4.0%	3	12.0%
2015	25	17.0%	22	88.0%	0	0.0%	3	12.0%
2016	10	6.8%	6	60.0%	2	20.0%	2	20.0%
TOTAL	147		109	74.1%	9	6.1%	29	19.7%



APPENDIX B

Clearances and Approvals

- 7/25/16, Huff & Huff PESA Review, Executive Summary
- 10/24/16, IDOT PESA Review Summary and Recommendations
- 2/8/17 and 10/20/17 (Addendum), Cultural Resources Clearance
- 10/25/17 IDOT/GSU Approval of No Lighting at Bridge over US Route 30
- 10/30/17, COSIM Pre-screening Clearance
- 11/17/17, Wetland Resource Clearance
- 11/20/17, Biological Resources Clearance
- 12/15/17, IDNR Wetland Clearance Concurrence
- 2/2/18, Section 4(f) Temporary Occupancy Document
- 2/8/18 IDOT Design Variance Approval
- 2/15/18, IDOA Approval Letter and NRCS Form AD-1006
- 4/6/18, IDOT Approval of Section 4(f) Temporary Occupancy
- 4/30/18, IDOT Approval of Signal Warrant and New Signal
- 5/4/18, IDOT Approval of Noise Report
- 6/12/18 IDOT Approval of Change in Wetland Bank
- 6/27/18, IDOT GSU Approval of Geometrics
- 7/24/18, IDOT Traffic Approval of IDS
- 10/9/18, IDOT Approval of the TS&L's
- 10/24/18, IDOT Approval of the Location Drainage Study
- 11/7/18, IDOT Maintenance Approval

Project Overview

Submittal Date: 04/29/2016 **Sequence No:** 19997
District: 1 **Requesting Agency:** Local Kane County DOT **Project No:**
Contract #: **Job No.:** P-91-992-16
Counties: Kane
Route: FAS 1107 **Marked:**
Street: CH 62 / Dauberman Road **Section:** 15-00277-01-BR
Municipality(ies): Big Rock **Project Length:** 1.6093 km 1 miles
FromTo (At): Granart Road (south) to US 30 (north)
Quadrangle: Big Rock & Plano **Township-Range-Section:** S22, S23, S14, S15 T38N R6E
Survey Target Date: 01/15/2017 **Anticipated Design Appr.:** 01/15/2017 **Anticipated Processing:** CE
Funding: ☒ Federal ☐ State ☐ TBP ☐ MFT ☐ Local Non-MFT

Consultant:
PTB No.: **Item No.:** **PTB Date:** **Prequal Level:**

Sequence No:	19997	Biological	Wetlands	Cultural	Special Waste
Entered By	BDE	BDE	BDE	BDE	
Cleared for DA	11/20/2017	12/15/2017	2/8/2017	10/24/2016	
Cleared for Letting	11/20/2017	12/15/2017	2/8/2017		
Resubmittal					
ResubmittalCleared					
Section:	15-00277-01-BR	Job No.:	P-	91-992-16	
FromTo (At):	Granart Road (south) to US 30 (north)				

Sequence No:	19997 A	Biological	Wetlands	Cultural	Special Waste
Entered By	BDE	BDE	BDE		
Cleared for DA	11/20/2017	12/15/2017	10/20/2017		
Cleared for Letting	11/20/2017	12/15/2017	10/20/2017		
Resubmittal					
ResubmittalCleared					
Section:	15-00277-01-BR	Job No.:	P-	91-992-16	
FromTo (At):	Granart Road (south) to US Route 30 (North)				

Notice of Intent	Project Initiation Ltr to FHWA	Public Info Meeting(s)		Notice of Availability		Public Hearing	Draft	ROD/FONSI
		1st	2nd	Draft	Final			
								Approved

Project Phase Comments:

**PRELIMINARY ENVIRONMENTAL
SITE ASSESSMENT**

**Dauberman Road Extension from Granart Road to US Route 30
Big Rock, Kane County, Illinois**

Prepared for

**TranSystems Corporation
1475 East Woodfield Road, Suite 600
Schaumburg, Illinois 60173**

**Prepared by:
Huff & Huff, Inc.**

July 2016

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APPENDICES

Appendix A – Photograph Log

Appendix B – Initial Field Survey Checklist

ISGS Preliminary Environmental Property Assessment Checklist

Appendix C – Project Information

Soil Survey Map

Berg Map

Stack Unit Map

Glacial Drift Map

Bedrock Geologic Map

Zoning Map of Kane County, Illinois

ISGS Coal Map

USGS Seismic Risk and Landslide Incidence

Earthquake Probability Map

ISGS Karst Area Map

FEMA Floodplain Insurance Map

National Wetland Inventory Map

National Pipeline Mapping System Map

Historical Inventory of Solid Waste Disposal Sites

Aerial Photographs

Appendix D – ERIS Report

Appendix E FOIA Information

GLOSSARY OF ACRONYMS

ACM	Asbestos Containing Material	LUST	Leaking Underground Storage Tank
ASTM	ASTM International	MAC	Maximum Allowable Concentration
AST	Aboveground Storage Tank	NIPC	Northeastern Illinois Planning Commission
BDE	Bureau of Design and Environment	PESA	Preliminary Environmental Site Assessment
BLRS	Bureau of Local Roads and Streets	PGA	Peak Ground Acceleration
BNSF	Burlington Northern Santa Fe Railroad	PIP	Potentially Impacted Property
CCDD	Clean Construction or Demolition Debris	PSI	Preliminary Site Investigation
ERIS	Environmental Risk Information Service, Ltd	RCRA	Resource Conservation and Recovery Act
FEMA	Federal Emergency Management Agency	REC	Recognized Environmental Condition
IAC	Illinois Administration Code	ROW	Right-of-Way
IDOT	Illinois Department of Transportation	SPILLS	Spills and Incidences Database
IEPA	Illinois Environmental Protection Agency	USEPA	United States Environmental Protection Agency
IPCB	Illinois Pollution Control Board	USGS	United States Geological Survey
ISGS	Illinois State Geological Survey	UST	Underground Storage Tank

EXECUTIVE SUMMARY

This is the Preliminary Environmental Site Assessment (PESA) of man-made hazards that may be encountered during the planned extension of Dauberman Road from Granart Road to US Route 30 in Big Rock and Unincorporated Kane County, Illinois (Project Corridor).

The screening process, used to identify sites that may pose a hazard to the Project Corridor, included a historical review, database search, the Illinois Environmental Protection Agency's (IEPA) online document explorer, and site reconnaissance. Historical resources included historical aerial photos of the Project Corridor, which were reviewed for evidence of former sites that may pose a hazard to the Project Corridor. The database search provided information (on a local, state, or federal level) on properties that may pose a hazard to the Project Corridor. Site reconnaissance was conducted to inspect the sites identified in the database search, as well as, identify additional sites adjacent to the Project Corridor with storage areas, spills, staining, or other indications of potential environmental concern. Every site identified through the screening process was reviewed to determine its status as a potentially impacted property (PIP) in connection to the Project Corridor. The following tables (Tables ES-1 through ES-3) summarize these sites.

PESA site reconnaissance activities were completed on June 6, 2016. The database search, historical review, and review of water quality data for the Project Corridor were performed prior to the site visit. Based on the information presented in this PESA and data collected during the screening process, *this assessment has revealed evidence of potentially impacted properties* (PIPs) in connection to the Project Corridor. The identified PIP is the Burlington Northern Santa Fe Railroad (BNSF) with the identified area limited to the right-of-way along the BNSF. Based upon preliminary engineering, the proposed Dauberman Road extension will be elevated over the BNSF and may not disturb soils along the BNSF right-of-way. *Therefore, a Preliminary Site Investigation (PSI) along the Project Corridor is recommended only if soils within BNSF right-of-way will be disturbed.*

TABLE ES-1 SUMMARY OF SITES DETERMINED TO BE PIPs

Site Name	Figure 5-1 Site ID	Address	Reason(s)
BNSF Railroad	3	NA	Potential Herbicide, Pesticide, and Chemical Usage

NA – Not available

TABLE ES-2 SUMMARY OF SITES IDENTIFIED WITH DE MINIMIS CONDITIONS ¹

Site Name	Figure 5-1 Site ID	Address	Reason(s)
Farm Residence	2	46W628 Granart Road	AST – Separation Distance of Approximately 220 feet
Rayvac Plastic Decorators	4	47W171 US Route 30	RCRA SQG – Separation Distance of Approximately 515 feet

¹De minimis based on definition included in ASTM Standard E 1527-13

For the purposes of this report, the following are considered to be de minimis conditions:

- Pole-mounted transformers in normal use, unless the transformers were observed to be leaking, appear on an environmental regulatory list, or were otherwise determined to pose a hazard not related to normal use
- Lead-based paint
- Asbestos-Containing Material (ACM)
- Radon and Biological Hazards
- Natural gas pipelines

All conditions listed above were considered while developing the report. However, as sites with these conditions are seen often, each site was not specifically referenced as de minimis based on these conditions alone.

**TABLE ES-3 SUMMARY OF SITES IDENTIFIED ADJACENT TO THE PROJECT CORRIDOR WITH
NO PIPs OR DE MINIMIS CONDITIONS**

Site Name	Figure 5-1 Site ID	Address	Reason(s)
Farm Residence	1	7S731 Granart Road	Site Reconnaissance



Illinois Department of Transportation

Memorandum

To:	John Fortmann	Attn:	Christopher Holt, c/o Sam Mead
From:	Maureen M. Addis	By:	Scott E. Stitt
Subject:	PESA Review		
Date:	October 24, 2016		

Project:	FAS 1107 (Dauberman Road): Granart Road to US 30	Job #:	P-91-992-16
District 1:	Kane County	Contract #:	Not provided
Requesting Agency:	Kane County DOT	Anticipated DA:	01/15/2017
Survey Target Date:	01/15/2017	Section:	15-00277-01-BR
Anticipated Letting:	Not provided	ISGS PESA #:	3284
BDE Sequence #:	19997		

Attached is a copy of the Preliminary Environmental Site Assessment (PESA) report prepared by the Illinois State Geological Survey (ISGS) for the subject project as described in your Special Waste Environmental Survey Request (ESR). Table 1 identifies sites along the project route that were determined to contain recognized environmental conditions (RECs). It is the opinion of this office, in consultation with the Chief Counsel's Office, that a preliminary site investigation (PSI) is required if any site identified in Table 1 of the PESA report involves any of the following situations:

- New right of way or easement (temporary or permanent);
- Railroad right-of-way, other than single rail rural with no maintenance facilities; or
- Building demolition / modification.

Additionally, a PSI is required if the project will have excavation or subsurface utility relocation on existing right-of-way adjoining a site identified in Table 1 of the PESA report.

If the district determines that they can avoid all the sites that contain RECs, then a PSI is not required and the project will be in compliance with Departmental Policy D&E-11. If the district determines that the project will involve a site containing a REC(s), then a PSI is required and the statewide special waste consultant should be requested to perform the PSI. Please notify this office of any actions you may decide to take concerning these sites (avoidance or further investigation). The PESA Response form can be found on PMA.

The District's Bureau of Land Acquisition (DBLA) should determine if any new right-of-way or easement will involve any site identified in Table 1 or any site adjoining a site listed in Table 4 of the PESA report. On those identified situations, DBLA shall coordinate the acquisition with this office, Central Bureau of Land Acquisition, and the Chief Counsel's Office to determine if an "All Appropriate Inquiries" (AAI) assessment is required for additional liability protection under CERCLA.

Other findings and recommendations of the report should be carefully considered. If you have any questions regarding this report or the tasking of the statewide consultant, please contact Duffy Dessalines at 217/524-2269 or James R. Curtis at 217/558-4653.

Attachments

cc:	Office of Chief Counsel – Rm. 313	Central Bureau of Land Acquisition – Rm. 210
	District Bureau of Land Acquisition	District Utility Coordinator

IDOT Sequence #: 19997
IDOT Job #: P91-992-16

ISGS: 3284
IDOT District #: 1

PRELIMINARY ENVIRONMENTAL SITE ASSESSMENT

FINAL REPORT

DATE: October 20, 2016

IDOT DESIGN DATE: January 15, 2017

SURVEY TARGET DATE: January 15, 2017

DATE REQUEST RECEIVED: August 3, 2016

LOCATION: FAS 1107 (CH 62/Dauberman Road), Granert Road to US 30, Big Rock, Kane County; Big Rock quadrangle (USGS 7.5-minute topographic map), T38N, R6E, Sections 14, 15, 16, 21, 22, and 23.



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GLOSSARY OF ACRONYMS

AAI	-	All Appropriate Inquiries	M.M.	-	mile marker
ACM	-	asbestos-containing material	M.P.	-	mile post
AST	-	aboveground storage tank	MSDS	-	material safety data sheet
ASTM	-	American Society for Testing and Materials	MTBE	-	methyl tertiary butyl ether
AULs	-	activity and use limitations (includes institutional controls, engineered barriers, and HAAs)	NFR	-	No Further Remediation
bgs	-	below ground surface	NPL	-	National Priorities List
BOL	-	Bureau of Land (IEPA)	NRCS	-	Natural Resources Conservation Service
BTEX	-	benzene, toluene, ethylbenzene, and total xylenes	OSFM	-	Office of the State Fire Marshal
CDPH	-	Chicago Department of Public Health	PAA	-	Permit Access Agreement
CCDD	-	Clean construction and demolition debris	PAH/PNA-	-	polynuclear aromatic hydrocarbons
CERCLIS-	-	Comprehensive Environmental Response, Compensation, and Liability Information System	PCB	-	polychlorinated biphenyls
CH	-	County highway	PESA	-	Preliminary Environmental Site Assessment
CTA	-	Chicago Transit Authority	P.G.	-	Professional Geologist
ERNS	-	Emergency Response Notification System	ppb	-	parts per billion (equivalent to $\mu\text{g/kg}$ for solids, and $\mu\text{g/l}$ in liquids)
FEMA	-	Federal Emergency Management Agency	ppm	-	parts per million (equivalent to mg/kg in solids, and mg/l in liquids)
FIRM	-	Flood Insurance Rate map	PRP	-	Potentially Responsible Party
FOIA	-	Freedom of Information Act	PSI	-	Preliminary Site Investigation
GIS	-	Geographic Information System	RCRA	-	Resource Conservation and Recovery Act
GRO	-	Groundwater Remediation Objective	REC	-	recognized environmental condition
HAA	-	Highway Authority Agreement	ROW	-	right-of-way
IDNR	-	Illinois Department of Natural Resources	SEMS	-	Superfund Enterprise Management System
IDOT	-	Illinois Department of Transportation	SIC	-	Standard Industrial Classification
IEMA	-	Illinois Emergency Management Agency	SPLP	-	synthetic precipitation leaching procedure
IEPA	-	Illinois Environmental Protection Agency	SRO	-	Soil Remediation Objective
IMD	-	Illinois Manufacturers Directory	SRP	-	Site Remediation Program
ISGS	-	Illinois State Geological Survey	SSTS	-	Section Seven Tracking System (USEPA)
ISTC	-	Illinois Sustainable Technology Center (formerly Waste Management and Research Center)	SVOCs	-	semi-volatile organic compounds
ISWS	-	Illinois State Water Survey	TACO	-	Tiered Approach to Corrective Action Objectives (IEPA)
LUST	-	leaking underground storage tank	TCLP	-	toxicity characteristic leaching procedure
$\mu\text{g/kg}$	-	micrograms per kilogram (ppb)	TPH	-	total petroleum hydrocarbons
$\mu\text{g/l}$	-	micrograms per liter (ppb)	TRI	-	Toxics Release Inventory
mg/kg	-	milligrams per kilogram (ppm)	TVOC	-	Total volatile organic compounds
mg/l	-	milligrams per liter (ppm)	USDA	-	United States Department of Agriculture
			USEPA	-	United States Environmental Protection Agency
			USGS	-	United States Geological Survey
			UST	-	underground storage tank
			VOC	-	volatile organic compounds

EXECUTIVE SUMMARY

This report presents the results of an environmental site assessment for the improvements to US 30, Big Rock, Kane County. This report was prepared on behalf of the Illinois Department of Transportation (IDOT) by the Illinois State Geological Survey (ISGS).

The following sites were examined for this project. The tables below list sites along the project for which recognized environmental conditions (RECs)* were identified for each address or address range (Table 1); sites along the project for which only de minimis conditions were identified (Table 2); sites along the project for which no RECs or de minimis conditions were identified (Table 3); and sites adjoining but not on the project that were identified on environmental databases (Table 4). Further investigation of sites with RECs may be desired.

Table 1. The following sites along the project were determined to contain RECs:

Property name IDOT parcel #	ISGS site #	REC(s), including de minimis conditions	Regulatory database(s)	Land use
Residence NA	3284-3	Evidence of chemical use; potential ACM and lead paint	RCRA, BOL	Residential
Hinckley-Big Rock Middle School NA	3284-5	Former USTs with documented releases; potential UST(s); AST; monitoring well; former monitoring wells; transformer; potential ACM and lead paint	LUST, UST, BOL, IEMA	Education
AT&T NA	3284-9	AST; potential ACM and lead paint	None	Commercial
Big Rock Café NA	3284-11	Former USTs with a documented release; potential UST(s); potential former chemical use; transformer; potential ACM and lead paint	LUST, UST, BOL, IEMA	Commercial
Agricultural land NA	3284-17	Petroleum pipeline; likely pesticide and/or herbicide use	None	Agricultural
Dekane Equipment Corporation NA	3284-18	Former USTs; potential UST(s); evidence of chemical use; drums; ASTs; transformers; potential ACM and lead paint	RCRA, UST, BOL	Commercial

J.C. Customs NA	3284-19	Potential UST(s); evidence of chemical use; potential ACM and lead paint	RCRA, BOL	Commercial/ residential
Vacationland NA	3284-20	Likely petroleum pipeline; transformer; potential ACM and lead paint	None	Commercial
Residence NA	3284-26	AST; potential ACM and lead paint	None	Residential
Ray-Vac Distributors NA	3284-30	Evidence of chemical use; metals; transformers; potential ACM and lead paint	RCRA, BOL	Commercial
Dugan's Automotive Service NA	3284-35	Potential UST(s); potential chemical use; potential ACM and lead paint	None	Commercial

Table 2. The following sites along the project were determined to contain de minimis conditions only:

Property name IDOT parcel #	ISGS site #	De minimis condition(s)	Land use
Residences NA	3284-1	Potential ACM and lead paint	Residential
Residence NA	3284-2	Potential ACM and lead paint	Residential
Storage building NA	3284-4	Potential ACM and lead paint	Residential
Park NA	3284-6	Potential ACM and lead paint	Recreational
Residences NA	3284-7	Potential ACM and lead paint	Residential
Residence NA	3284-8	Potential ACM and lead paint	Residential
Residence NA	3284-10	Potential ACM and lead paint	Residential
Residence NA	3284-12	Potential ACM and lead paint	Residential

Nourishing Medicine NA	3284-13	Potential ACM and lead paint	Commercial/ residential
Residences NA	3284-14	Transformer; potential ACM and lead paint	Residential
Residences NA	3284-15	Transformers; potential ACM and lead paint	Residential
Agricultural land NA	3284-16	Likely pesticide and/or herbicide use	Agricultural
Agricultural land NA	3284-21	Likely pesticide and/or herbicide use	Agricultural
Residence NA	3284-22	Potential ACM and lead paint	Residential
STA Roofing and Siding NA	3284-23	Transformer; potential ACM and lead paint	Commercial/ residential
Residence NA	3284-24	Transformer; potential ACM and lead paint	Residential
Residences NA	3284-25	Solid waste; natural gas pipelines; transformers; potential ACM and lead paint	Residential
Residence NA	3284-27	Natural gas pipeline; likely natural gas pipelines; potential ACM and lead paint	Residential
Executive Affiliates NA	3284-28	Natural gas pipelines; transformer; potential ACM and lead paint	Commercial
B&R Storage NA	3284-29	Natural gas pipelines; transformers; potential ACM and lead paint	Commercial
Agricultural land NA	3284-31	Transformer; likely pesticide and/or herbicide use	Agricultural
Farmstead NA	3284-32	Transformer; potential pesticide and/or herbicide presence	Residential
Cyrus Ministries International NA	3284-33	Potential ACM and lead paint	Religious
Sugar Grove Self- Storage NA	3284-34	Transformers; potential ACM and lead paint	Commercial

Residence NA	3284-36	Transformer; potential ACM and lead paint	Residential
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Table 3. The following site along the project was determined not to contain RECs or de minimis conditions:

Property name IDOT parcel #	ISGS site #	Land use
Vacant land NA	3284-37	Vacant

Table 4. The following additional site, adjoining but not on the project, was identified on environmental databases:

Property name	ISGS site #	Regulatory database(s)	Land use
BNSF Railroad	3284-A	IEMA	Transportation

* For all sites:

Where REC(s) are indicated as present, a condition was noted that may be indicative of releases or potential releases of hazardous substances on, at, in, or to the site, as discussed in the text. Potential hazards were not verified by ISGS testing. Radon, biological hazards (such as mold, medical waste, or septic waste), and non-agricultural pesticides and/or herbicides may also be of concern. No further investigation concerning the presence or use of these factors was conducted for this PESA.

Where RECs are not indicated as present, radon, biological hazards (such as mold, medical waste, or septic waste), and non-agricultural pesticides and/or herbicides may still be of concern. No further investigation concerning the presence or use of these factors was conducted for this PESA.

For the purposes of this report, the following are considered to be de minimis conditions:

- Normal use of lead-based paint on exteriors and interiors of buildings and structures.
- Use of asbestos-containing materials in building construction.
- Transformers in normal use, unless the transformers were observed to be leaking, appear on an environmental regulatory list, or were otherwise determined to pose a hazard not related to normal use.
- Agricultural use of pesticides and herbicides. In addition, most land in Illinois was under agricultural use prior to its conversion to residential, industrial, or commercial development. Pesticides, both regulated and otherwise, may have been used throughout the project area at any time. Unless specifically discussed elsewhere in this report, no information regarding past pesticide use that would be subject to enforcement action was located for this project, and such use is considered a de minimis condition.

The following data gaps exist for all PESAs:

- For residences, only areas visible from public roads are inspected.
- Interiors of buildings are not inspected.
- Interiors of agricultural areas are not inspected during growing seasons.

Radon and biological hazards are not considered in this PESA unless specifically noted.

NA = No parcel number was supplied by IDOT for this site.

Although potential natural hazards and undermining, if present, are described in this report, they are not considered as RECs or de minimis conditions for the purposes of this report, and are therefore not listed in the tables above.

CONCLUSIONS

(1) RECs were identified at the following sites along the project:

- Site 3284-3: Residence. Evidence of chemical use; potential ACM and lead paint.
- Site 3284-5: Hinckley-Big Rock Middle School. Former USTs with documented releases; potential UST(s); AST; monitoring well; former monitoring wells; transformer; potential ACM and lead paint.
- Site 3284-9: AT&T. AST; potential ACM and lead paint.
- Site 3284-11: Big Rock Café. Former USTs with a documented release; potential UST(s); potential former chemical use; transformer; potential ACM and lead paint.
- Site 3284-17: Agricultural land. Petroleum pipeline; likely pesticide and/or herbicide use.
- Site 3284-18: Dekane Equipment Corporation. Former USTs; potential UST(s); evidence of chemical use; drums; ASTs; transformers; potential ACM and lead paint.
- Site 3284-19: J.C Customs. Potential UST(s); evidence of chemical use; potential ACM and lead paint.
- Site 3284-20: Vacationland. Likely petroleum pipeline; transformer; potential ACM and lead paint.
- Site 3284-26: Residence. AST; natural gas pipeline; potential ACM and lead paint.
- Site 3284-30: Ray-Vac Distributors. Evidence of chemical use; metals; transformers; potential ACM and lead paint.
- Site 3284-35: Dugan's Automotive Service. Potential UST(s); potential chemical use; potential ACM and lead paint.

(2) De minimis conditions were identified at the following sites along the project:

- Site 3284-1: Residences. Potential ACM and lead paint.
- Site 3284-2: Residence. Potential ACM and lead paint.
- Site 3284-4: Residential building. Potential ACM and lead paint.
- Site 3284-6: Park. Potential ACM and lead paint.
- Site 3284-7: Residences. Potential ACM and lead paint.
- Site 3284-8: Residence. Potential ACM and lead paint.
- Site 3284-10: Residence. Potential ACM and lead paint.
- Site 3284-12: Residence. Potential ACM and lead paint.
- Site 3284-13: Nourishing Medicine. Potential ACM and lead paint.
- Site 3284-14: Residences. Transformer; potential ACM and lead paint.
- Site 3284-15: Residences. Transformers; potential ACM and lead paint.
- Site 3284-16: Agricultural land. Likely pesticide and/or herbicide use.
- Site 3284-21: Agricultural land. Likely pesticide and/or herbicide use.
- Site 3284-22: Residence. Potential ACM and lead paint.
- Site 3284-23: STA Roofing and Siding. Transformer; potential ACM and lead paint.
- Site 3284-24: Residence. Transformer; potential ACM and lead paint.
- Site 3284-25: Residences. Solid waste; natural gas pipelines; transformers; potential ACM and lead paint.
- Site 3284-27: Residence. Natural gas pipeline; likely natural gas pipelines; potential ACM and lead paint.
- Site 3284-28: Executive Affiliates. Natural gas pipelines; transformer; potential ACM and lead paint.
- Site 3284-29: B&R Storage. Natural gas pipelines; transformers; potential ACM and lead

- paint.
 - Site 3284-31: Agricultural land. Transformer; likely pesticide and/or herbicide use.
 - Site 3284-32: Farmstead. Transformer; potential pesticide and/or herbicide presence.
 - Site 3284-33: Cyrus Ministries International. Potential ACM and lead paint.
 - Site 3284-34: Sugar Grove Self-Storage. Transformers; potential ACM and lead paint.
 - Site 3284-36: Residence. Transformer; potential ACM and lead paint.
 -
- (3) No RECs or de minimis conditions were identified at the following site along the project:
- Site 3284-37. Vacant land.
- (4) The following property that appears on environmental databases was identified as being adjoining, but not along, the project.
- Site 3284-A. BNSF Railroad.
- (5) According to Flood Insurance Rate maps, the far-eastern portion of the project area is located within the Special Flood Hazard Area of Welch Creek. Flooding, standing water, and saturated soils may be encountered in this area, particularly during periods of high or extended rainfall or spring snowmelt.
- (6) For the purposes of this report, the following are considered to be de minimis conditions:
- Normal use of lead-based paint on exteriors and interiors of buildings and structures.
 - Use of asbestos-containing materials in building construction.
 - Transformers in normal use, unless the transformers were observed to be leaking, appear on an environmental regulatory list, or were otherwise determined to pose a hazard not related to normal use.
 - Agricultural use of pesticides and herbicides. In addition, most land in Illinois was under agricultural use prior to its conversion to residential, industrial, or commercial development. Pesticides, both regulated and otherwise, may have been used throughout the project area at any time. Unless specifically discussed elsewhere in this report, no information regarding past pesticide use that would be subject to enforcement action was located for this project, and such use is considered a de minimis condition.

ADDRESS LISTINGS

The following addresses along the project were evaluated for this project. Addresses of sites, if any, adjoining but not along the project are not listed here; see text for discussion of these sites.

Property name and address	ISGS site #	Parcel #
Residence 100 W. Galena Street, Big Rock	3284-1	NA
Residence 101 W. Galena Street, Big Rock	3284-1	NA
Residence 103 W. Galena Street, Big Rock	3284-1	NA
Residence 105 W. Galena Street, Big Rock	3284-1	NA
Residence 107 W. Galena Street, Big Rock	3284-1	NA
Residence 102 W. Galena Street, Big Rock	3284-2	NA
Residence 102 W. First Street, Big Rock	3284-3	NA
Storage building 103 W. First Street, Big Rock	3284-4	NA
Hinckley-Big Rock Middle School 47W984 US 30, Big Rock	3284-5	NA
Park 408 Rhodes Avenue, Big Rock	3284-6	NA
Residence 102 Rhodes Avenue, Big Rock	3284-7	NA
Residence 104 Rhodes Avenue, Big Rock	3284-7	NA
Residence 103 Jefferson Avenue, Big Rock	3284-8	NA
AT&T 100 block of Galena Street, Big Rock	3284-9	NA
Residence 101 E. Galena Street, Big Rock	3284-10	NA

Big Rock Café 103 E. Galena Street, Big Rock	3284-11	NA
Residence 200 E. Galena Street, Big Rock	3284-12	NA
Nourishing Medicine 202 E. Galena Street, Big Rock	3284-13	NA
Residence 204 E. Galena Street, Big Rock	3284-14	NA
Residence 206 E. Galena Street, Big Rock	3284-14	NA
Residence 208 E. Galena Street, Big Rock	3284-14	NA
Residence 47W690 US 30, Big Rock	3284-14	NA
Residence 47W780 US 30, Big Rock	3284-14	NA
Residence 100 Jefferson Avenue, Big Rock	3284-15	NA
Residence 102 Jefferson Avenue, Big Rock	3284-15	NA
Residence 207 E. Galena Street, Big Rock	3284-15	NA
Residence 209 E. Galena Street, Big Rock	3284-15	NA
Residence 211 E. Galena Street, Big Rock	3284-15	NA
Residence 213 E. Galena Street, Big Rock	3284-15	NA
Residence 215 E. Galena Street, Big Rock	3284-15	NA
Residence 47W893 US 30, Big Rock	3284-15	NA
Agricultural land 47W600 block of US 30, Big Rock	3284-16	NA
Agricultural land 47W600 block of US 30, Big Rock	3284-17	NA

Dekane Equipment Corporation 47W619 US 30, Big Rock	3284-18	NA
J.C Customs 47W518 US 30, Big Rock	3284-19	NA
Vacationland 47W529 US 30, Big Rock	3284-20	NA
Agricultural land 47W400 block of US 30, Big Rock	3284-21	NA
Residence 47W481 US 30, Big Rock	3284-22	NA
STA Roofing and Siding 47W435 US 30, Big Rock	3284-23	NA
Residence 47W415 US 30, Big Rock	3284-24	NA
Residence 47W305 US 30, Big Rock	3284-25	NA
Residence 47W341 US 30, Big Rock	3284-25	NA
Residence 47W379 US 30, Big Rock	3284-25	NA
Residence 47W366 US 30, Big Rock	3284-26	NA
Residence 47W302 US 30, Big Rock	3284-27	NA
Executive Affiliates 47W210 US 30, Big Rock	3284-28	NA
B&R Storage 47W247 US 30, Big Rock	3284-29	NA
Ray-Vac Distributors 47W171 US 30, Big Rock	3284-30	NA
Agricultural land 46W700-47W800 blocks of US 30, Big Rock	3284-31	NA
Farmstead 46W795 US 30, Big Rock	3284-32	NA
Cyrus Ministries International 46W779 US 30, Big Rock	3284-33	NA

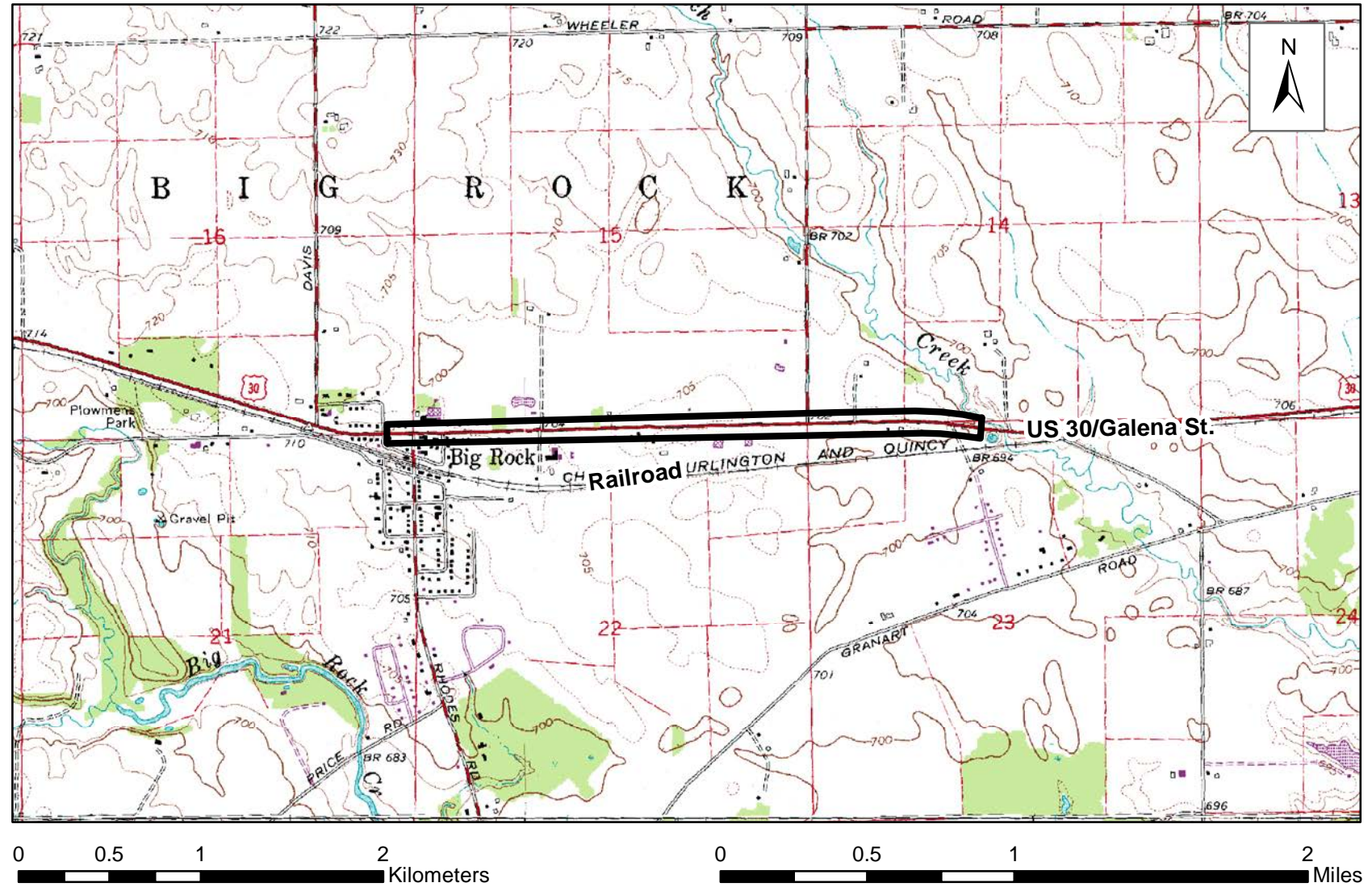
Sugar Grove Self-Storage 46W657 US 30, Big Rock	3284-34	NA
Dugan's Automotive Service 46W637 US 30, Big Rock	3284-35	NA
Residence 46W563 US 30, Big Rock	3284-36	NA
Vacant land 46W500 block of US 30, Big Rock	3284-37	NA
The following site has easement access from US 30/Galena Street, and has an address on US 30, but does not adjoin US 30:		
Former residence 46W880 US 30, Big Rock	NA	NA

LIST OF ATTACHMENTS

1. Project location map.
2. Site location maps (3 pages).
3. Site 3284-5. Monitoring well location (MW4), and former monitoring well locations (MW0-MW3, and MW5-MW6).
4. Site 3284-5. NFR letter, IEMA #932200, #992201, and #20000701 (10 pages).
5. Site 3284-11. Former UST locations.
6. Site 3284-11. NFR letter, IEMA #890327.

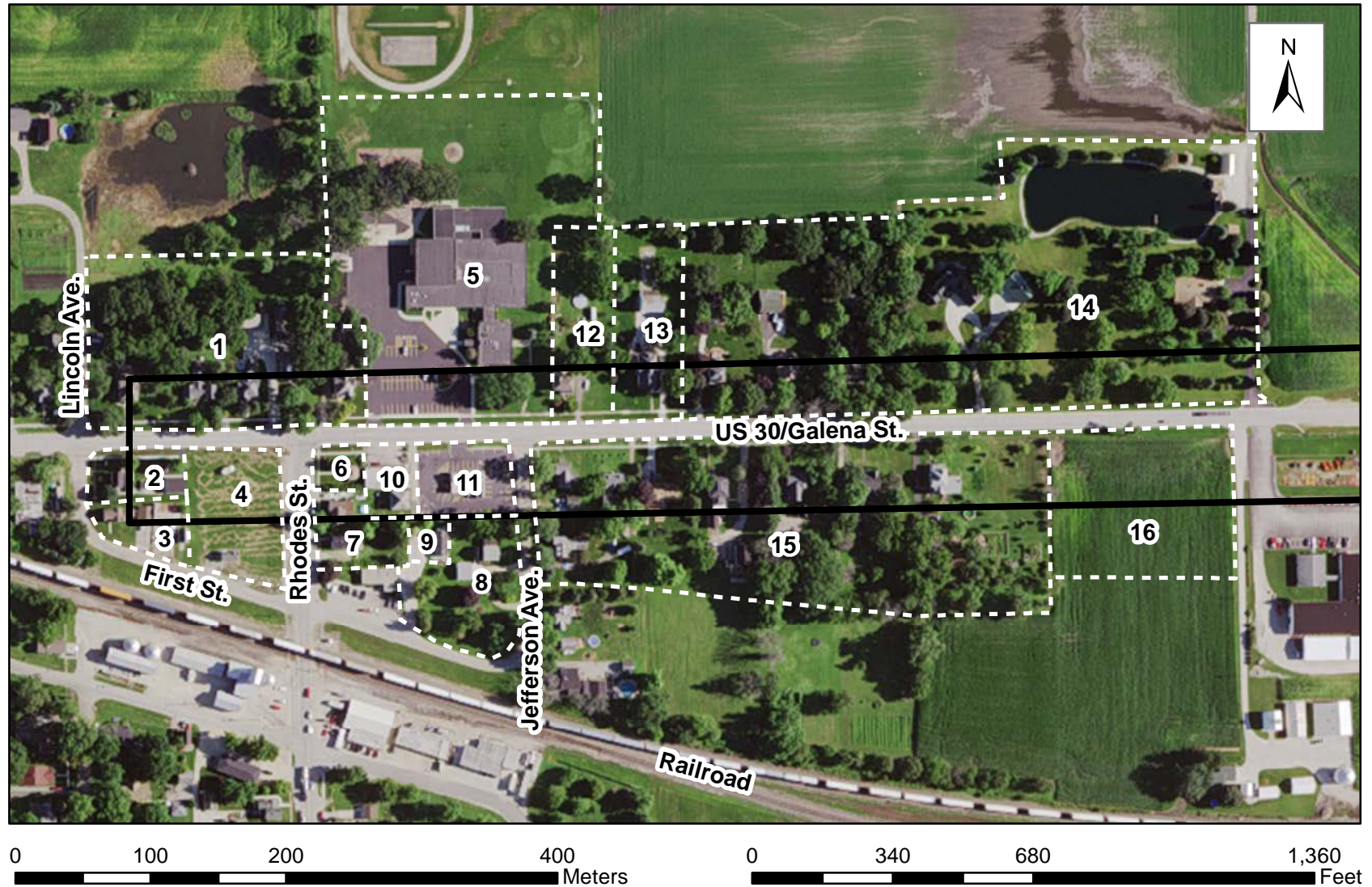
Attachment 1. Project location map, ISGS #3284.

Project area indicated by heavy black lines.



Attachment 2, page 1. Site location map, Sites 3284-1 through 3284-16.

All site boundaries are approximate and should not be used as actual parcel boundaries.



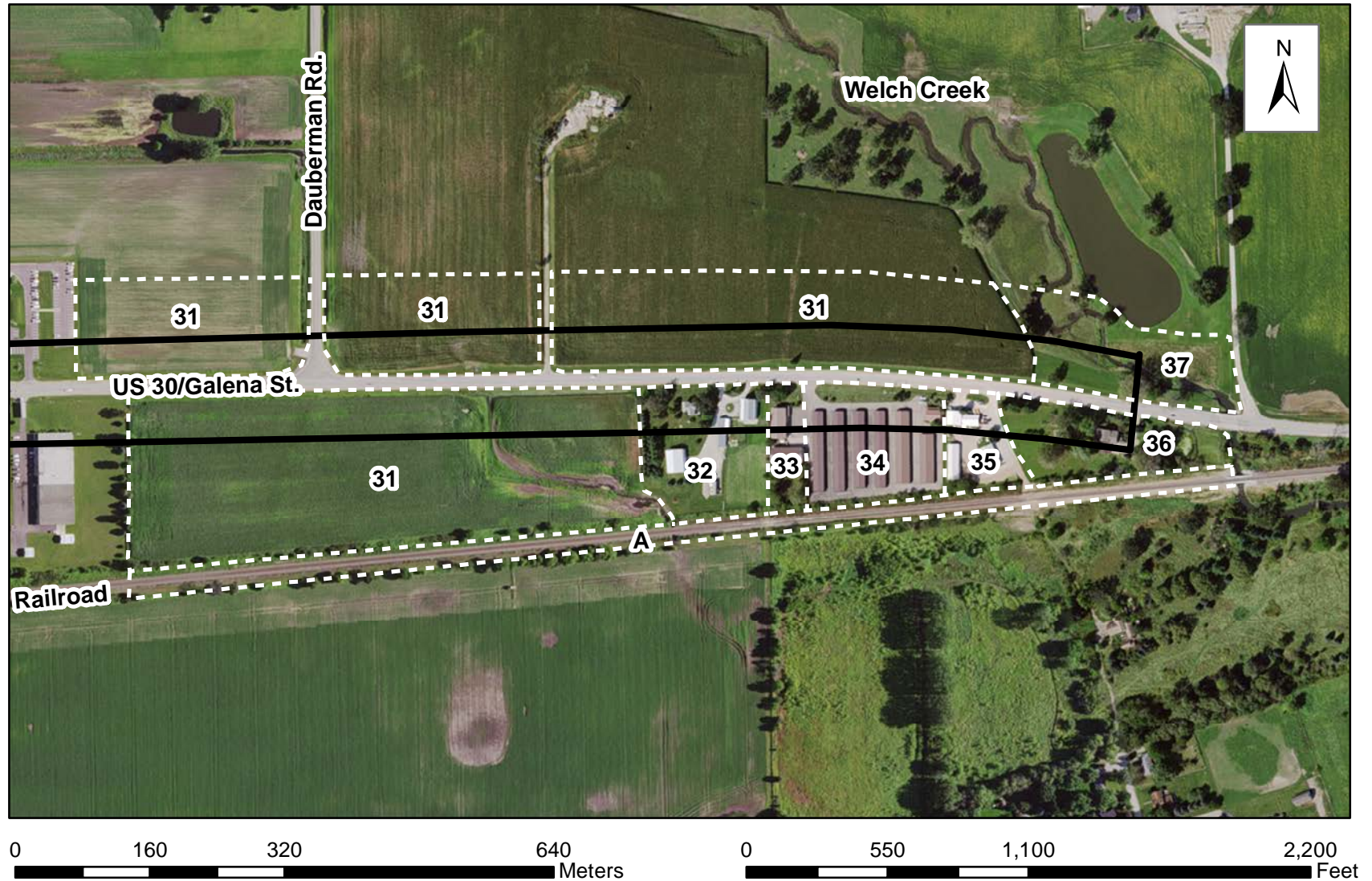
Attachment 2, page 2. Site location map, Sites 3284-17 through 3284-30.

All site boundaries are approximate and should not be used as actual parcel boundaries.

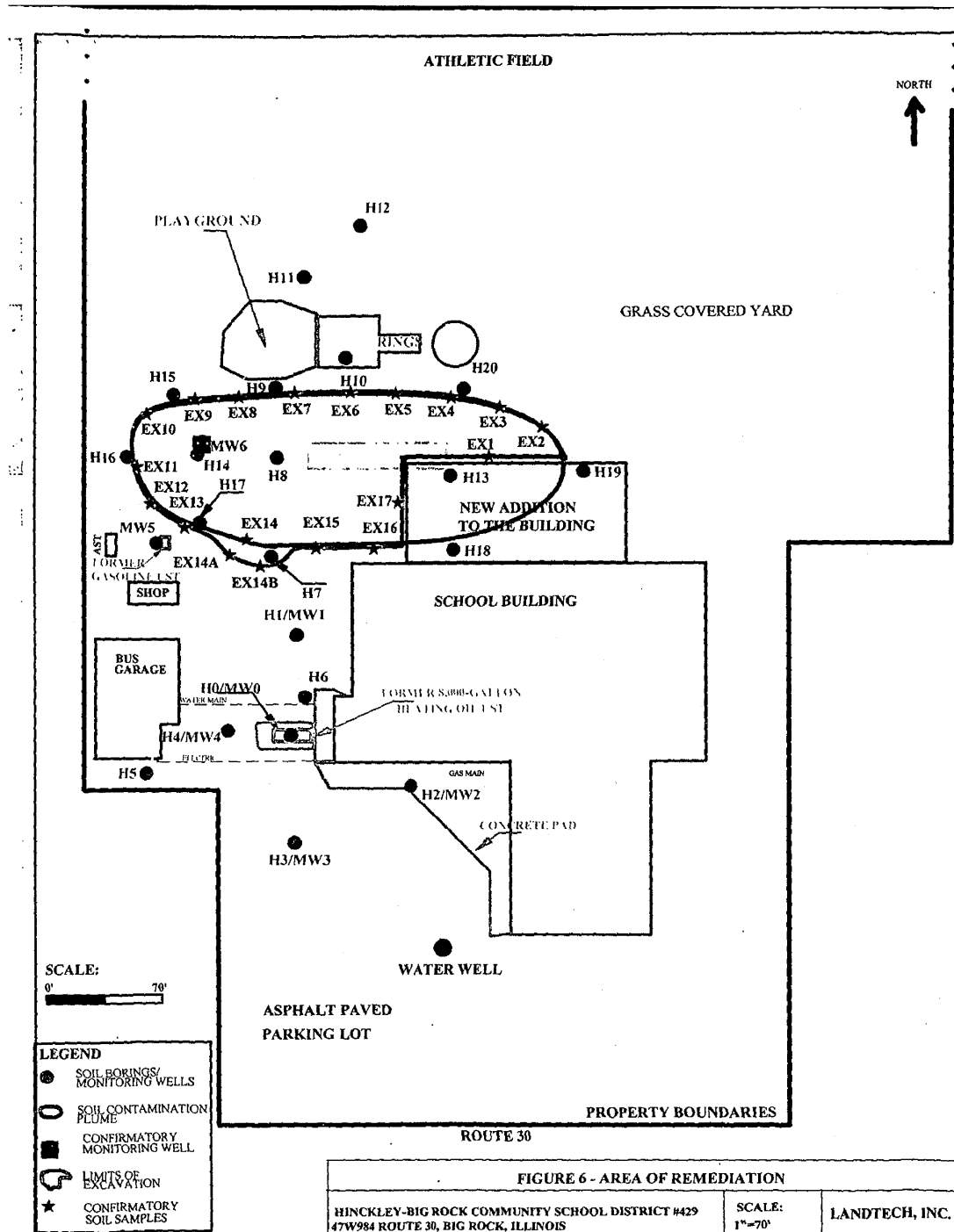


Attachment 2, page 3. Site location map, Sites 3284-31 through 3284-37.

All site boundaries are approximate and should not be used as actual parcel boundaries.



Attachment 3. Site 3284-5. Monitoring well location (MW4), and former monitoring well locations (MW0-MW3, and MW5-MW6). IEMA #932200, #992201, and #20000701. Map from Landtech, Inc.





ILLINOIS ENVIRONMENTAL PROTECTION AGENCY

1021 NORTH GRAND AVENUE EAST, P.O. BOX 19276, SPRINGFIELD, ILLINOIS 62794-9276, 217-782-3397
JAMES R. THOMPSON CENTER, 100 WEST RANDOLPH, SUITE 11-300, CHICAGO, IL 60601, 312-814-6026

ROD R. BLAGOJEVICH, GOVERNOR

RENEE CIPRIANO, DIRECTOR

217/782-6762

CERTIFIED MAIL

7002 3150 0000 1255 2018

FEB 20 2004

Hinckley Big Rock Community School Dist. #429
Attn: Glen Littlefield
P.O. Box 1210
Hinckley, IL 60520-1210

Re: LPC #0898025005 -- Kane County
Big Rock/ Hinckley Big Rock Community School Dist. #429
Rt 30
LUST Incident No. 20000701, 992201, & 932200
LUST Technical File

Dear Mr. Littlefield:

The Illinois Environmental Protection Agency (Illinois EPA) has reviewed the Corrective Action Completion Report submitted for the above-referenced incident. This information is dated December 11, 2003 and was received by the Illinois EPA on December 24, 2003. Citations in this letter are from the Environmental Protection Act (Act) and 35 Illinois Administrative Code (35 Ill. Adm. Code).

The High Priority Corrective Action Completion Report and associated Professional Engineer Certification indicate corrective action for the above-referenced site was conducted in accordance with the Corrective Action Plan approved by the Illinois EPA. The Corrective Action Completion Report demonstrates that the requirements of Section 57.7(c)(1)(E) of the Act and 35 Ill. Adm. Code 732.409(a)(2) have been satisfied.

Based upon the certification by Daniel Carolan, a Licensed Professional Engineer, and pursuant to Section 57.10 of the Act (415 ILCS 5/57.10), your request for a no further remediation determination is granted under the conditions and terms specified in this letter.

Issuance of this No Further Remediation Letter (Letter), based on the certification of the Licensed Professional Engineer, signifies that: (1) all statutory and regulatory corrective action requirements applicable to the occurrence have been complied with; (2) all corrective action concerning the remediation of the occurrence has been completed; and (3) no further corrective action concerning the occurrence is necessary for the protection of human health, safety, and the environment. Pursuant to Section 57.10(d) of the Act, this Letter shall apply in favor of the following parties:

RELEASABLE

ROCKFORD - 4302 North Main Street, Rockford, IL 61103 - (815) 987-7760 • DES PLAINES - 9511 W. Harrison St., Des Plaines, IL 60018-0087 294-4000
ELGIN - 595 South State, Elgin, IL 60123 - (847) 608-3131 • PEORIA - 5415 N. University St., Peoria, IL 61614 - (309) 693-5463
BUREAU OF LAND - PEORIA - 7620 N. University St., Peoria, IL 61614 - (309) 693-5462 • CHAMPAIGN - 2125 South First Street, Champaign, IL 61820 - (217) 278-5800
SPRINGFIELD - 4500 S. Sixth Street Rd., Springfield, IL 62706 - (217) 786-6892 • COLLINSVILLE - 2009 Mall Street, Collinsville, IL 62234 - (618) 993-7200
MARION - 2309 W. Main St., Suite 116, Marion, IL 62959 - (618) 993-7200

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1. Hinckley-Big Rock Community School District #429, the owner or operator of the underground storage tank system(s).
2. Any parent corporation or subsidiary of such owner or operator.
3. Any co-owner or co-operator, either by joint tenancy, right-of-survivorship, or any other party sharing a legal relationship with the owner or operator to whom the Letter is issued.
4. Any holder of a beneficial interest of a land trust or inter vivos trust whether revocable or irrevocable.
5. Any mortgagee or trustee of a deed of trust of such owner or operator.
6. Any successor-in-interest of such owner or operator.
7. Any transferee of such owner or operator whether the transfer was by sale, bankruptcy proceeding, partition, dissolution of marriage, settlement or adjudication of any civil action, charitable gift, or bequest.
8. Any heir or devisee of such owner or operator.

This Letter and all attachments, including but not limited to the Leaking Underground Storage Tank Environmental Notice, must be filed within 45 days of receipt as a single instrument with the Office of the Recorder or Registrar of Titles in the county in which the above-referenced site is located. This Letter shall not be effective until officially recorded by the Office of the Recorder or Registrar of Titles of the applicable county in accordance with Illinois law so it forms a permanent part of the chain of title for the above-referenced property. Within 30 days of this Letter being recorded, an accurate and official copy of this Letter, as recorded, shall be obtained and submitted to the Illinois EPA. For recording purposes, it is recommended that the Leaking Underground Storage Tank Environmental Notice of this Letter be the first page of the instrument filed.

CONDITIONS AND TERMS OF APPROVAL

LEVEL OF REMEDIATION AND LAND USE LIMITATIONS

1. The remediation objectives for the above-referenced site, more particularly described in the Leaking Underground Storage Tank Environmental Notice of this Letter, were established in accordance with the requirements of the Tiered Approach to Corrective Action Objectives (35 Ill. Adm. Code 742) rules.

Page 3

2. As a result of the release from the underground storage tank system(s) associated with the above-referenced incident, the above-referenced site, more particularly described in the attached Leaking Underground Storage Tank Environmental Notice of this Letter, shall not be used in a manner inconsistent with the following land use limitation: There are no land use limitations.
3. The land use limitation specified in this Letter may be revised if:
 - a. Further investigation or remedial action has been conducted that documents the attainment of objectives appropriate for the new land use; and
 - b. A new No Further Remediation Letter is obtained and recorded in accordance with Title XVII of the Act and regulations adopted thereunder.

PREVENTIVE, ENGINEERING, AND INSTITUTIONAL CONTROLS

4. Preventive: None.

Engineering: A three-foot thick clean soil barrier must be maintained within three (3) feet of the land surface to inhibit the ingestion of the contaminated media, as outlined in the area of concern on the attached Site Base Map.

Institutional: This Letter shall be recorded as a permanent part of the chain of title for the above-referenced site, more particularly described in the attached Leaking Underground Storage Tank Environmental Notice of this Letter.
5. Failure to establish, operate, and maintain controls in full compliance with the Act, applicable regulations, and the approved Corrective Action Plan, if applicable, may result in voidance of this Letter.

OTHER TERMS

6. Any contaminated soil or groundwater removed or excavated from, or disturbed at, the above-referenced site, more particularly described in the Leaking Underground Storage Tank Environmental Notice of this Letter, must be handled in accordance with all applicable laws and regulations under 35 Ill. Adm. Code Subtitle G.

Page 4

7. Further information regarding the above-referenced site can be obtained through a written request under the Freedom of Information Act (5 ILCS 140) to:

Illinois Environmental Protection Agency
Attention: Freedom of Information Act Officer
Bureau of Land - #24
1021 North Grand Avenue East
Post Office Box 19276
Springfield, IL 62794-9276

8. Pursuant to 35 Ill. Adm. Code 732.704, should the Illinois EPA seek to void this Letter, the Illinois EPA shall provide notice to the owner or operator of the leaking underground storage tank system(s) associated with the above-referenced incident and the current title holder of the real estate on which the tanks were located, at their last known addresses. The notice shall specify the cause for the voidance, explain the provisions for appeal, and describe the facts in support of the voidance. Specific acts or omissions that may result in the voidance of this Letter include, but shall not be limited to:

- a. Any violation of institutional controls or industrial/commercial land use restrictions;
- b. The failure to operate and maintain preventive or engineering controls or to comply with any applicable groundwater monitoring plan;
- c. The disturbance or removal of contamination that has been left in-place in accordance with the Corrective Action Plan or Completion Report;
- d. The failure to comply with the recording requirements for the Letter;
- e. Obtaining the Letter by fraud or misrepresentation; or
- f. Subsequent discovery of contaminants, not identified as part of the investigative or remedial activities upon which the issuance of the Letter was based, that pose a threat to human health or the environment.

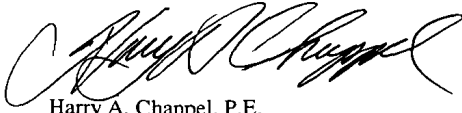
Submit an accurate and official copy of this Letter, as recorded, to:

Illinois Environmental Protection Agency
Bureau of Land - #24
Leaking Underground Storage Tank Section
1021 North Grand Avenue East
Post Office Box 19276
Springfield, IL 62794-9276

Page 5

If you have any questions or need further assistance, please contact the Illinois EPA project manager, Jason Donnelly, at (217) 557-8764.

Sincerely,

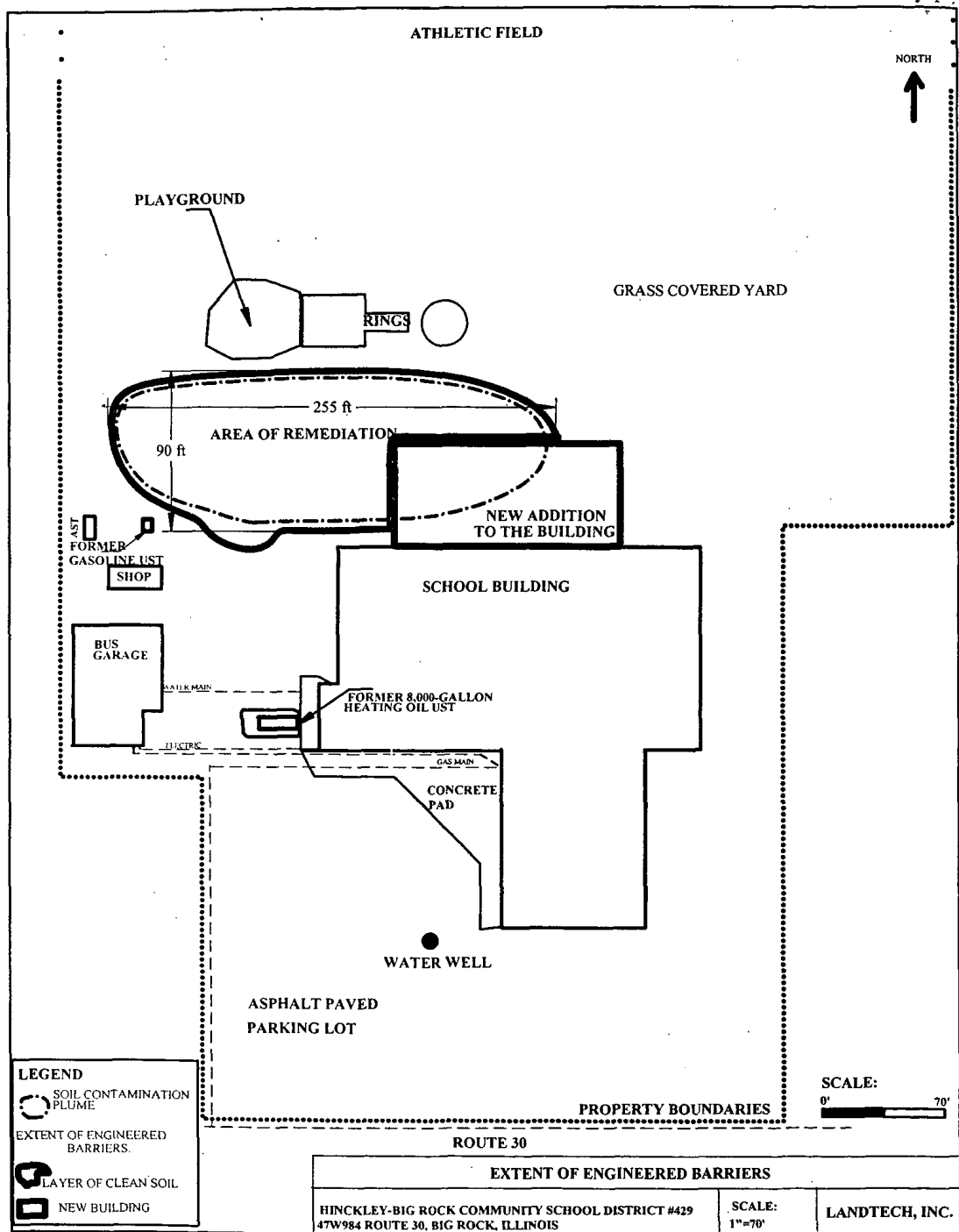


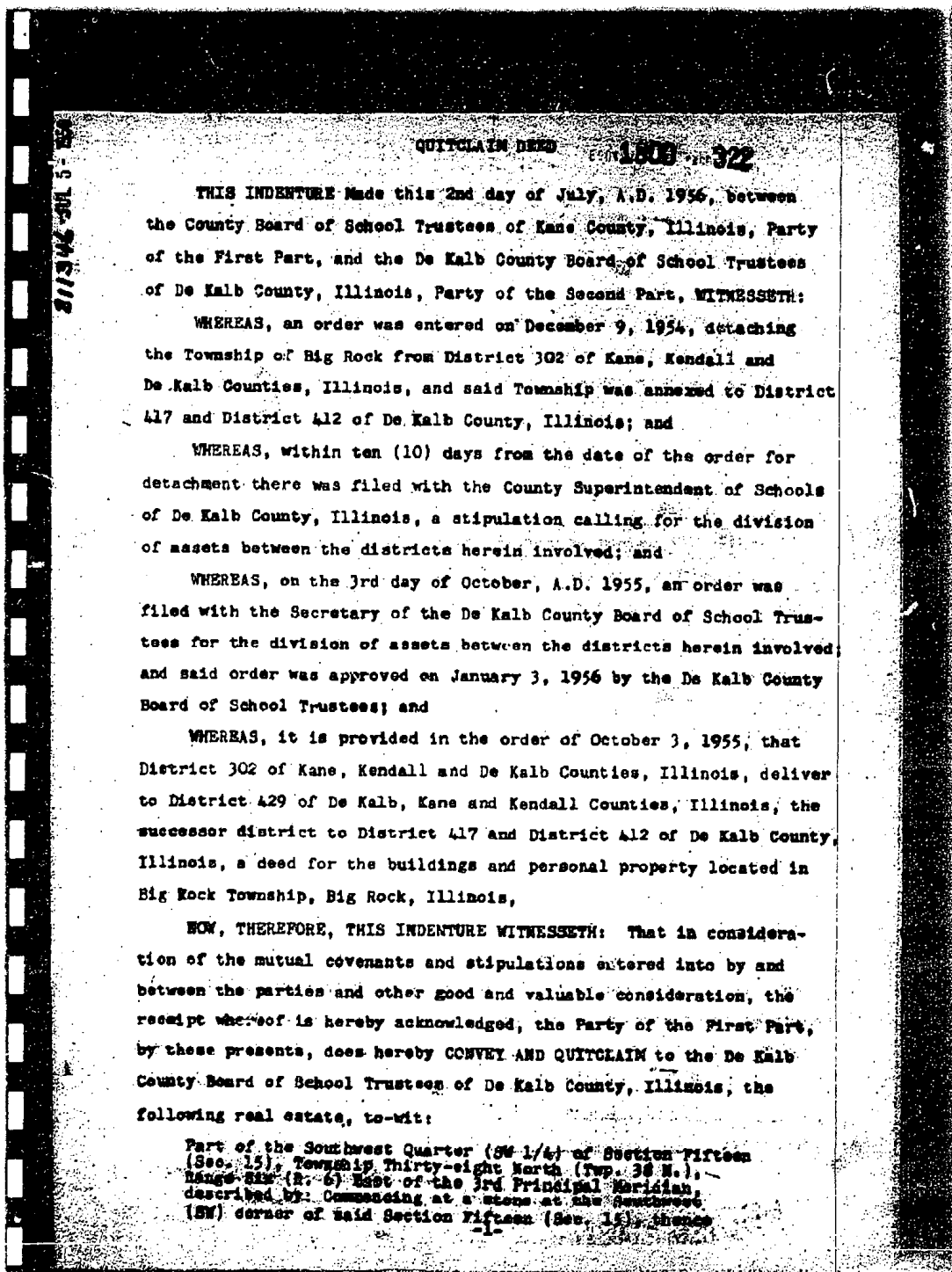
Harry A. Chappel, P.E.
Unit Manager
Leaking Underground Storage Tank Section
Division of Remediation Management
Bureau of Land

HAC:JD\000701,992201,932200-NFR

Attachments: Leaking Underground Storage Tank Environmental Notice
Site Base Map (Engineered Barriers)
Legal Description

c: LandTech, Inc., Ramona Cornea
Division File



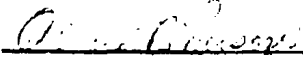



North (N) along said Section Line Two Hundred Six and Twenty-five Hundredths Feet (206.25') for a place of beginning; thence continuing north on said Section Line Four Hundred Five and Ninety-five Hundredths Feet (405.95'); thence east and parallel to the South Line of said Section Fifteen (Sec. 15) Five Hundred and Four and One Tenth Feet (504.1'); thence south and parallel to the West Line of said Section Fifteen, Two Hundred Forty-three Feet (243'); thence west and parallel to the South Line of said Section Fifteen (Sec. 15) One Hundred Thirty-four and no tenths Feet (134.0'); thence south and parallel to the West Line of said Section Fifteen (Sec. 15) One Hundred Fifty-four and One Tenth Feet (154.1'); thence west and parallel to the South Line of said Section Fifteen (Sec. 15), Six and Seventy-seven Hundredths Feet (6.77'); thence south and parallel to the West Line of said Section Fifteen, Two Hundred Six and Twenty-five Hundredths Feet (206.25'), to the South Line of said Section Fifteen (Sec. 15); thence west along said Section Line Two Hundred Eighty and Thirty-three Hundredths Feet (280.33'); thence north and parallel to the West Line of said Section Fifteen (Sec. 15) Two Hundred Six and Twenty-five Hundredths Feet (206.25'); thence west and parallel to the South Line of said Section Fifteen (Sec. 15), Eighty-three and no Tenths Feet (83.0') to the beginning place, all situated in the Township of Big Rock, Kane County, Illinois, together with all personal property located thereon.

TOGETHER WITH ALL AND SINGULAR the hereditaments and appurtenances thereto belonging or in any wise appertaining, and all the right, title, interest, claim, and demand, whatsoever at law or equity, which was given to the Kane County Board of School Trustees in Chapter 122, Section 4b-26 of the Revised Statutes of the State of Illinois.

TO HAVE AND TO HOLD, the same unto the said Party of the Second Part and their successors in office forever, as fully and effectually to all intents and purposes in law as the Party of the First Part might, could or ought to convey by virtue of the powers given and granted to it in Chapter 122, Section 4b-26 of the Revised Statutes of the State of Illinois.

IN WITNESS WHEREOF, the said Kane County Board of School Trustees of Kane County, Illinois, has caused these presents to be signed by its President and attested by its Secretary, this 2nd day of July, A.D. 1956.


By _____ President.

Attest: 
Secretary for Kane

-2-

1800-323

State of Illinois)
County of Kane)

I, Stannie Olson, a notary public in and for said County, in the State aforesaid, do hereby certify that Arnold P. Benson and ^{PAUL G. WITTE} ~~St. S. Wades~~ are personally known to me to be the President and Secretary respectively of the Kane County Board of School Trustees of Kane County, Illinois, whose names subscribed to the foregoing instrument, appeared before me this day in person and severally acknowledged that as such President and Secretary, they signed and delivered the said instrument as President and Secretary of said Kane County Board of School Trustees, and pursuant to the authority given to them by a majority of the Kane County Board of School Trustees of Kane County, Illinois, as their free and voluntary act and as the free and voluntary act and deed of said Board, for the uses and purposes therein set forth.

Given under my hand and notarial seal this 2nd day of July, A.D.

1956.



Stannie Olson
Notary Public.

Document No. 811348 Filed JUL -5 1956
Filed for record in Recorder's Office of Kane County, Illinois
at 9 O'clock A.M. Shaw C. Markson
Recorder of Deeds

PREPARED BY:

Name: Hinckley-Big Rock Community School District #429

Address: Route 30
Big Rock, IL 60511

RETURN TO:

Name: Hinckley-Big Rock Community School District #429
Attn: Glen Littlefield

Address: P.O. Box 1210
Hinckley, IL 60520-1210

(THE ABOVE SPACE FOR RECORDER'S OFFICE)

LEAKING UNDERGROUND STORAGE TANK ENVIRONMENTAL NOTICE

THE OWNER AND/OR OPERATOR OF THE LEAKING UNDERGROUND STORAGE TANK SYSTEM(S) ASSOCIATED WITH THE RELEASE REFERENCED BELOW, WITHIN 45 DAYS OF RECEIVING THE NO FURTHER REMEDIATION LETTER CONTAINING THIS NOTICE, MUST SUBMIT THIS NOTICE AND THE REMAINDER OF THE NO FURTHER REMEDIATION LETTER TO THE OFFICE OF THE RECORDER OR REGISTRAR OF TITLES OF KANE COUNTY IN WHICH THE SITE DESCRIBED BELOW IS LOCATED.

Illinois EPA Number: 0898025005

LUST Incident No.: 20000701, 992201, and 932200

Hinckley-Big Rock Community School District #429, the owner and/or operator of the leaking underground storage tank system(s) associated with the above-referenced incident, whose address is P.O. Box 1210, Hinckley, IL has performed investigative and/or remedial activities for the site identified as follows and depicted on the attached Site Base Map:

1. Legal Description or Reference to a Plat Showing the Boundaries: See attached
2. Common Address: Route 30, Big Rock, IL
3. Real Estate Tax Index/Parcel Index Number: 13-15-300-002, 13-15-300-004, 13-15-300-027, and 13-15-300-028
4. Site Owner: Hinckley-Big Rock Community School District #429
5. Land Use Limitation: There are no land use limitations.
6. See the attached No Further Remediation Letter for other terms.

HAC:JD

Attachment: Site Base Map
Legal Description

Leaking Underground Storage Tank Environmental Notice

RECEIVED
MAR 08 1989
UST PROGRAM

BIG ROCK FIRE PROTECTION DISTRICT
BIG ROCK, ILL.
MARCH 3, 1989

RTE 30

JEFFERSON

FIRE HOUSE

24'

46'

60'

20'

44'

1N

2N

TRUCK (Lined)

TRUCKS MOVED
THIS LOCATION

N

DRAWING NOT TO SCALE
DRAWN BY: P. QUINN
3/6/89

652000*88000

RECEIVED
MAR 08 1989
UST PROGRAM

BIG ROCK FIRE PROTECTION DISTRICT
BIG ROCK, ILL.
MARCH 3, 1989

RTE 30

FIRE HOUSE

JEFFERSON

DRAWING NOT TO SCALE
DRAWN BY: P. QUINN
3/6/69



Illinois Environmental Protection Agency

1701 First Avenue, Maywood, IL 60153

Refer to:

0898033004
0890000000/Kane Co.
Big Rock/Fire Protection Dist.
LUST/890327
Leak Report

April 26, 1990

Mr. John Ruh
Big Rock Fire Dept.
103 E. Route 30
Big Rock, IL 60511

Dear Mr. Ruh:

The Agency has reviewed the remediation report prepared by HUNTER/KECK for the subject site. The sample results indicate, and the Agency concurs, that the underground storage tank area requires no further remediation.

Please contact me at the above number if you have any questions.

Sincerely,

Kenneth L. Page
Kenneth L. Page, Project Manager
Immediate Removal Unit
Remedial Project Management Section
Division of Land Pollution Control

KLP:ML:bh:4915B

cc: ~~Division File~~
Maywood Region

RECEIVED

- 1 MAY 1990

IEPA/DLPC



Illinois Department of Transportation

Memorandum

To: Maureen Kastl Attn: William Raffensperger
From: Maureen Addis By: Brad Koldehoff
Subject: Cultural Resource Clearance
Date: October 20, 2017

**Kane County
FAS 1107, Dauberman Road
Sec. 15-00277-01-BR
Seq. 19997A**

For the above referenced project, IDOT's qualified professional Cultural Resources staff hereby make a **"No Historic Properties Affected"** finding pursuant to Section 106 of the National Historic Preservation Act.

This determination follows the stipulations of the Section 106 Programmatic Agreement for the Delegation of Authority for Minor Projects of the Federal Aid Highway Program in the State of Illinois, executed by FHWA, Illinois SHPO, IDOT and the Advisory Council on Historic Preservation. This project is consistent with the minor project types listed in Appendix B of the agreement.

The attached stamped IDOT Environmental Survey Request form documents that no further coordination for this project is required for cultural resources.

Attachment

BK:km

Environmental Survey Request Addendum

A. Project Information

☒ Bio ☒ Cultural ☐ Wetlands ☐ Special Waste

Submittal Date: 10/04/2017 Sequence No: 19997 A
District: 1 Requesting Agency: Local Kane County DOT Project No:
Contract #: Job No.: P- 91-992-16
County: Kane
Route: FAS 1107 Marked: CH 62
Street: Dauberman Road Section: 15-00277-01-BR
Municipality(ies): Big Rock Project Length: 1.6093 km 1 miles
From To (At): Granart Road (south) to US Route 30 (North)
Quadrangle: Big Rock and Plano Township-Range-Section: T38N, R6E, S22, S23, S14, S15
Survey Target Date: 04/15/2018 Anticipated Design Approval: 04/30/2018 Anticipated NEPA Processing:

B. Reason for Submittal: (Check all that apply) ; includes SW Level 1 Screening Criteria

Survey Types: B = Biological; C = Cultural; SW = Special Waste

B,C,SW ☒ Involves Acquisition of additional ROW or temporary or permanent easements. Addendum: 82 acres
Total Project: 15.200 acres
SW ☐ On a state-maintained route, crosses or involves RR ROW (except a single rail rural ROW with no maintenance facilities).
B,C ☐ Requires in-stream work (e.g., drainage structure runaround). Stream Name:
C ☐ Potential to affect a historic district or historic property.
C ☐ Involves replacement or rehabilitation of a bridge/culvert 40 years old or older.
SW ☐ Involves acquisition of, excavation (defined in BDE Manual 27-3.01) on, or subsurface utility relocation on State ROW.
☐ Other:

C. Addendum Description:

Reasons for addendum:
Shift in location of proposed jughandle connector from NW quadrant of Dauberman/Route 30 to NE Quadrant Also, a potential bikepath along Granart connecting to the Big Rock Forest Preserve path is being considered.

D. Tree Removal?: No Number?: ha/ acres Within Mahomet SSA Project Review Area

Wetland delineation performed by: Consultant End. Species Consultation performed by: BDE

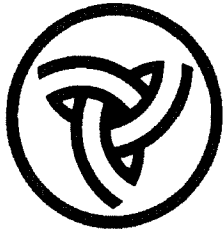
E. Contact Person: Marilyn Solomon Local Contact Person: Candi Thomas
Telephone #: (847) 705-4407 ext. Telephone #: (630) 406-7355 ext.
Env.Contact: E-Mail: ThomasCandance@co.kane.il.us
Telephone #: Title/Company:

F. ☐ Update Entire Project

☐ Addendum Only

☐ Closed ☐ PSI/RMP Only ☐ ESR Rec'd In CO ☐ SW Rec'd

**CULTURAL
RESOURCES:**
NO SURVEY OR FURTHER
COORDINATION REQUIRED
Paul Kellehoff 10/20/17
SIGNED DATE



Illinois Department of Transportation

Memorandum

To: Maureen Kastl Attn: Bill Raffensperger
From: Maureen Addis By: Brad Koldehoff
Subject: Cultural Resource Concurrence
Date: February 8, 2017

**Kane County
Big Rock
FAS 1107, CH 62, Dauberman Road
Road Construction
Section # 15-00277-01-BR
IDOT Seq. # 19997**

The attached letter documents the concurrence of the Illinois State Historic Preservation Officer (SHPO) supporting the "No Historic Properties Affected" determination found by IDOT's professional cultural resources staff for this roadway construction project.

This concurrence completes the necessary cultural resource coordination for the above referenced project.

Attachment

BK:el



Illinois Department of Transportation

2300 South Dirksen Parkway / Springfield, Illinois / 62764

RECEIVED
JAN - 6 2017
Dile 010617
Preservation Services

Kane County
Big Rock
FAS 1107, CH 62, Dauberman Road
Road Construction
Section: 15-00277-01-BR
IDOT Sequence #19997
ISAS Log #16086

January 6, 2017



IHPA REVIEW
H/A _____
AC _____
AR _____
File _____

FEDERAL - Section 106 Project

NO HISTORIC PROPERTIES AFFECTED

Dr. Rachel Leibowitz
Deputy State Historic Preservation Officer
Illinois Historic Preservation Agency
1 Old State Capitol Plaza
Springfield, Illinois 62701

Dear Dr. Leibowitz:

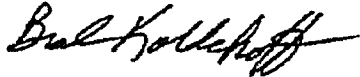
Enclosed are copies of the Phase I Survey Report completed by Illinois State Archaeological Survey (ISAS) personnel concerning archaeological and historical resources potentially impacted by the above referenced project. Survey of the 225-acre project area, or Area of Potential Effect (APE), resulted in the identification of 11 archaeological sites: 11K182, 11K1072, 11K1075, 11K1082, 11K1083, 11K1085, 11K1128, 11K1182, 11K1298, 11K1405, and 11K1406. Two are newly identified, while nine are previously recorded. All lack integrity and information potential and do not warrant National Register consideration.

Architectural resources eligible for National Register consideration were identified within the APE by Megan Gilbert, ISAS Historic Architecture Specialist, but none will be affected by the undertaking (see attached memo).

In coordination with the Federal Highway Administration (FHWA) and in accordance with the **Programmatic Agreement for Minor Projects of the Federal Aid Highway Program in Illinois**, we requests the concurrence of the State Historic Preservation Officer (SHPO) in our determination that no historic properties subject to protection under Section 106 of the National Historic Preservation Act of 1966 will be affected by the project.

If SHPO does not object to the "no historic properties affected" finding within 30 days, FHWA's responsibilities under section 106 are fulfilled (36 CFR 800.4(d)(1)(i)).

Sincerely,



Brad H. Koldehoff
Cultural Resources Unit
Bureau of Design & Environment

CONCUR

By: Rachel Leibowitz
Deputy State Historic Preservation Officer

Date: 2-7-17



**ILLINOIS STATE
ARCHAEOLOGICAL SURVEY**
PRAIRIE RESEARCH INSTITUTE

Central Offices
23 Stadium Drive • Champaign, IL 61820
T 217-244-4244 • F 217-244-7458
www.isas.illinois.edu

June 28, 2016

**To: Brad H. Koldehoff, Cultural Resources Unit Chief, IDOT
Emilie M. Land, Historic Architectural Compliance Specialist, ISAS**

From: Megan E. Gilbert, Historic Architectural Compliance Specialist, ISAS

Subject: Dauberman Road Extension/New Road Alignment Project, Historic Resource Inventory, Big Rock, Kane County, Illinois, IDOT Seq. # 19997

The Dauberman Road Extension/New Road Alignment project noted above contains numerous historic resources in the Environmental Survey Report (ESR) Limits, but the project has little to no potential of impacting these historic resources. Based on the scope of the project, which includes the creation of a new roadway alignment to extend Dauberman Rd. from Granart Rd. (south) to U.S. 30 (north) with a grade separation over the railroad and U.S. 30, the Area of Potential Effect (APE) was determined to be the properties directly bordering the roadways of U.S. 30, Dauberman Rd., and Granart Rd. as well as the land directly south of the Dauberman Rd. and U.S. 30 intersection to Granart Rd.

In accordance with 36 CFR §800.4, historic resources within and/or adjacent to the APE were identified. Based on a review of the National Register of Historic Places (NRHP) listings, National Historic Landmark (NHL) listings, Illinois Historic Preservation Agency's (IHPA) Historic Architectural Resources Geographic Information System (HARGIS) database, and IHPA files, our office has found the following historic resources in the APE. They will be categorized as either being listed on the NRHP (NRHP), previously determined to be NRHP-eligible by IHPA (NRHP-Eligible), designated NHLs, local landmarks (LL), extant HARGIS listings (HARGIS Property), and/or warrant NRHP consideration based on minimal information provided to our office (Warrants NRHP Consideration). All of the following historic resources are located in or directly adjacent to the APE for the project:

- **U.S. 30 (Rhodes St. to Welch Creek)**
 - *Big Rock Rock*, U.S. 30 and Rhodes St., Big Rock (HARGIS Property)
 - *Building*, 100 Galena St. (Warrants NRHP Consideration)
 - *Building*, 101 W. Galena St. (Warrants NRHP Consideration)
 - *Building*, 103 W. Galena St. (Warrants NRHP Consideration)
 - *Building*, Galena Blvd. [Parcel # 13-22-102-011] (Warrants NRHP Consideration)
 - *Building*, 206 E. Galena St. (Warrants NRHP Consideration)

- *Building*, 211 E. Galena Blvd. (Warrants NRHP Consideration)
 - *Building*, 215 E. Galena St. (Warrants NRHP Consideration)
 - *Building*, 47W379 U.S. 30 (Warrants NRHP Consideration)
- **Dauberman Road (Wheeler Rd. to U.S. 30)**
 - *None*
- **Granart Road (Camp Dean Rd. to Rhodes St.)**
 - *Building*, 7S731 Granart Rd. (Warrants NRHP Consideration)

If there are any questions concerning this project review, please contact me at mgilber@illinois.edu or 217-244-0776.

Megan E. Gilbert
Historic Architectural Compliance Specialist
Illinois State Archaeological Survey



Illinois Department of Transportation

Informal Transmittal

EP/SAS
Due 10/19/17

To:	Pete Harmet <i>ISSAM RAYAN</i>
Bureau:	Programming - Geometrics Section <i>UNIT</i>
Attn:	Jason Salley

From:	Chris Holt
Bureau:	Local Roads and Streets
By:	Marilyn Solomon/ Hannah Knight
Subject:	Kane County*
Section No.:	15-00277-01-BR

Date:	9/7/17 <i>(2ND SUBMITAL)</i>
-------	------------------------------

Please check appropriate box below:

- ☐ Take Necessary Action
- ☒ For Your Comments
- ☐ Per Your Request
- ☒ For Your Approval

- ☐ For Your Information
- ☐ See Me About the Attached
- ☐ Draft (Letter)(Memo) For my signature

- ☒ Reply
- ☒ Return
- ☐ Route
- ☐ File

Message

*Dauberman Road Extension at US Route 30

Attached is the Dauberman Road and US Route 30 Jughandle Analysis of Intersection Options. Please review and provide your comments.

Should you have any questions please contact Marilyn Solomon at 4407 or Hannah Knight at 4205. Thanks.

Marilyn Solomon / HK

Signature

Copies to

file

Response

10/25/17

MARILYN- PLEASE ASK THE COUNTY TO SUBMIT A TRAFFIC SIGNAL WARRANT STUDY FOR REVIEW & APPROVAL AT US 30 & JUG HANDLE BY THE BUREAU OF TRAFFIC. BASED ON TODAY'S MEETING, THE PROPOSED SHOULDERS ON US 30 SHOULD BE ENTIRELY HMA.

Statement that underpass lighting is not required

IN FOLLOW UP TO OUR MEETING - THE UNDERPASS LIGHTING AT DAUBERMAN & US 30 IS NOT REQUIRED.

THANKS, JASON SALLEY 4085

PROVIDE AN IDS ALONG WITH PLAN & PROFILES FOR US 30 & JUG HANDLE, CC: J L & J K (BOT)

COSIM 4.0 PRE-SCREEN MODELING RESULTS



10-30-17

01:07 PM

Dauberman Road Extension from US Route 30 to Granart Road

Performed by:	Sam Mead
Intersection Location:	Kane County
Intersection Name:	US Route 30 at Dauberman Raod
Highest Approach Volume:	1330 vph
Closest Receptor:	54 feet

Pass

Intersection PASSES Pre-Screen. COSIM analysis not required.
Highest design-year approach volume on the busiest leg of the intersection
is less than 5,000 vph or 62,500 ADT.

Please include the following statement in the project report or NEPA document:

In accordance with the IDOT-IEPA Agreement on Microscale Air Quality Assessments for IDOT Sponsored Transportation Projects, this project is exempt from a project-level carbon monoxide air quality analysis because the highest design-year approach volume on the busiest leg of the intersection is less than 5,000 vph or 62,500 ADT.

Wetlands

Submittal Date:	04/29/2016	Sequence No:	19997	
District:	1	Requesting Agency:	Local	Kane County DOT
Contract #:		Job No.:	P-	91-992-16
Counties:	Kane			
Route:	FAS 1107	Marked:		
Street:	CH 62 / Dauberman Road	Section:	15-00277-01-BR	
Municipality(ies):	Big Rock	Project Length:	1.6093 km	1 miles
FromTo (At):	Granart Road (south) to US 30 (north)			
Quadrangle:	Big Rock & Plano	Township-Range-Section:	S22, S23, S14, S15 T38N R6E	
Survey Target Date:	01/15/2017	Anticipated Design Apprvl:	01/15/2017	Cleared for Design Apprvl:
Cleared for Letting:	12/15/2017	Mitigation:	Yes	Mitigation Completed:

Initial Survey and WIE		Addendum No:							
Initiated	Due Date	Results Received	Wetland Present	District Notified	WIE Requested	WIE Received	Wetland Impacts	Resp to District	Coord Complete
						11/03/2017	Yes	11/17/2017	Yes

Comments:

Clearances: Cultural: Bio SW:

Submittal Date:	10/04/2017	Sequence No:	19997	A
District:	1	Requesting Agency:	Local	Kane County DOT
Contract #:		Job No.:	P-	91-992-16
Counties:	Kane			
Route:	FAS 1107	Marked:	CH 62	
Street:	Dauberman Road	Section:	15-00277-01-BR	
Municipality(ies):	Big Rock	Project Length:	1.6093 km	1 miles
FromTo (At):	Granart Road (south) to US Route 30 (North)			
Quadrangle:	Big Rock and Plano	Township-Range-Section:	T38N, R6E, S22, S23, S14, S15	
Survey Target Date:	04/15/2018	Anticipated Design Apprvl:	04/30/2018	Cleared for Design Apprvl:
Cleared for Letting:	12/15/2017	Mitigation:	Yes	Mitigation Completed:

Initial Survey and WIE		Addendum No:		A					
Initiated	Due Date	Results Received	Wetland Present	District Notified	WIE Requested	WIE Received	Wetland Impacts	Resp to District	Coord Complete

Comments:

Clearances: Cultural: Bio SW:

Processing

Individual Compensation Plan Required:

404 Individual Permit Required:

Mitigation Site:

Mitigation Basin:

Bank:

Accumulation:

Owner:

Name:

Location:

Size:

Types:

Quad:

Basin:

10	Farmed	No	No	No	No	0.05	.050	2.0	.100
Basin	07120007	Quadrangle	Big Rock			FQI	0.0		
Describe the work:		Excavation							
Total						3.520		11.720	

Mitigation Site Suitability Study:

Wetland Compensation Plan:

Preparer:

Preparer:

Conceptual					Final				
Plan Received	Agency	Report Sent and District Notified	Agency Response	District Notified	Plan Received	Agency	Report Sent and District Notified	Agency Response	District Notified
	IDNR					IDNR			
	USFWS					USFWS			
	COE					COE			

Monitoring

	Monitoring Reports				Monitoring Agency:
	Received	COE Notified	IDNR Notified	District Notified	
Year 1					Construction Begin Date: <input type="text"/> Construction Complete Date: <input type="text"/> Tasked Date: <input type="text"/> Monitoring Begin Date: <input type="text"/> Monitoring Complete Date: <input type="text"/>
Year 2					
Year 3					
Year 4					
Year 5					

Monitoring Comments:

Permit(s) Type: Corps Dist.: Permit Issued:

☐ Special Conditions:

☐ Permit Agreements/Commitments:

Project Phase

Project Phase Comments:

Project Overview

Submittal Date:	04/29/2016	Sequence No:	19997			
District:	1	Requesting Agency:	Local	Kane County DOT	Project No:	
Contract #:		Job No.:	P-	91-992-16		
Counties:	Kane					
Route:	FAS 1107		Marked:			
Street:	CH 62 / Dauberman Road		Section:	15-00277-01-BR		
Municipality(ies):	Big Rock		Project Length:	1.6093 km	1 miles	
FromTo (At):	Granart Road (south) to US 30 (north)					
Quadrangle:	Big Rock & Plano		Township-Range-Section:	S22, S23, S14, S15 T38N R6E		
Survey Target Date:	01/15/2017	Anticipated Design Appr.:	01/15/2017	Anticipated Processing:	CE	
Funding:	<input checked="" type="checkbox"/> Federal <input type="checkbox"/> State <input type="checkbox"/> TBP <input type="checkbox"/> MFT <input type="checkbox"/> Local Non-MFT					

Consultant:							
PTB No.:		Item No.:		PTB Date:		Prequal Level:	

Sequence No:	19997		Biological	Wetlands	Cultural	Special Waste
	Entered By		BDE	BDE	BDE	BDE
	Cleared for DA	11/20/2017	12/15/2017	2/8/2017	10/24/2016	
	Cleared for Letting	11/20/2017	12/15/2017	2/8/2017		
	Resubmittal					
	ResubmittalCleared					
	Section:	15-00277-01-BR	Job No.:	P-	91-992-16	
	FromTo (At):	Granart Road (south) to US 30 (north)				
Sequence No:	19997	A	Biological	Wetlands	Cultural	Special Waste
	Entered By		BDE	BDE	BDE	
	Cleared for DA	11/20/2017	12/15/2017	10/20/2017		
	Cleared for Letting	11/20/2017	12/15/2017	10/20/2017		
	Resubmittal					
	ResubmittalCleared					
	Section:	15-00277-01-BR	Job No.:	P-	91-992-16	
	FromTo (At):	Granart Road (south) to US Route 30 (North)				

Notice of Intent	Project Initiation Ltr to FHWA	Public Info Meeting(s)		Notice of Availability		Public Hearing	Draft	ROD/FONSI Approved
		1st	2nd	Draft	Final			

Project Phase Comments:

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Illinois Department of Transportation

Memorandum

To: Maureen E. Kastl Attn: Greg S. Lupton
From: Maureen M. Addis By: Thomas C. Brooks
Subject: Natural Resources Review
Date: November 20, 2017

Dauberman Road and Addendum A
Sec. 15-00277-01-BR
T38N/R6E/S 14
Seq. No.: 19997 and 19997A
Kane County

The proposed project involves new roadway alignment extending Dauberman Road from Granart Road to U.S. 30 with a grade separation over the railroad and U.S. 30. Addendum A is for a shift in location for a proposed jughandle connector from the northwest quadrant of Dauberman/U.S. 30 to the northeast quadrant. A bike path is being considered that would be built along Granart Road connecting to the Big Rock Forest Preserve path. The project takes place in Big Rock.

The overall project requires 15.2 acres of land acquisition. There will be no in stream work. There will be an unknown but likely minimal quantity of trees removed. The land cover in the vicinity of the project is agricultural. There are very few trees in the project area.

Review for Illinois Endangered Species Protection and Illinois Natural Areas Preservation – Part 1075

The Illinois Natural Heritage Database contains no record of State-listed threatened or endangered species, Illinois Natural Area Inventory sites, dedicated Illinois Nature Preserves, or registered Land and Water Reserves in the vicinity of the project. **Therefore, consultation under Part 1075 is terminated.**

This review for compliance with 17 Ill. Adm. Code Part 1075 is valid for two years unless new information becomes available that was not previously considered; the proposed improvement is modified; or additional species, essential habitat, or Natural Areas are identified in the vicinity. If the proposed improvement has not been implemented within two years of the date of this memorandum, or any of the above listed conditions develop, a new review will be necessary.

Review for Illinois Interagency Wetland Policy Act – Part 1090

The proposed improvement was surveyed for wetlands. We reviewed the wetland survey report and the Wetlands Impact Evaluation form and approve both. There are eight wetlands located within the ESR limits. There will be impacts to seven wetlands totaling

3.52 acres. Compensation for permanent losses will be provided at a commercial wetland bank. Because the project will be constructed on new alignment, it is being processed as a Standard Review Action in accordance with the IDOT Wetlands Action Plan. Coordination and concurrence with IDNR is required. **Our Wetland Review under Part 1090 remains open pending receipt of IDNR's response regarding the WIE.**

Review for Endangered Species Act - Section 7

The proposed improvement was reviewed in fulfillment of our obligation under Section 7(a)(2) of the Endangered Species Act. Our review included use of the US Fish and Wildlife Service's Information for Planning and Conservation (IPaC) web-based review tool. Through IPaC, an official species list was received and is saved to the project folder. The list contains the endangered, threatened, proposed and candidate species and proposed and designated critical habitat that may be present within or in the vicinity of the proposed improvement. The following species are listed in Kane County: Northern long-eared bat (NLEB), Eastern prairie fringed orchid (EPFO), and Rusty patched bumble bee. There is no Critical Habitat in the project vicinity. Under 50 CFR 402.12(e), **the accuracy of the species list is limited to 90 days.**

Northern long-eared bat

Northern long-eared bat suitable summer habitat consists of a wide variety of forested or wooded habitats where they roost, forage, and travel and may also include some adjacent and interspersed non-forested habitats such as emergent wetlands and adjacent edges of agricultural fields, old fields and pastures. This includes forests and woodlots containing potential roosts (i.e., live trees or snags ≥ 3 inches dbh that have exfoliating bark, cracks, crevices, or hollows) as well as linear features such as fencerows, riparian forests, and other wooded corridors. These wooded areas may be dense or loose aggregates of trees with variable amounts of canopy closure. Individual trees may be considered suitable habitat when they exhibit characteristics of suitable roost trees and are within 1,000 feet of other forested or wooded habitat. Trees found in highly-developed urban areas (e.g., street trees, downtown areas) are extremely unlikely to be suitable NLEB habitat.

There will be an unknown but likely minimal quantity of trees removed as a result of this project. There are few trees within the project area, which is agricultural with several wooded patches. Examination of the Wetland Impact Evaluation form showed that the wooded patches would be avoided. There are no records of maternity roost trees, maternity colonies or hibernacula in the vicinity of the project corridor.

We assessed the potential for adverse impacts to the NLEB in accordance with the Programmatic Biological Opinion on Final 4(d) Rule for the Northern Long-Eared Bat and Activities Excepted from Take Prohibitions and determined that the proposed improvement will have no effect to the NLEB.

Eastern prairie fringed orchid

Eastern prairie fringed orchid occurs in a wide variety of habitats, from mesic prairie to wetland communities such as sedge meadows, marsh edges and even bogs. It requires full sunlight for optimum growth and flowering, which restricts it to grass- and sedge-

dominated plant communities. The substrate of the sites where it occurs ranges from neutral to mildly calcareous. Occasionally the orchid colonizes successional habitats or recolonizes previously occupied areas.

We evaluated the limits of the proposed improvement for the presence of potentially suitable EPFO habitat. Our evaluation included the use of EPFO guidance from the US Fish and Wildlife Service, Chicago Ecological Services Field Office. There are no prairies or wetlands in the project corridor. We determined there would be no effect to EPFO from the proposed improvement.

Rusty patched bumble bee

We evaluated the limits of the proposed improvement for the presence of potentially suitable Rusty patched bumble bee habitat. Our evaluation included the use of the guidance issued by USFWS dated March 21, 2017 and titled "The Rusty Patched Bumble Bee (*Bombus affinis*), Interagency Cooperation under Section 7(a)(2) of the Endangered Species Act, Voluntary Implementation Guidance" ("USFWS Interagency Guidance"). According to the guidance, if a project is outside of a high potential zone, then the USFWS advises that the incidental take coverage is not necessary (<https://www.fws.gov/midwest/endangered/insects/rpbb/guidance.html>). Therefore, if the project is outside of a high potential zone, then a "no effect" determination is appropriate.

We cross referenced the preferred habitat of the Rusty patched bumble bee with our knowledge of the project areas and determined that there is no USFWS High Potential Zone or RPBB record in the project vicinity. In accordance with Section 7 of the Endangered Species Act, we determined that there will be no effect to the Rusty patched bumble bee.

Other Federally Listed Species

We cross-referenced the preferred habitat of each of the remaining listed species with our knowledge of the project area and determined that there are no suitable habitats present. We have determined that the proposed improvement will have no effect on any of the remaining listed species.

We have determined that the proposed improvement is not likely to jeopardize the continued existence of any endangered species or threatened species or result in the destruction or adverse modification of any critical habitat.

Should the proposed improvement be modified or new information indicate listed or proposed species may be affected, consultation or additional coordination should be initiated.

Attachment — USFWS species list

SDH

Sequence #: 19997A

Dauberman Rd Add. A

Resource in Vicinity of Project Polygon

*Ducks Unlimited Wetlands

*INHS Wetland

*National Wetlands Inventory

INAI & NP w/in 1 mile

*none found

No Resource Found

*INAI

*T&E

*Nature Preserve

*Roadside Prairie Inventory

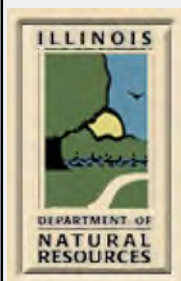
County: KANE

Section(PLSS): 3 38N6E14

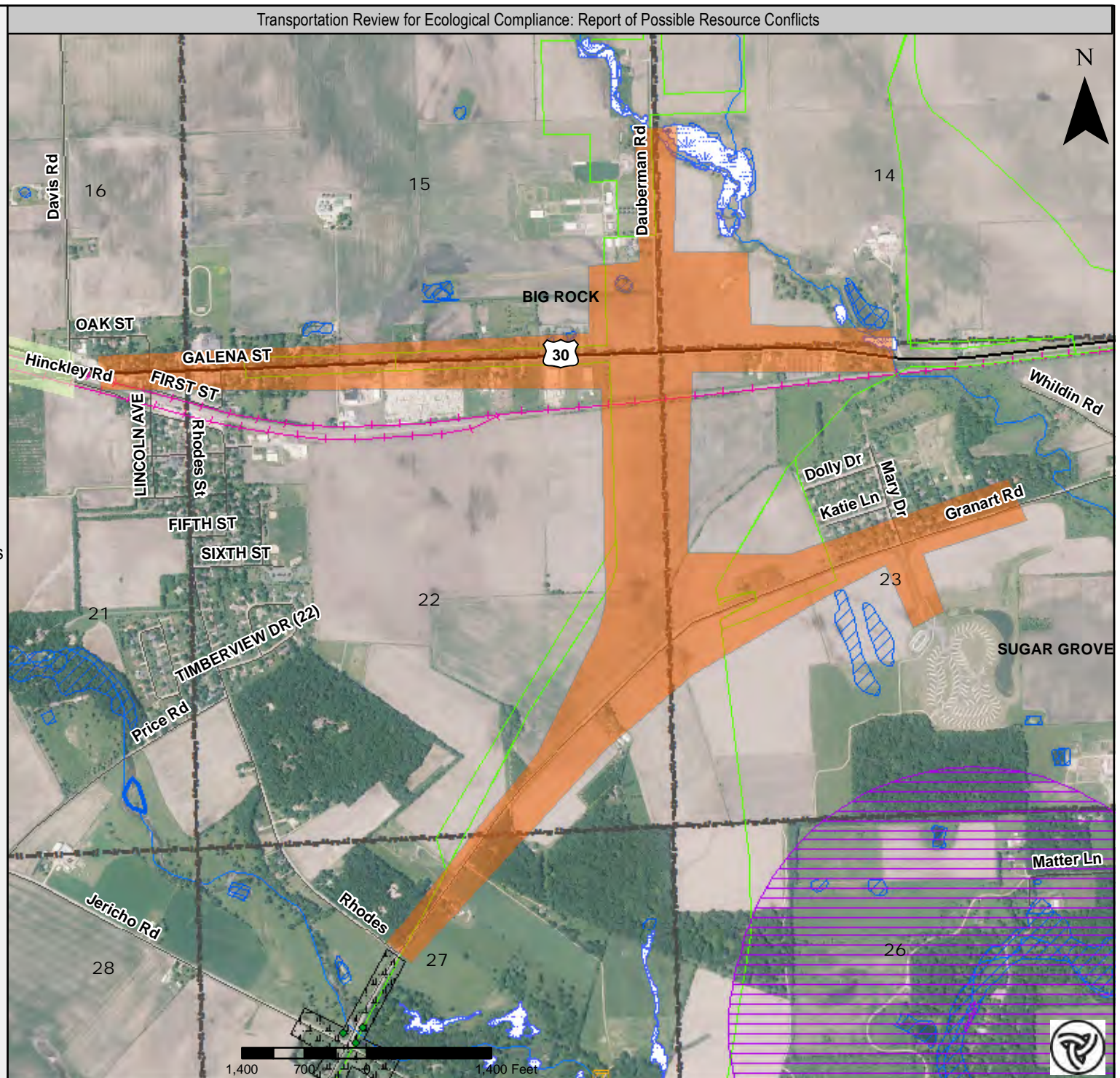
Area: 0.53992 sq. miles = 345.54607 acres

Report created by Susan Dees Hargrove

-  Threatened & Endangered Species (T&E)
-  Nature Preserve (NP)
-  Illinois Natural Areas Inventory (INAI)
-  Wetlands
-  INHS Wetland
-  Roadside Prairie Inventory



Include as additional documentation with permit applications (USACE).





United States Department of the Interior



FISH AND WILDLIFE SERVICE

Chicago Ecological Service Field Office
U.s. Fish And Wildlife Service Chicago Ecological Services Office
230 South Dearborn St., Suite 2938
Chicago, IL 60604-1507
Phone: (312) 216-4720 Fax:

<http://www.fws.gov/midwest/endangered/section7/s7process/7a2process.html>

In Reply Refer To:

November 20, 2017

Consultation Code: 03E13000-2018-SLI-0039

Event Code: 03E13000-2018-E-00124

Project Name: Dauberman Rd & Add. A, Big Rock, Kane Co, seq. 19997/A

Subject: List of threatened and endangered species that may occur in your proposed project location, and/or may be affected by your proposed project

To Whom It May Concern:

To Whom It May Concern:

The attached species list identifies any federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Please note! For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

For all other projects, continue the Section 7 Consultation process by going to our Section 7 Technical Assistance website at <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. If you are familiar with this website, you may want to go to Step 2 of the Section 7 Consultation process at <http://www.fws.gov/midwest/endangered/section7/s7process/step2.html>.

Under 50 CFR 402.12(e) (the regulations that implement Section 7 of the Endangered Species Act) the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally. You may verify the list by visiting the ECOS-IPaC website <http://ecos.fws.gov/ipac/> at regular intervals during project planning and implementation and

completing the same process you used to receive the attached list. As an alternative, you may contact this Ecological Services Field Office for updates.

Although no longer protected under the Endangered Species Act, be aware that bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 et seq.), as are golden eagles. Projects affecting these species may require measures to avoid harming eagles or may require a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website at <http://www.fws.gov/midwest/midwestbird/EaglePermits/index.html> to help you determine if you can avoid impacting eagles or if a permit may be necessary.

We appreciate your concern for threatened and endangered species. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List

Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Chicago Ecological Service Field Office

U.S. Fish And Wildlife Service Chicago Ecological Services Office

230 South Dearborn St., Suite 2938

Chicago, IL 60604-1507

(312) 216-4720

Project Summary

Consultation Code: 03E13000-2018-SLI-0039

Event Code: 03E13000-2018-E-00124

Project Name: Dauberman Rd & Add. A, Big Rock, Kane Co, seq. 19997/A

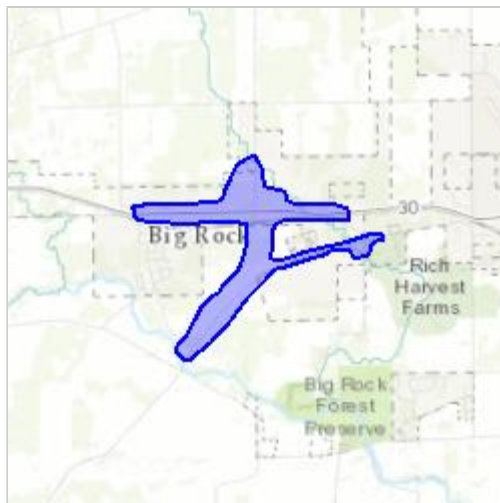
Project Type: TRANSPORTATION

Project Description: Extend Dauberman Rd from Granart Rd to US 30 w/ grade separation over RR & US 30; Add. A is shift for connector and potential bike path along Granart Rd connecting to Big Rock FP path. New ROW 15.2 ac, no instream work, unknown but likely minimal tree removal. Unknown construction date.

Project Location:

Approximate location of the project can be viewed in Google Maps:

<https://www.google.com/maps/place/41.75818613703895N88.52747740522828W>



Counties: Kane, IL

Endangered Species Act Species

There is a total of 2 threatened, endangered, or candidate species on this species list. Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions. See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

Mammals

NAME	STATUS
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/9045	Threatened

Flowering Plants

NAME	STATUS
Eastern Prairie Fringed Orchid <i>Platanthera leucophaea</i> No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: <ul style="list-style-type: none">Follow the guidance provided at https://www.fws.gov/midwest/endangered/section7/s7process/plants/epfos7guide.html Species profile: https://ecos.fws.gov/ecp/species/601 Species survey guidelines: https://ecos.fws.gov/ipac/guideline/survey/population/984/office/31131.pdf	Threatened

Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.



Illinois Department of Natural Resources

One Natural Resources Way Springfield, Illinois 62702-1271
www.dnr.illinois.gov

Bruce Rauner, Governor

Wayne A. Rosenthal, Director

December 15, 2017

Susan Dees Hargrove
Biological Resources Specialist
Illinois Department of Transportation
2300 South Dirksen Parkway, Room 330
Springfield, Illinois 62764

RE: WIE Response Seq. 19997/A, Dauberman Road, Kane County Coordination

Dear Ms. Hargrove:

The Illinois Department of Natural Resources has reviewed the above-mentioned project and has no objections. The Department concurs with the mitigation ratios and use of any of the listed mitigation banks (Gray Willows, V3 Blackberry 2, or Prairie Green 1) located within drainage basin 07120007.

Please contact me if you have any questions regarding this review.

A handwritten signature in black ink, appearing to read "Adam Rawe".

Adam Rawe
Division of Ecosystems and Environment
217-785-4991

Section 4(f) Temporary Occupancy Approval (per 23 CFR 774.13(d))

Summary Table

Date:	February 2, 2018
IDOT Region:	1
Project:	Dauberman Road Extension: US Route 30 to Granart Road See Exhibit 1, Project Location Map .
Project Description:	See Attachment A for a detailed Project Description.
Section 4(f) Resource:	Big Rock Camp Ground, Forest Preserve District of Kane County (Forest Preserve)
Type of 4(f) Resource:	Forest Preserve/Recreational Area Camping (tents and RVs), hiking trails, equestrian trails, water access (Welch Creek and Siegler Lake), fishing, cropland
Impact on 4(f) Resource:	Construction of a multi-use path along the south side of Granart Road, within property owned by the Forest Preserve, west of the Big Rock Forest Preserve entrance. The proposed multi-use path will be constructed on forest preserve property from approximately Sta. 707+90 to the entrance driveway, a distance of approximately 2,000 feet. The total area required is approximately 30 feet wide and 2,003 feet long for a total area of approximately 1.38 acres. See Exhibit 2, Plan and Profile Sheet.
Official with Jurisdiction:	Forest Preserve District of Kane County

Describe how the conditions for Temporary Occupancy are met

1. **Duration must be temporary, i.e., less than the time needed for construction of the project, and there should be no change in ownership of the land.**

The Forest Preserve District of Kane County (Forest Preserve) is a partnering agency to the Kane County Division of Transportation (KDOT). The Big Rock Forest Preserve is owned by the Forest Preserve and was purchased with OSLAD funds. The Open Space Lands Acquisition and Development (OSLAD) Program is a state-financed grant program that provides funding assistance to local government agencies for acquisition and/or development of land for public parks and open space.

The duration of the temporary occupancy will be limited to the time required to construct the multi-use path along the frontage of the Big Rock Forest Preserve property, on the south side of Granart Road. After construction, all disturbed areas will be graded and restored. The ownership and maintenance of the land will not change and will remain with the Forest Preserve.

The duration of the construction of the Dauberman Road extension is anticipated to occur over two construction seasons. To actually construct the 2,000-foot long multi-use path along Granart Road, within the forest preserve, would take about 6 weeks to complete. However, depending on the construction sequencing and staging of the other construction elements of the project, it would be reasonable to estimate the multi-use path construction would be completed within two months.

2. Scope of the work must be minor, i.e., both the nature and the magnitude of the changes of the Section 4 (f) property are minimal.

The proposed scope of work within the Section 4(f) property is to construct a paved multi-use path in the vicinity of an existing mowed grass trail and cropland along the northwest lot line of the forest preserve, which is located along the south side of Granart Road. See **Exhibit 3, Big Rock Forest Preserve Trail and Use Map**. The construction of the paved multi-use path provides an improved facility and will enhance the current outdoor use of the existing trail system and improve access to the forest preserve.

3. There are no anticipated permanent adverse physical impacts, nor will there be interference with the protected activities, features, or attributes of the property, on either a temporary or permanent basis.

The project is located in the northwest corner of the forest preserve directly adjacent to the Granart Road right-of-way. During construction of the project, there are no anticipated permanent adverse physical impacts, nor anticipated interference with the protected activities, features, or attributes of the property, on either a temporary or permanent basis. Access to and use of the camping areas, hiking trails, fishing, etc. will be available during construction.

Construction of the multi-use path will require a 30-foot wide strip along the forest preserve frontage for approximately 2,000 feet. It will require removal of crops in an area designated as “cropland”. See **Exhibit 3, Big Rock Forest Preserve Trail and Use Map**. The proposed multi-use path will replace the mowed grass trail along Granart Road. The new paved multi-use path will still allow and promote outdoor activity as the existing mowed grass trail currently does.

At the northwest corner of the forest preserve property, the existing grass trail (the Northwestern Lot Line Trail) turns south away from Granart Road. The proposed improvements will maintain the existing connection and provide a transition from the paved trail to the mowed grass trail as it turns south.

To protect the forest preserve holdings and to provide a clear delineation of the construction area, temporary fencing or silt fencing will be provided to demarcate the construction boundaries. It will separate the construction zone from the existing mowed grass trail (the Northwestern Lot Line Trail) and the cropland. See **Exhibit 4, Proposed Typical Section – Granart Road Multi-use Path**.

4. The land being used must be fully restored, i.e., the property must be returned to a condition which is at least as good as that which existed prior to the project.

The existing area is currently a mowed grass area. After completion of the multi-use path construction, all disturbed areas will be graded and seeded. A mowed grass separation will be reestablished between the multi-use path and the cropland.

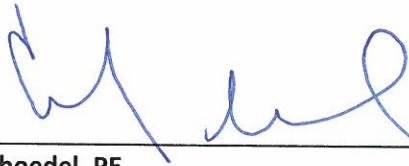
5. There must be documented agreement of the official(s) with jurisdiction over the section 4(f) resource regarding the above conditions.

The Forest Preserve District of Kane County is a cooperating agency. KDOT has initiated coordination with the Forest Preserve and coordination efforts are ongoing. The Forest Preserve is a non-financial project co-sponsor and will be signatory to the Categorical Exclusion Report and the Design Contract Plans. It is anticipated that the multi-use path within the forest preserve will be owned and maintained by the Forest Preserve District after construction is complete.

See **Attachment B, Coordination** for the Interagency Governmental Agreement and pertinent project correspondence.

Request for Approval

Based upon this analysis we request FHWA concurrence that this project's temporary occupancy of the section 4(f) resource described above satisfies the conditions set forth in 23 CFR 771.13(d) and is so minimal as to not constitute a use within the meaning of Section 4(f).



Carl Schoedel, PE
County Engineer, Kane County Division of Transportation

2-8-18

Date

FHWA Approval

Omar Qudus, PE
Engineering Team Leader, FHWA Illinois Division

Date

Attachment A – Project Description

General Introduction

The proposed improvements will extend Dauberman Road south approximately 0.8 miles from its current terminus at US Route 30 to Granart Road. The proposed roadway extension will tie into existing Granart Road by implementing a southwesterly horizontal curve to provide a new continuous through movement along Dauberman Road. Two structures are proposed along the extension to carry Dauberman Road over US Route 30 and the BNSF Railroad. The existing Dauberman Road/US Route 30 T-intersection will be replaced with a proposed jughandle type intersection, also referred to as the Connector Road. Three T-intersections are proposed: US Route 30/Connector Road, Dauberman Road/Connector Road, and Dauberman Road Extension/Granart Road. A multi-use path will be constructed as part of the project along Dauberman Road and Granart Road.

Multi-Use Path (Bicycle and Pedestrian Facility)

The proposed path will be 10 feet wide and paved with 2-foot graded grass shoulders. A minimum 10-foot clear zone from the outside edge of the paved roadway shoulder to the edge of the multi-use path will be provided. In those sections where the clear zone is less than 10 feet, a barrier with a minimum height of 3 feet-6 inches will be provided. At intersections, a striped pedestrian crosswalk will be provided across the roadway. Tactile surfaces will be provided at the crossings.

The proposed vertical grades follow the roadway grades, 0.1 to 0.7 percent along the project roadways and the Connector Road, and 4 percent on the bridge approaches. This is less than the 5 percent maximum allowed by ADA, without requiring flat areas. The project is fully compliant with Complete Streets, ADA, KDOT and IDOT requirements.

Within the project improvement limits, logical termini will be provided from US Route 30, at the north, to Big Rock Campground, at the south.

Along Dauberman Road, the proposed path will begin in the north at the northeast quadrant of the Collector Road intersection. It will continue south along the east side of Dauberman Road to the proposed Dauberman/Granart intersection. It will then cross the east leg of the intersection and continue east.

Along Granart Road, the proposed path will be along the south side of the road. It will continue east along Granart Road to the Big Rock Forest Preserve entrance (across from Mary Drive), terminating in the southwest quadrant of the entrance to Big Rock Campground.

Along the Connector Road, the proposed path will continue from the northeast quadrant of the Dauberman/Connector Road intersection. The path will be on the “outside” of the Connector Road continuing to the US Route 30/Connector Road intersection. At this intersection the path will cross the north leg of the intersection and terminate in the northwest quadrant. The paved path will be stubbed out at both corners. However, continuing west and east from the Connector Road, along the north side of US Route 30, the proposed right-of-way will be acquired and a shelf graded to accommodate a future path to be constructed by others.

The proposed multi-use path is consistent with the 2015 Kane/Kendall County Bicycle Planning Map and will be part of a north-south regional trail along Dauberman Road.

Attachment B, Coordination



MAR 07 2018

1996 South Kirk Road, Suite 320, Geneva, Illinois 60134

Michael J. Kenyon, President
Monica A. Meyers, Executive Director

(630) 232-5980
Fax: (630) 232-5924
www.kaneforest.com

March 5, 2018

Carl Schoedel, PE: Director of Transportation
Kane County Division of Transportation
41W011 Burlington Road
St. Charles, IL 60175
Attention: Ms. Candance Thomas, Senior Project Engineer

Re: Dauberman Road Extension – *Support for the Project and Section 4(f) Temporary Occupancy Use*
US Route 30 to Granart Road; Section 15-00277-01-BR; Kane County Hwy. No. 62

Dear Ms. Thomas,

This letter serves as a statement of support from the Forest Preserve District of Kane County for the Dauberman Road Extension project and the Section 4(f) Temporary Occupancy use of District's property along the Granart Road frontage.

As part of the project, a paved multi-use path will be constructed along the northwest lot line of the Big Rock Forest Preserve. The path will extend west from our entrance (across from Mary Drive) along the south side of Granart Road to the Dauberman Road extension at a new T-intersection.

We understand that a temporary construction easement will be needed to construct the path along our frontage. The easement is approximately 2,003 feet long and 30 feet wide, for a total area of the approximately 1.38 acres.

During construction of the project, there are no anticipated permanent adverse physical impacts, nor anticipated interference with the protected activities, features, or attributes of the Big Rock Forest Preserve, on either a temporary or permanent basis. Access to and use of the camping areas, hiking trails, fishing, etc. will be available during construction. After construction of the multi-use path, all disturbed areas will be graded, seeded, and restored.

We appreciate the opportunity to be a non-financial project co-sponsor. The District supports the construction of the path on District lands. Post construction, we will retain ownership of the temporary easement and own and maintain the path within our property. The details and terms of the temporary easement are outlined in the draft Interagency Governmental Agreement (IGA) which we are currently reviewing. We fully endorse the project improvements and the Section 4(f) Temporary Occupancy use of Big Rock Forest Preserve lands to construct the multi-use path along our frontage.

Sincerely,


Jerry Culp
Director of Planning & Development
Forest Preserve District of Kane County

To preserve and restore the nature of Kane County

**INTERGOVERNMENTAL AGREEMENT
BETWEEN THE COUNTY OF KANE
AND THE KANE COUNTY FOREST PRESERVE DISTRICT
DAUBERMAN ROAD EXTENSION**

This Agreement, entered into by and between the County of Kane, a body corporate and politic of the State of Illinois (hereinafter "County"), and the Forest Preservice District of Kane County, a downstate forest preserve district of the State of Illinois (hereinafter the "District"). The County and the District are each sometimes hereinafter individually referred to as a "Party" and collectively as the Parties".

WITNESSETH:

WHEREAS, the County and the District are public agencies within the meaning of the Illinois Intergovernmental Cooperation Act, as provided in the Illinois Compiled Statutes, 5 ILCS 220/1 *et. seq.*; and are units of local government within the meaning of the Constitution of Illinois, 1970, Article VII, Section 10; and,

WHEREAS, the District owns certain real property utilized as a forest preserve in the Township of Big Rock, County of Kane, State of Illinois and in particular a forest preserve referred to as "Big Rock Forest Preserve"; and,

WHEREAS, the County desires to improve Kane County Highway No. 62 (Dauberman Road) at and near its intersection with United States Route 30 (U.S. Route 30) by constructing a grade separation over U.S. Route 30 and the railroad tracks of the Burlington Northern Santa Fe Railroad and extend Dauberman Road south to intersect with Kane County Highway No. 35 (Granart Road) and in conjunction therewith constructing a pedestrian/bicycle path (hereinafter collectively the "Improvement") according to the approved plans and specifications therefor (hereinafter the "Plans"). Parts of the aforesaid Improvement are adjacent to and in the Big Rock Forest Preserve; and

WHEREAS, the District desires to plan for the future development and use of the Big Rock Forest Preserve for the benefit of the citizens of Kane County.

WHEREAS, the County and the District desire to co-operate amongst themselves in order to facilitate their respective statutory responsibilities and duties.

NOW, THEREFORE, in consideration of the foregoing preambles, the mutual covenants contained herein and for good and valuable consideration, the sufficiency of which is agreed to by the Parties, both the County and the District covenant, agree and bind themselves as follows, to wit:

1. The District agrees to grant and convey to the County and the County shall accept from the District a temporary construction easement from the District, (hereinafter the “Temporary Construction Easement”), the area of which is legally described in Exhibit “A” which exhibit is attached hereto and incorporated herein. The real property legally described in Exhibit “A” is hereinafter referred to as the “Temporary Easement Premises”.
2. As consideration for the Temporary Construction Easement the County agrees, at its sole cost and expense, to design, lay out and construct a pedestrian/bicycle path in the Big Rock Forest Preserve on the Temporary Easement Premises as set forth and in general conformance with the specifications as set forth in the Plans as contained in “B” which is attached hereto and incorporated herein.
3. The District hereby warrants its fee simple ownership of the Temporary Easement Premises.
4. The County and the District agree that the Path shall be constructed at the time the Improvement is constructed.
5. Prior to construction the County shall seek and incorporate the District’s input regarding Path design and configuration. The District agrees that its input shall not be unreasonable.
6. Closing and possession for the grant of Temporary Construction Easement shall be held at 1:00 p.m. on or before February 28th, 2018. Said closing shall be at the offices of the County or as otherwise agreed to by the Parties. The closing and possession date is legally significant to both the County and the District. The Parties understand that when this Agreement is signed by both the County and the District, closing and possession may only be changed by mutual agreement of the Parties.
7. The County, at its sole cost, shall prepare a plat of survey for the Temporary Construction Easement Premises and shall also prepare for the signature of the District and the signature of the County, which will be executed by the District and the County at closing, in the same form and substance as set forth in Exhibit “C”, the Temporary Construction Easement Agreement and any other documents as required by the County, the District and the Internal Revenue Code. The County shall also be responsible for obtaining title commitments for the Temporary Easement Premises at the sole cost of the County.
8. The District agrees to leave the Temporary Easement Premises in a clean and orderly condition. All refuse and personal matter on the Temporary Easement Premises shall be removed at the District’s expense prior to the date of possession by the County.
9. The District represents that no notice has been received of any zoning, building, fire or health code violations, environmental regulation or of any pending special assessment proceedings affecting the Temporary Easement Premises.

10. Any and all notices given pursuant to this Agreement shall be in writing and signed by the attorney for the District and the attorney for the County and shall be given by certified mail or in person at the addresses hereinbelow. Notice to any one of a multiple person Party shall be notice to all.

To the County:

Carl Schoedel, Director
41W011 Burlington Road
St. Charles, Illinois 60175

To the District:

Monica Meyers, Director
1996 South Kirk Road
Geneva, Illinois 60134

11. The date of this Contract shall be the last date of acceptance of this Agreement as provided herein below.
12. District hereby represents and warrants to the County as follows, which representations and warranties shall be deemed remade by District to County at the closing, and which shall survive the closing:
- (a) To District's knowledge, there is no pending or threatened litigation affecting the Temporary Easement Premises or to the best knowledge and belief of District is any such litigation contemplated by any party;
 - (b) The District has received no notice of, and has no actual knowledge of, any change that is contemplated with respect to the zoning of the Temporary Easement Premises the availability of utility services to the Temporary Easement Premises violation of any existing law, municipal ordinance or other governmental regulation, contemplated or threatened condemnation, or any other matter which would affect the Right of Way;
 - (c) The District has the authority to execute and perform the terms of this Agreement; and,
 - (d) The District has not received notice of any violations of local, state or federal laws including environmental laws, rules or regulations concerning the Temporary Easement Premises.
13. From and after the date hereof and so long as this Agreement is in effect, the District shall not, without County's prior written consent, execute any lease, license, contract, easement or other agreement affecting the Temporary Easement Premises that will survive the Closing.
14. The District acknowledges that upon the completion of the Path, the County shall have no further interest in or responsibility therefor and the District agrees that the District shall maintain or cause to be maintained the Path. For purposes of this Agreement "maintain" shall mean keeping the Path in good and sufficient repair and appearance. Such maintenance may include the full responsibility for the reconstruction, removal, replacement of the Path or portions thereof when needed, including day to day pavement maintenance, pothole repair, debris removal etc. This provision shall survive the closing.

15. This Agreement may be amended only in writing upon the signatures of all the Parties.
16. This Agreement is executed and submitted by the District as of the date set forth below. A duplicate original of this Contract, duly executed by the District shall be delivered to the County not later than 5 business days from such date.
17. This Agreement shall be effective upon approval by the respective legislative bodies of the County and the District.
18. The Parties acknowledge and agree that in the event that any section, paragraph, subdivision or sentence of this Agreement shall be for any reason held invalid or to be unconstitutional, such decision or holding shall not affect the validity of the remainder of this Agreement.

IN WITNESS WHEREOF, the County of Kane has executed this Agreement as of the ____ day of February 2018 at Geneva, Illinois.

COUNTY OF KANE

BY: _____
Christopher Lauzen, County Board Chairman

ATTEST:

John A. Cunningham, County Clerk

IN WITNESS WHEREOF, the District has executed this Agreement as of the ____ day of February 2018 at Geneva, Illinois.

FOREST PRESERVE DISTRICT OF KANE COUNTY

BY: _____
Michael Kenyon, President

ATTEST:

Phil Lewis, Secretary

EXHIBIT LIST

EXHIBIT “A” Temporary Easement Premises Legal Description and Plat

EXHIBIT “B” Plans, Profile and Specifications for the Path

EXHIBIT “C” Temporary Easement Agreement

DRAFT

EXHIBIT "C"

TEMPORARY CONSTRUCTION EASEMENT

This Grant of Temporary Construction Easement is made this ____ day of February 2018, between the KANE COUNTY FOREST PRESERVE DISTRICT, an Illinois downstate forest preserve district, located at 1996 South Kirk Road, Geneva, Illinois (hereinafter the "GRANTOR") and the County of Kane, 719 South Batavia Avenue, Geneva, Illinois acting by and through its Division of Transportation (hereinafter the "GRANTEE").

WHEREAS, GRANTEE has requested from GRANTOR a temporary construction easement for the purpose of designing, laying out, and constructing and maintaining a public highway and a highway overpass including any and everything appurtenant thereto (hereinafter referred to as the "improvement"); and,

WHEREAS the GRANTOR has agreed to grant unto the GRANTEE a temporary construction easement for the purpose of the improvement.

NOW, THEREFORE, in consideration of the sum of One Dollar (\$1.00) and other good and valuable consideration, receipt and sufficiency of which are hereby acknowledged by the GRANTOR and the GRANTEE, it is mutually agreed as follows:

1. The GRANTOR does hereby grant and convey to GRANTEE, its successors and assigns, a temporary construction easement for the purpose of improvement; within the real property as shown and legally described on Exhibit 1 attached hereto and made a part hereof;

2. The GRANTOR, for itself and its successors and assigns, covenants and agrees that it shall not interfere with the lawful use of the real property described in Paragraph No. 1 by the GRANTEE, its successors and assigns, pursuant to the terms of this temporary construction easement.

3. The GRANTEE, for itself and its successors and assigns, covenants and agrees that the improvement shall include the following obligations of the GRANTEE:

- a) During construction, the Grantee shall fence and/or cover any excavation opening with protective cover at all times when construction personnel are not present at the site of the improvement.

- b) GRANTEE and its contractor(s) will be responsible for safety measures to protect the public and the GRANTOR.
- c) GRANTEE shall construct the improvement in accordance with approved plans therefor.
- d) GRANTEE represents to the GRANTOR that prior to construction, all permits, licenses, authorizations and approvals required by all federal, state and local governmental agencies having jurisdiction over the work will be obtained.
- e) GRANTEE shall restore sod, plant material, trees, bicycle trail or other improvements (if any) damaged in constructing the improvement.
- f) GRANTEE shall restore the GRANTOR's property in accordance with the approved construction plan.
- g) GRANTEE will be fully responsible for the actions of its employees and agents while on the real property described in Exhibit 1.

4. GRANTEE hereby agrees to indemnify and save harmless the GRANTOR from and against any loss, damage or expense which the GRANTOR may suffer, incur or sustain or for which the GRANTOR may become legally liable arising or growing out of GRANTEE's activities and/or negligence and that of its contractors, agents and employees in designing, construction or maintaining the improvement.

5. GRANTOR makes no covenant, representation or warranty as to the suitability of the property for any purpose whatsoever or as to the physical condition of the property. GRANTEE acknowledges that portions of the property could have been, used for operations and for storage of products and by-products from those operations, some of which may be toxic or hazardous substances. As a result of such uses and activities, physical and other changes may have occurred in the property, including without limitation the disposition of solid and hazardous wastes and hazardous substances. GRANTEE acknowledges having inspected the property, having observed its physical characteristics and existing conditions, and having had the opportunity to conduct such investigation and study on and of said property as it deems necessary, and hereby waives any and all objections to or complaints about physical characteristics and existing conditions, including without limitation subsurface conditions and solid and hazardous wastes and hazardous substances on, under or related to the property. GRANTEE further acknowledges and agrees that the easements over the property are granted to and accepted by GRANTEE in its present condition "as is", and GRANTEE hereby assumes the risk that adverse physical characteristics and existing conditions may not have been revealed by its investigation.

6. The term of this temporary construction easement shall be for a period of five (5) years or

upon completion of the construction of the improvement, whichever occurs first.

7. The covenants and agreements herein contained shall inure to the benefit of and be binding upon the parties hereto, their heir's successors and assigns.

GRANTOR:
KANE COUNTY FOREST
PRESERVE DISTRICT

By:

President

Attest:

Secretary

GRANTEE:
COUNTY OF KANE

By:

Chairman
Kane County Board

Attest:

County

EXHIBIT “1”

DRAFT

Record of Telephone Conversation

By: Candi Thomas Date: November 21, 2017 Time: 8:30 ☒ AM ☐ PM

Project: Dauberman Road Extension

Individual: Jerry Culp Title:

Organization: Forest Preserve District of Kane County Location:

Phone No.: (630) 232-5980

Subject:

Items Discussed:

I spoke with Jerry Culp regarding the proposed multi-use path accommodations along Granart Road. The attached exhibit was also shared with him.

We discussed the property south of Granart Road and east of the Dauberman Road extension. This property is owned by the Forest Preserve District of Kane County (FPDKC) and was purchased with OSLAD funds. I informed Jerry of my contact with IDNR to discuss the impacts of placing the multi-use path on the OSLAD property and the project impacts. IDNR had responded 'since the proposed trail is outdoor recreation and the Forest Preserve District is not giving up ownership we are viewing this as additional outdoor recreational development on the OSLAD assisted site.' Jerry agreed with their response.

We discussed the option of the FPDKC being a 'non-financial' co-sponsor of the project and providing their signature on the cover sheet of the Project Development Report (PDR). Jerry agreed that this would be acceptable.

Jerry said the Forest Preserve was in support of the proposed multi-use path and the proposed location reflects the Forest Preserves long-range plan, by providing the path on the south side of Granart Road. He also stated that there is currently an existing grass path along the south side of Granart Road. Therefore, the proposed HMA path will provide a more durable surface to the existing conditions.

We discussed maintenance of the proposed multi-use path and Jerry said the FPDKC would be in agreement to maintain the proposed path. He asked about the portion of the proposed path which is outside of the FPDKC property limits and how this would be maintained. I told him I would look into this.

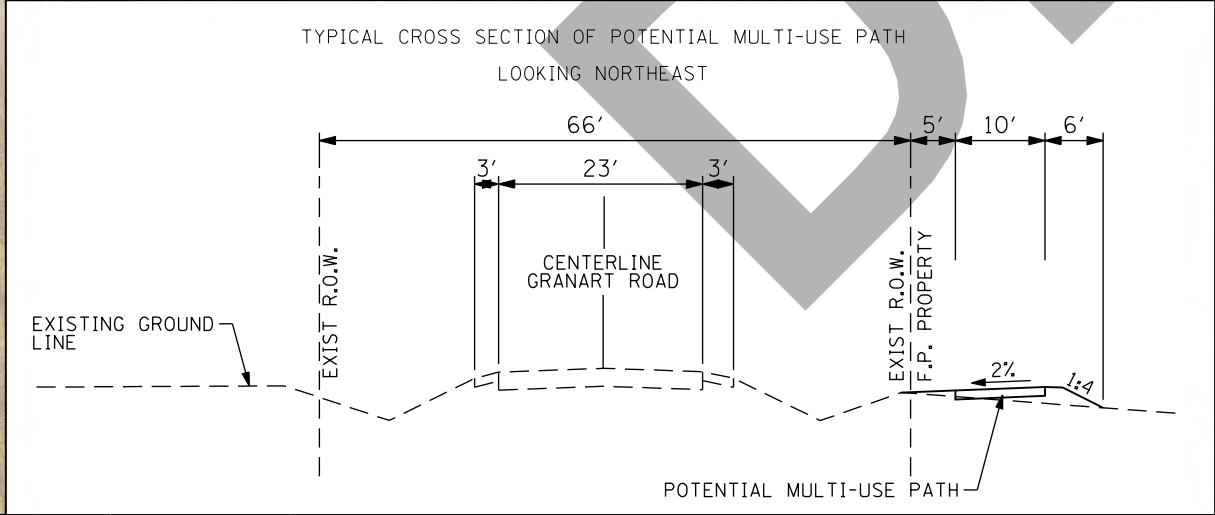
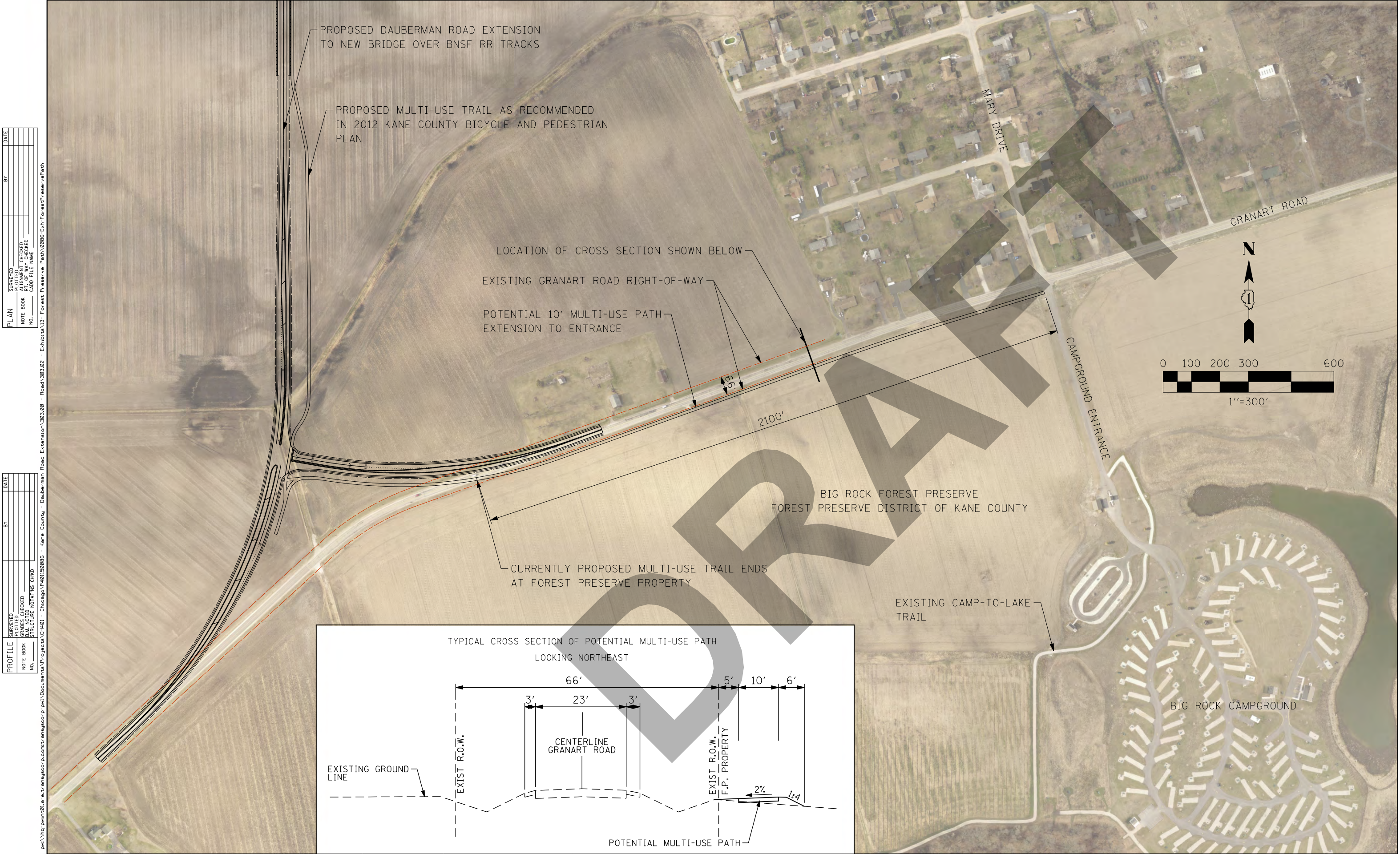
Comments or Action Required:

I will follow-up with Jerry on how the proposed multi-use path will be maintained, in its entirety, along Granart Road.

PLAN	SURVEYED	DATE
NO.	PLOTTED	BY
	CHECKED	
	FILE NAME	

PROFILE	SURVEYED	DATE
NO.	PLOTTED	BY
	CHECKED	
	FILE NAME	

FILE NAME = p:\hwy\p101\101-transportation\corporate\Documents\Projects\CH01 - Chicago\150086 - Kane County - Dauberman Road Extension\303.00 - Road\303.02 - Exhibits\13 - Forest Preserve Path\0086-En-ForestPreservePath



USER NAME = bjhalman	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 300.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/11/2017	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DAUBERMAN ROAD EXTENSION POTENTIAL MULTI-USE PATH EXTENSION THROUGH FOREST PRESERVE
SCALE: 1"=300'

FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1107	15-00277-01-BR	KANE	1	1
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

From: [Larimore, Kelly](#)
To: [Thomas, Candance](#)
Subject: RE: KDOT Dauberman Extension Improvements - OSLAD funds
Date: Monday, November 13, 2017 8:33:50 AM
Attachments: [image002.jpg](#)
[image003.jpg](#)

Good Morning Candi,

After speaking with the Senior Grant Administrator, all I will need is a copy of the agreement between KDOT and Kane County F.P.D.

Since the proposed trail is outdoor recreation and the Forest Preserve District is not giving up ownership we are viewing this as additional outdoor recreational development on the OSLAD assisted site.

Thank you for your patience.

Kelly Larimore

Grant Administrator

Illinois Department of Natural Resources

Division of Grant Administration

One Natural Resources Way

Springfield, IL 62702

Main: 217-782-7481

From: Thomas, Candance [<mailto:ThomasCandance@co.kane.il.us>]
Sent: Monday, November 13, 2017 8:07 AM
To: Larimore, Kelly <Kelly.Larimore@Illinois.gov>
Subject: [External] RE: KDOT Dauberman Extension Improvements - OSLAD funds

Good Morning –

I was following up from our emails last week and was wondering if you were able to talk to the Senior Grant Administrator about the multi-use path in regards to the OSLAD property?

Please let me know if you have any further questions.

Thanks,

Candi Thomas, P.E.
Senior Project Manager
Kane County Division of Transportation
Direct Phone: (630) 406-7355

Main Phone: (630) 584-1170
Fax: (630) 584-5265
Email: thomascandance@co.kane.il.us<<mailto:thomascandance@co.kane.il.us>>

From: Larimore, Kelly [<mailto:Kelly.Larimore@Illinois.gov>]
Sent: Tuesday, November 7, 2017 1:42 PM
To: Thomas, Candance <ThomasCandance@co.kane.il.us<<mailto:ThomasCandance@co.kane.il.us>>>
Subject: RE: KDOT Dauberman Extension Improvements - OSLAD funds

Is this associated with Big Rock Forest Preserve – OSLAD 10-1724? If the Forest Preserve is interested in this cooperative agreement they, as the grantee through OSLAD funds, will need to provide us with information regarding the agreement they wish to enter in to.

Additional development on the OSLAD funded site that is strictly for public outdoor recreational purposes does not involve a conversion of use. Additionally, OSLAD funds are funded through real estate transfer taxes and are strictly State funds so there will be no requirement for NPS approval. However, there may still be a need for reviews of the impacts of the proposed trail.

Our Senior Grant Administrator will be in tomorrow and I will ask her opinion on the requirements for this situation.

Thank you for your patience.

Kelly Larimore

Grant Administrator

Illinois Department of Natural Resources

Division of Grant Administration

One Natural Resources Way

Springfield, IL 62702

Main: 217-782-7481

From: Thomas, Candance [<mailto:ThomasCandance@co.kane.il.us>]

Sent: Tuesday, November 07, 2017 1:23 PM

To: Larimore, Kelly <Kelly.Larimore@Illinois.gov<<mailto:Kelly.Larimore@Illinois.gov>>>

Subject: [External] RE: KDOT Dauberman Extension Improvements - OSLAD funds

No problem at all...I was just checking to ensure I had the proper contact information (email) for you.

Thanks,

Candi Thomas, P.E.

Senior Project Manager

Kane County Division of Transportation

Direct Phone: (630) 406-7355

Main Phone: (630) 584-1170

Fax: (630) 584-5265

Email: thomascandance@co.kane.il.us<<mailto:thomascandance@co.kane.il.us>>

From: Larimore, Kelly [<mailto:Kelly.Larimore@Illinois.gov>]

Sent: Tuesday, November 7, 2017 1:09 PM

To: Thomas, Candance <ThomasCandance@co.kane.il.us<<mailto:ThomasCandance@co.kane.il.us>>>

Subject: RE: KDOT Dauberman Extension Improvements - OSLAD funds

Good Afternoon Candance,

Yes I received the previous email. We are currently in the middle of a large conversion and have just had a significant reduction in staff that are causing a delay in our email responses.

I will get back to you with answers to your questions as quickly as possible. Sorry for the delay.

Kelly Larimore

Grant Administrator

Illinois Department of Natural Resources

Division of Grant Administration

One Natural Resources Way

Springfield, IL 62702

From: Thomas, Candance [<mailto:ThomasCandance@co.kane.il.us>]

Sent: Tuesday, November 07, 2017 12:07 PM

To: Larimore, Kelly <Kelly.Larimore@Illinois.gov<<mailto:Kelly.Larimore@Illinois.gov>>>

Subject: [External] FW: KDOT Dauberman Extension Improvements - OSLAD funds

Good Afternoon Kelly –

Just following up to ensure you received the following email?

Thanks,

Candi Thomas, P.E.
Senior Project Manager
Kane County Division of Transportation
Direct Phone: (630) 406-7355

Main Phone: (630) 584-1170
Fax: (630) 584-5265
Email: thomascandance@co.kane.il.us<<mailto:thomascandance@co.kane.il.us>>

From: Thomas, Candance
Sent: Thursday, November 2, 2017 3:50 PM
To: 'Kelly.Larimore@illinois.gov' <Kelly.Larimore@illinois.gov><<mailto:Kelly.Larimore@illinois.gov>>>
Subject: KDOT Dauberman Extension Improvements - OSLAD funds

Good Afternoon Kelly –

I am a project engineer at Kane County Division of Transportation (KDOT) and I had a question regarding one of our proposed improvements, the Dauberman Extension.

We are looking to provide a separate Multi-use Path along the proposed roadway limits connecting from the Big Rock Forest Preserve/Campground (located off of Granart Road) to US 30. See Exhibit 1 for a plan view and typical section of the currently proposed Granart Rd section.

We have been informed that the land south of Granart Rd is owned by the Forest Preserve and was purchased with OSLAD funds. Please see Exhibit 2 for the deed.

Question:

If we were to do a 'non-financial' co-sponsor of the project with the Forest Preserve (and have them sign the cover of the Project Development Report) would this avoid Section 4(f) and Section 6(f) impacts? The Forest Preserve is supportive of the proposed path and partnering with KDOT to allow construction of the path on Forest Preserve property without KDOT acquiring the property. However, I was not sure how this would affect the processing and approval of the project.

We were thinking that if the Forest Preserve were to do a 'non-financial' co-sponsor, and we got prior written approval from IDNR/NPS, we would be able to avoid any 4(f) or 6(f) impacts, per:

The 17 IL Adm. Code 3025 and the U.S. Code Sec. 200305 stating that property acquired with OSLAD/LWCF grant assistance are prohibited to be utilized or developed or converted for any use other than public outdoor recreation. No easements, roadway dedications, leases or other such land encumbrances shall take place on OSLAD/LWCF assisted lands without prior written approval from IDNR/NPS.

Based on the above, we feel it may be interpreted that to build the path on forest preserve property, with prior written approval from IDNR/NPS, it is not considered a conversion of land use since the path will be used for recreational use.

What are your thoughts on this?

Please let me know if you have any additional questions/concerns or if you need any additional information.

Thanks so much,

Candi Thomas, P.E.
Senior Project Manager
Kane County Division of Transportation
Direct Phone: (630) 406-7355

Main Phone: (630) 584-1170
Fax: (630) 584-5265
Email: thomascandance@co.kane.il.us<<mailto:thomascandance@co.kane.il.us>>

State of Illinois - CONFIDENTIALITY NOTICE: The information contained in this communication is confidential, may be attorney-client privileged or attorney work product, may constitute inside information or internal

deliberative staff communication, and is intended only for the use of the addressee. Unauthorized use, disclosure or copying of this communication or any part thereof is strictly prohibited and may be unlawful. If you have received this communication in error, please notify the sender immediately by return e-mail and destroy this communication and all copies thereof, including all attachments. Receipt by an unintended recipient does not waive attorney-client privilege, attorney work product privilege, or any other exemption from disclosure.

Exhibit 1, Project Location Map

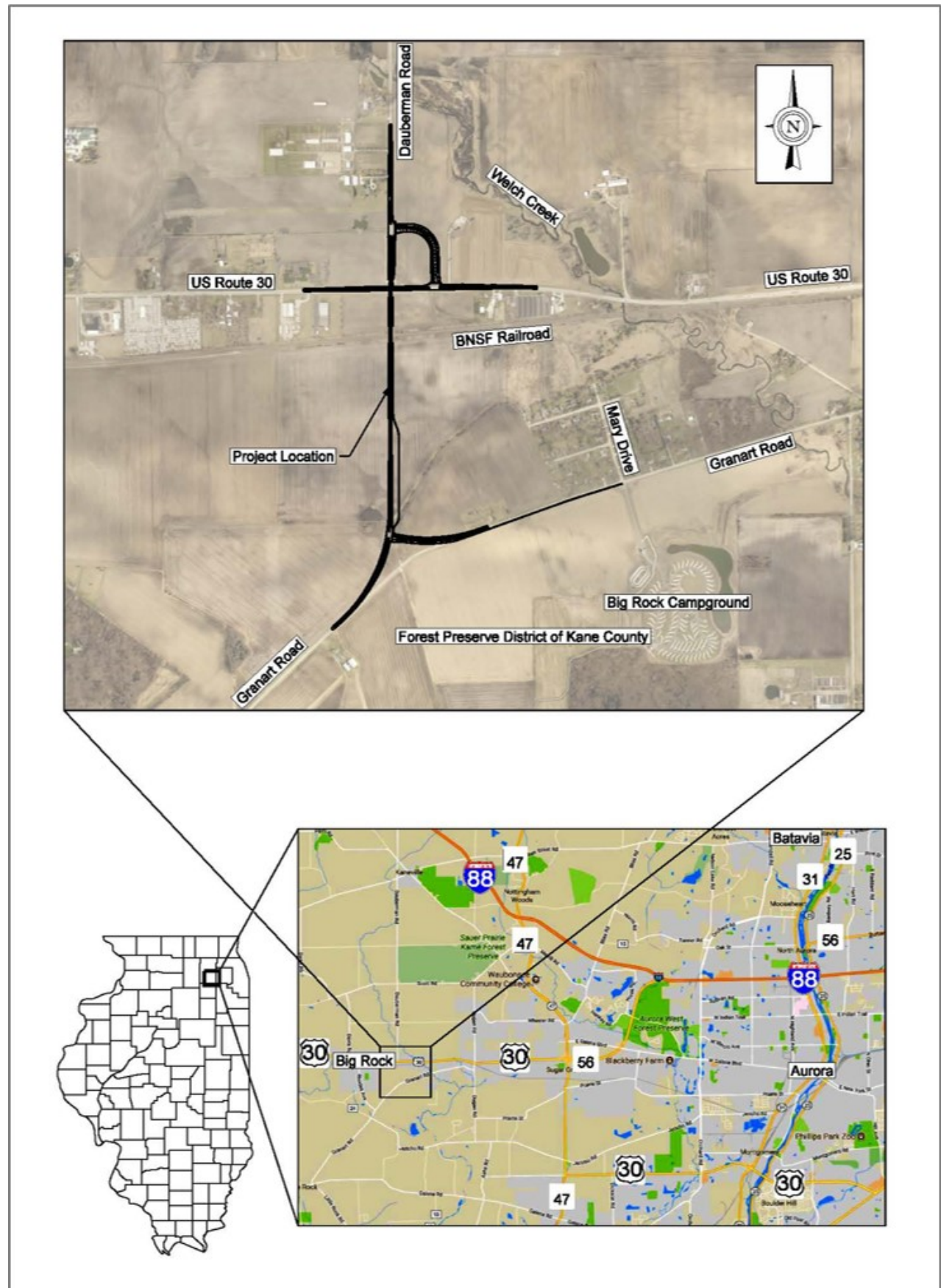


Exhibit 2, Plan and Profile Sheet

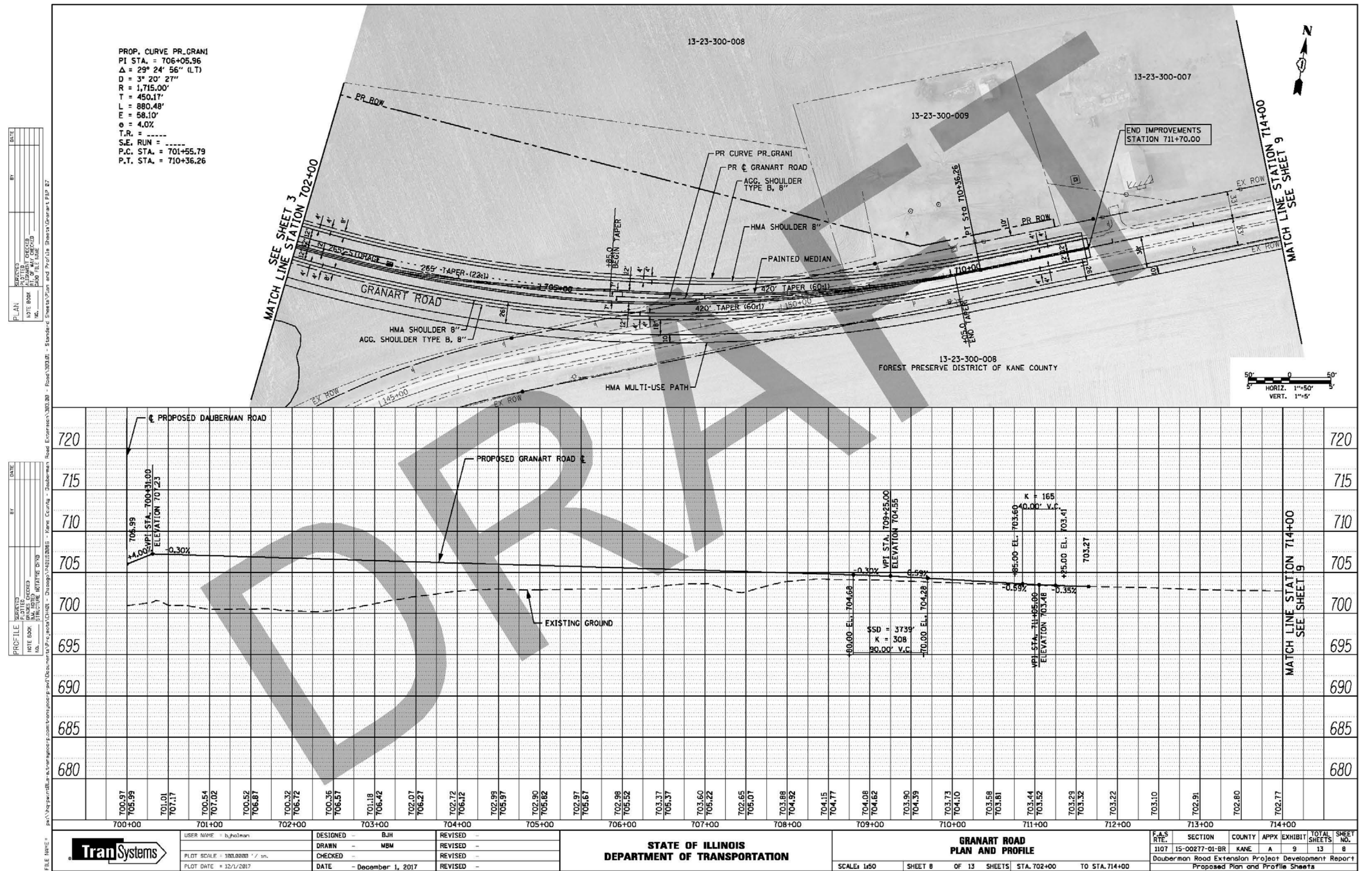


Exhibit 2, Plan and Profile Sheet

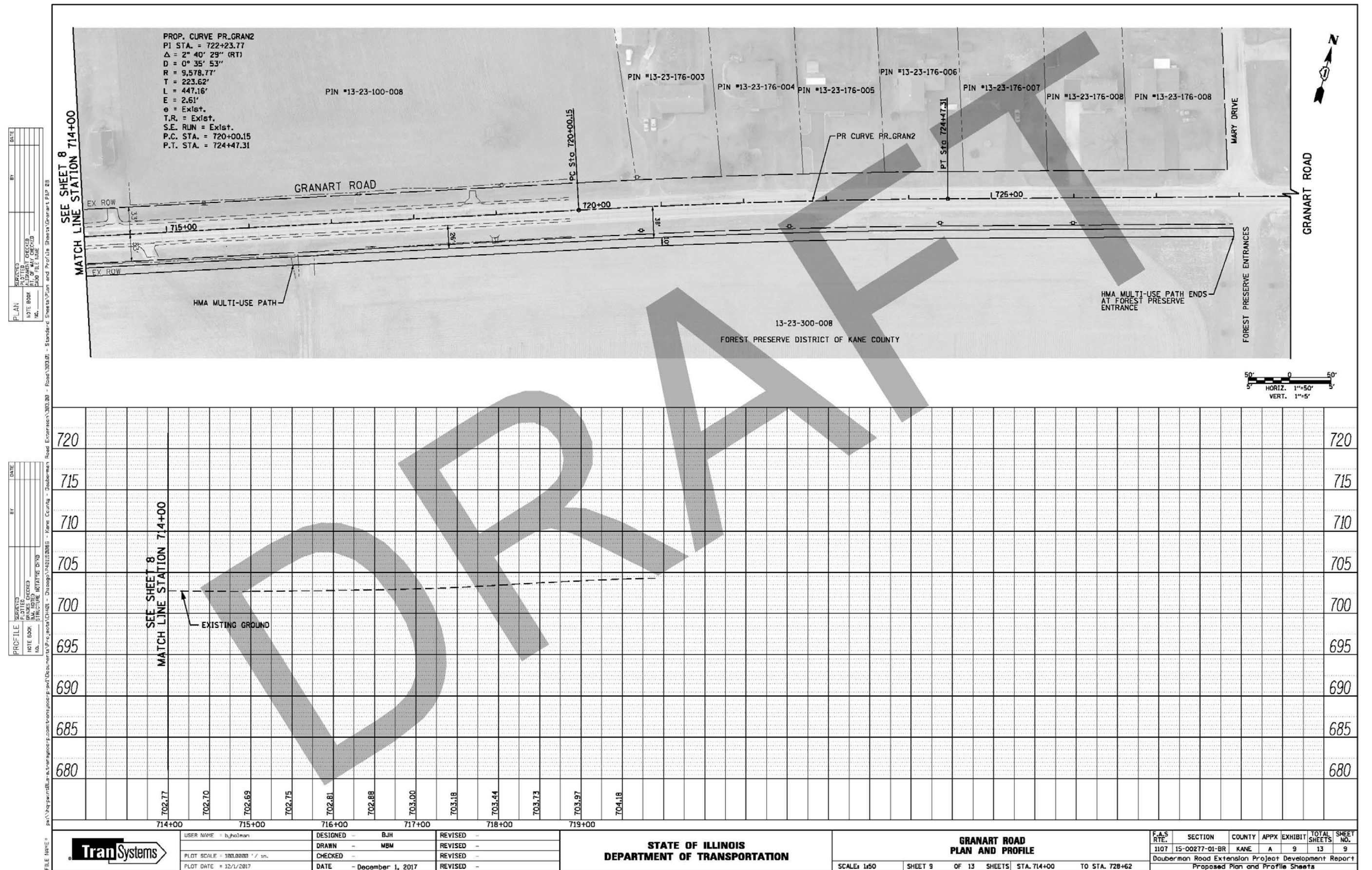
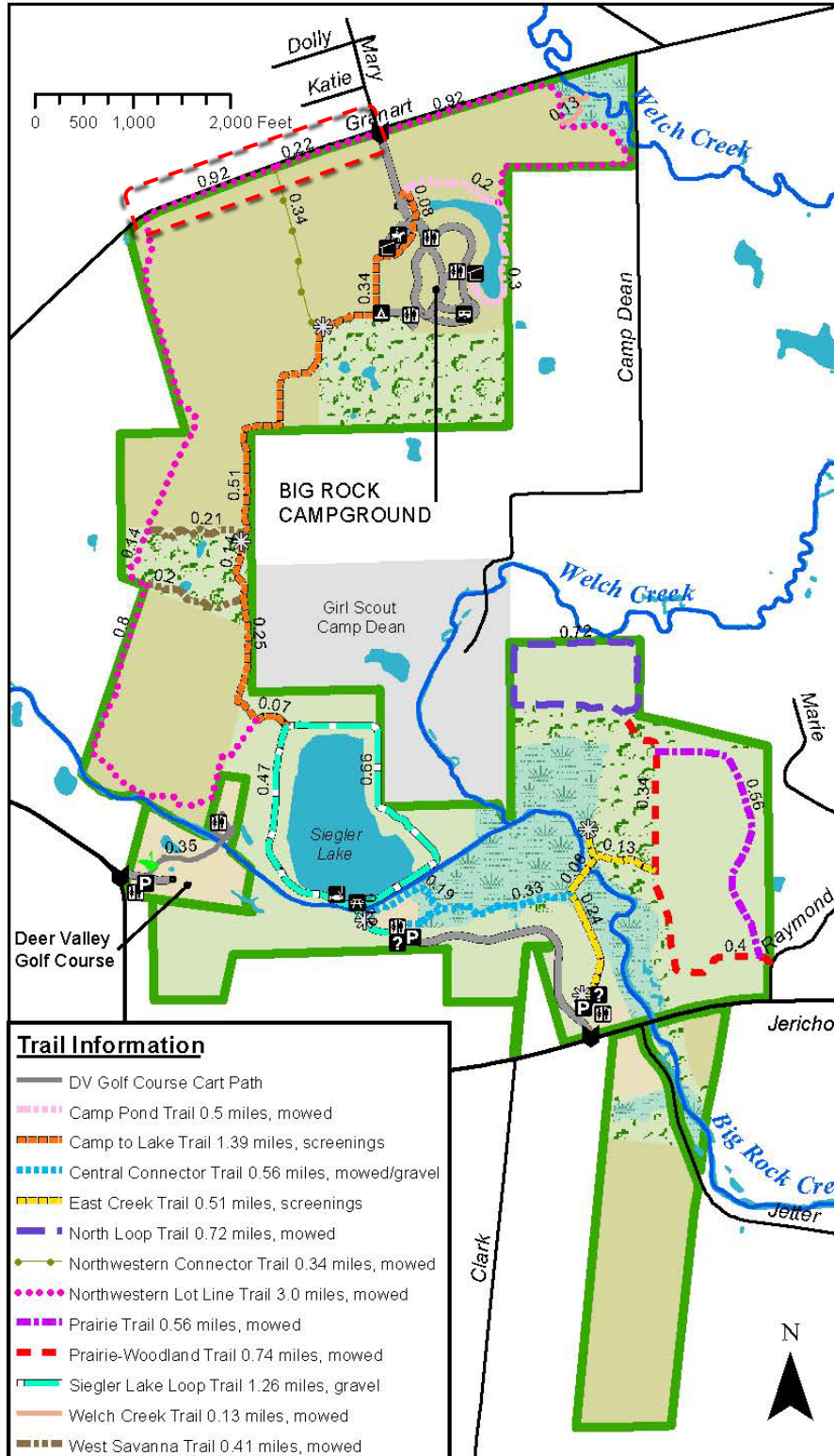


Exhibit 3, Big Rock Forest Preserve Trail and Use Map

Project Location



Big Rock Forest Preserve



Legend	
	Main Entrance
	Information
	Parking
	Shelter
	Picnic Area
	Restroom
	RV Campground
	Tent Campground
	Equestrian Campground
	Fishing
	Golf
	Interpretive Sign
	Road
	Forest Preserve Road
	Creek
	Forest Preserve
Natural Area -	
	Cropland
	Grassland
	Maintained
	Open Water
	Wetland
	Woodland

Preserve Main Entrance:
46W072 Jericho Road,
Big Rock, IL 60511
Approx. 3.54 miles west
of IL Route 47

Campground:
46W499 Granart Road,
Big Rock, IL 60511

Acreage = 840
Total trail miles = 10.12

Information: (630) 232-5980
www.kaneforest.com
Hours: Daily, Sunrise-Sunset

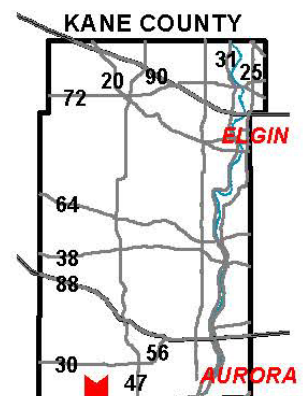
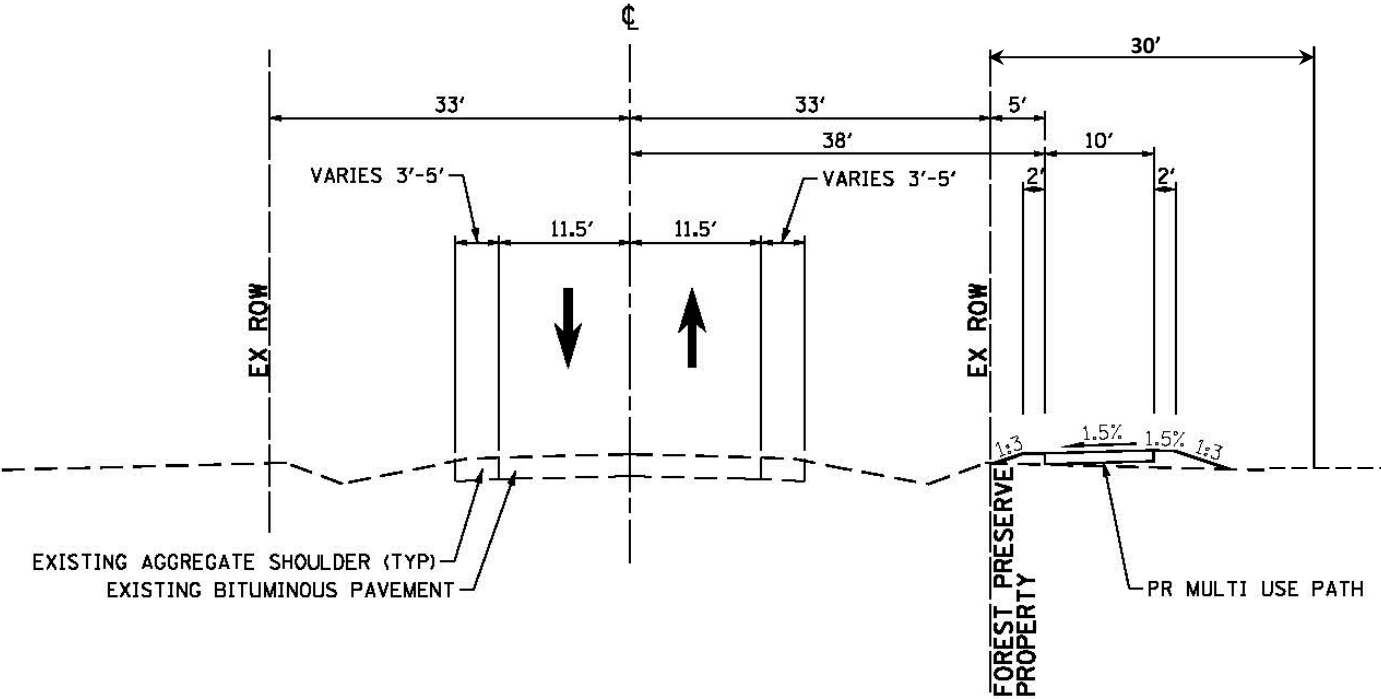


Exhibit 4, Proposed Typical Section – Granart Road Multi-use Path



Granart Road Multi-use Path
Station 711+70 to Station 727+93



Illinois Department of Transportation

Memorandum

To: Anthony Quigley, Dist. 1 Attn: Christopher Holt
From: Maureen Kastl, P.E.
Subject: Design Variance Request
Date: February 8, 2018

Program
Kane County
Dauberman Road Extension
Section 15-00277-01-BR

We have reviewed the Level One design variance request for the subject project. The design variance for shoulder width is approved based on the following:

1. The project abuts a restricted airstrip, J. Maddock Air Strip, in the Northwest portion of the project.
2. The justification states that alternates were studied to shift the alignment to the east to provide the required shoulder width. These studies found that the additional impacts still have the potential to interfere with the air strip.
3. The proposed 4 foot wide pave shoulder is to avoid operational impacts to the grass landing strip.
4. The 4 foot wide paved shoulder is an improvement over the existing narrow turf shoulder.
5. The total length of the 4 foot wide pave shoulder is approximately 390 feet, across the frontage of the air strip.

The design exception is approved contingent on the County installing advanced warning signs to alert the traveling public to the narrow shoulder.

Attached is a copy of the approved BLR 22120 form was send to your office via email. If you have any questions, please contact William Raffensperger at 217-785-1676.

Engineer of Local Roads and Streets

By: Gregory S. Lupton, P.E.

Local Project Implementation Engineer



Project Identification

Local Agency: Kane County Division of Transportation County: Kane
(County, Municipality, Road District / Township)
 Section No.: 15 - 00277 - 01 - BR Route: Kane County Hwy. No. 62
 Street/Road Name: Dauberman Road Extension
 Project Limits: US Route 30 to Granart Road

Project Length: 1.1 miles Functional Classification: Major Collector
 Design Year: 2040 Design Traffic: ☐ DHV ☒ ADT 7800
 Existing Structure No.: N/A Proposed Structure No.: 045-3401 & 045-3402

Project Scope of Work

- a. Is this project located on the NHS? ☐ Yes ☒ No
- b. Is this project on a Strategic Regional Arterial (SRA) route? ☐ Yes ☒ No
- c. Funding ☐ MFT/State Assistance ☒ Federal
- d. Type of Work ☒ New Construction ☐ Reconstruction ☐ 3R
- e. Design Guidelines ☐ Urban ☐ Suburban ☒ Rural ☐ 3R ☐ Other _____
- f. Provide a brief project description (major construction elements):

The proposed improvements will extend Dauberman Road south as a two-lane roadway approximately 0.8 miles from its current terminus at US Route 30 to Granart Road. The new proposed roadway extension will tie into existing Granart Road by implementing a southwesterly horizontal curve to provide a new continuous through movement along Dauberman Road. Two structures are proposed along the extension to carry Dauberman Road over US Route 30 (SN 045-3402) and the BNSF Railroad (SN 045-3401). The existing Dauberman Road/US Route 30 T-intersection will be replaced with a proposed new jughandle type intersection, also referred to as the Connector Road. Three T-intersections are proposed: US Route 30/Connector Road, Dauberman/Connector Road, and Dauberman Road Extension/Granart Road. A multi-use path will be constructed as part of the project. The 10-foot paved path will be along the east side of Dauberman Road and will extend from US Route 30 to Granart Road, then continue east along the south side of Granart Road to the entrance of the Big Rock Forest Preserve and Campgrounds.

District Coordination Meetings

Has project been previously discussed at district coordination meetings? ☒ Yes ☐ No
 (If yes, attach minutes of variance approvals)

Dates: 1/12/16; 3/14/17; 5/9/17; 7/11/17; 12/05/17

Level One Design Variance Approval

Local Agency: Kane County Division of Transportation

Section No.: 15-00277-01-BR

Design Criteria for Project (Provide numerical value where indicated)	BLR&S Criteria	Variance		Summary of Variance and Justification
		Yes	No	
1. Design Speed: 60 mph		<input type="checkbox"/>	<input type="checkbox"/>	
2. Level of Service (Mainline):		<input type="checkbox"/>	<input type="checkbox"/>	
3. Lane Widths				
a. Through Lanes: _____ feet		<input type="checkbox"/>	<input type="checkbox"/>	
b. Turn Lanes: _____ feet		<input type="checkbox"/>	<input type="checkbox"/>	
c. Parking Lanes: _____ feet		<input type="checkbox"/>	<input type="checkbox"/>	
d. Bike Lanes: _____ feet		<input type="checkbox"/>	<input type="checkbox"/>	
4. Through Travel Lane Cross Slopes				
Inside Lane: _____ %		<input type="checkbox"/>	<input type="checkbox"/>	
Outside Lane: _____ % (if more than 2 lanes)		<input type="checkbox"/>	<input type="checkbox"/>	
5. Shoulder Widths: Typical: 8 feet. From Sta. 667+10 to Sta. 671+00: 4 feet	8 feet (BLRS Figure 32-2B)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<p>The J. Maddock Air Strip is a private grass landing strip located at Sta. 669+35 (Left). To avoid impacts to the runway operation and use, while maintaining the tangent Dauberman Road alignment, a 4-foot wide paved shoulder with 1:2 (V:H) side slope is proposed along the west edge-of-pavement of Dauberman Road instead of the standard 8-foot wide shoulder (4-foot paved and 4-foot aggregate). This is proposed from Sta. 667+10 to Sta. 671+00, a distance of approximately 390 feet adjacent to the air strip property frontage.</p> <p>Shifting the alignment east with tapers or curves is not recommended because it would cause the Connector Road intersection to move farther north, potentially interfering with the air strip in the future if the intersection were to become signalized. Additionally, the location of the recently constructed Welch Creek Bridge to the north, limits other roadway realignment options.</p>

Level One Design Variance Approval

Local Agency: Kane County Division of Transportation

Section No.: 15-00277-01-BR

<p>6. Horizontal Curvature (Minimum Radius) <u> </u> feet List curves not meeting criteria <u>Sta.</u> <u>Radius</u> <u>Design Speed</u></p>		<input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/>	
<p>7. Superelevation Rates <u> </u> e_{max} curve % List curves for which e does not meet criteria <u>PI Sta.</u> <u>Radius</u> <u>e</u> <u>Design Speed</u></p>		<input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/>	
<p>8. Maximum Grade: <u> </u> %</p>		<input type="checkbox"/>	
<p>9. Minimum Intersection Sight Distance <u> </u> feet List locations not meeting the criteria <u>Cross Road</u> <u>Distance</u></p>		<input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/>	
<p>10. Minimum Stopping Sight Distance <u> </u> feet a. Crest Vertical Curves – Min. K value <u> </u> List curves not meeting the criteria <u>VPI Sta.</u> <u>Sight Distance</u> <u>Design Speed</u> <u>Curve Length</u> b. Sag Vertical Curves – Min. K value <u> </u> List curves not meeting the criteria <u>VPI Sta.</u> <u>Sight Distance</u> <u>Design Speed</u> <u>Curve Length</u> c. Inside of Horizontal Curves List curves not meeting the criteria <u>Sta.</u> <u>Sight Distance</u> <u>Design Speed</u> <u>Radius</u></p>		<input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/>	
<p>11. Clear Roadway Bridge Widths: <u> </u> feet</p>		<input type="checkbox"/>	
<p>12. Freeboard Above Design High Water: <u> </u> feet</p>		<input type="checkbox"/>	

Level One Design Variance Approval

Local Agency: Kane County Division of Transportation

Section No.: 15-00277-01-BR

13. Vertical Clearances:

- ☐ Over Roadway/RR _____ feet
☐ Under Structure _____ feet

<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

14. Accessibility Criteria for Disabled Persons
 List any feature not meeting ADA Criteria

<input type="checkbox"/>	<input type="checkbox"/>
--------------------------	--------------------------

15. Roadside Clear Zone:

- a. Tangent _____ feet
 b. Outside of Curve _____
 List criteria for each radius

Radius (ft) Clear Zone (ft)

<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

16. Intersection(s) Level of Service: _____

<input type="checkbox"/>	<input type="checkbox"/>
--------------------------	--------------------------

17. Warrants for Stop Signs or Signals

Cross Road Warrant

<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

18. Pavement Design (list any variance to policy)

<input type="checkbox"/>	<input type="checkbox"/>
--------------------------	--------------------------

Prepared By: Matthew J. Smith TransSystems
 Designer (Local Agency or Consultant)

Date: 1/17/2018

When Prepared by Consultant
 Local Agency Concurrence:

Date: 1.17.2018

Anthony J. Quigley Sr. 2/6/18
 IDOT Regional Engineer Concurrence Date

Maureen Katt 2/8/2018
 Central BLR&S Approval Date



Level Two Design Variance Approval

Local Agency: Kane County Division of Transportation

Section No.: 15-00277-01-BR

Design Criteria for Project (Provide numerical value where indicated)		BLR&S Criteria		Variance Yes No		Summary of Variance and Justification
1.	Design Period: _____ years	20 years		<input type="checkbox"/>	<input type="checkbox"/>	
2.	Horizontal Alignment (Mainline)					
	a. Minimum Superelevation Transition Lengths: _____ feet			<input type="checkbox"/>	<input type="checkbox"/>	
	b. Superelevation Distribution Between Tangent and Curve: _____	2/3 : 1/3		<input type="checkbox"/>	<input type="checkbox"/>	
3.	Vertical Alignment (Mainline)					
	a. Minimum Grade of Urban Cross Section _____ %	0.3%		<input type="checkbox"/>	<input type="checkbox"/>	
	b. Minimum Length of Vertical Curves _____ feet			<input type="checkbox"/>	<input type="checkbox"/>	
	c. Maximum K value of Vertical Curves _____ (for curbed facilities)	167		<input type="checkbox"/>	<input type="checkbox"/>	
4.	Cross Section Elements (Mainline)					
	a. Design of Parking Lanes			<input type="checkbox"/>	<input type="checkbox"/>	
	• Cross Slope: _____ %					
	b. Design of Sidewalks			<input type="checkbox"/>	<input type="checkbox"/>	
	• Width: _____ feet	4 feet		<input type="checkbox"/>	<input type="checkbox"/>	
	• Buffer Distance: _____ feet	2 feet		<input type="checkbox"/>	<input type="checkbox"/>	
	• Cross Slope: _____ %	2% max.		<input type="checkbox"/>	<input type="checkbox"/>	
	• Longitudinal Grades: _____ %	5% max.		<input type="checkbox"/>	<input type="checkbox"/>	
	c. Median			<input type="checkbox"/>	<input type="checkbox"/>	
	• Type: _____			<input type="checkbox"/>	<input type="checkbox"/>	
	• Width: _____ feet			<input type="checkbox"/>	<input type="checkbox"/>	
	d. Shoulder Cross Slopes: _____ %			<input type="checkbox"/>	<input type="checkbox"/>	
	e. Rollover Factor _____ %			<input type="checkbox"/>	<input type="checkbox"/>	
	f. Curb and Gutter Type _____			<input type="checkbox"/>	<input type="checkbox"/>	
	g. Roadway Element			<input type="checkbox"/>	<input type="checkbox"/>	
	• Steepest Front Slopes: 2:1 (H:V)	4:1 <25' Fill (BLRS Figure 32-2B)		<input checked="" type="checkbox"/>	<input type="checkbox"/>	A short section of 2:1 slope is used near the J. Maddock Airport private grass landing strip to avoid property take from the runway. The steeper slope will allow development of a small ditch to drain offsite flow. The roadway fill height is only 2.2 feet, shallow enough that guardrail is not necessary (BDE Fig 34-4.A)
	• Steepest Back Slopes: _____ (H:V)			<input type="checkbox"/>	<input type="checkbox"/>	
5.	Drainage (Flood Frequency)					
	a. Pavement: _____ years			<input type="checkbox"/>	<input type="checkbox"/>	
	b. Structure: _____ years			<input type="checkbox"/>	<input type="checkbox"/>	
	c. Storm Sewer: _____ years			<input type="checkbox"/>	<input type="checkbox"/>	

Level Two Design Variance Approval

Local Agency: Kane County Division of Transportation

Section No.: 15-00277-01-BR

6. Intersections

- a. **Level of Service for Individual Movement:**

- Through Lanes: _____
- Turn Lanes: _____

- b. Skew Angle: _____ Degrees

- c. Approach Grades: _____ %

- d. Design Vehicle: _____

- e. Turning Radius for Design Vehicle: _____

- f. Minimum Corner Island Size: _____

- g. Minimum Turn Lane Length _____ feet

- Approach Taper: _____ feet
- Departure Taper: _____ feet
- Bay Taper: _____ feet

h. Entrances

<u>Entrance Type</u>	<u>Max. Width (ft.)</u>	<u>Min. Width (ft.)</u>	<u>Max. Grade(%)</u>
----------------------	-------------------------	-------------------------	----------------------

Commercial _____

Residential _____

7. RR Crossings

- a. Type of Railroad Protection:

- b. Crossing Width (at 90° angle) _____ feet

8. Lighting

- a. Illuminance _____ lux

- b. Uniformity Ratio _____

9. Other Items

Prepared By: Matthew J. Smith Transystems
Designer (Local Agency or Consultant)

Date: 1/17/2018

When Prepared by Consultant
Local Agency Concurrence:

Date: 1.17.18

_____ IDOT Regional Engineer Concurrence

Date _____

Central BLR&S Approval

Date _____





Bruce Rauner, Governor
Raymond Poe, Director

Bureau of Land and Water Resources

State Fairgrounds • P.O. Box 19281 • Springfield, IL 62794-9281 • 217/782-6297 • TDD 866/287-2999 • Fax 217/557-0993

February 15, 2018

Mr. Richard Ray
Huff & Huff/GZA, Inc.
915 Harger Road – Suite 330
Oak Brook, Illinois 60523

Re: Dauberman Road Extension
U.S. Route 30 to Granart Road – 44 acres
Kane County, Illinois
USDA NRCS Form AD-1006

Dear Mr. Ray:

The Illinois Department of Agriculture (IDOA) has completed its review of the agricultural impacts associated with extending Dauberman Road 0.6 miles south from the existing road at U.S. 30 to Granart Road in the Village of Big Rock, Illinois. The IDOA conducted a Study of Agricultural Impacts (copy enclosed) associated with the request in accordance with the rules governing the state's Farmland Preservation Act (505 ILCS 75/1 et seq.).

Dauberman Road is an important north-south roadway connection in southwest Kane County. The Dauberman Road extension project is one of five alignments in the western third of the county recommended by the Kane County 2040 Transportation Plan to provide a continuous north-south corridor for improved connectivity throughout the County and region. The extension will create an additional north-south roadway connection over the Burlington Northern Sante Fe (BNSF) Railroad and provide a safer, more effective emergency route. Its construction results in the conversion of 44 acres of agricultural land to a non-agricultural use.

Based upon our Study, the IDOA has no objection to Kane County proceeding with the highway improvements. The IDOA would consider such an action to be consistent with the IDOT's Agricultural Land Preservation Policy and in compliance with the state's Farmland Preservation Act.

Sincerely,

A handwritten signature in cursive script that reads "Steven D. Chard".

Steven D. Chard, Acting Chief
Bureau of Land and Water Resources

SDC:JL/TS

Enclosures (2)

cc: Governor Bruce Rauner
Sen. John Cullerton
Sen. Bill Brady
Rep. Michael Madigan
Rep. Jim Durkin

Sen. Jim Oberweis
Rep. Keith R. Wheeler
Dir. Raymond Poe, IDOA
Warren Goetsch, IDOA
Craig Sondgeroth, IDOA

Inter-Agency Committee
Ashley Curran, RC, Kane-
DuPage Co. SWCD
Agency project file

**ILLINOIS DEPARTMENT OF AGRICULTURE
STUDY OF AGRICULTURAL IMPACTS**

**Dauberman Road Extension
U.S. Route 30 to Granart Road
Kane County, Illinois**

The Illinois Department of Agriculture (IDOA) conducted a Study of Agricultural Impacts on the proposal by the Kane County Division of Transportation to extend Dauberman Road from US Route 30 to Granart Road. The project consists of constructing new right-of-way (ROW) and some construction along the existing ROW. The results of our Study are as follows:

New right-of-way – Approximately 57.3 acres of ROW are needed for the project. Of the 57.3 acres, 13.3 acres are existing ROW while 44 acres will be acquired from the adjoining farm operations.

Land use – Of the 57.3 ROW acres necessary for the project, 44 acres (76.8%) are in agricultural use while 13.3 acres (23.2%) are existing highway ROW.

Prime farmland status – According to the Kane County Soil Survey that was prepared by the USDA Natural Resources Conservation Service, the 44 agricultural acres are comprised of Prime soils.

Minimum design standards – This project has been designed to minimize the amount of additional right-of-way required to construct the project.

No landlocked parcels, severed parcels, uneconomical remnants, or adverse travel will be created.

Relocations – There will be no relocations of farmsteads, farm buildings, or rural residences.

Secondary agricultural land conversion – No additional agricultural land will be used for wetland mitigation, tree replacement or floodplain compensatory storage. Borrow sites could come from affected landowners or provided from an unknown source.

Field entrances – All field entrances will be rebuilt to meet the new grade line of the road and widened, if necessary, to accommodate wider farm equipment now in use.

Drainage impacts – All tile and drainage systems will be retained, with repairs and new outlets being provided for any tile that is affected by the construction of the corridor.

Agricultural Conservation and Protection Area – None of the proposed ROW acquisition acres are located in a County-designated Ag Area under the Agricultural Areas Conservation and Protection Act 505 ILCS 5/1.

Loss of gross agricultural receipts – The conversion of all agricultural land within the 44 cropland acres will result in a potential annual loss of **\$51,216** cash receipts from crops and livestock based upon statistics from the *2012 Census of Agriculture*. (*Kane Co. Total Cash Receipts*) + (*Land in Farms in Kane Co.*) = (\$1,164/ac) x (44 ac in ag use.)

IDOA Opinion – The road extension project has been designed to minimize impacts to agricultural land by utilizing the existing alignment to the greatest extent possible. Because the project has been designed to meet safety standards as well as to minimize agricultural impacts, the IDOA has determined the project meets the intent of the IDOA-IDOT Cooperative Working Agreement on the protection of Illinois farmland.

**Dauberman Road Extension
Kane County, Illinois
Federal Highway Administration Funds**

PART VI-B		
Illinois Site Assessment <i>CORRIDOR</i> Factors	Maximum Points	Site A
1. Amount of agricultural land required	30	30
2. Location of the proposed alignment	30	20
3. Acres of off-site agricultural land required for borrow materials	15	15
4. Acres of Prime and Important farmland required for mitigation	15	15
5. Creation of severed farm parcels	10	0
6. Creation of uneconomical remnants	10	0
7. Creation of landlocked parcels	10	0
8. Creation of adverse travel	10	0
9. Relocations of rural residences and farm buildings	10	0
10. Utilization of minimum design standards	10	0
TOTAL SITE ASSESSMENT <i>CORRIDOR</i> POINTS	150	80

PART VII

Relative Value of Farmland	150	145
Total Site Assessment <i>CORRIDOR</i> Factors	150	80
TOTAL ILLINOIS LESA POINTS	300	225

11618
JL

** The Illinois LESA System applies the 225 point cutoff when evaluating state and federally funded projects. Site or Corridor alternatives receiving 175 or fewer points have a low rating for protection, and it is not necessary to evaluate additional alternatives. Those alternatives receiving 176 to 225 points are in the moderate range for protection. In most cases, alternatives exceeding the 225 point level should be retained for agricultural use, and an alternate site should be utilized for the intended project. Selecting the alternative with the lowest total points will usually protect the best farmland located in the most agriculturally viable areas. LESA also serves to maintain and promote the agricultural industry in Illinois.*

U.S. Department of Agriculture

FARMLAND CONVERSION IMPACT RATING

PART I (To be completed by Federal Agency)		Date Of Land Evaluation Request 12/27/17			
Name Of Project Dauberman Road		Federal Agency Involved Federal Highway Authority			
Proposed Land Use Roadway/Right-of-Way		County And State Kane County, Illinois			
PART II (To be completed by NRCS)		Date Request Received By NRCS 12/27/17			
Does the site contain prime, unique, statewide or local important farmland? (If no, the FPPA does not apply – do not complete additional parts of this form):		Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	Acres Irrigated N/A	Average Farm Size 372
Major Crop(s) Corn, soybeans, wheat, hay	Farmable Land In Govt. Jurisdiction Acres: 29,633,500 % 97	Amount Of Farmland As Defined In FPPA Acres: 27,695,900 % 91			
Name Of Land Evaluation System Used Illinois	Name Of Local Site Assessment System Statewide	Date Land Evaluation Returned By NRCS 12/27/17			
PART III (To be completed by Federal Agency)		Alternative Site Rating			
		Site A	Site B	Site C	Site D
A. Total Acres To Be Converted Directly		44.0			
B. Total Acres To Be Converted Indirectly		0.0			
C. Total Acres In Site		44.0	0.0	0.0	0.0
PART IV (To be completed by NRCS) Land Evaluation Information					
A. Total Acres Prime And Unique Farmland		44.0			
B. Total Acres Statewide And Local Important Farmland		0.0			
C. Percentage Of Farmland In County Or Local Govt. Unit To Be Converted		0.0			
D. Percentage Of Farmland In Govt. Jurisdiction With Same Or Higher Relative Value		12.5			
PART V (To be completed by NRCS) Land Evaluation Criterion Relative Value Of Farmland To Be Converted (Scale of 0 to 100 Points)		145	0	0	0
PART VI (To be completed by Federal Agency) Site Assessment Criteria (These criteria are explained in 7 CFR 658.5(b))		Maximum Points			
1. Area In Nonurban Use					
2. Perimeter In Nonurban Use					
3. Percent Of Site Being Farmed					
4. Protection Provided By State					
5. Distance From Urban Builtup					
6. Distance To Urban Support S					
7. Size Of Present Farm Unit Cl					
8. Creation Of Nonfarmable Far					
9. Availability Of Farm Support					
10. On-Farm Investments					
11. Effects Of Conversion On Farm Support Services					
12. Compatibility With Existing Agricultural Use					
TOTAL SITE ASSESSMENT POINTS		0	0	0	0
PART VII (To be completed by Federal Agency)					
Relative Value Of Farmland (From Part V)		150	145	0	0
Total Site Assessment (From Part VI above or a local site assessment)		150	80	0	0
TOTAL POINTS (Total of above 2 lines)		300	225	0	0
Site Selected:		Date Of Selection		Was A Local Site Assessment Used? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/>	
Reason For Selection:					

See attached
Illinois LESA Site Assessment
Corridor Factors

** When utilizing the Illinois State Site Assessment Corridor factors, 150 points are assigned to the Land Evaluation portion, and 150 points are assigned to the Site Assessment portion of the LESA System for a maximum score of 300 points.

From: Solomon, Marilyn D
To: [CH-Grace Dysico](#)
Cc: [Kawash, Mohammad](#); ThomasCandance@co.kane.il.us; [CH-Matthew Smith](#); [CH-Brian Fairwood](#)
Subject: FW: Kane County; Dauberman Road, Sec 15-00277-01-BR; Section 4(f) Approval
Date: Monday, April 09, 2018 1:29:57 PM
Attachments: [image001.png](#)

Hi Grace,

FYI.

Thanks,

Marilyn

847-705-4407



Illinois Department of Transportation (IDOT), District 1

"Please consider the environment before printing this e-mail"

From: Raffensperger, William
Sent: Friday, April 06, 2018 7:25 AM
To: Solomon, Marilyn D <Marilyn.Solomon@illinois.gov>
Cc: Kawash, Mohammad <Mohammad.Kawash@illinois.gov>; 'casshighway@casscomm.com' <casshighway@casscomm.com>; Omar Qudus <omar.qudus@dot.gov>
Subject: FW: Kane County; Dauberman Road, Sec 15-00277-01-BR; Section 4(f) Approval

Ms. Solomon –

Under 23 CFR 774.13(d) the temporary occupancy of the Big Rock Camp Ground, owned by the Forest Preserve District of Kane County, is approved by exception. The County has satisfactorily documented the impacts to the Section 4(f) property are of a temporary nature, the scope of work is minor, there are no permanent adverse physical impacts, the land will be fully restored, and there is documented agreement with the OWJ.

Please ensure that a copy of the Section 4(f) temporary occupancy report and this email are included in the final PDR when it is submitted for approval. As noted below, FHWA approval of the temporary occupancy report is not required.

If you have any questions, please call.

William Raffensperger, PE, PTOE, PTP
Project Development Engineer
Illinois Department of Transportation
Bureau of Local Roads and Streets

2300 S. Dirksen Parkway
Springfield, IL 62764
O - 217.785.1676
C - 217.720.2787
Sent from laptop

From: Qudus, Omar [<mailto:omar.qudus@dot.gov>]
Sent: Thursday, April 5, 2018 4:06 PM
To: Raffensperger, William <William.Raffensperger@illinois.gov>
Cc: Cassem, Craig (FHWA) <craig.cassem@dot.gov>
Subject: [External] RE: Kane County; Dauberman Road, Sec 15-00277-01-BR; Section 4(f) Approval

Mr. Raffensperger,

For temporary occupancies of Section 4(f) properties that qualify for the exemption under 23 CFR 774.113(d), FHWA does not need to "approve" the temporary occupancy. We would only need to ensure, prior to NEPA approval, that the temporary occupancy criteria have been satisfied and documented in the project record.
Kane County will need to gain concurrence from the Owners with Jurisdiction for the exemption to be applicable.

Thank you,
Omar

From: Raffensperger, William [<mailto:William.Raffensperger@illinois.gov>]
Sent: Thursday, April 05, 2018 3:02 PM
To: Cassem, Craig (FHWA) <craig.cassem@dot.gov>
Cc: Qudus, Omar <omar.qudus@dot.gov>
Subject: FW: Kane County; Dauberman Road, Sec 15-00277-01-BR; Section 4(f) Approval

Mr. Cassem –

Attached is a Section 4(f) temporary occupancy request for impacts to the Big Rock Camp Ground owned by the Forest Preserve District of Kane County. This project proposes to construct a shared use path on the Section 4(f) property, which enhances to the outdoor recreational activities promoted by the Forest Preserve.

The coordination with the OWJ is a letter from the Kane County Forest Preserve District, dated March 5, 2018 supporting the temporary easement and proposed construction of the multi-use path on the District's property. This letter is page 7 in the attached pdf document.

IDOT recommends approval of this Section 4(f) temporary occupancy request.

If you have any questions, please call.

William Raffensperger, PE, PTOE, PTP
Project Development Engineer

Illinois Department of Transportation
Bureau of Local Roads and Streets
2300 S. Dirksen Parkway
Springfield, IL 62764
O - 217.785.1676
C - 217.720.2787
Sent from laptop

From: Raffensperger, William
Sent: Tuesday, March 20, 2018 12:53 PM
To: 'Qudus, Omar' <omar.qudus@dot.gov>
Subject: RE: Kane County; Dauberman Road, Sec 15-00277-01-BR; Section 4(f) Approval

Mr. Qudus –

The coordination with the OWJ is a letter from the Kane County Forest Preserve District, dated March 5, 2018 supporting the temporary easement and proposed construction of the multi-use path on the District's property. This letter is page 7 in the attached pdf document.

Is this letter satisfactory to the FHWA? Thank you for your assistance,

William Raffensperger, PE, PTOE, PTP
Project Development Engineer
Illinois Department of Transportation
Bureau of Local Roads and Streets
2300 S. Dirksen Parkway
Springfield, IL 62764
O - 217.785.1676
C - 217.720.2787
Sent from laptop

From: Qudus, Omar [<mailto:omar.qudus@dot.gov>]
Sent: Monday, March 19, 2018 1:07 PM
To: Raffensperger, William <William.Raffensperger@illinois.gov>
Subject: [External] RE: Kane County; Dauberman Road, Sec 15-00277-01-BR; Section 4(f) Approval

Mr. Raffensperger,

In accordance with 23 CFR 774.113(d), this project could qualify for a temporary occupancy exception.
FHWA does not need to approve this exception, we would only need to ensure, prior to NEPA approval, that the temporary occupancy criteria have been satisfied and documented in the project record.
Please provide the concurrence from the Owners with Jurisdiction for the exemption to be applicable.

Thank you,

Omar

From: Raffensperger, William [<mailto:William.Raffensperger@illinois.gov>]
Sent: Friday, March 09, 2018 9:18 AM
To: Helmerichs, Robin (FHWA) <Robin.Helmerichs@dot.gov>
Cc: Qudus, Omar <omar.qudus@dot.gov>
Subject: FW: Kane County; Dauberman Road, Sec 15-00277-01-BR; Section 4(f) Approval

Ms. Helmerichs –

Attached is a Section 4(f) temporary occupancy request for impacts to the Big Rock Camp Ground owned by the Forest Preserve District of Kane County. This project proposes to construct a shared use path on the Section 4(f) property, which enhances to the outdoor recreational activities promoted by the Forest Preserve.

IDOT recommends approval of this Section 4(f) temporary occupancy request.

If you have any questions, please call.

William Raffensperger, PE, PTOE, PTP
Project Development Engineer

Illinois Department of Transportation
Bureau of Local Roads and Streets
2300 S. Dirksen Parkway
Springfield, IL 62764

Work hours: 7:00 am to 3:00 pm.
(O) 217-785-1676
(C) 217-720-2787
(F) 217-782-3971
william.raffensperger@illinois.gov

From: Solomon, Marilyn D
Sent: Friday, March 09, 2018 7:58 AM
To: Raffensperger, William <William.Raffensperger@illinois.gov>
Cc: Kawash, Mohammad <Mohammad.Kawash@illinois.gov>
Subject: FW: Kane County; Dauberman Road, Sec 15-00277-01-BR; Section 4(f) Approval

William,

Please see below email. We are forwarding the attachment/update of Forest Preserve's support for the temporary occupancy.

Thanks,
Marilyn
847-705-4407



Illinois Department of Transportation (IDOT), District 1

"Please consider the environment before printing this e-mail"

From: gldysico@transystems.com [<mailto:gldysico@transystems.com>]
Sent: Thursday, March 08, 2018 2:56 PM
To: Solomon, Marilyn D <Marilin.Solomon@illinois.gov>
Cc: Kawash, Mohammad <Mohammad.Kawash@illinois.gov>; mjsmith@transystems.com;
ThomasCandance@co.kane.il.us; bjholman@transystems.com; blfairwood@transystems.com
Subject: [External] RE: Kane County; Dauberman Road, Sec 15-00277-01-BR; Section 4(f) Approval

Good afternoon Marilyn,

As follow-up to the Section 4(f) TO, the County has obtained a support letter from the Kane County Forest Preserve District. It clearly states the Forest Preserve's support for the temporary occupancy. The letter has been included in Appendix B of the attached FINAL Section 4(f) TO document.

Please forward the Section 4(f) TO document to CBLRS to coordinate the FHWA approval. If you have questions or require other information, please let us know.

Thank you,
Grace

From: Solomon, Marilyn D [<mailto:Marilin.Solomon@illinois.gov>]
Sent: Tuesday, February 13, 2018 1:46 PM
To: CH-Grace Dysico <gldysico@transystems.com>
Cc: Kawash, Mohammad <Mohammad.Kawash@illinois.gov>; CH-Matthew Smith <mjsmith@transystems.com>; Thomas, Candance <ThomasCandance@co.kane.il.us>; 'Mike Zakosek' <zakosekmike@co.kane.il.us>
Subject: FW: Kane County; Dauberman Road, Sec 15-00277-01-BR; Section 4(f) Approval

Grace,

We are forwarding the email below (another item in the list) for the approval/process of TO Report.

Thanks,
Marilyn
847-705-4407



Illinois Department of Transportation (IDOT), District 1

"Please consider the environment before printing this e-mail"

From: Raffensperger, William
Sent: Tuesday, February 13, 2018 11:36 AM
To: Solomon, Marilyn D <Marilyn.Solomon@illinois.gov>
Cc: Kawash, Mohammad <Mohammad.Kawash@illinois.gov>
Subject: FW: Dauberman Road, Sec 15-00277-01-BR; Section 4(f) Approval

Ms. Solomon –

Please have the county provide a letter from the forest preserve district, dated after the public hearing, supporting the temporary occupancy. Once this is available, the FHWA will approve the temporary occupancy report.

Thank you for your assistance,

William Raffensperger, PE, PTOE, PTP
Project Development Engineer

Illinois Department of Transportation
Bureau of Local Roads and Streets
2300 S. Dirksen Parkway
Springfield, IL 62764

Work hours: 7:00 am to 3:00 pm.
(O) 217-785-1676
(C) 217-720-2787
(F) 217-782-3971
william.raffensperger@illinois.gov

From: Helmerichs, Robin (FHWA) [<mailto:Robin.Helmerichs@dot.gov>]
Sent: Tuesday, February 13, 2018 8:14 AM
To: Raffensperger, William <William.Raffensperger@illinois.gov>
Cc: Qudus, Omar <omar.qudus@dot.gov>
Subject: [External] RE: Dauberman Road, Sec 15-00277-01-BR; Section 4(f) Approval

Bill,

Please have Kane County include a letter of support/agreement from the Forest Preserve agreeing with this process. Then the temporary occupancy can be signed by FHWA.

Thanks,
Robin

From: Raffensperger, William [<mailto:William.Raffensperger@illinois.gov>]
Sent: Tuesday, February 13, 2018 6:48 AM

To: Helmerichs, Robin (FHWA) <Robin.Helmerichs@dot.gov>

Cc: Qudus, Omar <omar.qudus@dot.gov>

Subject: FW: Dauberman Road, Sec 15-00277-01-BR; Section 4(f) Approval

Ms. Helmerichs –

Please review and approve the attached Section 4(f) report for a temporary occupancy of the Big Rock Camp Ground owned and operated by the Forest Preserve District of Kane County. The temporary occupancy is necessary to construct a shared use path on the forest preserve property. The Forest preserve supports this construction. During the Phase 2 design stage of the project, the Kane County Department of Transportation and the Forest Preserve will execute an intergovernmental agreement for this work. A draft copy of the IGA is included in the report.

IDOT recommends approval of this Section 4(f) temporary occupancy.

If you have any questions, please call,

William Raffensperger, PE, PTOE, PTP
Project Development Engineer

Illinois Department of Transportation
Bureau of Local Roads and Streets
2300 S. Dirksen Parkway
Springfield, IL 62764

Work hours: 7:00 am to 3:00 pm.

(O) 217-785-1676

(C) 217-720-2787

(F) 217-782-3971

william.raffensperger@illinois.gov

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Bureau of Traffic Operations-Traffic Programs Section-Local Roads Review

Traffic Signal Warrant and Intersection Design Study (IDS) for
US Route 30 and Dauberman Road Connector Roadway
Village of Big Rock, Kane County (Section #15-00277-01-BR)

April 30, 2018

- 1) Intersection vertical Sight-Distance and Stopping Sight-Distance Exhibit-No further comments.
- 2) On the Intersection Design Study (IDS), the storage for the southbound left-turn lane and right-turn lane on the new Connector Roadway is shown as 600 feet. The length of the taper to develop the turn lane storage, however, is not provided on the Intersection Design Study.
- 3) As a follow up to our previous comment regarding the field entrance along the south side of US 30, there is an existing field entrance located approximately 400' east of Dauberman Rd. This entrance will need to be relocated, if necessary, to keep the field entrance out of the influence of the proposed intersection of US 30 and the Dauberman Rd connector.
- 4) As a follow up to our previous comment regarding the potential for a fourth leg to be developed at the intersection of US 30 and the Dauberman Rd connector in the future, sufficient setback of any detention within the northwest quadrant of the intersection is required. This is required to accommodate possible widening westward of the Dauberman Rd connector should the proposed right turn lane need to be relocated to accommodate an additional, dedicated through lane to access the south side of US 30. Additional geometric improvements may be needed to accommodate a future 4th leg. Construction of a fourth leg would require a separate IDS.

Bureau of Traffic Approval of Traffic Signal on US Route 30

- 5) The Department agrees that the traffic signal at US Route 30 and Dauberman Road connector roadway is warranted under opening day Year 2020 satisfaction of MUTCD Traffic Signal Warrant #3 (Peak Hour Volume). A traffic signal is approved conditional upon construction of all proposed geometric improvements in the final approved IDS.
- 6) Is a traffic signal proposed for the intersection of Dauberman Rd and the Dauberman Rd connector? If so, please provide a capacity analysis and report including queues up to and including the 95th percentile.
- 7) Submit a revised Intersection Design Study (IDS) for US Route 30 and Dauberman Road connector roadway for continued review.

From: [CH-Grace Dysico](#)
To: [Candi Thomas](#); [CH-Brian Fairwood](#); [CH-Matthew Smith](#); [CH-John A. Fortmann](#); [Dan Bruckelmeyer](#); [CH-Brian Holman](#)
Subject: FW: Kane County, Section No: 15-00277-01-BR; Dauberman Road; Bureau of Traffic IDS Review
Date: Friday, July 27, 2018 3:29:53 PM
Attachments: [image001.png](#)
[image002.png](#)

Per the response from BOT below, the signal warrant is satisfactory and approved.

From: Lloyd, Jonathan M.
Sent: Friday, July 27, 2018 12:41 PM
To: Kawash, Mohammad <Mohammad.Kawash@illinois.gov>
Cc: Solomon, Marilyn D <Marilyn.Solomon@illinois.gov>
Subject: RE: Kane County, Section No: 15-00277-01-BR; Dauberman Road; Bureau of Traffic IDS Review

Dear Moe,

We already approved the traffic signal. In our comments from April 30, 2018 we stated the following:

“The Department agrees that the traffic signal at US Route 30 and Dauberman Road connector roadway is warranted under opening day Year 2020 satisfaction of MUTCD Traffic Signal Warrant #3 (Peak Hour Volume). A traffic signal is approved conditional upon construction of all proposed geometric improvements in the final approved IDS.”

Please let me know if there are any other questions.

Sincerely,

Jonathan M. Lloyd, P.E.
Traffic Studies Engineer
IDOT – District 1: Traffic Operations
847-705-4135

***A goal we can all live with
Drive Zero Fatalities to a Reality***

 Please consider the environment before printing this e-mail.

PDR Note: previous email chain truncated for clarity

CH-Brian Holman

From: CH-Grace Dysico
Sent: Monday, May 07, 2018 2:17 PM
To: Thomas, Candance
Cc: CH-Brian Fairwood; CH-Matthew Smith; CH-Brian Holman
Subject: RE: Dauberman - Noise Analysis
Attachments: VoiceMessage.wav

Noise Report Approval

Hi Candi,

Acknowledge. We will not be making any revisions to the Noise Report. We will include this email from you in the PDR as proof of approval from BLRS.

Brian H.

Please save this email with the attached voicemail message to the files.

Thank you,
Grace

From: Thomas, Candance [<mailto:ThomasCandance@co.kane.il.us>]
Sent: Friday, May 04, 2018 7:47 AM
To: CH-Grace Dysico <gldysico@transystems.com>
Cc: CH-Brian Fairwood <blfairwood@transystems.com>
Subject: Dauberman - Noise Analysis

Good Morning –

Mr Raffensperger left me a voicemail that the report is fine as is.

Please let me know if you have any questions.

Thanks,

Candi Thomas, P.E.

Senior Project Manager
Kane County Division of Transportation
Direct Phone: (630) 406-7355
Main Phone: (630) 584-1170
Fax: (630) 584-5265
Email: thomascandance@co.kane.il.us



CH-Brian Holman

From: CH-Grace Dysico
Sent: Tuesday, June 12, 2018 2:29 PM
To: 'Solomon, Marilyn D'
Cc: ThomasCandance@co.kane.il.us; CH-Matthew Smith; CH-Brian Holman; CH-Brian Fairwood; Raffensperger, William; Kawash, Mohammad; CH-Dave Block
Subject: RE: PMA Seq's 19997/A - D1 - Kane Co Sec 15-00277-01-BR; Wetlands Form
Attachments: FW: PMA Seq's 19997/A - D1 - Kane Co Sec 15-00277-01-BR; Wetlands Form

Per the attached email, the requested change has been noted and Sue Dees Hargrove updated the IDOT PMA.

Thanks for the assistance on this. Moving forward into Phase 2, we will continue the coordination through Kane County for the required mitigation credits from the County's Sauer Family Forest Preserve Wetland Bank.

From: Solomon, Marilyn D [mailto:Marilyn.Solomon@illinois.gov]
Sent: Tuesday, June 12, 2018 2:09 PM
To: CH-Grace Dysico <gldysico@transystems.com>
Cc: ThomasCandance@co.kane.il.us; CH-Matthew Smith <mjsmith@transystems.com>; CH-Brian Holman <bjholman@transystems.com>; CH-Brian Fairwood <blfairwood@transystems.com>; Raffensperger, William <William.Raffensperger@illinois.gov>; Kawash, Mohammad <Mohammad.Kawash@illinois.gov>
Subject: RE: PMA Seq's 19997/A - D1 - Kane Co Sec 15-00277-01-BR; Wetlands Form

Hi Grace,

Thank you for the follow-up. Please see attached email from CBLRS for the update.

Thanks,
Marilyn
847-705-4407



Illinois Department of Transportation (IDOT), District 1

"Please consider the environment before printing this e-mail"

From: gldysico@transystems.com [mailto:gldysico@transystems.com]
Sent: Tuesday, June 12, 2018 10:43 AM
To: Solomon, Marilyn D <Marilyn.Solomon@illinois.gov>; Kawash, Mohammad <Mohammad.Kawash@illinois.gov>
Cc: ThomasCandance@co.kane.il.us; mjsmith@transystems.com; bjholman@transystems.com; blfairwood@transystems.com
Subject: [External] RE: PMA Seq's 19997/A - D1 - Kane Co Sec 15-00277-01-BR; Wetlands Form

Hi Marilyn & Moe,
Any word from CLBRS & FHWA on the wetland bank change? Please let us know if they have questions or need other information.

Thank you,
Grace

From: Solomon, Marilyn D [<mailto:Marilyn.Solomon@illinois.gov>]
Sent: Thursday, May 24, 2018 4:08 PM
To: CH-Grace Dysico <gldysico@transystems.com>
Cc: ThomasCandance@co.kane.il.us; CH-Matthew Smith <mjsmith@transystems.com>; CH-Brian Holman <bjholman@transystems.com>; CH-Brian Fairwood <blfairwood@transystems.com>; Kawash, Mohammad <Mohammad.Kawash@illinois.gov>
Subject: RE: PMA Seq's 19997/A - D1 - Kane Co Sec 15-00277-01-BR; Wetlands Form

Hi Grace,

Receipt acknowledged; forwarded to CBLRS & FHWA.

Thanks,
Marilyn
847-705-4407



Illinois Department of Transportation (IDOT), District 1

"Please consider the environment before printing this e-mail"

From: gldysico@transystems.com [<mailto:gldysico@transystems.com>]
Sent: Thursday, May 24, 2018 1:42 PM
To: Solomon, Marilyn D <Marilyn.Solomon@illinois.gov>; Kawash, Mohammad <Mohammad.Kawash@illinois.gov>
Cc: ThomasCandance@co.kane.il.us; mjsmith@transystems.com; bjholman@transystems.com; blfairwood@transystems.com
Subject: [External] RE: PMA Seq's 19997/A - D1 - Kane Co Sec 15-00277-01-BR; Wetlands Form

Good afternoon Marilyn and Moe,

As you may be aware the Dauberman Road project received the Wetland Resources Clearance December 15, 2017. See attachments for the clearance documents. The project was cleared for design approval and letting. The proposed mitigation measure was to obtain credits from a wetland bank site in the Fox River basin. The banks listed in the clearance form were Gray Willows, V3 Blackberry 2, and Prairie Green 1.

This email is to inform IDOT that KDOT has identified an alternate wetland bank site, also in the Fox River basin, that they would like to use. The bank is approximately 4 miles northeast of the project area. Here are the particulars about the bank:

Bank Name: Sauer Family Forest Preserve Wetland Bank
Location: Southwest quadrant of Lasher Road and Harter Road, Sugar Grove, IL
Owned and Operated by: the Kane County Forest Preserve (KCFP)
Constructed: early 2000's

It is not a commercial bank. This wetland bank is used only for KDOT and KCFP projects. Most recently, it was used for the KDOT Longmeadow project (Section 94-00215-01-ES) for 6.06 acres.

The Dauberman Road project will require 11.72 acres of wetland bank credits. KDOT anticipates that the needed credits for Dauberman Road will be available from the Sauer Family Forest Preserve Wetland Bank. Using this wetland bank will afford KDOT with some cost savings because they will not need to buy credits from a commercial facility. We anticipate approval of KDOT's intent to use the bank as it was done for the Longmeadow project.

Please inform and coordinate with CBLRS and BDE accordingly. Let us know if you have questions or require other information.

Thank you for the assistance.

Best,
Grace

From: Knight, Hannah M. [<mailto:Hannah.Knight@illinois.gov>]
Sent: Monday, December 18, 2017 10:04 AM
To: CH-Grace Dysico <gldysico@transystems.com>; CH-Matthew Smith <mjsmith@transystems.com>
Cc: Solomon, Marilyn D <Marilyn.Solomon@illinois.gov>
Subject: FW: PMA Seq's 19997/A - D1 - Kane Co Sec 15-00277-01-BR; Wetlands Form

Grace / Matt,

Please see attached for the wetlands clearance for the subject project. Project Overview is also attached.

Thanks,

Hannah Knight, P.E.
HR Green, Inc.
Consultant to Bureau of Local Roads and Streets
Illinois Department of Transportation
201 West Center Court
Schaumburg, IL 60196
Tel: (847)705-4205
Fax: (847)705-4203
hannah.knight@illinois.gov

 **Please consider the environment before printing this email**

This e-mail, and any attachments thereto, is intended only for use by the addressee(s) named herein and may contain legally privileged and/or confidential information. If you are not the intended recipient of this e-mail (or the person responsible for delivering this document to the intended recipient), you are hereby notified that any dissemination, distribution, printing or copying of this e-mail, and any attachment thereto, is strictly prohibited. If you have received this e-mail in error, please respond to the individual sending the message, and permanently delete the original and any copy of any e-mail and printout thereof.

From: DOT.LocalAgencyESR
Sent: Monday, December 18, 2017 9:55 AM
To: Solomon, Marilyn D
Cc: Knight, Hannah M.; Raffensperger, William; DOT.LocalAgencyESR
Subject: PMA Seq's 19997/A - D1 - Kane Co Sec 15-00277-01-BR; Wetlands Form

FYI

The subject section has been cleared for letting with respect to **Wetlands**.
Clearance dates have been entered on the Wetlands forms within the subject PMA.
The attached form scan and IDNR concurrence letter have been posted to your project SharePoint folders and included herewith for reference.

Jr

ELMER (JR) PEARCY
CBLRS CONSULTANT

From: Hargrove, Susan Dees

Sent: Monday, December 18, 2017 8:02 AM

To: Percy, Elmer; Raffensperger, William

Subject: PMA Seq. #19997 , 'Cleared for Design Approval' - Wetlands Form

All 'Cleared for Design Approval' dates have been entered for the following PMA record:

IDNR concurrence dated 12/15/17 received.

Sequence #: 19997

District: 1

Route: FAS 1107

Marked Route:

County: Kane

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Illinois Department of Transportation

201 West Center Court
Schaumburg, IL 60196-1096

Informal Transmittal

EP/JRS
Due → 6/28/18

To:	Issam Rayyan
Bureau:	Programming (Geometrics)
Attention:	Jason Salley/Evelina Perry
	(6th SUBMITTAL)
Date:	May 17, 2018 ✓

From:	Chris Holt
Bureau:	Local Roads & Streets
By:	Marilin Solomon/Moe Kawash
Subject:	Kane County/Dauberman Road/US Route 30
Section:	15-00277-01-BR

Please check appropriate box below:

- ☒ Take Necessary Action
- ☐ For Your Comments
- ☐ Per Your Request
- ☒ For Your Approval

- ☐ For Your Information
- ☐ See Me About the Attached
- ☐ Draft (Letter)(Memo) For My signature

- ☒ Reply
- ☒ Return
- ☐ Route
- ☐ File

Message

Jason,

Attached are two copies of the IDS, typical sections and cross sections of the subject project.

Please review and provide your comments and/or your approval.

Feel free to call me at x4205 or email me at Mohammad.Kawash@Illinois.gov.

Thanks.

Moe Kawash
Signature

Mohammad K

Copies to

File

Response

6/27/18

Geometrics Approval of IDS

Moe - This project's Geometry & IDS at the US 30 AND CONNECTION NO INTERSECTION ONLY ARE APPROVED. THE REMAINING COMPONENTS ARE NOT WITHIN MY PURVIEW.

CC: JL & JK (BOT)

THANK YOU,

Signature



To:	Chris Holt
Attn:	Marlin Solomon / Moe Kawash
Bureau:	Local Roads & Streets
Phone:	(847) 705-4407
Date:	June 27, 2018

From:	Jason Salley
Bureau:	Programming / Geometrics Unit
Phone:	(847) 705-4085
Subject:	Dauberman Road – Granart Road To US Route 30 LR&S Section # 15-00277-01-BR Geometric & IDS Approval

Please check appropriate box below:

- ☐ Take Necessary Action
☐ For Your Comments
☐ Per Your Request
☒ For Your Approval

- ☐ For Your Information
☐ See Me About the Attached
☐ Draft (Letter)(Memo) For
My signature

- ☐ Reply
☐ Return
☐ Route
☐ File

Message

Moe,
All of the Design Elements for this project meet current BDE Standards.

A PDF copy of the project's Intersection Design Study (IDS) has been received by the Geometric Studies Unit and has been placed onto the District's H Drive for future reference.

Geometrics Approval

Therefore, I approve this project's geometry as well as its IDS as it relates to only the US 30 at Connector Rd intersection. The remaining components of this project fall outside of my purview.

This project's BDE 2602 Form will be forwarded to IDOT BDE for their records.

Please contact me if you have any questions or comments.

Thanks,

Jason Salley, P.E.
Signature

Copies to

File	BDE	BOT	
------	-----	-----	--

Response

Signature



District

1

Consultant

Bollinger, Lach & Associates, Inc.

Type of Design:

- ☒ Intersection Design Study ☐ Interchange Design Study
☐ Interchange Type Study ☐ Other

Route

FAS 1107A

Marked

Street

Dauberman Road

Intersecting Route

FAP 573

Marked

US Route 30

Street

Contract Number

State Job Number

Section Number

Counties

Kane

Municipalities

Big Rock

Local Agency

Kane County DOT

LRS Section Number

15-00277-01-BR

Permit Applicant

Permit Number

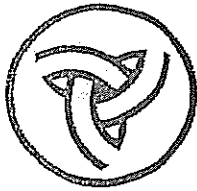
Brief Project Description

The proposed improvements will extend Dauberman Road south as a two-lane roadway approximate 0.8 miles from its current terminus at US Route 30 to Granart Road. The new proposed roadway extension will tie into existing Granart Road. Two structures are proposed along the extension to carry Dauberman Road over US Route 30 and the BNSF Railroad. The existing Dauberman Road & US Route 30 T-intersection will be replaced with a proposed new jug-handle type intersection, also referred to as the Connector Road. A multi-use path will be constructed as part of the project. The 10-foot paved path will be along the east side of Dauberman Road and will extend from US Route 30 to Granart Road, then continue east along the south side of Granart Road to the entrance of the Big Rock Forest Preserve and Campgrounds.

Date Approved by Qualified Geometrics Engineer

06/27/2018

Comments



Illinois Department of Transportation

Memorandum

Traffic Approval of IDS
Page 1/2

To: *Local Roads* *ATTN: M. Solomon/M. Kawash*
From: *Lisa Heaven-Baum* By: *Connie LaRocco-Stresino*
Subject: *Plan Review*
Date: *7-24-18*

 X We have completed our review of the above referenced project.
The comments/recommendations by the various Sections are
attached.

 We have completed a partial review of the above referenced
project.

 We have completed the balance of our review of the above
referenced project. The additional comments are attached.

 Final Plans Comments

File Name: *RT1 (K) Dauberman Rd Ext @ US 30*

RECEIVED
BUREAU OF

JUL 26

LOCAL ROADS & STREETS



3L 5/16
JML 7/19

5/31

Illinois Department of Transportation

Memorandum

✓ **FROM:** Bureau of Traffic Operations Attn: C. LaRocco-Stresino

✓ **TO:** ✓ Bureau of Local Roads

Subject: Plan Review

Date: May 10, 2018

SUBMITTAL/TYPE OF REVIEW

- | | | | |
|---|---|--|---|
| <input checked="" type="checkbox"/> Phase I | <input type="checkbox"/> LOI/LOU/Agreements | <input type="checkbox"/> Detector Loop Request | <input type="checkbox"/> Preliminary PS&E |
| <input type="checkbox"/> Warrants | <input type="checkbox"/> Cost Estimates | <input type="checkbox"/> Traffic Signal Design Request | <input type="checkbox"/> Pre-final PS&E |
| <input type="checkbox"/> TIS | <input type="checkbox"/> Work Zone Safety | <input type="checkbox"/> Railroad Report | <input type="checkbox"/> Final PS&E |
| <input checked="" type="checkbox"/> IDS | <input type="checkbox"/> Utility Permits | <input type="checkbox"/> Catalog Cuts | <input type="checkbox"/> HSIP |

✓ **CONTACT PERSON:** Marilyn Solomon/Moe Kawash **SECTION CHIEF:** Chris Holt

ROUTE: Dauberman Road Extension

LIMITS: US Route 30

TOWN/VILLAGE: Big Rock

SECTION: 15-00277-01-BR

COUNTY: Kane

JOB NUMBER: NA

CONTRACT #: NA

LETTING DATE: 2020

WERE COMMENTS PREVIOUSLY RECEIVED: ☒ YES ☐ NO

If yes, please attach a copy of the disposition of comments.

SECTION(S) TO REVIEW

(see attachment for section description)

Each section requires their own set of plans.

- | | |
|---|---|
| <input type="checkbox"/> PERMITS | ELECTRICAL DESIGN |
| <input type="checkbox"/> EXPRESSWAYS | <input type="checkbox"/> Lighting |
| <input type="checkbox"/> ELECTRICAL MAINTENANCE/TSC | <input type="checkbox"/> ITS |
| <input type="checkbox"/> STRUCTURAL OPS. | <input type="checkbox"/> Surveillance |
| <input type="checkbox"/> ARTERIALS | TRAFFIC PROGRAMS |
| <input type="checkbox"/> TRAFFIC CONTROL STAGING | <input type="checkbox"/> Design |
| | <input checked="" type="checkbox"/> Studies |
| | <input type="checkbox"/> Signals |

Traffic Approval of IDS
Page 2/2

Please allow up to three (3) weeks from the date of this request for review process.

For Traffic Operations only

Return to Connie by: 5/31

File Name: RT 1(K) Dauberman Rd. Ext @ US 30

Comments:

TRAFFIC STAFF: On Page 1 of the IDS, revise Matchline STA. 802+00 to read "See Sheet 4" instead of Sheet 3

No Further
Comments



Illinois Department of Transportation

Office of Highways Project Implementation / Region 1 / District 1
201 West Center Court / Schaumburg, Illinois 60196-1096

LOCAL ROADS AND STREETS

Preliminary Bridge Design and Hydraulic Report Approval Notification

County of Kane

Location: Dauberman Road over BNSF Railroad & US Route 30

Section No.: 15-00277-01-BR

Proposed Structure No.: 045-3401

Cook County

October 9, 2018

Mr. Matthew D. Santeford, P.E.
Vice President
TranSystems Corporation
1475 E. Woodfield Road, Suite 600
Schaumburg, IL 60173

Dear Mr. Santeford:


Enclosed is the Approved Preliminary Bridge Design and Hydraulic Report (PBDHR) for the above referenced project. Please include the Approval Memorandum in the Project development Report.

Once the bridge design is finalized, three copies of the Final Specifications and reduced-size Structural Plans along with appropriate documentation should be submitted to our office for the Central Bureau of Bridges and Structures' approval.

If you have any questions or need additional information, please contact Marilyn Solomon, Field Engineer, at (847) 705-4407 or via email at Marilyn.Solomon@illinois.gov.

Very truly yours,

Anthony J. Quigley, P.E.
Region One Engineer

By: 
Christopher J. Holt, P.E.
Bureau Chief of Local Roads and Streets

Enclosure

RECEIVED

OCT 15 2018





Illinois Department of Transportation

Memorandum

FILE
MDH

To: Anthony J. Quigley, District 1 Attn: Christopher J. Holt
From: D. Carl Puzey By: Timothy A. Armbrrecht
Subject: PRELIMINARY BRIDGE DESIGN APPROVAL
Date: September 19, 2018

Timothy A. Armbrrecht

STP – Rural / IL Grade Crossing Protection Funding
Kane County
Section 15-00277-01-BR

SN 045-3401
045-3402

FAU 1107 (Dauberman Road) over BNSF Railroad & US Route 30

The preliminary bridge designs, for the above-designated project are satisfactory based on the consultant's disposition to our review comments and revised Type, Size, & Location drawings, received by e-mail correspondence, which adequately address our review comments. The preliminary bridge designs are hereby approved. This approval is consistent with our provisional approval sent by e-mail correspondence on September 18, 2018.

SN 045-3401. The final structural plans and specifications for this project may be accepted by the District based on the Structural Engineer's seal, certification and signature per BLRS Manual Section 23-7.02.1, and on a **sealed structure load rating** submittal as described in BLRS Circular Letter 2017-16 and Structural Services Manual Section 4.2.2.

SN 045-3402. Three (3) sets of the final specifications and **reduced-size** structural plans for this project must be submitted to the Bureau of Bridges and Structures, through the District, for final review and approval. After approval, submittal of the original of the General Plan & Elevation (GP&E) sheet, signed and sealed by the Structural Engineer of record, will be required to affix the approval of the Engineer of Bridges and Structures. Please provide an approximately 4"H x 2"V area on the GP&E sheet for affixing the approval stamp. After approval of the final plans, a **sealed structure load rating** submittal as described above will be required.

If you have any questions, contact Matt Humke at 217/782-5929 or matt.humke@illinois.gov.

Two copies of each of the approved reports are being returned to you and we will retain one copy for our files.

MDH



Illinois Department of Transportation

201 West Center Court
Schaumburg, IL 60196-1096

Informal Transmittal

IDOT Approval of LDS
October 24, 2018

To:	Issam Rayyan
Bureau:	Programming (Hydraulics Unit)
Attention:	Perry Masouridis
Date:	October 23, 2018 ✓

From:	Chris Holt
Bureau:	Local Roads & Streets
By:	Marilyn Solomon/Moe Kawash
Subject:	Kane County *
Section:	15-00277-01-BR

Please check appropriate box below:

- ☒ Take Necessary Action
☐ For Your Comments
☐ Per Your Request
☒ For Your Approval

- ☐ For Your Information
☐ See Me About the Attached
☐ Draft (Letter)(Memo) For
My signature

- ☒ Reply
☒ Return
☐ Route
☐ File

Message

*Dauberman Road Extension LDS
Granart Road to US Route 30

Perry,

Enclosed for your review and/or approval:

- (2) Report Location Drainage Study (Vol. 1&2)
- (2) CD Location Drainage Study PDF files (Vol. 1&2)
- (2) CD HEC-HMS & HEC-RAS Files (Location inside Vol.2 – Tab D-1 Cover)

Please review and provide your comments and/or your approval.

Feel free to call me at x4205 or email me.

Thank you,

Moe Kawash
Signature

Mohammed D.K

Copies to

File

Response

we have no comments.

m. Carmona

10/24/18

Signature

From: [Kawash, Mohammad](#)
To: [CH-Grace Dysico](#)
Cc: [Solomon, Marilyn D](#); [CH-Matthew Smith](#); [CH-Brian Holman](#); [CH-Brian Fairwood](#); [Dan Bruckelmeyer](#); [ThomasCandance@co.kane.il.us](#)
Subject: RE: Kane Co., Sec. 15-00277-01-BR, FAU 1107 (Dauberman Rd.), Location Drainage Study Approval
Date: Wednesday, October 24, 2018 3:19:43 PM
Attachments: [image001.png](#)

Hi Grace,

Only transmittal form that you will be getting along with four hard copies.

Thanks,
Moe Kawash

From: gldysico@transystems.com <gldysico@transystems.com>
Sent: Wednesday, October 24, 2018 3:12 PM
To: Kawash, Mohammad <Mohammad.Kawash@illinois.gov>
Cc: Solomon, Marilyn D <Marilyn.Solomon@illinois.gov>; mjsmith@transystems.com; bjholman@transystems.com; blfairwood@transystems.com; DBruckelmeyer@bollingerlach.com; ThomasCandance@co.kane.il.us
Subject: [External] Kane Co., Sec. 15-00277-01-BR, FAU 1107 (Dauberman Rd.), Location Drainage Study Approval

Hi Moe,

Thanks for sending this on. Will we be getting a formal memo/letter from BLR of the LDS approval, similar to what was done for the TS&L approval, or is this Transmittal form all that will be coming?

Dan,

Please assemble the electronic file of the LDS and post it to this link:
<https://transystems.sharefile.com/r-r35b4c84a9e143269> We will follow-up and provide to BLR.

Candi, would you like the hard copies that IDOT is returning?

Thank you,
Grace

Grace L. Dysico PE
Master Professional
Vice President



TranSystems
1475 East Woodfield Road, Suite 600

Schaumburg, IL 60173-5440
Main: 847-605-9600
Direct: 847-407-5247
Fax: 847-463-0565
Cell: 847-477-3404
www.transystems.com

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From: Kawash, Mohammad [<mailto:Mohammad.Kawash@illinois.gov>]

Sent: Wednesday, October 24, 2018 2:10 PM

To: CH-Matthew Santeford <mdsanteford@transystems.com>; Humke, Matt <Matt.Humke@Illinois.gov>

Cc: CH-Brian Fairwood <blfairwood@transystems.com>; CH-Matthew Smith <mjsmith@transystems.com>; jthede@bollingerlach.com; CH-Grace Dysico <gldysico@transystems.com>; CD-Tina Revzin <trevzin@transystems.com>; Dan Bruckelmeyer <DBruckelmeyer@bollingerlach.com>; CH-Brian Holman <bjholman@transystems.com>; Solomon, Marilyn D <Marilyn.Solomon@illinois.gov>; jihde@bla-inc.com; hbrierton@bla-inc.com; ThomasCandance@co.kane.il.us; zakosekmike@co.kane.il.us; Klein, James K <James.Klein@illinois.gov>

Subject: RE: Kane Co., Sec. 15-00277-01-BR, FAU 1107 (Dauberman Rd.) over BNSF Railroad & US RT. 30, Prop. SN 045-3401 & 045-3402

Matt,

Per Bureau of Hydraulics, there are no further comments. Please provide us with scanned copy of the LDS for our records, hard copies are being sent to you through mail.

Attached is a transmittal for your reference.

Thanks,

Moe Kawash

Illinois Department of Transportation, Dist. 1
Bureau of Local Roads and Streets
Associate Field Engineer
P: (847) 705 - 4205

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From: [Kawash, Mohammad](#)
To: [CH-Grace Dysico](#)
Cc: [Solomon, Marilin D](#); [CH-Matthew Smith](#); [CH-Brian Fairwood](#); ThomasCandance@co.kane.il.us
Subject: FW: Kane County 15-00277-01-BR Dauberman Rd over US 30 & RR
Date: Wednesday, November 07, 2018 10:13:05 AM

Hi Grace,

FYI

Thanks,

Moe Kawash

IDOT D1 Bureau of Maintenance Approval;
Dated 11/7/18;
Sarah Wilson, D1 Bridge Engineer

From: Wilson, Sarah M
Sent: Wednesday, November 7, 2018 10:10 AM
To: Kawash, Mohammad <Mohammad.Kawash@illinois.gov>
Cc: Solomon, Marilin D <Marilin.Solomon@illinois.gov>
Subject: Re: Kane County 15-00277-01-BR Dauberman Rd over US 30 & RR

Sounds good - I'm fine with this information; consider this Maintenance's approval

Sarah

On Nov 7, 2018, at 8:08 AM, Kawash, Mohammad <Mohammad.Kawash@illinois.gov> wrote:

Good morning Sarah,

Please see below email and attachment.

Thanks,

Moe Kawash

Illinois Department of Transportation, Dist. 1
Bureau of Local Roads and Streets
Associate Field Engineer
P: (847) 705 - 4205
E-Mail: Mohammad.Kawash@illinois.gov

From: gldysico@transystems.com <gldysico@transystems.com>
Sent: Monday, November 5, 2018 5:08 PM
To: Solomon, Marilin D <Marilin.Solomon@illinois.gov>; Kawash, Mohammad <Mohammad.Kawash@illinois.gov>
Cc: mjsmith@transystems.com; ThomasCandance@co.kane.il.us; blfairwood@transystems.com
Subject: [External] RE: Kane County 15-00277-01-BR Dauberman Rd over US 30 & RR

Good afternoon Marilyn & Moe,
The County has responded that the maintenance and responsibility of the bridges and ramp will be KDOT's. See the attached email from Candi Thomas, County Project Manager. Please forward this information to Sarah Wilson so she can provide her approval. The necessary agreement documents will be completed in Phase 2 Engineering. Thank you.

Grace

From: CH-Grace Dysico
Sent: Friday, November 02, 2018 12:24 PM
To: 'Solomon, Marilyn D' <Marilyn.Solomon@illinois.gov>
Cc: Kawash, Mohammad <Mohammad.Kawash@illinois.gov>; CH-Matt Smith <mjsmith@transystems.com>
Subject: RE: Kane County 15-00277-01-BR Dauberman Rd over US 30 & RR

Marilyn,
It doesn't appear that Sarah provided her "approval". Re-read her email more closely. I don't think this issue is resolved. Did I misinterpret Sarah's email? Could you check with Program Development if an LOI was initiated? I've inquired with KDOT and awaiting a response. Thanks and have a nice weekend.

Grace

From: Solomon, Marilyn D [<mailto:Marilyn.Solomon@illinois.gov>]
Sent: Thursday, November 01, 2018 2:55 PM
To: CH-Grace Dysico <gldysico@transystems.com>
Cc: Kawash, Mohammad <Mohammad.Kawash@illinois.gov>
Subject: RE: Kane County 15-00277-01-BR Dauberman Rd over US 30 & RR

Grace,

1. Sarah Wilson - Yes, it is. We will insert to the PDR.
2. Bill R. – waiting for concurrence from FHWA (will staple upon receipt- haven't checked yet)

Thx- MS

From: gldysico@transystems.com <gldysico@transystems.com>
Sent: Thursday, November 1, 2018 2:35 PM
To: Solomon, Marilyn D <Marilyn.Solomon@illinois.gov>
Cc: Kawash, Mohammad <Mohammad.Kawash@illinois.gov>
Subject: [External] RE: Kane County 15-00277-01-BR Dauberman Rd over US 30 & RR

Marilyn,

Is this all we need from Sarah Wilson? If so, is the last item that remains for the Final PDR approval the email from Bill about the CE Concurrence? He said it would just be stapled to the signature sheet. Please advise.

Thanks,
Grace

From: Solomon, Marilyn D [<mailto:Marilyn.Solomon@illinois.gov>]
Sent: Thursday, November 01, 2018 12:59 PM
To: CH-Grace Dysico <gldysico@transystems.com>
Cc: Kawash, Mohammad <Mohammad.Kawash@illinois.gov>
Subject: RE: Kane County 15-00277-01-BR Dauberman Rd over US 30 & RR

Grace,

We will add this email to the PDR.

Thanks,
Marilyn

From: Kawash, Mohammad
Sent: Wednesday, October 31, 2018 11:34 AM
To: gldysico@transystems.com
Cc: Solomon, Marilyn D <Marilyn.Solomon@illinois.gov>
Subject: FW: Kane County 15-00277-01-BR Dauberman Rd over US 30 & RR

Hi Grace,

Please see below email.

*Thanks,
Moe Kawash*

From: Wilson, Sarah M
Sent: Wednesday, October 31, 2018 11:28 AM
To: Kawash, Mohammad <Mohammad.Kawash@illinois.gov>
Cc: Solomon, Marilyn D <Marilyn.Solomon@illinois.gov>
Subject: RE: Kane County 15-00277-01-BR Dauberman Rd over US 30 & RR

Well – I don't want to 'approve it' if the ownership isn't worked out. Normally something like this is worked out in Phase I using a letter of intent, after which Phase II uses the LOI as the framework of the final agreement.

If the structure is crossing an IDOT road, and it's not an IDOT initiated project, then the local entity should own the structure, and doesn't require Maintenance's 'approval' if they are meeting BDE standards – which they are.

Sarah

From: Kawash, Mohammad
Sent: Wednesday, October 31, 2018 11:20 AM
To: Wilson, Sarah M <Sarah.Wilson@illinois.gov>
Cc: Solomon, Marilin D <Marilin.Solomon@illinois.gov>
Subject: FW: Kane County 15-00277-01-BR Dauberman Rd over US 30 & RR

Sarah,

Please see below email.

Thanks,

Moe Kawash
Illinois Department of Transportation, Dist. 1
Bureau of Local Roads and Streets
Associate Field Engineer
P: (847) 705 - 4205
E-Mail: Mohammad.Kawash@illinois.gov

From: gldysico@transystems.com <gldysico@transystems.com>
Sent: Wednesday, October 31, 2018 10:16 AM
To: Kawash, Mohammad <Mohammad.Kawash@illinois.gov>
Cc: Solomon, Marilin D <Marilin.Solomon@illinois.gov>;
ThomasCandance@co.kane.il.us; blfairwood@transystems.com;
mjsmith@transystems.com; bjholman@transystems.com
Subject: [External] RE: Kane County 15-00277-01-BR Dauberman Rd over US 30 & RR

Good morning Moe,

I conferred with Kane County. The maintenance and responsibility of the structures (and ramp) have not yet been decided upon as the County has not executed agreements or correspondence with IDOT on this as of yet. This is typically a Phase 2 item and the project is not there yet. Therefore, this will be determined and finalized in Phase 2 when the coordination between the County and IDOT are made on this topic.

However, please note that the clearances (vertically and horizontally) over US 30 have been made to accommodate any future widening of US 30 and construction of a bike path along the north side by IDOT.

Let me know if you have questions or need additional information so that we can conclude this aspect of the Phase 1 coordination to move towards approval of the Final PDR.

Thanks,
Grace

From: Kawash, Mohammad [<mailto:Mohammad.Kawash@illinois.gov>]
Sent: Wednesday, October 31, 2018 8:20 AM
To: CH-Grace Dysico <gldysico@transystems.com>
Cc: Solomon, Marilin D <Marilin.Solomon@illinois.gov>
Subject: FW: Kane County 15-00277-01-BR Dauberman Rd over US 30 & RR

Grace,

Please see below email and advise.

Thanks,

Moe Kawash
Illinois Department of Transportation, Dist. 1
Bureau of Local Roads and Streets
Associate Field Engineer
P: (847) 705 - 4205
E-Mail: Mohammad.Kawash@illinois.gov

From: Wilson, Sarah M
Sent: Wednesday, October 31, 2018 7:08 AM
To: Solomon, Marilin D <Marilin.Solomon@illinois.gov>
Cc: Kawash, Mohammad <Mohammad.Kawash@illinois.gov>
Subject: Kane County 15-00277-01-BR Dauberman Rd over US 30 & RR

SN: 045-3401 & 045-3402

Marilin – can you confirm that the local entity will be responsible for 100% of these structures? Also, what agency will be maintaining the ‘ramp’ connecting US 30 & Dauberman?

As long as they are locally maintained, with the 16 ft 6 inch proposed vertical clearance, has well as the proposed horizontal clearances shown on the TS&L, everything looks good to me on this project.

Sarah Wilson
Illinois Department of Transportation
D1 Bridge Maint Eng

APPENDIX C

FHWA/IDOT Coordination Meeting Minutes

- 1/8/16, KDOT & IDOT Meeting
- 1/12/16, FHWA/IDOT Meeting #1
- 3/14/17, FHWA/IDOT Meeting #2
- 5/9/17, FHWA/IDOT Meeting #3
- 7/11/17, FHWA/IDOT Meeting #4
- 12/5/17, FHWA/IDOT Meeting #5
- Final Purpose and Need Document

**Dauberman Road Extension
Kane County DOT**

**IDOT Coordination Meeting
January 8, 2016**

Minutes to Meeting

The purpose of the meeting was to provide IDOT Bureau of Local Roads & Streets (BLRS) an introduction to the project and provide a preview of the presentation at the upcoming FHWA/IDOT coordination meeting on January 12, 2016. BLRS invited the Bureau of Programming to attend because of the US 30 interfaces. The discussions followed the proposed draft for the FHWA/IDOT meeting.

Grace Dysico with TranSystems began the discussions providing a brief history of the project. The Kane County Division of Transportation (KDOT) is the project sponsor. The County has selected TranSystems to conduct the Phase I Study for the Dauberman Road extension and grade separation over the BNSF Railroad. The Dauberman Road extension has been included in the County's Long Range Plan for many years. It was also identified in the Prairie Parkway Study Alternatives Analysis. The ultimate purpose of the project is to improve safety and provide another north-south corridor over the railroad for the west side of the County. It is anticipated that an abbreviated Environmental Assessment (EA) will be required for the NEPA/environmental documentation.

Discussion then ensued regarding the US 30 corridor and the understanding of conversations between IDOT Programming staff and KDOT. Programming has an ongoing Phase 1 study of US 30 from IL 47 to Dugan Road. It involves an add-lane widening and an EA is being prepared. IDOT requested KDOT consider including the segment of US 30 between Dugan Road and Dauberman Road to the EA study limits for Dauberman. Ms. Dysico explained that the County has considered the request and prefers to conduct two separate studies; one for Dauberman and one for US 30. The projects each have separate independent utilities and the County felt this was the most prudent way to proceed if they were to undertake the US 30 study.

Brian Carlson with IDOT acknowledged that he was aware of the County's intent. John Baczek with IDOT questioned why the County felt separating the two projects was necessary. He asked if the County thought that the US 30 segment would be more complicated and pointed out that the Dauberman Road extension would be on a new alignment which could present certain challenges as well.

Brian Fairwood with TranSystems agreed that both projects had unique challenges, separating the projects would, in a sense, protect each from the other from a schedule standpoint. He further explained that the County's interest was with Dauberman Road. The County has budgeted local funding for the Phase 1 and 2 engineering and they have secured ICC Grade Crossing Protection funding for construction. The timetable for KDOT to implement the Dauberman Road improvements is sooner than IDOT's US 30 projects. Currently there are no IDOT funds programmed for Phase 2, land acquisition, and construction for US 30.

John Baczek described another approach which would be to extend the limits of the current Phase 1 EA for US 30 (IL 47 to Dugan Road). IDOT would make a request to the FHWA to extend the limits along US 30 west to match into the limits along US 30 of the County's Dauberman Road study.

Grace Dysico explained that due to its proximity to the BNSF RR, the Dauberman/US 30 intersection would most likely need to be grade separated as well. Initial concept level reviews include a Dauberman Road connection to US 30 with a jug-handle configuration within the northeast or northwest quadrant. Widening for channelization improvements would be required along US 30 with this configuration. The

County's limits would extend with the tapers (as needed) to tie into the existing cross section. The County anticipates avoiding significant impacts along US 30 such as Welch Creek. The Dauberman Road improvements would be designed to accommodate any future widening for US 30 that was identified through IDOT's Phase 1 studies (most likely a 4 or 5-lane section).

John Baczek explained that regional traffic from the Dauberman Road extension and the potential US 30 impacts would need to be reviewed by IDOT. All were in agreement; however, all IDOT coordination issues and items would be channeled through the Bureau of Local Roads & Streets.

IDOT and KDOT staff agreed with keeping the projects separate and the request would be presented to the FHWA for concurrence at the upcoming coordination meeting.

The meeting ended at 2:30 pm.

Minutes prepared by TranSystems (B. Fairwood)

Attachments: Attendance Roster, Draft FHWA/IDOT Agenda, Exhibit 1 - Vicinity Map, Exhibit 2 - Aerial, Exhibit 3 – ADT map

ATTENDANCE ROSTER

BUREAU OF LOCAL ROADS AND STREETS

PROJECT: Kane County; Phase I, Dauberman Road: US 30 to Granart Road

DATE: 01/08/2016

TIME: 01:45 PM

LOCATION: Schaumburg / District One ROOM: Conference Room A

	NAME (Please Print)	REPRESENTING	PHONE NUMBER
1	Marilin Solomon <i>MS</i>	IDOT - D1 - BLRS	(847) 705-4407 Marilyn.Solomon@illinois.gov
2	<i>Brian Carlson</i>	<i>IDOT - Proj Dev</i>	<i>(847) 705-4080</i>
3	<i>Brian Fairwood</i>	<i>TransSystems</i>	<i>(847) 407-5280</i>
4	<i>Grace Dysico</i>	<i>TransSystems</i>	<i>(847) 407-5247</i>
5	<i>John Baczek</i>	<i>IDOT PROGRAMMING</i>	<i>(847) 705 4104</i>
6	<i>Bonnie Flock</i>	<i>7 KDOT (called in)</i>	()
7	<i>Mike Zakosek</i>		()
8	<i>Jennifer Becker</i>		()
9			()
10			()
11			()
12			()
13			()
14			()

**Dauberman Road Extension
Kane County DOT
Section No. 15-00277-01-BR**

**FHWA/IDOT Initial Meeting
January 12, 2016**

Minutes to Meeting

This was the first presentation of the project. The purpose of the meeting was to provide a project introduction, review the proposed scope of work, and determine the project study logical termini.

1. KDOT Dauberman Road Extension Project Overview

- a. The project sponsor is the Kane County Division of Transportation (KDOT).
- b. KDOT has selected the TranSystems team as the Phase 1 consultant for their project. The engineering agreement documents have been submitted to the County for review and approval. The Notice to Proceed (NTP) has not yet been issued. Transportation Committee approval is anticipated in February, with Board approval and NTP in early March.
- c. The KDOT project is the extension of Dauberman Road from where it currently terminates at US 30, south to Granart Road with a grade separation structure over the BNSF Railroad, a distance of approximately 0.6 miles.
- d. Existing Dauberman Road is a county route. It has 2 travel lanes with aggregate shoulders. Existing US 30 is a state route with 2 travel lanes with combination paved/aggregate shoulders.
- e. The Dauberman roadway extension is included in the Kane County 20-year Long Range Transportation Plans. It was also identified in the Prairie Parkway Study Alternatives Analyses as a grade separation over the railroad.
- f. The area is generally agricultural with the Village of Big Rock to the west and the Village of Sugar Grove to the east.
- g. The intent of the project is to construct a grade-separated crossing of the BNSF Railroad to provide uninterrupted access for emergency responders, and local and regional users.
- h. The nearest existing grade separations over the railroad from Dauberman Road are IL 47 (approximately 4.5 miles east) and US 30 (approximately 20 miles west). The adjacent intersections of US 30 with Dugan Road and with Rhodes Street are both at-grade crossings, to the east and west, respectively.
- i. The project will include a connection between Dauberman Road and US 30. It is likely to be in the form of a jug handle or other intersection. The southern terminus will be Granart Road, with some type of intersection.
- j. The type of intersection at US 30 will be determined once the traffic counts and projected traffic volumes have been made. The traffic counts from the IDOT US 30/Dugan IDS are inconsistent with the ADTs shown on the IDOT web site.
- k. KDOT has an agreement with the ICC for partial construction funding.

2. IDOT US 30 Project Overview

- a. IDOT is currently conducting a Phase 1 Study along US 30, its limits are from west of IL 47 to west of Dugan. IDOT is about 3 years into the Phase 1 Study. An abbreviated Environmental Assessment (EA) is being prepared.
 - i. IDOT is conducting a full CSS approach and has formed a Community Advisory Group (CAG) for the project.
 - ii. Public meetings have been conducted.
- b. IDOT is proposing a 5-lane cross section along US 30 providing for 2 lanes in each direction with a 30-foot median to allow for channelization at the intersections.
 - i. The project includes intersection improvements at its terminus at Dugan Road.
 - ii. Interim improvements at the Dugan Road intersection have been recently constructed by IDOT in 2015.
- c. Coordination with the FAA is ongoing with regards to proposed right-of-way (ROW) required from the Aurora Municipal Airport.
- d. Currently IDOT does not have funding identified for Phase 2, ROW nor construction for their project.

3. Potential for combining projects along US 30 and Dauberman Road

- a. So as not to leave a gap (approx. 2 miles long) on US 30 between Dugan Road and Dauberman Road, IDOT had asked KDOT to considering including it in their project.
- b. KDOT desires to progress from Phase 1 right into Phase 2, and through construction with the Dauberman Road extension. KDOT considered IDOT's request but only as a separate project along US 30.
- c. IDOT decided to extend the current study limits of their ongoing EA, west along US 30 to meet the new Dauberman Road extension. This would allow the KDOT Dauberman Road project to move forward independently of the IDOT US 30 project. The FHWA and IDOT Bureau of Local Road and Streets (BLRS) concurred this was the best way to proceed.

4. Project Scoping

- a. The County's Dauberman Road project is a grade separation (safety need) and roadway extension (connectivity need) improvement. The IDOT US 30 project is an add-lanes (capacity need) improvement along US 30. Each project has independent utility and will provide both regional and local benefits.
- b. Presuming the Dauberman Road project is ahead of the US 30 improvements, the Dauberman Road connection will need to consider matching the existing 2-lane roadway and the future widened roadway of US 30.

- c. Due to the grade separation over the BNSF and the short distance between the railroad and US 30, it is likely that US 30 will also be grade separated. The bridge opening will be made wide enough to accommodate the future US 30 roadway.
- d. The FHWA approved and IDOT BLRS concurred that the environmental study limits for the proposed Dauberman Road extension will be:
 - i. East terminus along US 30: Abutting the IDOT US 30 EA project, but on an interim basis, as needed to match into the existing roadway.
 - ii. West terminus along US 30: US 30 at Rhodes Street intersection
 - iii. North terminus along Dauberman Road: Dauberman Road at Wheeler Road intersection
 - iv. East Terminus along Granart Road: Granart Road at Camp Dean Road intersection
 - v. West Terminus along Granart Road: Granart Road at Rhodes Street intersection
- e. An Alternatives Analysis (AA) will be completed as part of the Phase 1 studies. The AA will include the grade separation over US 30 and the railroad, the connection to US 30, and the connection to Granart Road.
- f. The ESR form has not yet been submitted. However, after the NTP has been issued to TranSystems, it will be completed.

5. Dauberman Road Environmental Processing

- a. As a new roadway extension, it is anticipated that the KDOT Dauberman Road Phase 1 project will be an Abbreviated EA. FHWA and IDOT BLRS concurred with the environmental level of processing.
- b. The project coordination will be through IDOT BLRS. BLRS will coordinate with the Bureau of Programming for items involving US 30.
- c. Due to the level of the environmental documentation TranSystems asked if following the NEPA/404 merger meeting process will be likely. Since the NEPA/404 merger process relates to the 404 permitting, and since there are likely very small involvements/impacts relating to the 404 permit, the merger meeting process may not be needed. A final determination will be made when environmental studies are underway and more information is available.

The meeting ended at 11:50am.

Minutes prepared by TranSystems (M. Dever)

Attachments: Attendance Roster, Exhibit 1 - Vicinity Map, Exhibit 2 - Aerial, Exhibit 3 – ADT map

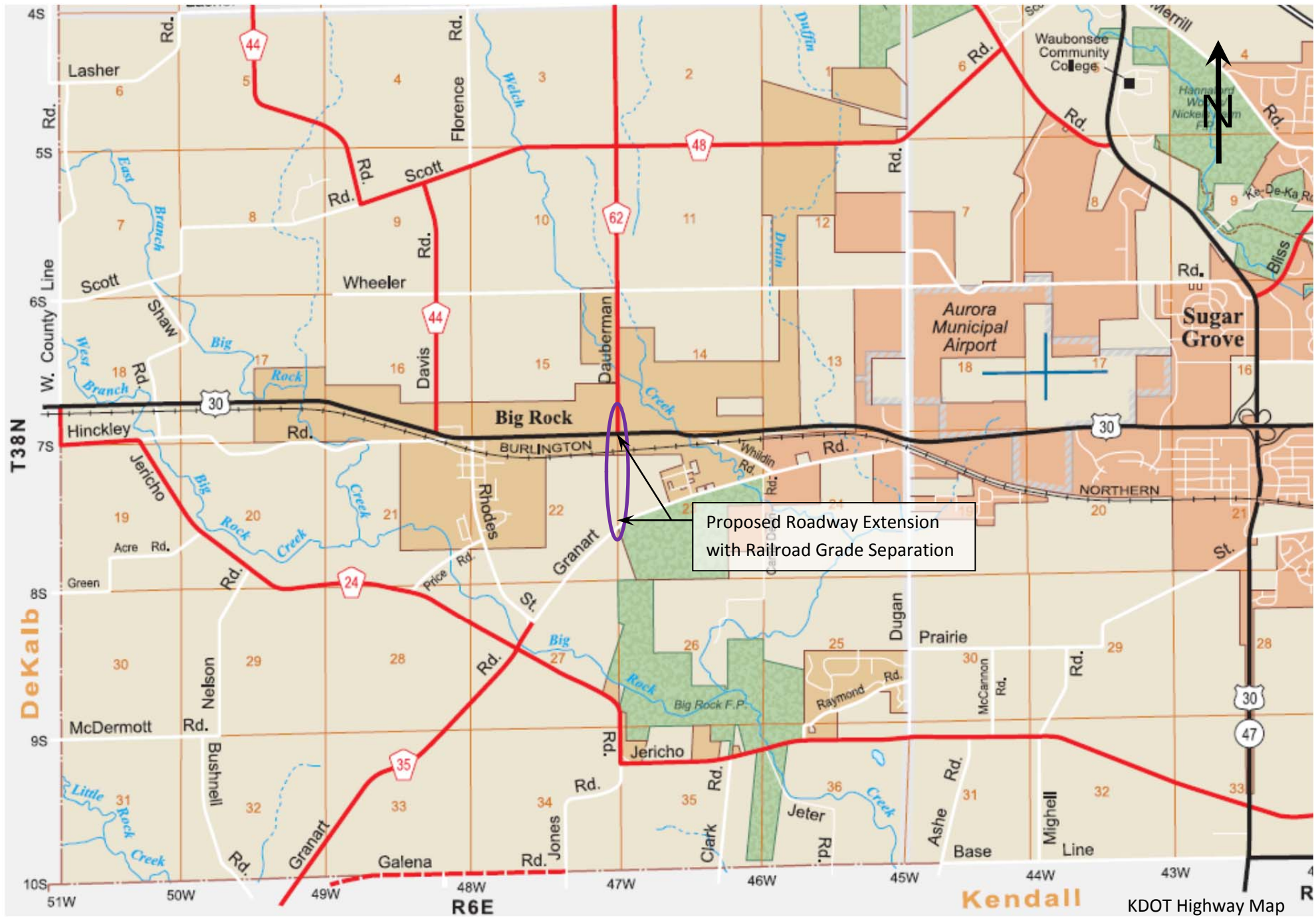


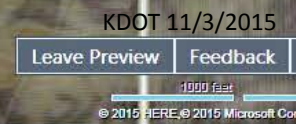
Illinois Department of Transportation

FHWA/Local Coordination Meeting Attendance Roster

Agency Name:	Kane County		
Project & Topic:	Dauberman Rd.-US 30 to Granart Road: New Rdwy Grade Separation;Project intro, scope, termini, env. processing, coord. w/ IDOT/Programming Phase I study US 30 from Dugan to IL 47		
Section No.:	15-00277-01-BR		
Date:	January 12, 2016	Time:	10:30 AM
Location:	Region/District One	Room:	Executive Conference Room
	NAME (Please Print)	REPRESENTING	PHONE NUMBER
1	Chris Holt <i>CH</i>	IDOT BLRS	(847) 705-4201
2	Hassan Dastgir	FHWA	(217) 492-4283
3	Chris Byars <i>WED</i>	FHWA	(312) 886-1606
4	Robin Helmerichs <i>DBA</i>	FHWA	(217) 492-4615
5	Gary Galecki <i>GG</i>	IDOT CBLRS	(217) 785-8564
6	John Sherrill	IDOT CD&E	(217) 785-4181
7	Ken Runkle	IDOT CD&E	(217) 785-0202
8	Scott McGuire	FHWA	217-492-4613
9	DWAYNE FERGUSON	IDOT-CBDE	217.785.2929
10	MIKE ZAKOSK	IDOT	630-584-1170
11	Jennifer Becker	IDOT	630-444-2957
12	Bonnie Flock	IDOT	630-584-1170
13	Kimberly Murphy	IDOT-Programming	847-705-4791
14	Brian Fairwood	TransSystems	847 407 5280
15	Grace Dysico	TransSystems	847 407 5247
16	MICHAEL DEVER	TRANSYSTENS	847-407-5351
17	MARILYN SOLOMON	IDOT-DA - BLRS	847-705-4407
18	SULESMAN TULGAR	" " " (HAR, INC.)	847 705 4205
19			
20			
21			
22			
23			
24			

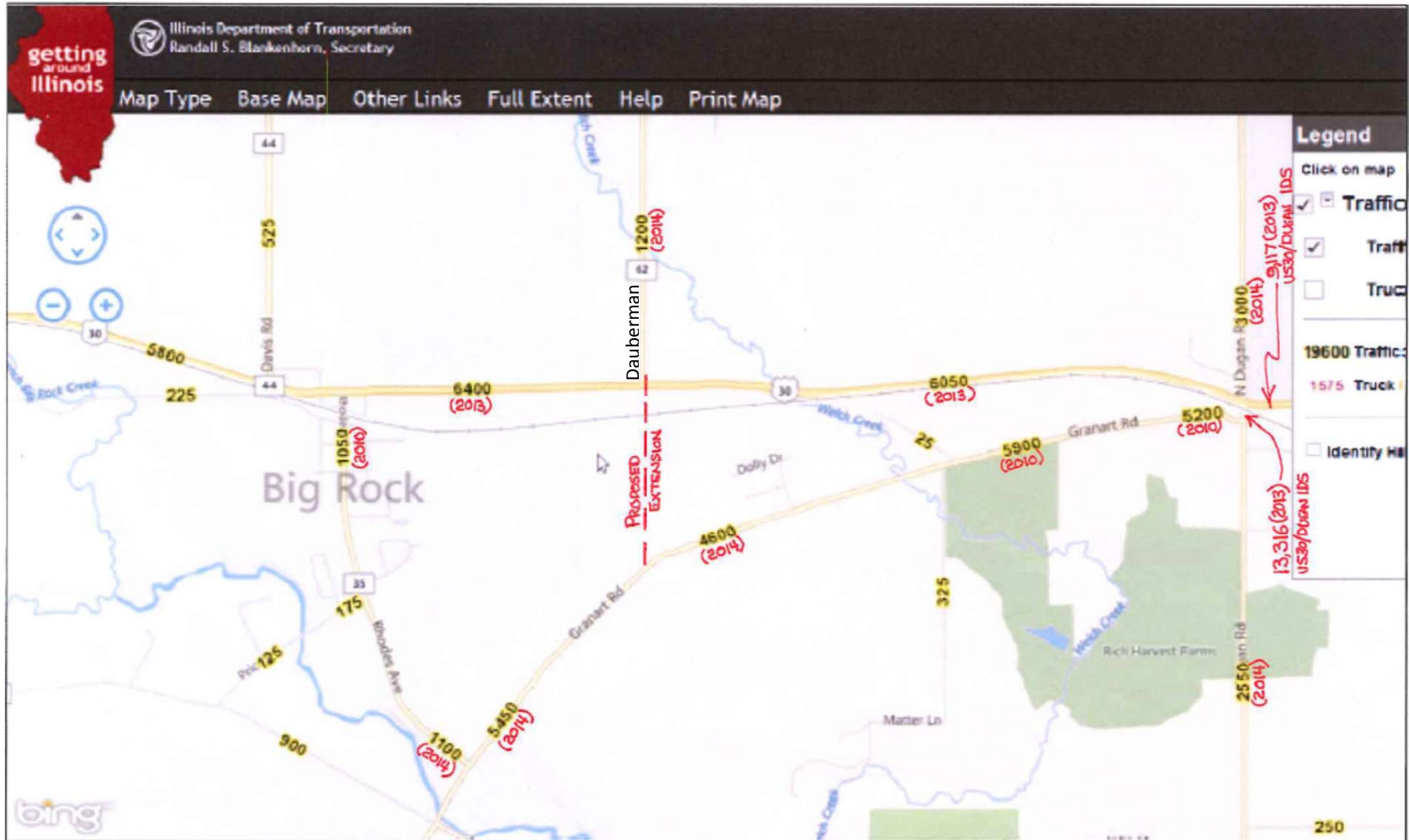
Dauberman Road Extension - Location Map





ADT from IDOT Web Site

08/17/2015



IDOT/FHWA Coordination Meeting

Dauberman Road Extension
Kane County DOT
Section No. 15-00277-01-BR

Kane County Division of Transportation
9:00 A.M.

March 14, 2017

The project was initially presented on January 12, 2016. This is the 2nd presentation of the project. The purpose of this presentation was to reintroduce the project, discuss the current project status, introduce the draft Purpose and Need, present the Environmental Assessment (EA) Timeframe schedule, and describe the next steps in the project.

The following points were discussed:

1) Project Overview

- a) The project sponsor is the Kane County Division of Transportation (KDOT).
- b) The KDOT project is the proposed extension of Dauberman Road from where it currently terminates at US Route 30, south to Granart Road with a grade separation at the BNSF Railroad (BNSF RR), a distance of approximately 0.6 miles.
- c) The project is located in the southwest area of Kane County between the villages of Big Rock and Sugar Grove, west of Aurora.
- d) The Aurora Airport is two miles east of the project and a private grass landing strip is adjacent to the project. The majority of the adjacent area is farm land, with pockets of residential developments and denser residential and commercial uses within the town proper of each municipality.
- e) The project was previously approved to be processed as an EA.
- f) The intent is to curve Granart Road from the west into the extended Dauberman Road, creating a new continuous north-south route connection.
- g) The remaining section of Granart Road east of Dauberman Road would remain and connect to Dauberman Road, likely at a T-intersection.
- h) IDOT is studying the section of US Route 30 from IL Route 47 to just west of Welch Creek and completing an EA. The IDOT project is adjacent to this project at the east limits. KDOT and IDOT are coordinating their efforts.

2) Public Meeting #1 (Introductory and Scoping)

- a) A public meeting was held at the Hinckley-Big Rock Middle School on February 9, 2017 from 5:00 PM to 7:30 PM.
- b) The purpose of the meeting was to introduce the project to the public, gather feedback, and refine the project scope. Only existing conditions were presented. There were no alternatives or proposed information shown.
- c) The meeting was publicized in local papers and on the internet through Big Rock's social media forums.
- d) The meeting was an open house format. Two presentations were made to the public during the meeting.
- e) One hundred people attended the meeting, and 11 comments were made via comment form or email.
- f) Six comments supported the project, one opposed it, and four expressed concerns.

3) Draft Purpose and Need

- a) The draft Purpose and Need was provided in advance to the FHWA and IDOT for their concurrent reviews and comment.
- a) Welch Creek was requested to be labeled on the location maps in the embedded figures of the draft Purpose and Need.
- b) The Purpose of the project is to add a new grade-separated crossing of the BNSF RR between the villages of Sugar Grove and Big Rock.
- c) The Needs of the project are:
 - To improve safety by providing a grade-separated alternative to crossing the railroad tracks at-grade and providing emergency responders located south of the BNSF tracks safe, dependable access across the railroad to respond to calls north of the tracks, and travel to nearby hospitals;
 - To improve mobility by providing an unimpeded route across the railroad tracks and connection to US Route 30; and
 - To improve system linkage and connectivity by providing a connection serving the predominant regional travel pattern (southwest to northeast) which includes access over major divides in Kane County, such as Interstate 88 and the BNSF RR.

4) Environmental Processing

- a) It was noted by FHWA that the project could be processed as a federal Categorical Exclusion (CE) instead of an Environmental Assessment (EA) if the proposed extension alignment could avoid or limit impacts to wetlands under one acre. This could simplify the project documentation and potentially shorten the schedule.
 - If the project was processed as a CE, a formal Alternative Analysis (AA) would not be required – eliminating the need to hold a second Public Meeting to formally present the alternatives considered and the results of the AA. Instead, after the Preferred Alternative was determined, it would be presented at a Public Hearing. The hearing format could be an open-house hybrid format which would include a court reporter and public comment period.
 - All property owners from which property would be acquired from for the project must be directly notified of and invited to the Public Hearing.
 - Chapter 21 of the Bureau of Local Roads and Streets (BLRS) Manual is undergoing an update. As part of this update, public hearings will require a certified transcript of the hearing be submitted to the FHWA. A sample submittal letter was provided to the consultant team.
- b) If the wetland impacts cannot be limited to an acre or less, the project would require an Individual 404 Permit. If this is the case, the US Army Corps and other regulatory agencies would likely require the project to follow the full NEPA/404 merger process, and the project would continue as an EA.
- c) Based on the discussions, IDOT Central Office BLRS and FHWA both concurred that the project should proceed as a federal CE, and continue with development of viable alternatives. Alternatives should be investigated to avoid and minimize impacts to the identified wetlands – keeping impacts below the one-acre threshold.
- a) After viable alternatives have been developed and a likely preferred alternative is selected which avoids or limits wetland impacts, the level of environmental processing as a federal CE can be confirmed with FHWA and IDOT via a conference call.

5) Environmental Clearances and Studies

- a) Environmental Survey Request (ESR) was submitted in April 2016
 - Cultural Clearance was received February 8, 2017

- IDOT PESA for US Route 30 was received October 24, 2016
 - The PESA for non-IDOT ROW and the Wetland Delineation Report were completed and submitted to IDOT for review September 2016 and January 2017, respectively.
- b) The following studies and coordination will be completed for the project:
- Noise Analysis – after the preferred alignment is determined
 - Wetland Impact Evaluation – after the preferred alignment is determined
 - FAA Coordination – after the preferred alignment is determined the online FAA screening will be completed to determine coordination requirements for both the Aurora Airport in Sugar Grove and J. Maddock Landing Strip near Dauberman Road and US Route 30. A commitment to complete Form 7460 during Phase 2 will be included in the CE document.
 - Agriculture – after the preferred alignment is determined coordination with the Department of Agriculture will be initiated and Form AD-1006 will be completed and submitted as appropriate.
- 6) EA Timeframe Schedule
- a) Portions of the EA Timeframe Schedule were reviewed. The sequence of activities was generally agreed to by IDOT and FHWA – in particular providing 30-day concurrent reviews and comment periods for FHWA and IDOT.
- b) As previously discussed, it was noted that if the project continues as a federal CE, the formal interim coordination meetings for the AA and the second public meeting would not be required.
- 7) Next Steps
- a) The consultant team will continue with development of viable alternatives, focusing on alignments that minimize or avoid wetland impacts. When it can be confirmed that a viable alignment and alternative can comply with the wetland impact threshold, a conference call with FHWA and IDOT will be scheduled to obtain concurrence on the environmental level of processing.
- b) IDOT and FHWA will review the draft Purpose and Need and provide comments.

The meeting concluded at 9:45 A.M.

Minutes prepared by TranSystems (B. Holman)

Attachments



Illinois Department of Transportation

FHWA/Local Coordination Meeting Attendance Roster

Agency Name:	Kane County		
Project & Topic:	Dauberman Rd. Extension (US 30 to Granart Rd.): New roadway, grade separation; update, public involvement, review timeframe schedule for EA		
Section No.:	15-00277-01-BR		
Date:	March 14, 2017	Time:	9:00 AM
Location:	Region/District One	Room:	Executive Conference Room

IDOT - DISTRICT ONE		
Chris Holt (847) 705-4201	Alex Househ (847) 705-4410	Zubair Haider (847) 705-4206
Marilyn Solomon (847) 705-4407	Kevin Stallworth (847) 705-4169	Gerardo Fierro (847) 705-4236
Michelle Davis-Byrd (847) 705-4795	Jim Skvarla (847) 705-4520	Moud Ahmad (847) 705-4409
Jason Salley (847) 705-4085	Hannah Knight (847) 705-4205	Joy Gustafson (847) 705-4296
Peter Stresino (847) 705-4135	Temi Latinwo (847) 705-4179	Jose Rios (847) 705-4118
IDOT - CENTRAL OFFICE		
John Sherrill (217) 785-4181	Ken Runkle (217) 785-0202	Bill Raffensperger (217) 785-1676
Scott Stitt (217) 785-0721	Maureen Kastl (217) 342-8321	
FHWA		
Hassan Dastgir (217) 492-4623	Chris Byars (312) 886-1606	Robin Helmerichs (217) 492-4615
Omar Qudus (217) 492-4634		

OTHER PARTICIPANTS	REPRESENTING	PHONE
1 Anand Patel	Programming / D 1	847-705-4677
2 Theresa Pelletier	Programming / D 1	847-705-4479
3 MIKE ZAKOSKE	KANE COUNTY DOT	630-584-1170
4 Patrick Knapp	KKCOM	630-584-1170
5 Grace Dysico	TransSystems	847-407-5247
6 Brian Holman	TransSystems	847-407-5232
7 Eric Neubauer	TransSystems	
8 Bonnie Klock	KDOT	630-584-1170
9 Kimberly Mung	IDOT - Proj	847-705-4191
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IDOT/FHWA Coordination Meeting #3

Dauberman Road Extension
Kane County DOT
Section No. 15-00277-01-BR

Kane County Division of Transportation
9:30 A.M.

May 9, 2017

This is the 3rd presentation of the project. The project was previously presented March 14, 2017. The purpose of this presentation was to provide an update on project status, present alternative alignments to minimize wetland impacts, and gain concurrence on processing the project as a Federal Categorical Exclusion.

The following points were discussed:

- 1) Introduction and Recap
 - a) The project sponsor is the Kane County Division of Transportation (KDOT).
 - b) The KDOT project is the proposed extension of Dauberman Road from where it currently terminates at US Route 30, south to Granart Road with a grade separation at the BNSF Railroad (BNSF RR), a distance of approximately 0.6 miles.
 - c) The project is located in the southwest area of Kane County between the villages of Big Rock and Sugar Grove, west of Aurora.
 - d) The Aurora Airport is two miles east of the project and a private grass landing strip is adjacent to the project. The majority of the adjacent area is farm land, with pockets of residential developments and denser residential and commercial uses within the town proper of each municipality.
- 2) Final Purpose and Need
 - a) TranSystems has received and addressed the comments from the FHWA on the draft Purpose and Need presented at the last meeting.
 - b) The final Purpose and Need will be submitted after concurrence is received from the Chicago Metropolitan Agency for Planning (CMAP) on the 2040 Build traffic volumes – the volume information will be included in the Purpose and Need.
- 3) Alternatives Development
 - a) At the previous meeting, TranSystems was encouraged to review alternate alignments that would avoid or minimize impacts to the delineated wetlands near the southern limits of the project (just north of Granart Road). Several wetlands were delineated – all were of low to poor quality with FQIs ranging between 6.3 and 0. These wetlands were determined by the Corps to be non-jurisdiction and isolated. See discussion in item 4 below for more details.
 - b) Several alternate alignments were designed and reviewed with KDOT. The County decided that Option F, going west of the original corridor (Option A) to avoid the wetlands was preferred.
 - c) Other options that went west of the wetlands added more ROW and left remnant parcels to the project, or added additional geometric curvature (undesirable reverse curves).
 - d) Options that went east of the original corridor were not preferred by KDOT, as they had the potential of going thru other farmed wetlands, increased noise to the residential neighborhood, and introduced problems with a potentially historic barn structure to the east along Granart Road.

- e) The original north/south alignment (Option A), Option F, and a representative east side alignment (Option H) were presented to the FHWA and IDOT. The Pros and Cons of the various alternates were explained.
 - f) It was noted that Option A, the original alignment, was still the preferred alignment from a geometric standpoint and that it would be analyzed against Option F to determine if the geometric benefits justified the additional impacts to the wetlands. FHWA concurred with this approach.
 - g) The crossings of US Route 30 and the BNSF railroad tracks were briefly discussed. In response to questioning, it was explained that the anticipated design is two bridges on Dauberman Road, one each spanning over US Route 30 and the BNSF right-of-ways, with a jug-handle connecting US Route 30 and Dauberman Road in the northeast quadrant of the intersection. However, it was noted that this information remained preliminary. It was explained that the analysis of alternatives was anticipated to be shown at the next presentation of the project in several months.
- 4) Environmental Processing
- a) At the previous meeting it was noted by FHWA that the project could be processed as a Federal Categorical Exclusion (CE) instead of an Environmental Assessment (EA) if the proposed extension alignment could avoid or limit impacts to wetlands under one acre. This could simplify the project documentation and potentially shorten the schedule.
 - b) TranSystems noted that on April 25, 2017 project representatives met with the Army Corps of Engineers for a Jurisdictional Determination at the project site.
 - c) The Corps determined that the delineated wetlands were non-jurisdictional, and that any farmed wetlands in the alternative corridors later delineated would also be non-jurisdictional. The Corps provided a verbal determination, with the official letter of determination anticipated in late May, 2017.
 - d) With the wetlands being non-jurisdictional, the FHWA agreed that the project would be processed as a Federal CE, subject to final results of the Environmental Survey Request. BLRS form 22210 will be used for the project report documentation. Both FHWA and IDOT concurred on this decision.
 - e) The FHWA noted with the project as a Federal CE, that a second Public Meeting would not be needed, and that a Public Hearing showing the preferred alignment at the conclusion of the process would satisfy the remaining public outreach. See the minutes for the March 14, 2017 FHWA/IDOT Coordination meeting for a discussion of the first Public Meeting.
 - f) As part of the Public Hearing, the local agency in charge of a project is required to certify to the FHWA that the public hearing was held, the results of that meeting, and the transcript of the meeting. A draft submittal letter of the certification listing all the items to be included in the transcript was provided to TranSystems. This submittal letter will be submitted by KDOT to IDOT to initiate the certification process with the FHWA. FHWA indicated that the transcript items can be provided on a CD or USB flash drive.
 - g) It was emphasized that Section 4(f) impacts to the Kane County Forest Preserve should be avoided or minimized if possible. TranSystems indicated that there will be no involvement with the forest preserve properties located south of Granart Road.
 - h) FHWA observed that the logical termini of the project remained unchanged and were still acceptable. Both FHWA and IDOT concurred on this decision.
 - i) In response to an FHWA request at the previous meeting TranSystems had run an online FAA screening tool for projects near airports. The database did not include the private landing strip along Dauberman Road, but indicated that the Aurora Airport was close enough to require coordination during Phase 2.
- 5) Next Steps
- a) TranSystems will finalize coordination of the 2040 Build traffic with CMAP and submit the final Purpose and Need. A copy of the 2040 Build traffic will be provided to Programming/Project Studies.

- b) Following the 2040 Build traffic finalization, number of lanes will be determined and Intersection Design Studies will be prepared. Reviews with the Bureau of Programming/Project Studies will be coordinated through the Bureau of Local Roads and Streets.
- c) At the next FHWA/IDOT Coordination meeting, the following are anticipated to be presented:
 - Profile Alternatives
 - Preliminary design of US Route 30 / Dauberman Road connection
 - Recommended Alignment
- d) FHWA requested that a preliminary cost estimate be available at the next meeting.
- e) In response to an IDOT request, TranSystems will coordinate the project with the IDOT Bureau of Aeronautics, primarily because of the private runway along Dauberman Road within the project limits.

The meeting concluded at 10:15 A.M.

Minutes prepared by TranSystems (B. Holman)

Attachments:

1. *Attendance Sheet*
2. *Exhibits*



Illinois Department of Transportation

FHWA/Local Coordination Meeting Attendance Roster

Agency Name:	Kane County		
Project & Topic:	Dauberman Rd. Extension: New Roadway, Grade Separation; project status update, review alternatives, request comments on purpose/need, request concurrence on project processing level chg from EA to CE		
Section No.:	15-00277-01-BR		
Date:	May 9, 2017	Time:	9:30 AM
Location:	Region/District One	Room:	Executive Conference Room

IDOT - DISTRICT ONE		
Chris Holt (847) 705-4201	Alex Househ (847) 705-4410	Zubair Haider (847) 705-4206
Marilyn Solomon (847) 705-4407	Kevin Stallworth (847) 705-4169	Gerardo Fierro (847) 705-4236
Michelle Davis-Byrd (847) 705-4795	Jim Skvarla (847) 705-4520	Moud Ahmad (847) 705-4409
Jason Salley (847) 705-4085	Hannah Knight (847) 705-4205	Joy Gustafson (847) 705-4206
Carmen E. Ramos (847) 705-4219	Jennifer Williams (847) 705-4229	David Herman (847) 705-4487
Peter Stresino (847) 705-4135	Temi Latinwo (847) 705-4179	Jose Rios (847) 705-4118
IDOT - CENTRAL OFFICE		
John Sherrill (217) 785-4181	Ken Runkle (217) 785-0202	Bill Raffensperger (217) 785-1676
Scott Stitt (217) 785-0721	Maureen Kastl (217) 342-8321	
FHWA		
Hassan Dastgir (217) 492-4623	Chris Byars (312) 886-1606	Robin Helmerichs (217) 492-4615
Omar Qudus (217) 492-4634		

OTHER PARTICIPANTS		REPRESENTING	PHONE
1	Kimberly Murphy	IDOT - SI Programming	847-705-4791
2	Scott Czaplinski	IDOT/Programming-Consultant	(847) 705-4678
3	Theresa Pelletier	IDOT/Programming Consultant	847-705-4479
4	MIKE ZAKOSKE	KANE COUNTY DOT	630-584-1170
5	Grace Dysico	TransSystems	847-605-9600
6	Brian Holman	TransSystems	847-605-9600
7	Eric Neubauer	TransSystems	(312) 669-5834
8	Jackie Forbes	KKCOM	630-444-3142
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IDOT/FHWA Coordination Meeting #4

Dauberman Road Extension
Kane County DOT
Section No. 15-00277-01-BR

Kane County Division of Transportation
1:15 P.M.

July 11, 2017

This is the 4th presentation of the project. The project was previously presented May 9, 2017. The purpose of this presentation was to provide an update on project status and gain concurrence on the following: the profile over the BNSF railroad tracks, the proposed alignment, and the concept and location of the connector between Dauberman Road and US Route 30 in the proposed condition.

The following points were discussed:

- 1) Introduction and Recap
 - a) The project sponsor is the Kane County Division of Transportation (KDOT). The project is the proposed extension of Dauberman Road from where it currently terminates at US Route 30, south to Granart Road with a grade separation at the BNSF Railroad (BNSF RR), a distance of approximately 0.6 miles.
 - b) The project is located in the southwest area of Kane County between the villages of Big Rock and Sugar Grove, west of Aurora.
 - c) At the previous meeting the final Purpose and Need was approved, the preliminary alternatives analyses avoiding the wetlands was presented and the project was approved to be processed as a Federal CE.
- 2) BNSF Crossing Recommendation
 - a) TranSystems explained the recommended crossing of the BNSF tracks was to elevate Dauberman Road on a bridge over the railroad tracks. This also requires that Dauberman Road be elevated on a bridge over US Route 30. The distance between the railroad tracks and US Route 30 is only 520 feet. It is insufficient to provide the required profile grade from the bridge over the railroad down to existing ground level without incurring major impacts to the US Route 30 alignment or needing a much higher and longer bridge over the railroad.
 - b) Other options reviewed but not considered further included an underpass, an at-grade crossing, and realigning US Route 30 to the north to allow enough room to bring Dauberman Road back down to existing grade.
 - c) The underpass was not selected as the preferred option because of additional impacts to the railroad and its associated costs, including the necessity for a pump station for drainage. The at-grade crossing option was not selected because it did not meet the Project Purpose and Need, which includes enhancing safety by providing an unobstructed route across the BNSF tracks for emergency responders. Realigning US Route 30 to the north was not selected because it was roughly cost-equivalent to bridging Dauberman over US Route 30, and would result in poor geometrics moving US Route 30 north by 300 to 500 feet via two sets of reverse curves. IDOT Geometrics indicated that this would not be acceptable to them.
 - d) IDOT Central Office BLRS approved the recommendation to bridge Dauberman Road over US Route 30 and the BNSF railroad tracks. The FHWA concurred.

3) Dauberman Road Alignment Recommendation

- a) TranSystems recapped the history of the alignment development for Dauberman Road.
- b) At the 2nd FHWA/IDOT Coordination Meeting on March 14, 2017, TranSystems was encouraged to review alternate alignments to avoid or minimize impacts to the delineated farmed wetlands near the southern limits of the project (just north of Granart Road). Several wetlands were delineated – all were of low to poor quality with FQIs ranging between 0 and 6.3. These wetlands were previously determined to be non-jurisdiction and isolated by the US Army Corps of Engineers during a site review.
- c) A technical memorandum was prepared by TranSystems reviewing eight different alignment options. Copies of the memo were submitted to IDOT District 1 BLRS for review and comments.
- d) The recommendation of the memo was Option A, the originally proposed alignment. Option A provides the most straight forward tangent alignment and the required curve to tie into Granart Road at the south terminus. Additional refinements will be made to contain the proposed improvement footprint to minimize impacts to the wetlands, surrounding properties, and connecting roadways.
- e) The other alternatives were considered and dismissed for several reasons, including:
 - identified fatal flaws – impacts to Section 4(f) properties (forest preserves and Girl Scout Camp) and did not meet the Project Purpose and Need
 - safety concerns introduced by additional curves and reverse curves
 - skewed bridge crossings, requiring more right-of-way, leaving land-locked remnant parcels, additional costs from longer alignment lengths, and potential noise and visual impacts to the nearby residential land uses near Mary Drive and Granart Road
- f) Moving forward, it was discussed that the proposed cross section for Dauberman Road would be a rural cross section. IDOT Central Office BLRS approved this recommendation and the FHWA concurred.
- g) It was noted that the wetlands impacts will likely be mitigated at a wetland bank in accordance with the Kane County Stormwater Ordinance.
- h) IDOT Central Office BLRS approved the alignment recommendation of Option A. FHWA concurred; they agreed that for a new roadway facility, safety is paramount over avoidance of very low quality wetlands.

4) Dauberman Road/US Route 30 Connector Recommendation

- a) With Dauberman Road proposed to be elevated over US Route 30, a connection will need to be reestablished between these roads.
- b) TranSystems presented a technical memorandum discussing the options that were reviewed to connect the two roadways. Copies of the memo were submitted to IDOT District 1 BLRS for review and comments. The memo recommends a jug-handle be constructed in the northeast quadrant of the Dauberman Road/US Route 30 intersection.
- c) Of the four quadrants, the northeast quadrant was most viable. The two southern quadrant, located between US Route 30 and the railroad did not have enough room to accommodate the maximum allowable slopes for the jug-handle. The northwest quadrant was not as viable as the northeast for the following:
 - there were potential noise and visual resource impacts to the office building complex directly west
 - several driveways along US Route 30 would be impacted by the work along the route
 - a 4-acre remnant parcel would result
 - traffic movements for the predominant travel pattern was less equitable
- d) Higher level interchange configurations were considered, but the proposed 2040 Build traffic volumes did not support the need nor expense for it. See item 5 below for additional discussions of the 2040 volumes.

- e) Alternatives for a raised intersection were reviewed but were dismissed due to significant impacts including land acquisitions, business relocations, re-routing of US Route 30, and Welch Creek.
- f) IDOT BLRS Central Office approved and FHWA concurred that a jug-handle was an appropriate choice for the connection, and that the northeast quadrant was the most appropriate location for it.
- g) TranSystems noted that preliminary assumptions show both ends of the jug-handle as signalized intersections, but signal warrants analyses were still to be completed. IDOT and FHWA indicated that US Route 30 was not a Strategic Regional Arterial (SRA) in this area.
- h) IDOT Geometrics inquired if a roundabout had been considered for connecting the jug-handle to Dauberman Road and/or US Route 30. TranSystems noted it had not, but that it could be considered as part of the design – additional coordination with KDOT would be made.

5) Other Items

- a) TranSystems presented the 2040 Build Average Daily Traffic (ADT) and Peak Hour Volume as coordinated with CMAP. A hard copy of the CMAP concurrence letter and volume diagrams was provided to the IDOT Project Studies Project Manager of the IDOT US Route 30 project east of the Dauberman extension project.
- b) FHWA noted that a cost estimate should be provided in the future to determine if the project cost will require a Value Engineering (VE) study. The threshold for requiring a VE study \$40 million for non-bridge projects and \$50 million for bridge projects.

6) Next Steps

- a) FHWA confirmed that the project will still be processed as a Federal Categorical Exclusion.
- b) TranSystems will coordinate with IDOT regarding the traffic and design of the jug-handle connection, prior to starting Intersection Design Studies.
- c) TranSystems will prepare the plan and profile of the roadway, prior to beginning coordination with other agencies. These agencies will include the NRCS, the Illinois Department of Agriculture, the FAA, IDOT Bureau of Aviation, and the ICC.
- d) IDOT BLRS Central Office indicated that when the WIE Form is submitted, the name of the proposed wetland bank should be listed.

The meeting concluded at 1:45 P.M.

Minutes prepared by TranSystems (B. Holman)

Attachments:

- 1. *Attendance Sheet*
- 2. *Exhibits*



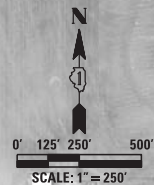
Illinois Department of Transportation

FHWA/Local Coordination Meeting Attendance Roster

Agency Name:	Kane County		
Project & Topic:	Dauberman Rd. Extension (US 30 to Granart Rd.); New Roadway grade separation; Status update, geometric analysis, alignment analysis for options A & F		
Section No.:	15-00277-01-BR		
Date:	July 11, 2017	Time:	1:15 PM
Location:	Region/District One	Room:	Executive Conference Room

IDOT - DISTRICT ONE			
Chris Holt (847) 705-4201	CH	Alex Househ (847) 705-4410	Zubair Haider (847) 705-4206
Marilyn Solomon (847) 705-4407	MS	Kevin Stallworth (847) 705-4169	Gerardo Fierro (847) 705-4236
Michelle Davis-Byrd (847) 705-4795		Jim Skvarla (847) 705-4520	Moud Ahmad (847) 705-4409
Jason Salley (847) 705-4085	JS	Hannah Knight (847) 705-4205	Joy Gustafson (847) 705-4334
Carmen E. Ramos (847) 705-4219		Jennifer Williams (847) 705-4229	David Herman (847) 705-4487
Peter Stresino (847) 705-4135		Temi Latinwo (847) 705-4179	Jose Rios (847) 705-4118
IDOT - CENTRAL OFFICE			
John Sherrill (217) 785-4181	JSS	Ken Runkle (217) 785-0202	Bill Raffensperger (217) 785-1676
Scott Stitt (217) 785-0721		Maureen Kastl (217) 342-8321	
FHWA			
Hassan Dastgir (217) 492-4623		Chris Byars (312) 886-1606	Robin Helmerichs (217) 492-4615
Omar Qudus (217) 492-4634	OQ		

OTHER PARTICIPANTS		REPRESENTING	PHONE
1	Theresa Pelle ties	IDOT Programming Cons Mgr	847 705 4479
2	Matt Smith	Tran Systems	847 407 5300
3	Grace Dysico	Tran Systems	
4	Brian Holman	Tran System	
5	Ryan Bigbie	KKCOM	630 444 3143
6	Chadi Thomas	KDOT	
7	MIKE ZAKOSKE	KDOT	630-584-1170
8	Brian Holman	Transystems	847 407 5300
9	Grace Dysico	Transystems	847 407 5300
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ALIGNMENT COMPARISON
REALIGNMENT OF DAUBERMAN ROAD AND GRANART ROAD TO
REDUCE WETLAND IMPACTS TO UNDER 1 ACRE

- OPTION A: ORIGINAL ALIGNMENT
- OPTION B: CURVE DAUBERMAN TO EAST
- OPTION C: SHIFT DAUBERMAN ALIGNMENT TO WEST & GRANART TO SOUTH
- OPTION D: SHIFT DAUBERMAN ALIGNMENT TO WEST WITH S-CURVE
- OPTION E: SHIFT DAUBERMAN ALIGNMENT TO WEST
- OPTION F: SHIFT DAUBERMAN ALIGNMENT TO WEST & CURVE NORTH OF 30
- OPTION G: SHIFT DAUBERMAN ALIGNMENT TO EAST & CURVE BETWEEN BNSF & 30
- OPTION H: SHIFT DAUBERMAN ALIGNMENT TO EAST & CURVE NORTH OF 30

FILE NAME: WFL04



DESIGNED - JLT
DRAWN - JLT
CHECKED - DBB
DATE - 03/24/2017

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DAUBERMAN ROAD
ALIGNMENT COMPARISON

SCALE: 1"=250'

SHEET ____ OF ____ SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		KANE		
CONTRACT NO.				

ILLINOIS FED. AID PROJECT

IDOT/FHWA Coordination Meeting #5

Dauberman Road Extension
Kane County DOT (KDOT)
Section No. 15-00277-01-BR

Kane County Division of Transportation
9:00 A.M.

December 5, 2017

This is the 5th presentation of the project. The project was previously presented July 11, 2017. The purpose of this presentation was to provide an update on the project, present the preferred alternative, and request approval for two design variances.

The following points were discussed:

1) Introduction and Recap

- a) At the previous meeting several alignment alternatives were presented to avoid some very low quality wetlands near the south end of the project limits. The wetland FQI's ranged from 0 to 7.0.
 - i) The alignment alternatives were able to reduce the wetland impacts but the geometrics of the original on-tangent alignment were still superior in its simplicity. Therefore, the original on-tangent alignment of the Dauberman Road extension was approved as the preferred alignment.
 - ii) The wetlands were determined to be non-jurisdictional by the US Army Corps of Engineers and therefore fall under the jurisdiction of the Kane County Stormwater Management Ordinance for permitting. *Subsequent to the meeting, additional coordination determined that the Illinois Wetland Policy act also has jurisdiction over the wetland impacts and mitigation requirements for this project.*
 - iii) IDOT Local Roads noted that in response to the WIE form submittal, the Natural Resources Review (NRR) should be completed sometime this month. There is the potential for 11.52 acres of wetland mitigation to be required. TranSystems indicated that wetland impacts are proposed to be mitigated by purchasing credits from a wetland bank. *Subsequent to the meeting, IDOT Local Roads provided clarification on the potential required mitigation of 11.52 acres. Dauberman Road, being on a new alignment, does not qualify as a programmatic review action. It meets the criteria for a standard review action with higher mitigation ratios. In addition, because of the total area of wetlands that are being adversely impacted, 3.52 acres, even higher mitigation ratios may apply. The NRR response will provide additional details.*
- b) 2040 Build traffic projections have been coordination with the Chicago Metropolitan Agency for Planning (CMAP) and were applied to the project development and design.

2) Preferred Alternative and Proposed Improvements

- a) The proposed improvements will extend Dauberman Road south from US Route 30 to Granart Road. The new north-south road will connect to Granart Road and continue west. Granart Road from the east will form a new T-intersection with the Dauberman Road extension.
- b) The project roadways will have a rural cross section with two 12' lanes and 8' shoulders.
 - i) Dauberman Road and Granart Road shoulders will be 4' paved and 4' aggregate.
 - ii) US Route 30 will be 8' paved shoulders.
- c) There are three proposed T-intersections within the project. Two of the intersections are formed by the new jughandle/Connector Road between Dauberman Road and US Route 30. The jughandle/Connector Road will be in the northeast quadrant of the existing intersection. The third intersection is the T-intersection formed by the Dauberman Road extension and Granart Road from the east.

- i) All intersections will be channelized with left and right turn lanes.
 - ii) A WB-65 design vehicle was used for the US Route 30/Connector Road intersection as US Route 30 is a Class II truck route. A WB-55 design vehicle was used for the Dauberman/Connector Road and the Dauberman Road extension/Granart intersections. Both intersections are under local jurisdiction and are not designated truck routes.
 - iii) The US Route 30/Connector Road intersection meets the Peak Hour warrant for 2020 opening day traffic and will be signalized. The other two intersections do not meet warrants and will be stop controlled on the side street. The through street will not stop.
 - d) Two bridges are proposed to span over US Route 30 and over the BNSF Railroad.
 - i) Dauberman Road will be on a bridge over US Route 30 because of insufficient distance to descend to grade between US Route 30 and the BNSF Railroad Tracks.
 - ii) The proposed bridge over the BNSF RR will span the 100' railroad right-of-way (ROW). No work is anticipated on the railroad tracks or within the railroad ROW. The proposed minimum vertical clearance over the railroad ROW is 23'-4" from the existing top-of-rail.
 - iii) The proposed span over US Route 30 will be long enough to allow for future widening of US Route 30 and a multi-use path along the north side of the roadway. The proposed minimum vertical clearance over US Route 30 is 16'-6".
 - e) A 10' wide multi-use path with 1' graded grass shoulders is proposed with the project.
 - i) The path will be along the east side of Dauberman Road, the northeast side of the Connector Road, and the south side of Granart Road. Along US Route 30, right-of-way will be acquired and a shelf graded for a 10' wide multi use path along the north side of the road, but no path will be constructed at this time due to the lack of a local sponsoring agency.
 - ii) Logical termini for the path is provided from US Route 30, at the north, to Big Rock Campground, at the south.
 - f) The proposed drainage concept was briefly described by TranSystems, explaining that the existing drainage patterns will be maintained, and that detention for the project will be provided in infield ponds and linear ditches and ponds along the new alignment. Existing drain tiles have been surveyed and accounted for, and coordination has been undertaken with local drainage agencies.
 - g) TranSystems noted that although a large amount of ROW will be acquired for the project, there are no residential or building relocations required.
 - h) TranSystems noted that although an 8' wide shoulder is proposed along the length of the US Route 30 reconstruction, an additional 2' of width will be added to the shoulder at the only mailbox along the project to accommodate mail delivery vehicles.
- 3) General Questions
- a) IDOT Local Roads inquired about existing and proposed street lighting on the project. TranSystems responded that the project is in a rural area with no street lighting, with the exception of one beacon light at the existing intersection of Dauberman Road and US Route 30. Accordingly, no lighting is proposed for the project. It was also noted that IDOT has confirmed that lighting underneath the proposed Dauberman Road bridge over US Route 30 will not be required.
 - b) IDOT Local Roads inquired if the Dauberman Road bridge over US Route 30 was checked for the required stopping sight distance (SSD) to the new traffic signal at the US Route 30/Connector Road intersection. TranSystems stated that they believed it did. *Subsequent to the meeting, it was confirmed that more than double the required 570' of SSD is provided.*
 - c) IDOT Local Roads asked how the proposed project would affect large events at the Rich Harvest Farms Golf Course located two miles east along Granart Road. TranSystems noted that any affect it had would be positive. The proposed project will improve and enhance the safety by providing a new grade-

separated crossing over the BNSF Railroad; improve mobility in the region by providing an additional north-south roadway in addition to Dugan Road; and enhance system linkage and connectivity of the transportation network in the County by providing a new connection between US Route 30 and Granart Road.

4) Granart Road and Big Rock Forest Preserve

- a) TranSystems reviewed the Granart Road section of the project. The roadway reconstruction would end once it had tied into the existing alignment, but the multi-use path running along the south side of the road would extend east approximately 3,200' past this point to its logical terminus at the Big Rock Forest Preserve entrance, providing a connection to the existing system of paths and trails within the forest preserve. The Forest Preserve currently has a designated mowed grass trail along the south side of Granart Road. TranSystems noted that because of ROW restrictions in this area, the path would be built on forest preserve property, essentially replacing the mowed grass trail.
- b) To avoid Section 4(f) and 6(f) concerns, KDOT has approached the Forest Preserve District of Kane County (FPDKC) on their willingness to become a non-financial project co-sponsor and allow the path to be built on Forest Preserve lands.
 - i) Jerry Culp, FPDKC Director of Planning and Development indicated that the FPDKC is willing to be a non-financial project co-sponsor and would sign the Project Development Report (PDR) cover sheet. IDOT Local Roads indicated that the FPDKC would also need to sign the Phase 2 design plans when completed.
 - ii) KDOT, in discussions with the FPDKC, indicated the path on forest preserve property would be constructed by KDOT with ownership and maintenance turned over to the FPDKC. However, details of the maintenance and ownership once the path is off the forest preserve property will be determined when the Inter-agency Governmental Agreement (IGA) is finalized.
 - iii) KDOT also coordinated with IDNR in regards to the Section 6(f)/OSLAD funds used to purchase property for the forest preserve. IDNR determined that construction of the path constituted outdoor recreation and will not violate the OSLAD requirements. The FPDKC will maintain ownership of the property.
- c) IDOT Local Roads indicated that this would be acceptable, although they requested that a Section 4(f) Temporary Occupancy Form be submitted for FHWA approval. Items to include with the form are:
 - i) KDOT Communication with FPDKC and IDNR
 - ii) Draft Intergovernmental Agreement (IGA) that would be finalized and executed sometime during Phase 2 design.

5) Project Development Report (PDR) Items

- a) TranSystems discussed the follow-up with the Federal Aviation Administration (FAA) online Part 77 Notice Criteria Tool. The tool noted the proposed project was "in proximity to a navigation facility, and may impact assurance of navigational signal reception." The online Part 77 Notice Criteria Tool was designed for a broad range of projects, and this result was likely in case the project in question would emit electromagnetic radiation or be of a height to interrupt navigational signals, such as a cell phone tower. The proposed Dauberman Road Extension is not such a project. At its highest point, the proposed project still maintains a minimum clearance of more than 120' to the nearest Part 77 surface.
 - i) The FAA does not regulate the flight paths for private airstrips such as the J. Maddock Airport. The project does not cause any interference with the Aurora Municipal Airport's Part 77 surfaces.
 - ii) IDOT Local Roads agreed it was unlikely that there would be any interference with the Aurora Municipal Airport (ARR) from the project, but re-emphasized the need to file with the FAA during Phase 2 to ensure that there were not any issues with construction equipment violating Part 77 surfaces.

- b) TranSystems presented the BLR Form 22120, Approval of Design Variance Request. Two variances were requested:
 - i) A 4' wide shoulder instead of an 8' wide shoulder on the west side of Dauberman Road from Station 667+50 to 672+70, a distance of approximately 570'. The variance is requested to avoid impacts to the use and operations of the J. Maddock Air Strip and to avoid property takes from the air strip.
 - ii) A 2:1 (H:V) side slope instead of 3:1 slopes used in two locations, 1) on the large embankments approaching the bridges over US Route 30 and the BNSF Railroad Tracks to minimize the proposed project foot print, and 2) near the J. Maddock Air Strip to avoid property takes from the air strip.
 - c) IDOT and FHWA approved the design variances with the following notes:
 - i) They requested that the length of narrow shoulder and 2:1 side slope along Dauberman Road in front of the J. Maddock Air strip be kept to the minimum length required to avoid the air strip itself, instead of stretching along the length of the property. TranSystems indicated this was feasible.
 - ii) It was noted that because guardrail is planned to shield the large embankments approaching both bridges, the 2:1 side slope on the approach to and between the US Route 30 and BNSF bridges is not considered a design variance.
 - d) TranSystems noted that the NRCS Form AD-1006 for coordination with the Illinois Department of Agriculture will be filled out and submitted in the near future.
 - e) TranSystems inquired what the correct response would be on the PDR form for the question about air quality Hot-Spot Analysis. IDOT Local Roads directed them to the Bureau of Design and Environment (BDE) Manual, Chapter 26 for additional information.
 - f) TranSystems noted the Noise Analysis Report was submitted on November 21, 2017. No noise abatement measures were found to be feasible and reasonable.
- 6) Project Document Submittals
- a) TranSystems reviewed the major submittals that have been made to IDOT for the project:
 - i) Wetland Impact Evaluation (WIE) forms submitted October 19, 2017, with minor updates submitted November 2, 2017.
 - ii) Intersection Design Study (IDS) for the US Route 30/Connector Road intersection submitted November 3, 2017.
 - iii) Draft Project Design Report (PDR) submitted December 1, 2017.
 - iv) Draft Location Drainage Study/Proposed Drainage Plan (LDS/PDP) submitted December 1, 2017.
 - v) Draft Type, Size, and Location (TS&Ls) and the BLR 10210, Preliminary Bridge Design and Hydraulic Report form submitted December 1, 2017 for both bridges.
- 7) Next Steps
- a) TranSystems noted that it was intended to take the project to a Public Hearing sometime early in 2018, probably in the February-March range.
 - b) IDOT and FHWA agreed with this plan. IDOT Local Roads provided an example of a transcript certification letter that will need to be provided to the FHWA after the hearing, and reminded the group that a court reporter will need to be present at the Hearing.
 - c) TranSystems finished by noting that the Final PDR will be submitted subsequent to the holding of the Public Hearing.

The meeting concluded at 9:45 A.M.

Minutes prepared by TranSystems (B. Holman)

Dauberman Road Extension
FHWA/IDOT Coordination Meeting #5
December 5, 2017
Page 5 of 5

Attachments:

1. *Attendance Sheet*



Illinois Department of Transportation

FHWA/Local Coordination Meeting Attendance Roster

Agency Name:	Kane County		
Project & Topic:	Dauberman Road Extension (US 30 to Granart Road) New Roadway Grade Separation; Project status update, preferred alternative, design variances, public hearing plans		
Section No.:	15-00277-01-BR		
Date:	December 5, 2017	Time:	9:00 AM
Location:	Region/District One	Room:	Executive Conference Room

IDOT - DISTRICT ONE			
Chris Holt (847) 705-4201	CH	Alex Househ (847) 705-4410	Zubair Haider (847) 705-4206
Marilyn Solomon (847) 705-4407	MS	Kevin Stallworth (847) 705-4169	Gerardo Fierro (847) 705-4236
Michelle Davis-Byrd (847) 705-4795		Jim Skvarla (847) 705-4520	Moud Ahmad (847) 705-4409
Jason Salley (847) 705-4085	JS	Hannah Knight (847) 705-4205	Joy Gustafson (847) 705-4334
Peter Stresino (847) 705-4135		Temi Latinwo (847) 705-4179	Jose Rios (847) 705-4118
		Jennifer Williams (847) 705-4229	David Herman (847) 705-4487

IDOT - CENTRAL OFFICE			
John Sherrill (217) 785-4181	JSS	Bill Raffensperger (217) 785-1676	Maureen Kastl (217) 342-8321
Scott Stitt (217) 785-0721		Dwayne Ferguson (217) 785-2929	DF

FHWA			
Hassan Dastgir (217) 492-4623	Ha	Chris Byars (312) 886-1606	WGP
Omar Qudus (217) 492-4634	OQ		

OTHER PARTICIPANTS		REPRESENTING	PHONE
1	Evelina Perry	IDOT GSU consultant	847 705 4085
2	Theresa Pelletier	IDOT Proj Studies consult	847.705.4479
3	Jackie Forbes	KDOT	630-444-3142
4	Candi Thomas	KDOT	
5	Grace Dysico	TransSystems	847-407-5247
6	Brian Holman	TransSystems	847 407-9600
7	BRIAN FAIRWOOD	TRANSYSTEMS	847.407.5280
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1 PURPOSE AND NEED

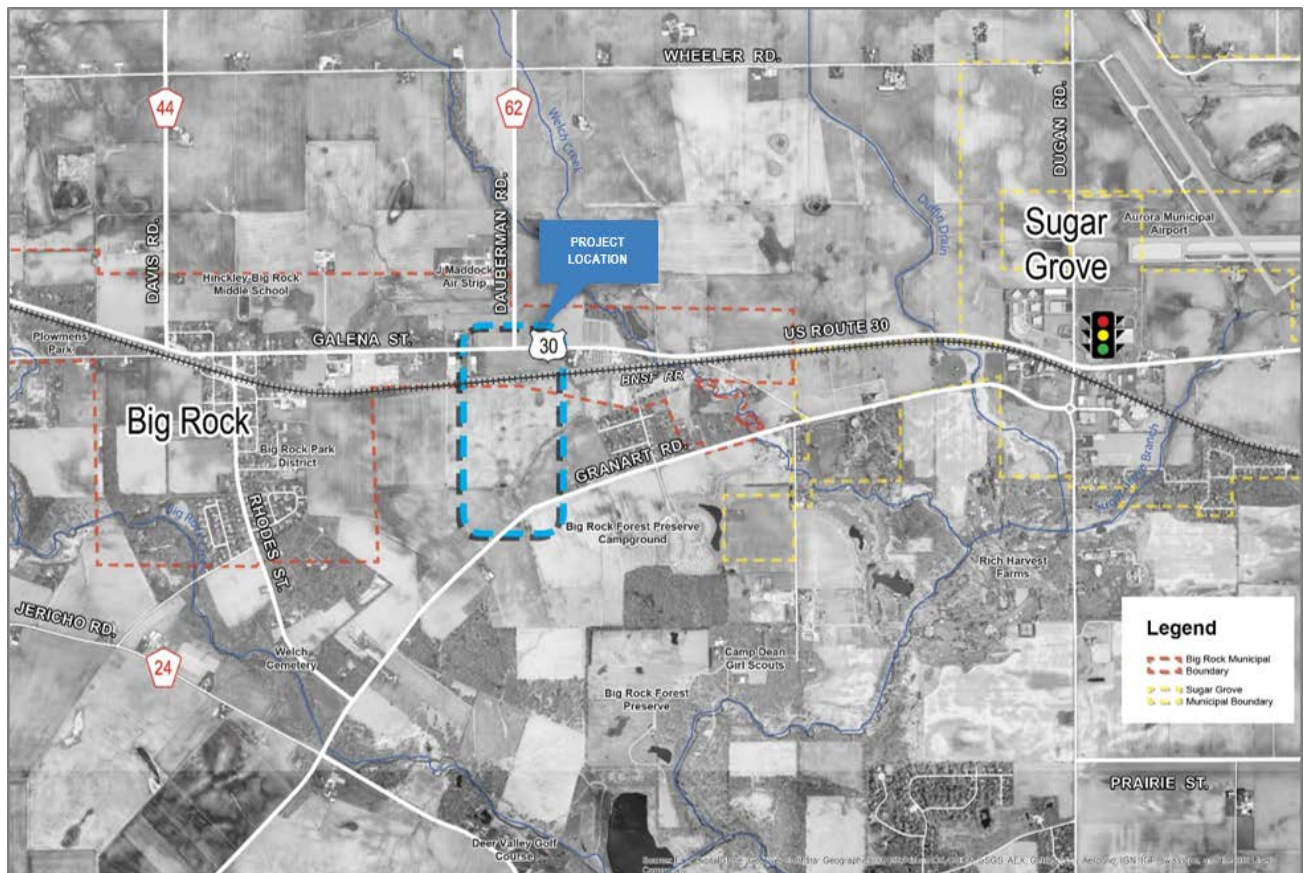
1.1 PURPOSE

The purpose of this project is to provide a roadway connection to improve access across the Burlington Northern Santa Fe (BNSF) Railroad tracks between the Village of Big Rock and the Village of Sugar Grove as well as address future transportation needs in accordance with the Kane County 2040 Transportation Plan and other local land use plans. The needs for this project include improving and enhancing the safety of the users, improving mobility in the region, and enhancing system linkage and connectivity of the transportation network in the County.

1.2 PROJECT LOCATION

The *Project Study Area* is in the southwest quadrant of Kane County in the Village of Big Rock, Big Rock Township, and unincorporated Kane County, west of the Village of Sugar Grove. The project limits are north along Dauberman Road to the bridge over Welch Creek, US Route 30 approximately 3,600 feet west to 2,600 feet east of Dauberman Road, and approximately 3,000 feet west of Mary Drive to 1,500 feet west of Mary Drive along Granart Road. (See **Figure 1.1** for the Project Limits Map)

Figure 1.1: Project Limits Map



Existing Dauberman Road is classified as a major collector under the jurisdiction of Kane County Division of Transportation (KDOT). Dauberman Road begins at US Route 30 to the south and continues for approximately

eight miles to the north to Keslinger Road in Maple Park. The other roads within the project study area are classified and under the jurisdiction as described below:

- US Route 30: minor arterial, Illinois Department of Transportation (IDOT)
- Granart Road: minor arterial, Big Rock Township and Village of Big Rock
- Rhodes Street: minor collector, Village of Big Rock and Big Rock Township
- Dugan Road: major collector, Village of Sugar Grove

The existing T-intersection of Dauberman Road and US Route 30 is stop controlled on Dauberman Road. The existing land use in the area is mostly agriculture. Approximately 930 feet north of the intersection of Dauberman Road and US Route 30 is a private airstrip. There are commercial properties along US Route 30, east and west of the Dauberman Road intersection. The BNSF Railroad runs east-west just south and parallel to US Route 30. Welch Creek flows northwest to southeast under Dauberman Road north of the intersection with US Route 30 and under US Route 30 east of the project limits. East of the project limits, the intersection of Dugan Road and US Route 30 was reconstructed by IDOT in 2016, and a new roundabout was completed at the intersection of Dugan Road and Granart Road by the Village of Sugar Grove in 2015.

Near the south end of the project study area and east along Granart Road is the Kane County Forest Preserve (Big Rock Campground) and the Girl Scouts of Northern Illinois Camp Dean which hosts various events from July through September. The forest preserve entrance is across from Marry Drive, while the Girl Scout camp entrance is approximately 0.5 miles further east at Camp Dean Road. Continuing east along Granart Road is Rich Harvest Farms, a private golf course which hosts private event and professional tournaments during the summer months.



Intersection of Dauberman Road and US Route 30 looking south

1.3 PROJECT BACKGROUND

1.3.1 Location History

Dauberman Road and Granart Road were originally constructed over 70 years ago and have undergone periodic resurfacing since their original construction. The section of US Route 30 within the project limits was originally constructed in 1944 and has undergone periodic resurfacing and widening by IDOT with the most recent resurfacing in 1999. The BNSF Railroad was built as the Chicago, Burlington and Quincy Railroad prior to 1868, and is now part of BNSF's Northern Transcontinental route.

The bridge north of the Dauberman Road and US Route 30 intersection (SN 045-3107), over Welch Creek, was recently replaced by KDOT and reopened in July 2016. The improvement consisted of a new superstructure and repairs to the existing substructure, with 2-12 foot lanes and 8-foot wide shoulders. The structure was raised approximately three inches and modernized guardrail systems were added in accordance with IDOT Highway Standards.

1.3.2 Project Studies

A feasibility study was conducted by KDOT in 1999 for a Dauberman Road extension from US Route 30 to Granart Road. The study considered many alternatives including an at-grade crossing of the BNSF Railroad, below-grade crossing, and an above-grade crossing of the railroad. The conclusion of the feasibility study found two alternatives to be feasible for further evaluation and study, the at-grade crossing and the above-grade crossing of the BNSF Railroad with an elevated intersection with US Route 30. The alternatives found to be feasible will be evaluated as part of this project.

The Kane County 2040 Transportation Plan (adopted April 10, 2012) outlines five alignments in the western third of the county to provide a continuous north-south corridor and improve connectivity throughout the County and the region. The Dauberman Road extension is one of the alignments that is recommended in order to create an additional north-south connection over the BNSF Railroad and relieve any traffic congestion at the intersection of Dugan Road and US Route 30.

The Village of Big Rock Comprehensive Plan (adopted April 22, 2014) was completed in conjunction with Chicago Metropolitan Agency for Planning (CMAP), the region's Metropolitan Planning Organization (MPO), and funded in part by the Federal Highway Administration (FHWA), Federal Transit Administration (FTA), US Department of Housing and Urban Development (HUD), Illinois Department of Transportation (IDOT), and the Chicago Community Trust. The Plan supports the extension of Dauberman Road because it recognizes the extension as an additional north-south route, an additional rail crossing for emergency vehicles, and potentially vital to the Village of Big Rock's future land use plan – it has identified the area south of the BNSF as a new industrial area and the area around US Route 30 and Dauberman Road as an area for potential commercial growth¹.

The Village of Sugar Grove Comprehensive Plan includes two new grade separated crossings of the BNSF railroad tracks between the Village of Big Rock and Dugan Road, with one crossing at the Dauberman Road corridor and another one mile east as part of a new north/south collector roadway. It anticipates the area south of the railroad tracks and east of an extension of Dauberman being developed as residential area.²

IDOT is conducting an Environmental Assessment (EA) Phase 1 study along US Route 30 from just east of Dauberman Road to Illinois Route 47 (IL Route 47). IDOT's west project limit will match into KDOT's east project limit along US Route 30. IDOT is considering potential improvements along the US Route 30 corridor to address future transportation needs. The project is anticipated to complete the EA in 2019, with construction to be programmed thereafter. To address current safety and capacity issues at the US Route 30 and Dugan Road intersection, IDOT advanced an interim design and construction project for the intersection. Construction of the intersection improvements was completed in late 2016.

Similarly, the Village of Sugar Grove improved the intersection of Granart Road and Dugan Road as a roundabout. This intersection is located immediately south of the US Route 30 and Dugan Road intersection. The roundabout was completed in August 2015.

¹ Source: *Village of Big Rock Comprehensive Plan*, Chicago Metropolitan Agency for Planning (CMAP), Adopted April 22, 2014, Page 79.

² Source: *Village of Sugar Grove Comprehensive Plan*, URS Corporation, Adopted 2004

1.4 NEED FOR THE PROPOSED ACTION

1.4.1 Improve Safety

Emergency Routes and Response

One of the key needs this project intends to address is more effective emergency routes. The emergency responders (fire department and paramedics) are currently located south of the BNSF Railroad in the Village of Big Rock. The fire department and paramedics service the areas north of US Route 30 such as the Kaneland High School located in Maple Park on Keslinger Road between Dauberman Road and Meredith Road, approximately eight miles from the project.

In cases where emergencies are north of the BNSF Railroad, the first responders are required to detour around the train for both reaching an emergency north of the railroad and then once again to travel to the nearest hospital. The nearest hospitals used for the Village of Big Rock are in Sandwich and Yorkville in Kendall County. Maneuvers to bypass a blocked at-grade rail crossing require responders to travel five to ten miles out of their way. At times emergency responders are forced to travel to IL Route 47 in Sugar Grove, which has the nearest grade-separated crossing to respond to emergencies north of the tracks.

This area is under the jurisdiction of the Kane County Sherriff, which is based out of St. Charles. They have also noted delays in response times due to blocked at-grade railroad crossings.

Per the Illinois Commerce Commission (ICC) and train counts conducted by the project team, there are between 25 and 32 trains per day on the BNSF Railroad line through Sugar Grove and Big Rock.

Railroad Crossing Safety

As of January 4, 2016 the crossings at Dugan Road (approximately two miles east of Dauberman Road) and Rhodes Street (approximately one mile west of Dauberman Road, in downtown Big Rock) were changed to no-horn crossings. These at-grade crossings have signals, crossing gates, and medians to discourage drivers from going around the gates. The potential for further safety improvements at these crossings, such as full grade separated overpass or underpass, are constrained by existing structures and other roadways.

Crash Experience/History

From 2010 through 2014 there were 109 crashes of all types reported within the project study area. Key trends within these crashes were:

- Rear end crashes were the most common, with a concentration at the Dugan Road and US Route 30 intersection, where a significant volume of traffic crosses the BNSF tracks between Granart Road and US Route 30.
- Collisions between vehicles and large animals like deer were the second most common type of crash, with the majority of the crashes that occurred on roadway segments between intersections.
- The Granart Road curve, approximately a half-mile west of Mary Drive, was a focal point for Loss of Control crashes, with many being attributed to either weather condition or traveling at a high rate of speed through the curve.

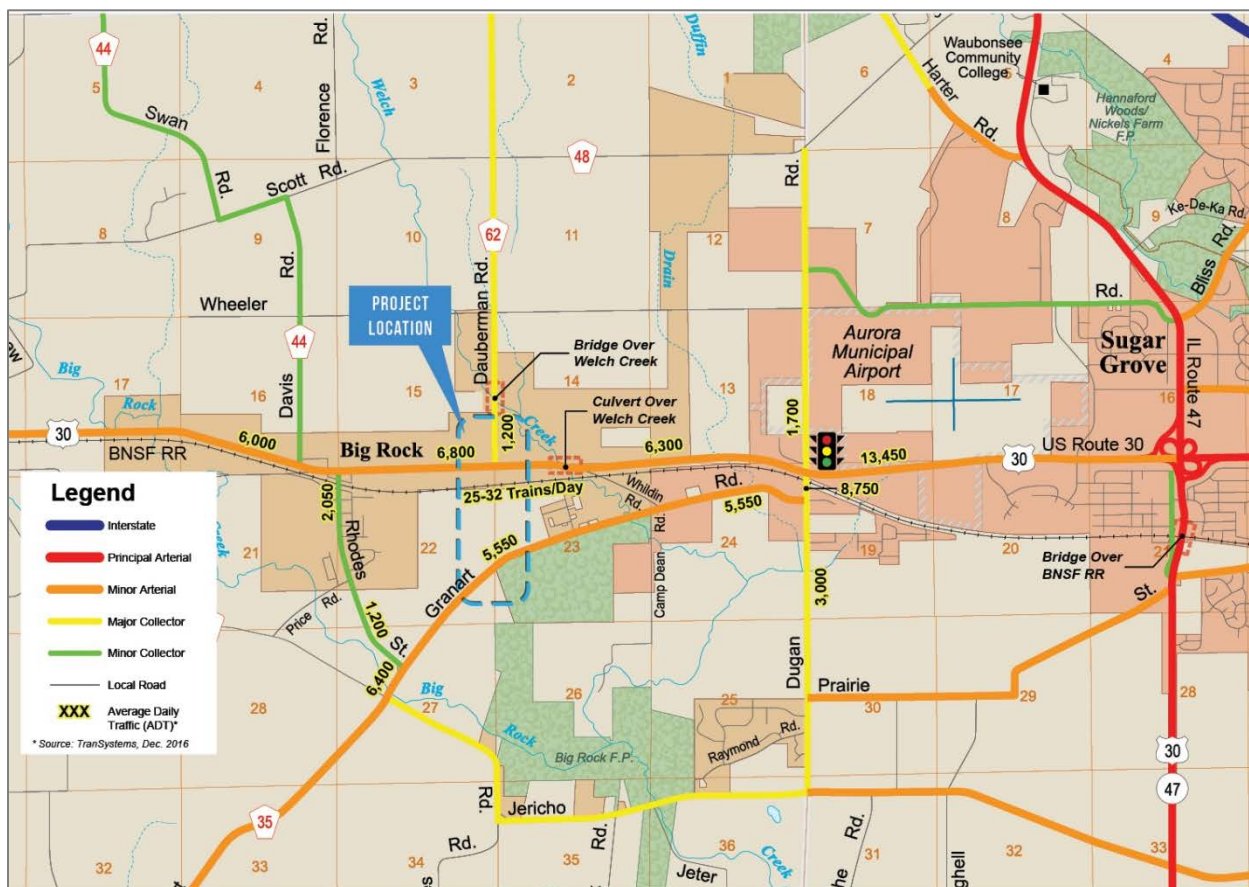
Animal collision crashes are common to many roads similar to the project, and are typically difficult to mitigate. However, rear end crashes and the loss-of-control issues at the Granart Road curve are indicative of safety needs caused by excessive queueing and geometric issues.

1.4.2 Improve Mobility

Traffic Volumes

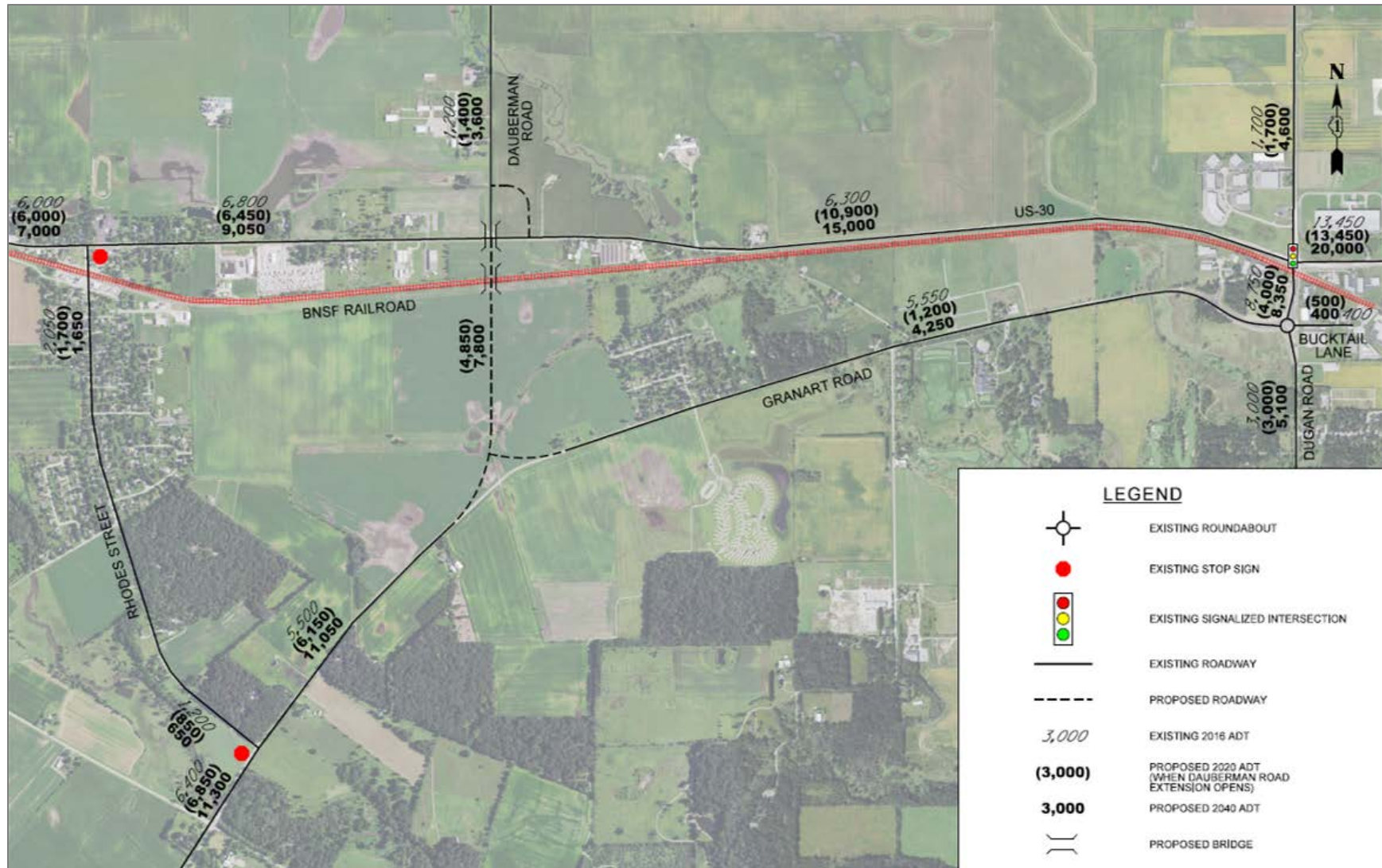
Based on traffic counts conducted in September 2016, traffic volumes were analyzed and then forecasted by the Chicago Metropolitan Agency for Planning (CMAP) based on assumed growth within the area. Per CMAP's model, traffic growth is anticipated to continue to increase based on population, new industrial and commercial businesses, and increased residential development. The existing average daily traffic (ADT) varies within the corridor as seen below (see **Figure 1.2** Existing Average Daily Traffic).

Figure 1.2: Existing Average Daily Traffic



The anticipated ADT traffic volume increases in the 2040 build condition are commensurate with implementation of the Dauberman Road extension and creation of the new north-south connection from US Route 30 to Granart Road. The growth shows the continued northeast and southwest predominant travel patterns. Along US Route 30, the section between Dugan Road and Dauberman Road will experience a 138 percent growth. Granart Road west of the Dauberman Road extension will experience a 100.9 percent growth. In contrast, Dugan Road between US Route 30 and Granart Road will realize a 4.6 percent reduction in volumes. Granart Road between the Dauberman Road extension and Dugan Road will experience a 23.4 percent reduction in volume as well (see **Figure 1.3** Projected 2040 Average Daily Traffic).

Figure 1.3: Projected 2040 Average Daily Traffic



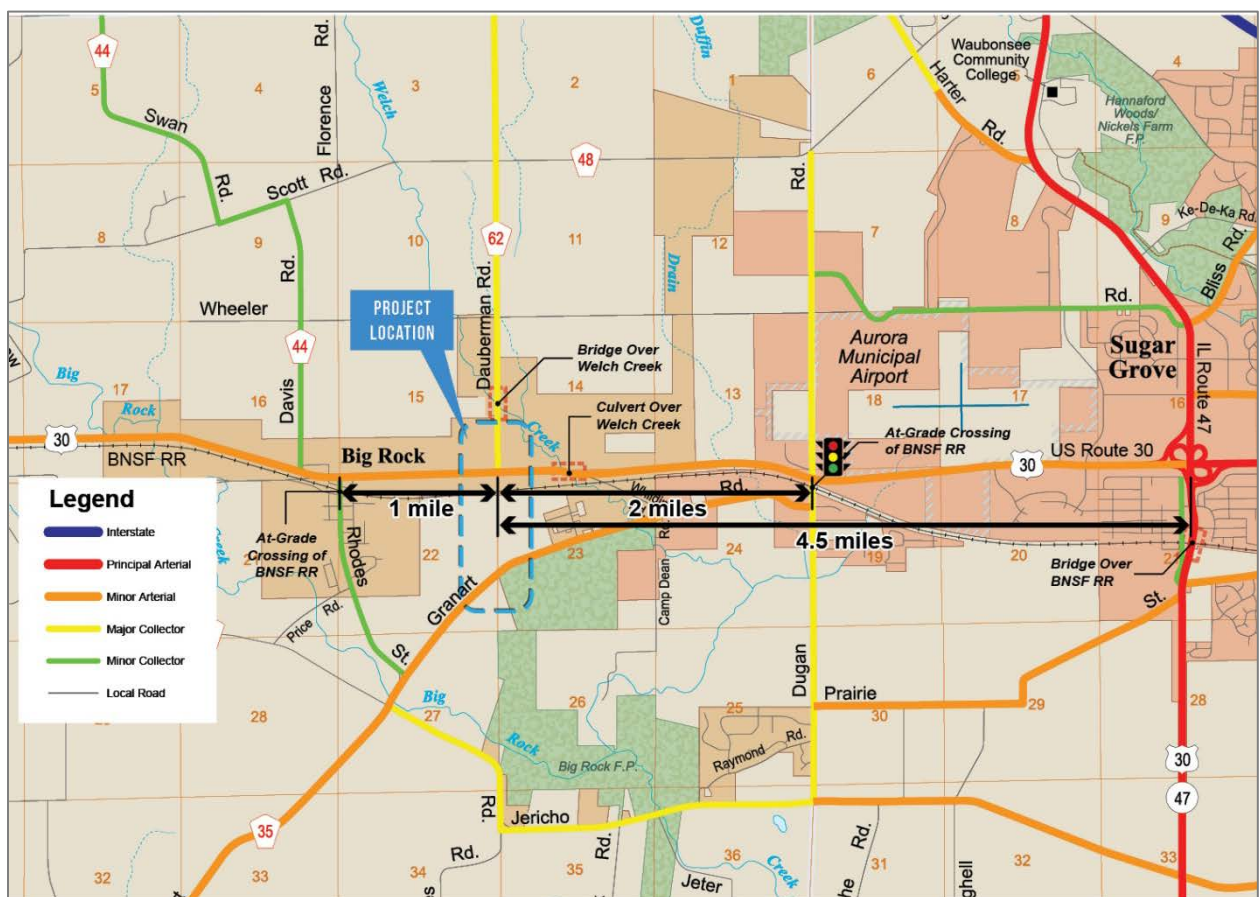
Train Volumes and Crossings

Based on train counts conducted over a 24-hour period in December 2016, over 50% of the trains were observed to stop for two or more minutes at the Dugan Road and BNSF Railroad crossing. Based on these same counts, it was seen that over 35% of the trains are running during the morning and evening commuter peak periods.

On average, the BNSF Railroad Aurora Subdivision has 25 to 32 trains per day at the Rhodes Street crossing in the Village of Big Rock and at the Dugan Road crossing in Sugar Grove. There are no passenger services currently on the rail line; all trains are freight trains which are typically longer and slower than passenger trains and create longer blockages of road crossings.

The nearest crossings of the BNSF Railroad to Dauberman Road are Rhodes Street in downtown Big Rock one mile to the west, and Dugan Road two miles to the east, both of which are at-grade crossings. The nearest grade separated crossings are IL Route 47, 4.5 miles to the east in Sugar Grove, and US Route 30, 17.5 miles to the west near the Village of Shabbona. Because of these large gaps between grade separated crossings of the railroad, there is a need for additional crossings to increase mobility for all road users. Because of the impacts to traffic mobility from the crossing trains, there is a need for new grade separated crossings (see **Figure 1.4 Railroad Crossings Locations**).

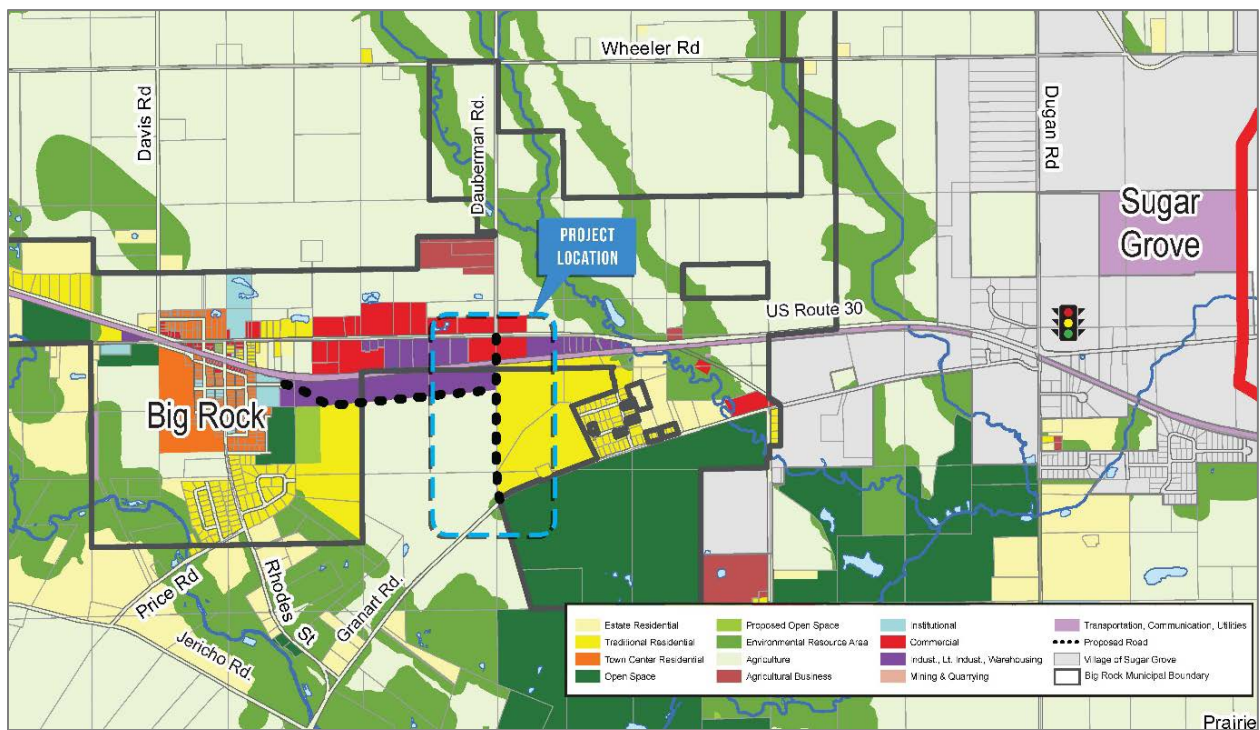
Figure 1.4: Railroad Crossings Locations



Local Land Use

The Village of Big Rock Comprehensive Plan includes an additional crossing of the BNSF tracks in the Dauberman Road corridor, and also includes an extension of 2nd Street east from downtown Big Rock, terminating at the Dauberman Road corridor. These roadway extensions, in addition to any other benefit seen in the plan, provide access to additional development in the area south of the BNSF railroad tracks and east of Big Rock. The Comprehensive Plan's land use in the project study area is a strip of industrial use along the south side of the BNSF railroad tracks, accessed primarily from the 2nd Street extension. In addition to this industrial development, a large residential development is seen directly east of the Dauberman Road corridor, with access from either a Dauberman Road extension or Granart Road (see **Figure 1.5 Village of Big Rock – Future Land Use Map**).

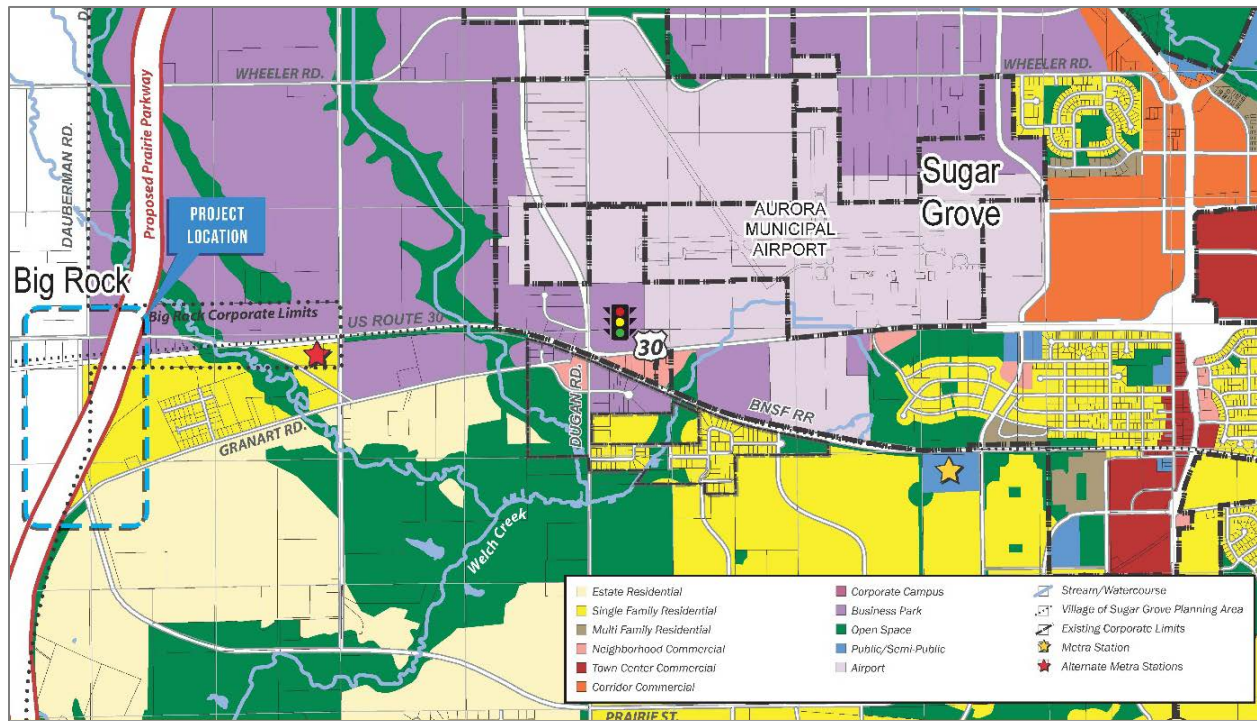
Figure 1.5: Village of Big Rock – Future Land Use Map



The Village of Sugar Grove's Comprehensive Plan is similar to Big Rock's, including both a crossing of the BNSF Railroad along the Dauberman Road corridor and a new collector roadway between Dauberman Road and Dugan Road. The future land use according to the Comprehensive Plan will include commercial and industrial development north of US Route 30 on both sides of Dugan Road to Dauberman Road. South of US Route 30 will be commercial/industrial development east of Dugan Road and residential development west to the Dauberman Road extension (see **Figure 1.6 Village of Sugar Grove – Future Land Use Map**).

Both municipalities within the project study area propose land uses that will encourage growth and continue to challenge the mobility of the area and the surrounding roadway network.

Figure 1.6: Village of Sugar Grove – Future Land Use Map



1.4.3 ENHANCE SYSTEM LINKAGE AND CONNECTIVITY

Dauberman Road is an important north-south roadway connection in Southwest Kane County. It currently extends approximately eight miles to the north of US Route 30 where it intersects with Keslinger Road in Maple Park. It provides connections to Kaneville and Maple Park from Sugar Grove and Big Rock. Additionally, Dauberman Road is one of only four roads in Kane County west of IL Route 47 that provides access across Interstate 88 (I-88).

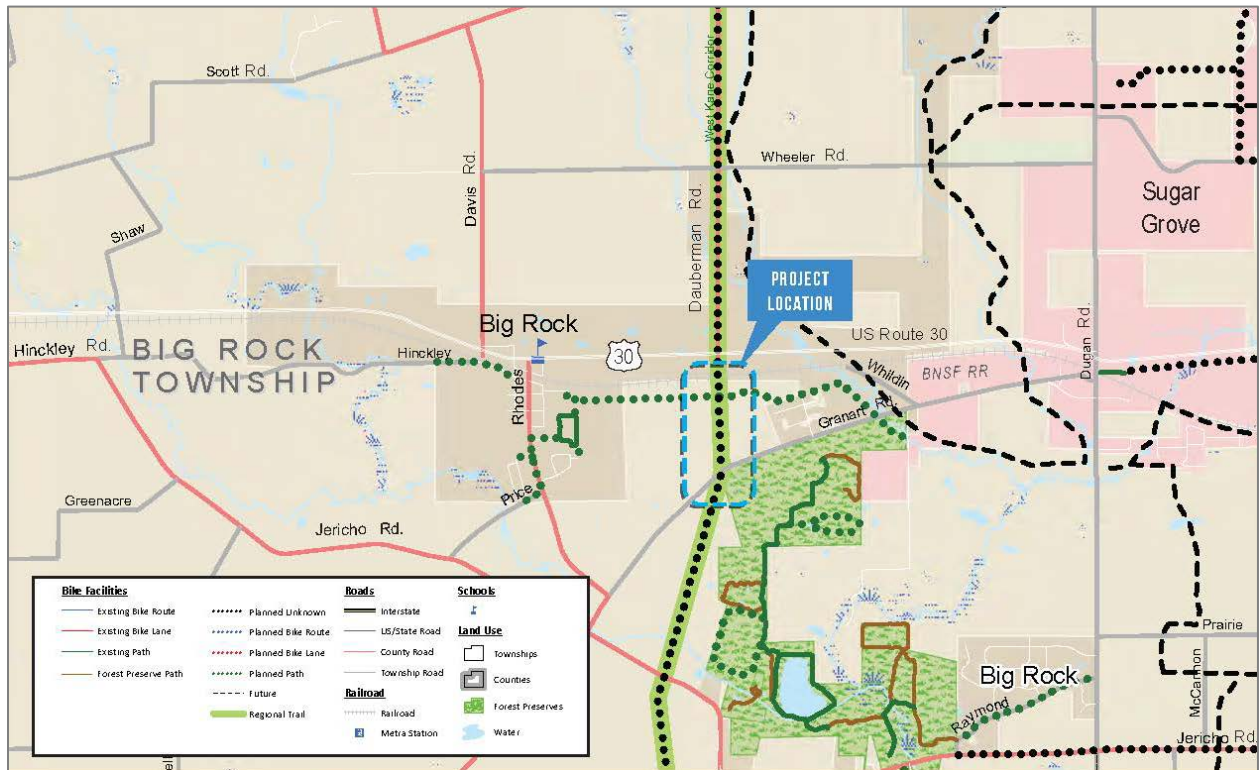
In the existing condition, Dauberman Road tees into US Route 30 directly north of the BNSF Railroad. Motorists wishing to travel further south are required to travel either west into downtown Big Rock to Rhodes Street or east to Dugan Road to use the railroad crossings at those locations, resulting in a mile or more of adverse travel, and deflection of vehicles from a primary north/south route that provides access over I-88. Three separate long-range planning documents, the Village of Big Rock and Village of Sugar Grove Comprehensive Plans as well as the KDOT 2040 Transportation Plan, all indicate a need to construct an additional grade separated crossing of the railroad to improve north/south travel in the area east of Big Rock.

In addition to motorized traffic, non-motorized bicycle and pedestrian traffic is impacted by the scarcity of railroad crossings in the area. The West Kane Corridor Regional Trail is a non-motorized trail that is included in the Kane County Bicycle and Pedestrian Plan (see **Figure 1.7** Kane/Kendall County Bicycle and Pedestrian Plan). This trail is planned to run north/south along the Dauberman Road corridor, provide non-motorized access in western Kane County, and connect to existing trails within the Big Rock Forest Preserve³. Again because of the distance from this planned corridor to existing railroad crossings at Dugan Road and Rhodes

³ Source: *Kane County Bicycle and Pedestrian Plan*, Kane County, Adopted April 2012

Street, there is a need to provide an additional crossing of the BNSF Railroad at or near the Dauberman Road corridor to enhance planned non-motorized transportation links.

Figure 1.7: Kane/Kendall County Bicycle and Pedestrian Plan



APPENDIX D

Public Meeting and Public Hearing

- 2/9/17, Public Meeting Summary
- 2/15/18, Public Hearing Summary
- 5/18/18, KDOT Public Hearing Certification Letter

DAUBERMAN ROAD EXTENSION

PHASE I STUDY



PUBLIC INFORMATION MEETING #I SUMMARY

HINCKLEY-BIG ROCK MIDDLE SCHOOL
FEBRUARY 9, 2017
5:00-7:30 P.M.

Public Meeting Summary

The first Public Information Meeting for the Dauberman Road Extension project was held on Thursday, February 9, 2017 at the Hinckley-Big Rock Middle School – 47W984 US Route 30, Big Rock, Illinois from 5:00 P.M. to 7:30 P.M. The meeting was an open house format with large-scale exhibits set up on display; a separate comment area for attendees to provide comments, questions, and concerns were provided. Two (2) PowerPoint presentations were made to the public at 5:30 P.M. and 6:30 P.M. by Brian Fairwood of TranSystems.

Representatives from the Kane County Division of Transportation, Illinois Department of Transportation (IDOT), TranSystems, and Bollinger, Lach & Associates (BLA) were available to discuss the project and answer questions.

Letters were mailed to approximately **440** stakeholders, **40** representatives from Local Agencies, and **23** Officials on January 23, 2017. Advertisements for the public meeting were published in local newspapers and online social media including:

- **Aurora Beacon News – January 26, 2017 and February 2, 2017**
- **Big Rock Community Crier – February 2017 Newsletter**
- **Daily Herald – January 26, 2017 and February 2, 2017**
- **Elburn Herald – January 26, 2017 and February 2, 2017**
- **Kane County Chronicle – January 26, 2017 and February 2, 2017**
- **Kane County Connects (Facebook) – February 1, 2017**

The meeting was attended by 100 people. Eleven (11) comments via comment form or email were received by attendees prior to the closure of Public Comment on February 23, 2017.

The following Agencies were represented:

- **Big Rock Drainage District #1**
- **Big Rock Fire Department**
- **Big Rock Community School District #429**
- **Illinois Department of Transportation**
- **Kane/Kendall Council of Mayors**
- **Kane County Board**
- **Kane County Division of Transportation**
- **Kaneland Community School District #302**
- **Kaneville Fire Protection District**
- **Kendall County Highway Department**
- **Sugar Grove Fire Department**

The following Municipalities were represented:

- **Big Rock Township**
- **Village of Big Rock**
- **Village of Sugar Grove**

Other Organizations represented include:

- **J. Maddock Airport**
- **EEL Engineering – Village Engineer for Sugar Grove**

The eleven (11) comments submitted via comment form or email covered a variety of topics including:

- **Air Quality**
- **Benefits**
- **Capacity**
- **Drainage Concerns**
- **Environmental Impacts**
- **Impacts on Wildlife**
- **Need for Project**
- **Neighborhood Impacts**
- **Opposition to Project**
- **Recreational Connectivity**
- **Safety**
- **Support for Project**

Of the comments received:

Support Project	Opposed to Project	Expressed Concerns
6	1	4

Of the received comments expressing concerns or opposition to the project, four (4) comments were made regarding the potential increase in traffic generated by the Dauberman Road Extension and two (2) additional comments were made regarding poor drainage in the area.

Several additional topics were discussed by attendees with members of the project team including:

- **Extension of Municipal Roadways to Dauberman Road Extension**
- **Farm Equipment Maneuverability**
- **Pavement Condition & Jurisdiction of Granart Road**
- **Project Timeline**
- **Confusion Regarding IDOT US 30 Route Phase I Study and Dauberman Road Extension Phase I Study**
- **Concerns Regarding Impacts Resulting from Potentially Widening US Route 30**
- **BNSF Railroad Drainage Maintenance Concerns**
- **Localized Flooding Issues**
- **Safety Concerns at Granart Road Curve**
- **Traffic Control Devices**
- **Usefulness of Project**

A log of all attendees at the meeting as well as a log of all comment forms, exhibits, brochures, presentations, and all other distributed items is attached as part of this summary.

Exhibits displayed at the meeting are displayed on the Kane County Division of Transportation website at <http://www.co.kane.il.us/dot/constProjects/Dauberman.aspx>.

Attachments:

- **Attachment 1: Sign-In Sheets**
- **Attachment 2: Received Written or Emailed Comments**
- **Attachment 3: Mailed Invitation Letter, Recipients, and Notification Area**
- **Attachment 4: Certifications of Publication**
- **Attachment 5: Informational Brochure**
- **Attachment 6: PowerPoint Presentation/Display Exhibits**
- **Attachment 7: Public Meeting Photos**

DAUBERMAN ROAD EXTENSION

PHASE I STUDY

ATTACHMENT I

SIGN-IN SHEETS



PUBLIC MEETING REGISTER

DAUBERMAN ROAD EXTENSION

INTRODUCTORY & SCOPING PUBLIC MEETING

February 9, 2017 - Hinckley-Big Rock Middle School - 5:00 to 7:30 P.M.

Please print your name and address below:

	Name	Address	Phone/E-Mail (optional)
1	Nancy McCraw	1050 Apples Dr Aurora Ill.	
2	Wayne Parson	25 S. Municipal Sugar Grove, IL	
3	Joe Whitely	47W210 US Highway 30 Big Rock, IL 60511	630.556.3731
4	Tom Rickett	41W011 BURLINGTON ST. CHARLES, IL 60175	
5	Shirley Wysocki	215 E. HALENA BIG ROCK 60511	630-556-3966
6	DENNIS STORZEN	107 E. 5TH ST P.O. 54 BIG ROCK, IL 60511	
7	Bill Van Kirk	105 E. 5TH ST Big Rock	
8	Mark and Marcia Lee	1521 Mary Dr Big Rock IL 60511	marcialynne71@gmail.com 630-556-4142
9	Jim Behm	25297 Audenrae Kaneland IL	630.557.0052
10	Larry & Judy Conrad	48W090 Birch Rd Big Rock	630 556 4413
11	Sarah Allen	400680 Dolly Drive Big Rock	
12	Sherry Lynn Sorensen	47W225 1st St Big Rock	
13	Beverly Nachtrieb	46W192 Granat Big Rock, IL	knach2018@gmail.com
14	John & Linda Voelker	231 S Glenwood Aurora IL	john462@sbcglobal
15	Tony Speciale	601 Heartland Pkwy Sugar Grove IL	aspeciale@sugargrovel.gov



PUBLIC MEETING REGISTER

DAUBERMAN ROAD EXTENSION

INTRODUCTORY & SCOPING PUBLIC MEETING

February 9, 2017 - Hinckley-Big Rock Middle School - 5:00 to 7:30 P.M.

Please print your name and address below:

	Name	Address	Phone/E-Mail (optional)
16	Brock Mathewson	45W487 Bergmon Dr Big Rock IL	
17	Kolt Odonnell		
18	DREW FRASZ KDOT		630-514-2153
19	Steve Michael		
20	Sherry Michael	55331 Deer Ridge Path Big Rock	630-556-3902
21	Bill Lenert	138 W. Burkhaman Sugar Grove, IL	630-319-4947
22	Lester Hummel	Big Rock, IL 75405 CAMP DENA	
23	MICHAEL BARNES	101 E GALENA	
24	CLAY HARMINEN	65920 DAVIS RD BIG ROCK	CLAYBOOB6@AOL.COM
25	Kim Augustine	460700 Dolly Dr. Big Rock	
26	Jennifer Becker KDOT/KKCOM		beckerjennifer@co.kane.il.us
27	Jules Ann Fuchs Kamland 1302	47W326 Kenilworth Rd. Maple Park, IL 60151	jfuchs@kamland.org
28	DAN KASBITT	46W628 GRANDVIEW RD BIG ROCK, IL	GUSTAFPLAYER1354@ATT.NET
29	Bruce Thompson	P.O. Box 113 Big Rock, IL	
30			630-608-3286



PUBLIC MEETING REGISTER

DAUBERMAN ROAD EXTENSION

INTRODUCTORY & SCOPING PUBLIC MEETING

February 9, 2017 - Hinckley-Big Rock Middle School - 5:00 to 7:30 P.M.

Please print your name and address below:

	Name	Address	Phone/E-Mail (optional)
31	Robert J. Raymond	45W468 Raymond Rd Big Rock IL 60511	
32	Janald Thompson	46W795 US Hwy 30 Sugar Grove, IL	
33	Dennis Baie	47W341 U.S. Hwy 30	bbaie1@att.net
34	Barbara Baie	Big Rock, IL 60511	
35	Theresa Pelletier	DOT Programming	theresa.pelletier@illinois.gov 8473054479
36	John H Ruh	Big Rock FD Box 59	JRUH & DEKAWIE.COM
37	Bridget Bulthaupt	46W597 Dolly Dr Big Rock, IL	tandb36@mcbsi.com
38	Jack Bulthaupt	same	same
39	Greg Green	47W 940 TIMBERVIEW B.R.	
40	Heinrich Mr. Mrs. James	46W563 Rt. 30 Sugar Grove	
41	RICH KOKES	46W622 KATIE BIG ROCK	
42	DAVE SIGMUND	KANEVILLE FD	
43	JAFF ODWYER	46W603 KATIE BIG ROCK	
44	Mike Pegan	300 Fays Ct Sugar Grove	
45	WADE THOMPSON	65642 DAVIS RD BIG ROCK	



PUBLIC MEETING REGISTER

DAUBERMAN ROAD EXTENSION

INTRODUCTORY & SCOPING PUBLIC MEETING

February 9, 2017 - Hinckley-Big Rock Middle School - 5:00 to 7:30 P.M.

Please print your name and address below:

	Name	Address	Phone/E-Mail (optional)
46	Jim & Marion Scott	S. A. 45W 33 Wheeler Rd.	
47	Ken Rojeh	49W 365 SCOTT Big Rock, IL	KenRoje@AOL.com
48	Bonnie Warchol	48W 58 Rt 30 Big Rock 60511	
49	Ray Warchol	48W 58 Rt 30 Big Rock	
50	William Anthony	46W 597 Dolly Dr Big rock	
51	Don & Joyce Siefert	47W 915 Sunbroom Big Rock	
52	Lynn W. Goben	46W 615 Dolly Dr. Big Rock, IL	
53	BARRY & LEAH MORSE	1808 FAIR LN SUGAR GROVE IL	
54	Sandy Carr	55430 Deer Ridge Ave Big Rock, IL	big rock township @ gmail.com
55	MURKINS TORC	75620 Rhodes St.	
56	Kareville F.P.D. Troop John T. LARKIN	P O Box 10 Kareville IL	
57	DAVID HALL	201 EAK ST BIG ROCK	
58	VINCE STRAZZABOSCO	401 RHODES (PO BOX 318) BIG ROCK, IL	V STRAZZABOSCO @YAHOO.COM
59	Tom Greyer	4700 24th Ave	630 330 1591
60	Thomas LOSHIECKI	214 Oak St Big Rock	



PUBLIC MEETING REGISTER

DAUBERMAN ROAD EXTENSION

INTRODUCTORY & SCOPING PUBLIC MEETING

February 9, 2017 - Hinckley-Big Rock Middle School - 5:00 to 7:30 P.M.

Please print your name and address below:

	Name	Address	Phone/E-Mail (optional)
61	RUTH DON PLAUCK	1918 ANNETTES CIR SUGAR GROVE IL	630-466-8443
62	Dean Hummer II	47W799 Timberline Big Rock	630 707 3542
63	Hannah Knight	IDOT	
64	Mauri Sopinski	Big Rock 115 Hwy 30	
65	John Miller	46 W 680 Dolly Dr	847-305-9981
66	Mark May	47W794 CAREY BIG ROCK	
67	SONNIE LOU	100 E GALENA RT 30 BIG ROCK	
68	MATTHEW CESARIO	123 Joy St. Sugar Grove, IL 60554	
69	Dale Kuhn	73270 May Big Rock	
70	Lori Marco	46 W 650 Katie Drive Big Rock IL 60511	lmarco@ameritech.net
71	GENE MERRICK	305 LINCOLN BIG ROCK, IL 60511	GMERRICK@AOL.COM
72	Robert Hulek		
73	Rich Dunlap	PO Box 37 Big Rock IL 60511	
74	MARY Stola	7558 Walton St Big Rock, IL	
75	TRAVIS McQuinn	476 Prairie View Lane Hinckley, IL 60520	



PUBLIC MEETING REGISTER

DAUBERMAN ROAD EXTENSION

INTRODUCTORY & SCOPING PUBLIC MEETING

February 9, 2017 - Hinckley-Big Rock Middle School - 5:00 to 7:30 P.M.

Please print your name and address below:

	Name	Address	Phone/E-Mail (optional)
76	Lulu Archule	98136 Granary Rd Big Rock	
77	Robert & Aaron Schoger	11830 Big Rock	
78	Rebecca Byrnes	48W456 Jericho Big Rock	
79	Kathy Nicks	101 W 2 Rd. 30 Big Rock IL	
80	CARL & Amy Schoger	47W662 Rt 30 Big Rock, IL 60511	
81	PAUL WEDEREN	45W789 Jericho Rd Big Rock	
82	Ted McCANNON	45W109 Raymond Rd Big Rock	Re-e-d-s@mchsi.com
83	Dr. BILL FISCHER	Big Rock	
84	Jay Mardak	Big Rock	
85	STEVE GORDON	GENOVA, IL	
86	Bill Guye	Big Rock Rt 30	
87	FRAN KLAAS	205 E. Kendall Yorkville, IL	fk1225@co.Kendall.il.us
88	Jim MICHELIS	43W852 OLD MIN TOPK ELDON, IL 60119	JMICHELIS@BELLWEB.COM 630/814-4290
89	ROBERT BROWN	SUFFERD AVE Big Rock	
90	Tim Stoca	75558 WELTON DR Big Rock	



PUBLIC MEETING REGISTER

DAUBERMAN ROAD EXTENSION

INTRODUCTORY & SCOPING PUBLIC MEETING

February 9, 2017 - Hinckley-Big Rock Middle School - 5:00 to 7:30 P.M.

Please print your name and address below:

	Name	Address	Phone/E-Mail (optional)
91	GARY PETERSEN	7551 E WILTON DR Big Rock IL 60511	
92	John & Nan Long	11461 Co. Line Big Rock IL 60511	J
93	Nicole Envergn	75589 Welton Dr. Big Rock IL	
94	WALTER MAGDZIARZ	601 HEARTLAND DRIVE SUGAR GROVE, IL 60554	WMAGDZIARZ@SUGARGROVE.IL.GOV
95	Mary Hauge	75626 Welton Dr. Big Rock IL 60511	
96	GARY Hauge		
97	C BAUMANN	343 S BATAVIA LESTER, VA 60134	
98	Dennis Owen	4660 Dolly Dr. Big Rock, IL	
99	Cathy Owen	" "	
100	Angel Veliz	75359 Camp Dean Rd Big Rock 60511	
101			
102			
103			
104			
105			



PUBLIC MEETING REGISTER

DAUBERMAN ROAD EXTENSION

INTRODUCTORY & SCOPING PUBLIC MEETING

February 9, 2017 - Hinckley-Big Rock Middle School - 5:00 to 7:30 P.M.

Please print your name and address below:

	Name	Address	Phone/E-Mail (optional)
106	Angela Carey	Big Rock 75702 WELTON DR.	acarey158@gmail.com
107	ROBERT CAREY	75702 WELTON DR BIG ROCK	630 336-0544
108			
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120			

DAUBERMAN ROAD EXTENSION

PHASE I STUDY

ATTACHMENT 2

RECEIVED WRITTEN OR EMAILED COMMENTS



FEBRUARY 9, 2017
DAUBERMAN ROAD EXTENSION
INTRODUCTORY & SCOPING PUBLIC MEETING
COMMENT FORM

Dauberman Road Extension is a new corridor study that would extend Dauberman Road to the south of US Route 30 and intersect with Granart Road. The road extension will improve connectivity between the surrounding areas and improve safety by providing a grade-separated crossing at the BNSF Railroad and US Route 30. The project is being studied by the Kane County Division of Transportation (KDOT).

This Public Meeting is an opportunity for you to provide input on the project at an early stage of the project study and development. KDOT encourages you to submit your input and comments at the public forum tonight by placing it in the COMMENTS box; or scan and email to FlockBonnie@co.kane.il.us or fold it in thirds, tape closed, place a stamp and mail.

All comments submitted through February 23, 2017 will become part of the public meeting record.

Comments/Questions:

I ENCOURAGE THE COUNTY TO CONTINUE DEVELOPMENT OF THE PROJECT. I WOULD ALSO RECOMMEND THAT AN EXTENSION OF ROADWAY TO BIG ROCK FIRE DEPARTMENT ACCESS BE INCLUDED IN THE PLAN. BNSF OFTEN BLOCKS THE TRACKS FOR SEVERAL MINUTES AT A TIME AND AS A BIG ROCK RESIDENT ON THE NORTH SIDE OF THE TRACKS I WOULD APPRECIATE HAVING A BRIDGE / UNDERPASS OPTION INSTEAD OF A GRADE CROSSING

(Optional, Please Print)

Name/Affiliation THOMAS LOSINIECKI

Address 214 OAK ST BOX 282

City/State BIG ROCK IL Zip Code 60511

Phone No. 630 667 4253

E-Mail Address KTMATRIZ@gmail.com

I would NOT like to receive information on this project

☐



FEBRUARY 9, 2017
DAUBERMAN ROAD EXTENSION
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Comments/Questions:

Drainage, water runoff

I Run a business @ 47W171 RT30 We have concerns with Traffic egress & ingress at our Facility.

(Optional, Please Print)

Name/Affiliation *Bob Raymond*

Address *45W 468 Raymond Rd*

City/State *Big Rock, IL* Zip Code *60511*

Phone No.

E-Mail Address *Bob@BigRockUSA.com*

I would NOT like to receive information on this project

☐



FEBRUARY 9, 2017
DAUBERMAN ROAD EXTENSION
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Comments/Questions:

Would there be a stop light on Granart for the extension?
If not, what would be the control system to maintain traffic flow?
~~These are~~ These questions actually apply to both ends of the extension.
I live on Granart Rd and the morning and afternoon traffic is heavy.
I am concerned that the traffic may increase.

(Optional, Please Print)

Name/Affiliation Leslie Aichele / Homeowner

Address 95136 Granart Rd

City/State Big Rock, IL Zip Code 60511

Phone No. 630-556-4865

E-Mail Address _____

I would NOT like to receive information on this project

☐



FEBRUARY 9, 2017
DAUBERMAN ROAD EXTENSION
INTRODUCTORY & SCOPING PUBLIC MEETING
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Comments/Questions:

I use to live in Susan Corone and use to frequent this intersection to school (Haneland), as well as to Hinkley. This is a well needed improvement and it can't be built soon enough.

(Optional, Please Print)

Name/Affiliation Matthew Cesario - Former Dauberman user

Address 2401 Rosehall Lane

City/State Aurora, IL Zip Code 60503

Phone No. _____

E-Mail Address matthewcesario14@yahoo.com

I would NOT like to receive information on this project

☐



FEBRUARY 9, 2017
DAUBERMAN ROAD EXTENSION
INTRODUCTORY & SCOPING PUBLIC MEETING
COMMENT FORM

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Comments/Questions:

I WAS INFORMED THAT THIS ROAD EXTENSION PUTS GRANART RD UNDER COUNTY CONTROL INSTEAD OF BIG ROCK TOWN CONTROL. SO THERE IS ONE REASON I SUPPORT THIS EXTENSION. BIG ROCK CANNOT AFFORD TO FIX OR MAINTAIN IT. SO GRANART IS A DANGEROUS TRAVEL ROUTE.

I HAVE GREAT HOPE THAT THIS ROAD EXTENSION CUTS TRAFFIC IN HALF THAT PASSES CONTINUOUSLY ON GRANART. TOO MANY SPEED PAST ESPECIALLY MORNING & NIGHT. THIS IS PART OF THE REASON THE ROAD IS SO BAD AND CANNOT BE REPAIRED. SO I GIVE 100% SUPPORT FOR THE DAUBERMAN ROAD EXTENSION.

(Optional, Please Print)

Name/Affiliation DAN KNIGHT

Address 56 W 628 GRANART RD

City/State BIG ROCK, IL Zip Code 60511

Phone No. 312-231-2427

E-Mail Address GUITARPLAYER2354@ATT.NET

I would NOT like to receive information on this project

☐



FEBRUARY 9, 2017
DAUBERMAN ROAD EXTENSION
INTRODUCTORY & SCOPING PUBLIC MEETING
COMMENT FORM

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Comments/Questions:

The Kaneland School District may be positively impacted by the construction of the proposed road, specifically, by providing a shorter route for the Big Rock ambulance to reach Kaneland High School more quickly.

(Optional, Please Print)

Name/Affiliation Julie-Ann Fuchs, Kaneland CUSD #302

Address 4760326 Kreslinger Blvd

City/State Maple Park IL Zip Code 60151

Phone No. (630) 365-4119

E-Mail Address jfuchs@Kaneland.org

I would NOT like to receive information on this project ☐



FEBRUARY 9, 2017
DAUBERMAN ROAD EXTENSION
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COMMENT FORM

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All comments submitted through February 23, 2017 will become part of the public meeting record.

Comments/Questions:

THE BIG ROCK PARK DISTRICT IS THE GOVERNMENTAL AGENCY
GOVERNING THE BIG ROCK TOWNSHIP AND THE AREA OF THIS
PROJECT. WE ARE CONCERNED WITH THE POTENTIAL RECREATIONAL
ASPECTS INVOLVED. CONNECTIVITY TO OUR COMMUNITY PARK
TRAIL BETWEEN OUR NORTHERN AND SOUTHERN SUBDIVISIONS, AND
A POTENTIAL BORROW PIT COULD MAKE A UNIQUE OPPORTUNITY FOR
A RECREATIONAL FEATURE AS WELL.

(Optional, Please Print)

Name/Affiliation GENE NEHRING BIG ROCK PARK DISTRICT

Address P.O. BOX 119

City/State BIG ROCK, IL Zip Code 60511

Phone No. 630-918-7831

E-Mail Address GCNEHRING@KOL.COM

I would NOT like to receive information on this project ☐



Kaneville Fire Protection District

46W536 Lovell Street P.O. Box 10
Kaneville, IL 60144-0010

RECEIVED
FEB 23 2017

KANE COUNTY
DIVISION OF TRANSPORTATION
Fire Station 630-557-2443
Fax 630-557-2201

Emergency 911

Bonnie Flock
Kane County Project Manager
Kane County Division of Transportation
41w011 Burlington Rd.
St. Charles, IL 60175

Re: Dauberman Rd. Extension US 30 to Granart Rd.

Dear Ms. Flock,

The Kaneville Fire Protection District is in full support of the above project due to the fact that we have a contract with the Big Rock Fire Protection District to provide us with Advanced Life Support/Paramedic ambulance service around the clock 365 days per year. Their use of the above grade railroad separation would be a great benefit to us as now they have to go to Dugan Rd. to the east or Shaw Rd. to the west if a BNSF railroad train is stopped at the Rhodes Ave. crossing in Big Rock and then come back to Dauberman Rd which is the most direct way up to our entire District. In addition, we have an access road to the Illinois Tollway (I88) at Dauberman Rd. We cover over 10 miles of the tollway for fire and EMS. Also, the Kaneland High School is at the intersection of Dauberman and Keslinger Rd. This grade separation will also help us in responding to mutual aid to BRFPD and other fire districts that we respond to south of the BNSF railroad.

So it is very important to the Kaneville Fire Protection District that this overpass is completed in an efficient and expedited manner. In addition, with the BNSF taking this rail line to double track from single track, train traffic will significantly increase from the present 25-32 trains per day to an unknown final number when completed in the not so distant future.

Respectfully,

Jim O'Connell
President

John Larkin
Sec/Treasurer
Lois Penninger
Trustee

David Sigmund
Fire Chief

Chief Dave Sigmund
Captain David Kovach
Lieutenant Gene Hunt
Lieutenant Jim Long
Lieutenant Brian Kaucnik

Captain Dan Kahl
Lieutenant Bart Needham
Lieutenant Jesse Sanchez
Lieutenant Harry Meyer

Dan Bruckelmeyer

Subject: FW: Dauberman Extension

From: Beverly Nachtrieb [<mailto:bnach2012@gmail.com>]

Sent: Tuesday, February 21, 2017 2:00 PM

To: KDOT Comments Email

Subject: Dauberman Extension

Under NO circumstances should this project be completed. It will bring minor benefit and cause extensive environmental problems.

Big Rock owns that stretch of Granart Road and it is already in terrible shape. The Village President wrote a newsletter article recently stating there is no money to fix it. Why increase/ double the traffic? Route 30 has up to 13,000 cars a day, Granart up to 6400. Dauberman only 1200. But if this goes in people will undoubtedly want to cut down to Jericho and other main roads that lead to **Hinckley and Yorkville, which increases the traffic on the already bad road, especially if they pick up Jericho from Dugan to get to Rte 47 and Ashe.**

As I understand it, two years ago the pot holes were so deep in some , areas they did not plow parts of it because no one really wanted to drop a blade in the deep holes. (unverified) It does not appear the County will take responsibility once the improvement is made.

Also, the intersection will be right at the curve, which is already the sight of the most accidents on Granart (by the study's own stats.) They said they would look into easing that curve but . . . the other side of the road is private farm land. And the commitment was far from definite.

The entire project will run behind Tenerelli - bringing all that dirt and noise with it. Why run a NS adjacent to a residential area? They should have done that at Dugan. What they did solved no problems - in fact, compounded a few. The tracks are still there and there are still backups. The circle is confusing and has several dangerous merges. I submit the project will increase that traffic not lesssen it.

The BR fire dept is concerned about being stopped at the tracks frequently. However, the average wait time is 2 minutes. **It will take longer than that to get to the Dauberman extension so that isn't going to help.**

That also does not address the environmental impact. This is farm land. People eat the food grown in this soil. More pollutants will not help.

The BIGGEST problem is the drainage. They showed the flood plain, the existing tiles in open field and the dry land. There are a lot of problems with the drainage - and they seemed to recognize that. One of the reps told me that the water would most likely go to Welch Creek. YOW! Last spring I watched the racoons fish in the circle portion of our driveway. wWe had close to 7' of rushing water in the meadows. The farmer next door lost half his crop. More water?????

I spoke to the Village President when they proposed the changes at Tenerelli. He told me they **expected** Welch Creek to flood. No solution proposed - except perhaps *some* remediation when they redid the bridge over the culvert. All they did was resurface the bridge. They **did** not widen the creek bed on the other side of Granart which would alleviate some of the problems.

And is this going to facilitate more train traffic? We don't need the impact of more trains that pollute and are possibly carrying hazardous material. 32+ trains are enough! Some communities, as I understand it, are compensated for that. Is Big Rock?

NO EXTENSION!!!!!!!

Respectfully submitted
Beverly Nachtrieb
46W192 Granart Road
Big Rock, IL 60511

708 525-2168

bnach2012@gmail.com

Dan Bruckelmeyer

Subject: FW: Dauberman Rd. Extention

From: Bill Dunteman [<mailto:bill@duntemanturf.com>]

Sent: Saturday, February 04, 2017 4:46 PM

To: KDOT Comments Email

Subject: Dauberman Rd. Extention

Greetings,

I am a Kane county farmer. I do not represent any specific group but I'm sure the agricultural community would agree with me on my viewpoint.

The recent construction of the turnabout, and primarily the taller curbing on eastbound Granart Rd., did not take into consideration the width of farm equipment. The tolerance of the curb to curb inner dimension is narrow than larger farm equipment and is harmful to the tires. Some tire widths may approach 17' not to mention overall widths exceeding 18'. The remedy? Shallow curbing on at least one side, or a wider gauging curb to curb.

Likewise, The grade crossing on Rhodes St. is too crowded with the newly installed divided lane concept.

Adding the Dauberman overpass, or underpass, will be of tremendous help to relieve the obstruction listed above. Please consider the following guidelines: 20' width for farm equipment, per direction, considering meeting of equipment from two directions. Three lane minimum, with 40' guardrail, light pole, and signage clearance. This would allow most equipment to squeeze by. And, 15' minimum vertical clearance for a underpass.

Thank you!

Bill Dunteman

Dunteman Turf & Grain Farms

Cell # 630/774-7167

Dan Bruckelmeyer

Subject: FW: Dauberman Road Extension Public Meeting Comments and Questions for Record

From: Bridget And Tim's Email Account [<mailto:tandb36@mchsi.com>]

Sent: Saturday, February 25, 2017 12:39 AM

To: Flock, Bonnie

Subject: Dauberman Road Extension Public Meeting Comments and Questions for Record

Ms. Flock, Please consider these questions as a part off the meeting's public record.

I am writing about the potential road extension of Dauberman to Granart. Rd. I have concerns that the Tenorrelli Subdivision will be impacted by a significant increase in noise and light pollution from the additional traffic through what is now a quiet corn field at the edge of our quiet neighborhood.

There have also been several sightings of Bald Eagles (an endangered species) in our area feeding, flying and soaring in this area. How will endangered species habitat be protected?

over and near Tenorrelli Subdivision and the potential road construction area.

Will there be an environmental impact study to consider the impacts to the local wildlife, flora and fauna? Will there be an archaeological study to determine if there are any Native American artifacts in the field? There have been known finds in this area. There is a collection at the Big Rock Library, many artifacts were donated by a collector or from farmers in this region.

I'm also concerned for the potential increase in traffic and potential accidents along an already dangerous curve on Granart Road. The southern extent near the curve is also a potential wetland and hydric soils that should be avoided. moving the potential road to the east, moves the potential road closer to the subdivision, increasing stormwater runoff, dust, noise and light pollution. The Tenorelli Subdivision (and his potential road extension) is in-between two high quality waterbodies in the Big Rock Creek Watershed. Additional traffic brings additional pollution.

This road construction would also take away additional farmland, prime farmland or farmland of statewide importance? Will there be plans to protect the loss of farmland and soil? Will there be air pollution impact studies? How will our air, soil and groundwater be protected from construction and operational impacts to the soil/farmland and shallow groundwater in this area?

I am concerned this road will invite additional urban sprawl to our quiet countryside. Many of us who live in Big Rock are quite fond of the quiet countryside and ability to see the stars at night. With the nearby Airport to the east, our western night sky is a star-filled treasure not to be lost to traffic lights. Would there be an addition of traffic lights or another roundabout at the proposed road connection to Granart? The curve along Granart is already a safety issue. How would safety be considered with the additional traffic? The addition of traffic also brings additional air pollution and with construction, the loss of trees which provide habitat for the bald eagles and carbon dioxide to help clean the air. How will tree loss and habitat for endangered species be mitigated?

The increase of traffic will ultimately bring the potential urban sprawl and additional housing.

As shown on one of your maps at the meeting, there is a proposed Metra Train station to the east of Welsh Creek. This would not be a welcome addition of additional noise and traffic, light pollution and several environmental impacts to the east side of Tenorelli.

Will there be foot paths into town to be able to connect to the Tenorelli subdivision to the Park, or connecting to Second Street, or across Rt. 30 to the Middle School as a part of this project Plan?

Finally, what are the alternate routes you are considering and what is the purpose and need for this road extension to connect to Granart?

Thank you for your consideration.

Bridget Bulthaup

Tenorelli Subdivision Resident

46w597 Dolly Drive

Big Rock, IL

DAUBERMAN ROAD EXTENSION

PHASE I STUDY

ATTACHMENT 3

**MAILED INVITATION LETTER, RECIPIENTS,
AND NOTIFICATION AREA**

KANE COUNTY

DIVISION of TRANSPORTATION

Carl Schoedel, P.E.
Director of Transportation
County Engineer



41W011 Burlington Road
St. Charles, IL 60175
Phone: (630) 584-1170
Fax: (630) 584-5265

January XX, 2017

Name
Address
City, IL zip

Re: **Dauberman Road Extension – Public Information Meeting**

To Whom It May Concern:

The Kane County Division of Transportation wishes to formally invite you to attend an Introductory and Scoping Public Meeting to be held concerning the potential extension of Dauberman Road from US Route 30 to Granart Road, with a potential grade-separated crossing at the BNSF Railroad and US Route 30.

Date: Thursday, February 9, 2017
Time: 5:00 P.M. to 7:30 P.M.
Location: Hinckley-Big Rock Middle School
47W984 US Route 30
Big Rock, IL 60511

The Public Meeting will be conducted on an informal open house format, with two 20-minute presentations at 5:30 P.M. and 6:30 P.M. Exhibits will also be available for viewing and discussion with project staff. The purpose of this public meeting is to introduce the project, describe the current planning process, review the existing conditions, and gather input and information from the community and public. Input, comments, and questions will be solicited on the project. Written statements will be received at the time of the meeting or can be mailed after the meeting to **Bonnie Flock, Project Manager, Kane County Division of Transportation, 41W011 Burlington Road, St. Charles, IL 60175**. Comments and questions are welcome at any time, but to be part of this meeting's record it must be received by the County on or before February 23, 2017 by close of business.

In compliance with the American with Disabilities Act and other Federal and State Laws, the Public Meeting will be accessible to those with disabilities. If any additional assistance is required, please contact Bonnie Flock, Project Manager, by telephone at (630) 406-7359 or by email at FlockBonnie@co.kane.il.us at least five (5) business days prior to the meeting such that any additional arrangements can be made.

Sincerely,

Carl Schoedel, P.E.
Director of Transportation/County Engineer



**KANE COUNTY
DIVISION OF TRANSPORTATION**

**DAUBERMAN ROAD EXTENSION
PHASE 1 STUDY**

PUBLIC MEETING NOTIFICATION AREA

BY: DBB DATE: 01/24/17 SCALE: N.T.S.

PREFIX	GREETING	LAST NAME	FIRST NAME	AGENCY	TITLE	ADDRESS	CITY	STATE	ZIP
THE HONORABLE	SENATOR	DURBIN	RICHARD	UNITED STATES SENATE	SENATOR	230 S. DEARBORN ST. SUITE 3892	CHICAGO	IL	60604
THE HONORABLE	SENATOR	DUCKWORTH	TAMMY	UNITED STATES SENATE	SENATOR	P.O. BOX 10793	CHICAGO	IL	60610
THE HONORABLE	CONGRESSMAN	HULTGREN	RANDY	UNITED STATES HOUSE OF REPRESENTATIVES	STATE REPRESENTATIVE	40W310 LAFOX ROAD #F2	CAMPTON HILLS	IL	60175
THE HONORABLE	REPRESENTATIVE	PRITCHARD	ROBERT	ILLINOIS HOUSE OF REPRESENTATIVES	ILLINOIS STATE REPRESENTATIVE	2600 DEKALB AVE.	SYCAMORE	IL	60178
THE HONORABLE	SENATOR	SYVERSON	DAVE	ILLINOIS STATE SENATE	ILLINOIS STATE SENATOR	200 S. WYMAN SUITE #302	ROCKFORD	IL	61101
MS.	SUPERVISOR	CARR	SANDRA	BIG ROCK TOWNSHIP	SUPERVISOR	PO BOX 252	BIG ROCK	IL	60511
MS.	EXECUTIVE DIRECTOR	MEYERS	MONICA	FOREST PRESERVE DISTRICT OF KANE COUNTY	EXECUTIVE DIRECTOR	1996 SOUTH KIRK ROAD, SUITE 320	GENEVA	IL	60134
THE HONORABLE	CHAIRMAN	LAUZEN	CHRIS	KANE COUNTY BOARD	KANE COUNTY BOARD CHAIRMAN	719 S. BATAVIA AVE.	GENEVA	IL	60134
MR.	KANE COUNTY BOARD	LENERT	BILL	KANE COUNTY BOARD	KANE COUNTY BOARD	719 S. BATAVIA AVE.	GENEVA	IL	60134
MR.	KANE COUNTY BOARD	KENYON	MICHAEL	KANE COUNTY BOARD	KANE COUNTY BOARD	719 S. BATAVIA AVE.	GENEVA	IL	60134
MR.	KANE COUNTY BOARD	FRASZ	DREW	KANE COUNTY BOARD	KANE COUNTY BOARD	719 S. BATAVIA AVE.	GENEVA	IL	60134
MR.	SUPERVISOR	ROWE	TOM	SUGAR GROVE TOWNSHIP	SUPERVISOR	54 SNOW STREET	SUGAR GROVE	IL	60554
MR.	PRESIDENT	HUMMELL	DEAN	VILLAGE OF BIG ROCK	PRESIDENT	PO BOX 128	BIG ROCK	IL	60511
MR.	PRESIDENT	MICHELS	P. SEAN	VILLAGE OF SUGAR GROVE	PRESIDENT	10 S MUNICIPAL DRIVE	SUGAR GROVE	IL	60554
THE HONORABLE	CHAIRMAN	GRYDER	SCOTT	KENDALL COUNTY BOARD	CHAIRMAN	4630 MCLAREN DRIVE	OSWEGO	IL	60543
MR.	HIGHWAY CHAIRMAN	KELLONG	MATT	KENDALL COUNTY HIGHWAY CHAIRMAN	CHAIRMAN	9750 SCHLAPP ROAD	YORKVILLE	IL	60560
MS.	VICE CHAIRMAN	CULLICK	LYNN	KENDALL COUNTY BOARD	KENDALL COUNTY BOARD	232 ANGELA CIRCLE	OSWEGO	IL	60543
MS.	KENDALL COUNTY BOARD	FLOWERS	ELIZABETH	KENDALL COUNTY BOARD	KENDALL COUNTY BOARD	1995 GRANDVIEW	MONTGOMERY	IL	60538
MR.	KENDALL COUTNY BOARD	DAVIDSON	BOB	KENDALL COUNTY BOARD	KENDALL COUNTY BOARD	244 JETER ROAD	PLANO	IL	60545
MR.	COUNTY ADMINISTRATOR	WILKINS	JEFF	KENDALL COUNTY	KENDALL COUNTY ADMINISTRATOR	111 W. FOX STREET	YORKVILLE	IL	60560
MR.	COUNTY ENGINEER	KLAAS	FRANCIS	KENDALL COUNTY	KENDALL COUNTY ENGINEER	6780 ROUTE 47	YORKVILLE	IL	60560
THE HONORABLE	CHAIRMAN	PIETROWSKI, JR.	MARK	DEKALB COUNTY BOARD	CHAIRMAN	200 N. MAIN STREET	SYCAMORE	IL	60178
MR.	COUNTY ENGINEER	SCHWARTZ	NATHAN	DEKALB COUNTY	DEKALB COUNTY ENGINEER	1826 BARBER GREENE ROAD	DEKALB	IL	60115

AGENCY	PREFIX	LAST NAME	FIRST NAME	TITLE	ADDRESS	CITY	STATE	ZIP
BIG ROCK FIRE DEPARTMENT	MR.	RUH	JOHN	FIRE CHIEF	47W863 E 2ND ST PO BOX 25	BIG ROCK	IL	60511
BIG ROCK HISTORICAL SOCIETY	MS.	BECK	SUE	PRESIDENT	48W455 HINCKLEY ROAD	BIG ROCK	IL	60511
BIG ROCK TOWNSHIP HIGHWAY DEPARTMENT	MR.	ROUSCH	RICK	HIGHWAY COMMISSIONER	47W860 E 2ND STREET	BIG ROCK	IL	60511
AURORA MUNICIPAL AIRPORT	MS.	PENESIS	BETH	INTERIM MANAGER	43W636 US ROUTE 30	SUGAR GROVE	IL	60554
BNSF RAILWAY - CHICAGO DIVISION	MR.	CAUGHRON	BRENNAN	PROJECT ENGINEER	3611 WEST 38TH STREET	CHICAGO	IL	60632
GIRL SCOUTS OF NORTHERN ILLINOIS					46W253 CAMP DEAN ROAD	BIG ROCK	IL	60511
HINCKLEY BIG ROCK CUSD #429	MR.	MCGUIRE	TRAVIS	SUPERINTENDENT	700 E LINCOLN HWY	HINCKLEY	IL	60520
HINCKLEY BIG ROCK ELEMENTARY SCHOOL	MS.	MELNYK	JULIE	PRINCIPAL	600 W LINCOLN HWY	HINCKLEY	IL	60520
HINCKLEY BIG ROCK HIGH SCHOOL	MR.	BRICKMAN	JAY	PRINCIPAL	700 E LINCOLN HWY	HINCKLEY	IL	60520
HINCKLEY BIG ROCK MIDDLE SCHOOL	MR.	STROUSS	JEFF	PRINCIPAL	47W984 US ROUTE 30	BIG ROCK	IL	60511
IDNR/OFFICE OF WATER RESOURCES					2050 WEST STEARNS ROAD	BARTLETT	IL	60103
ILLINOIS COMMERCE COMMISSION	MR.	VERCRUYSE	BRIAN	SENIOR RAIL SAFETY SPECIALIST	527 EAST CAPITOL AVENUE	SPRINGFIELD	IL	62701
ILLINOIS ENVIRONMENTAL PROTECTION AGENCY					1021 NORTH GRAND AVE. EAST, P.O. BOX 19276	SPRINGFIELD	IL	62794
ILLINOIS DEPARTMENT OF TRANSPORTATION	MR.	RIOS	JOSE	ACTING REGION 1 ENGINEER	201 WEST CENTER COURT	SCHAUMBURG	IL	60196
ILLINOIS DEPARTMENT OF TRANSPORTATION	MS.	MURPHY	KIMBERLY	CONSULTANT STUDIES UNIT HEAD	201 WEST CENTER COURT	SCHAUMBURG	IL	60196
ILLINOIS DEPARTMENT OF TRANSPORTATION	MS.	SOLOMON	MARILIN	FIELD ENGINEER	201 WEST CENTER COURT	SCHAUMBURG	IL	60196
ILLINOIS HISTORIC PRESERVATION AGENCY					1 N OLD STATE CAPITOL PLAZA	SPRINGFIELD	IL	62701
J MADDOCK AIRPORT	MR.	MADDOCK	JAY		PO BOX 232	SUGAR GROVE	IL	60554
KANE COUNTY ENVIRONMENTAL AND WATER RESOURCES DIVISION	MS.	WOLLNIK	JODIE	ASSISTANT DIRECTOR	719 S. BATAVIA AVE, BUILDING A	GENEVA	IL	60134
KANE COUNTY FARM BUREAU	MR.	WHITE	JOSEPH	PRESIDENT	2N710 RANDALL ROAD	ST CHARLES	IL	60174
KANE COUNTY SHERIFF'S OFFICE	MR.	KRAMER	DONALD	SHERIFF	37W755 IL RTE 38	ST CHARLES	IL	60175
KANE COUNTY SHERIFF'S OFFICE	MR.	BUMGARNER	TOM	CHIEF DEPUTY	37W755 IL RTE 38	ST CHARLES	IL	60175
KANE-DUPAGE SOIL & WATER CONSERVATION DISTRICT	MR.	FABRIZIUS	GERARD	CHAIRMAN	2315 DEAN STREET, SUITE 100	ST CHARLES	IL	60175
KANE-KENDALL COUNCIL OF MAYORS	MR.	KNAPP	PATRICK	REGIONAL PLANNING LIAISON	41W011 BURLINGTON ROAD	ST CHARLES	IL	60175
KANELAND HIGH SCHOOL	MS.	MARAS	JILL	PRINCIPAL	47W326 KESLINGER ROAD	MAPLE PARK	IL	60151
RIDE ILLINOIS	MR.	BARSOTTI	ED	CHIEF PROGRAMS OFFICER	2550 CHESHIRE DRIVE	AURORA	IL	60504
SUGAR GROVE CHAMBER OF COMMERCE					141 MAIN STREET	SUGAR GROVE	IL	60554
SUGAR GROVE DRAINAGE DISTRICT #1	MR.	JESSEMAN	SCOTT	PRESIDENT	54 SNOW STREET P.O. BOX 465	SUGAR GROVE	IL	60554
SUGAR GROVE FIRE DEPARTMENT	MR.	PERKINS	WILLIAM	FIRE CHIEF	25 S MUNICIPAL DRIVE	SUGAR GROVE	IL	60554
SUGAR GROVE POLICE DEPARTMENT	MR.	ROLLINGS	PAT	POLICE CHIEF	10 S MUNICIPAL DRIVE	SUGAR GROVE	IL	60554
SUGAR GROVE TOWNSHIP HIGHWAY DEPARTMENT	MR.	HUGGINS	GREG	HIGHWAY COMMISSIONER	54 SNOW STREET	SUGAR GROVE	IL	60554
US ARMY CORPS OF ENGINEERS					231 SOUTH LASALLE STREET, SUITE 1500	CHICAGO	IL	60604
US FISH & WILDLIFE SERVICES					1250 S. GROVE AVE. #103	BARRINGTON	IL	60010
VILLAGE OF BIG ROCK	MR.	MAY	TIM	CLERK	PO BOX 128	BIG ROCK	IL	60511
VILLAGE OF SUGAR GROVE	MR.	EICHELBERGER	BRENT	VILLAGE ADMINISTRATOR	10 S MUNICIPAL DRIVE	SUGAR GROVE	IL	60554
VILLAGE OF SUGAR GROVE (EEI ENGINEERING)	MS.	PIOTROWSKI	MICHELE	VILLAGE ENGINEER	10 S MUNICIPAL DRIVE	SUGAR GROVE	IL	60554
VILLAGE OF SUGAR GROVE	MR.	SPECIALE	TONY	DIRECTOR OF PUBLIC WORKS	601 HEARTLAND DRIVE	SUGAR GROVE	IL	60554
VILLAGE OF SUGAR GROVE	MR.	MAGDZIARZ	WALTER	COMMUNITY DEVELOPMENT DIRECTOR	10 S MUNICIPAL DRIVE	SUGAR GROVE	IL	60554
WELSH CEMETERY ASSOCIATION					PO BOX 145	BIG ROCK	IL	60511
FEDERAL HIGHWAY ADMINISTRATION	MR.	CHRIS	BYARS	TRANSPORTATION ENGINEER	3250 EXECUTIVE PARK DRIVE	SPRINGFIELD	IL	62703

ADDRESSEE (PROPERTY OWNER)	ADDRESS (PROPERTY OWNER)	CITY (PROPERTY OWNER)	STATE (PR)	ZIP (PROPERTY OWNER)
LESTER & VALERIE LANDMEIER	47W466 WHEELER ROAD	SUGAR GROVE	IL	60554
SANDRA BRACKETT	45W336 WHEELER ROAD	SUGAR GROVE	IL	60554
MARION & JAMES SCOTT	45W833 WHEELER RD	SUGAR GROVE	IL	60554-9534
WEST WIND PROPERTIES LLC	49W697 HINCKLEY RD	BIG ROCK	IL	60511-9324
F&H LEASING, INC.	1020 AIRPARK DRIVE	SUGAR GROVE	IL	60554
ERW LLC. C/O BRIAN LANG	1999 BUCKTAIL LANE, UNIT 1	SUGAR GROVE	IL	60554
OLD SECOND BANK TRUST #205349	37 S RIVER ST	AURORA	IL	60506
ERW LLC.	1999 BUCKTAIL LANE, SUITE 1	SUGAR GROVE	IL	60554
SCHEFF GROUP, LLC	1020 AIRPARK DRIVE	SUGAR GROVE	IL	60554
OLD SECOND NATIONAL BANK AURORA, GUM ROBERT	R F D Box 51	BIG ROCK	IL	60511
EDWARD A COX JR.	119 INDIAN TRAIL RD	OAKBROOK	IL	60523-2794
PAULA MESCH TRUST	P.O. BOX 127 - 222 E. MARKET ST.	PANORA	IA	50216
FRAZER & JUDITH ANN ECKLES	PO BOX 57	BIG ROCK	IL	60511
PAM BLACK	100 GALENA ST EAST RT 30	BIG ROCK	IL	60511
SUSAN L. DUKES	PO BOX 204 200 E GALENA ST	BIG ROCK	IL	60511
RYAN J. ZIMMERMAN	206 E GALENA	BIG ROCK	IL	60511
BANK OF NAPERVILLE, ALBIERO ERNEST F	PO BOX 75	BIG ROCK	IL	60511
BRIAN & KIM WARD	1775 FODDERSTACK MOUNTAIN LOOP	GREENEVILLE	TN	37745-2637
BRIAN J. COLWELL	47W518 RTE 30	BIG ROCK	IL	60511
SCOTT E & TRUDI SCHOGER	47W780 ROUTE 30, PO BOX13	BIG ROCK	IL	60511
ROBERT & SHIRLY SCHOGER	47W662 RTE 30	BIG ROCK	IL	60511
JAY & MARGARET MADDOCK	065772 DAUBERMAN RD PO BOX 232	SUGAR GROVE	IL	60554
EDMUND & STELLA BRONGIEL	9120 RTE 34	YORKVILLE	IL	60560
MARVIN L. & EUNICE K. GAPINSKE	47W366 RTE 30	BIG ROCK	IL	60511
JAMES R & MARY A PEPICH	47W302 RTE 30	BIG ROCK	IL	60511
OLD KENT BANK, EXECUTIVE CENTER	47W210 RTE 30	BIG ROCK	IL	60511
CORY SCHOLLMAYER	65578 DAUBERMAN RD	BIG ROCK	IL	60511
GARY LEE COLEMAN	212 N SCOVILLE AVE	OAK PARK	IL	60302
KAREN & CLAYTON HANNINEN	065920 DAVIS RD	BIG ROCK	IL	60511
WILLIAM M WACKERLIN	795 AUDREY AVE	AURORA	IL	60505-1003
ARLENE J STRAUS	65903 DAVIS RD	BIG ROCK	IL	60511-9475
RALPH & SUSAN HARKISON	PO BOX 3	BIG ROCK	IL	60511-0003
THOMAS IMBURGIA	209 OAK STREET	BIG ROCK	IL	60511
CATHERINE MILBURG	PO BOX 594	ELBURN	IL	60119
GEORGE & ASANEE ENGEL	PO BOX 315	BIG ROCK	IL	60511
DAVID K & LORRAINE K HALL	201 OAK ST	BIG ROCK	IL	60511
DREW OLSON	206 OAK ST	BIG ROCK	IL	60511
AARON W. LAHO	207 W GALENA	BIG ROCK	IL	60511
RODNEY D & CHERYL J BARTLETT	205 W GALENA RD	BIG ROCK	IL	60511
DONALD & JEAN WHITE	203 W RTE 30 - P O BOX 245	BIG ROCK	IL	60511
JORDAN S. BICKFORD	209 W GALENA BLVD	BIG ROCK	IL	60511
ANNA MAE MOODY	208 OAK ST	BIG ROCK	IL	60511-2028
THOMAS J & DAWN M LOSINIECKI	214 OAK ST	BIG ROCK	IL	60511
TERRY D & MARTHA L HUNT	1030 FOREST TRL	SUGAR GROVE	IL	60554-9253
ESTHERS PLACE LLC.	201 W GALENA WEST PO BOX 185	BIG ROCK	IL	60511
ANTHONY & CAROLE SMITH	7334 MARIDON RD	BIG ROCK	IL	60511
48 INC.	8930 WAUKEGAN RD STE 230	MORTON GROVE	IL	60053-2116
PATRICK H & JANIS E FINLEY	PO BOX 126 204 OAK ST	BIG ROCK	IL	60511-0126
BYRON G & DANA K INMAN	103 W GALENA ST	BIG ROCK	IL	60511
KATHY A. METZGER	PO BOX 29 101 W RTE 30	BIG ROCK	IL	60511
SAM A & VIRGINIA L WATERSTREET	105 W GALENA BOX 61	BIG ROCK	IL	60511
HENRY L BAGGE	14527 BASSWOOD RD	NEW AUBURN	WI	54757
LISA WEISSINGER	PO BOX 44	BIG ROCK	IL	60511-0044
KENNETH E. CARLSON	400 WEST SECOND ST	BIG ROCK	IL	60511
MARVEL DAVIS, TRUSTEE	47W066 JERICHO RD	BIG ROCK	IL	60511
GEORGE & ASANEE ENGEL	PO BOX 315	BIG ROCK	IL	60511
RON CHRISTOFFEL	13800 BUNNY LN	SOMONAUK	IL	60552
MID-CITY NATIONAL OF CHICAGO, ALBIERO ERNEST F	RTE 30 & LINCOLN HIGHWAY	BIG ROCK	IL	60511
PATRICIA A JOVANOVIC	PO BOX 234 208 W GALENA RD	BIG ROCK	IL	60511-0234
THOMAS CANZONERI	102 W 1ST ST	BIG ROCK	IL	60511
BARBARA CAESAR	1004 STATE ROAD 29	SPARLAND	IL	61565
SHRADER PROPERTY LLC, .	PO BOX 88	BIG ROCK	IL	60511
FREDERICK E. WILLIS	103 LINCOLN PO BOX 183	BIG ROCK	IL	60511
JOE LUDWIG LLC.	055500 DAVIS RD	BIG ROCK	IL	60511
DOUG & GINA K ALLEN	PO BOX 162	BIG ROCK	IL	60511
ROBERT & ELIZABETH NOKES	205 LINCOLN ST PO BOX 222	BIG ROCK	IL	60511
FIRST BAPTIST CHURCH OF BIG ROCK	201 LINCOLN AVE	BIG ROCK	IL	60511
JOSEPH & THERESA MORAR	200 S LINCOLN AVE BOX 273	BIG ROCK	IL	60511
GLENN A & ELIZABETH A FRANKLIN	204 S LINCOLN	BIG ROCK	IL	60511
JASON & DEBORAH E HALSTEAD	206 LINCOLN ST	BIG ROCK	IL	60511
THERESA M WASSON	14869 BUDD RD	YORKVILLE	IL	60560-9544
ROBERT & JAMIE BRASFIELD	209 RHODES AVE	BIG ROCK	IL	60511
ERNEST & DEBORAH L UEBEL	P O DRAWER 3318	LITTLE ROCK	AR	72203
GENE C & CYNTHIA L NEHRING	PO BOX 119	BIG ROCK	IL	60511-0119
ENGLISH CONGREGATIONAL CHURCH OF CHRIST	PO BOX 156	BIG ROCK	IL	60511
ALBERT E & EVELYN L LAWRENCE	BOX 215	BIG ROCK	IL	60511
WILLIAM WERNER & LAWRENCE CONANT	101 W FOURTH ST	BIG ROCK	IL	60511
DENNIS & NANCY FLANAGAN	305 RHODES RD	BIG ROCK	IL	60511
DAN K & BRENDA M ROMPF	307 RHODES AVE	BIG ROCK	IL	60511
JOHN H & DEBORAH A RUH	PO BOX 59	BIG ROCK	IL	60511
TRUST #102, MARY S ROGERSON, TRUSTEE	75515 TIMBERVIEW DR	BIG ROCK	IL	60511-9363
JOHN E. HAKE	104W FOURTH ST PO BOX 233	BIG ROCK	IL	60511

JACK & DOROTHY HAKE	PO BOX 104 403 RHODES ST	BIG ROCK	IL	60511-0104
KYLE D. DONKA	405 RHODES ST	BIG ROCK	IL	60511
DAVID A & JENNIFER S BAYLER	407 RHODES AVE	BIG ROCK	IL	60511
MICHAEL D. LONG	409 RHODES PO BOX 122	BIG ROCK	IL	60511
JESSICA M. CLARK	100 W 4TH ST	BIG ROCK	IL	60511
VINCENT R STRAZZABOSCO	75401 RHODES ST	BIG ROCK	IL	60511-9414
JAMES T & PATSY L RAYMOND	48W008 PRICE RD	BIG ROCK	IL	60511
ZACHARY L FRANZEN	75673 WELTON DR	BIG ROCK	IL	60511
TERRY BUTTS & BARBARA COLE	PO BOX 310	BIG ROCK	IL	60511-0310
ERIC ANDERSON & TRACI UNDERWOOD	75641 WELTON DR	BIG ROCK	IL	60511-9409
KEVIN D & DONNA L HOGLE	75623 WELTON DR	BIG ROCK	IL	60511
GARY E & MARY ELIZABETH HAUGE	75626 WELTON DR	BIG ROCK	IL	60511
DOUGLAS & JENNIFER SCHLACHTER	48W060 MARY ST	BIG ROCK	IL	60511
ZACHERY J RUH	075672 WELTON DR	BIG ROCK	IL	60511
ROBERT E & ANGELA M CAREY	075702 WELTON DR	BIG ROCK	IL	60511
CHARLES & SHARON FISHER	75530 RHODES RD	BIG ROCK	IL	60511
GARY M & PHYLLIS V INMAN	075541 WELTON DR	BIG ROCK	IL	60511
TIMOTHY B & MARY S KENNER	75557 WELTON DR	BIG ROCK	IL	60511
RICKY L & CARA RAUSCH	75571 WELTON DR	BIG ROCK	IL	60511
CESAER M & NICOLE M ENVERGA	75587 WELTON DR	BIG ROCK	IL	60511
MICHAEL J & SANDRA M UTES	75601 WELTON DR	BIG ROCK	IL	60511
JAMES A & RUTHANN DUDEK	48W124 MARY ST	BIG ROCK	IL	60511
GARY R & DEBORAH J PETERSEN	75518 WELTON DR	BIG ROCK	IL	60511
COLLEEN S CHRISTENSEN	75546 MARY ST	BIG ROCK	IL	60511
DENNIS W & DENISE A SNYDER	75560 MARY ST	BIG ROCK	IL	60511
MARY FRANCIS WELTON	PO BOX 236	BIG ROCK	IL	60511
JAMES A. FEIFHEIT	PO BOX 125	BIG ROCK	IL	60511
ALAN & DANA ARRIGHI	075634 MARY ST	BIG ROCK	IL	60511
NANCY M ANDERSON & STANLEY P SINKS	75660 MARY ST	BIG ROCK	IL	60511
MICHAEL R & JOAN BLAIR	75670 MARY ST	BIG ROCK	IL	60511
GERARD J & CYNTHIA S POLFER	48W095 MARY ST	BIG ROCK	IL	60511
TIMOTHY P & MARY A STOLA	75558 WELTON DR	BIG ROCK	IL	60511
RONALD D & ELLEN M WEITEN	75572 WELTON DR	BIG ROCK	IL	60511
WILBERT & RUTH HAGEMAN	75596 WELTON DR	BIG ROCK	IL	60511-9408
BYRON H JR & THERESA A SCOTT	075608 WELTON DR	BIG ROCK	IL	60511
CYNTHIA J & DALE L DEAN	48W100 PRICE RD	BIG ROCK	IL	60511
LAWRENCE L & JUDITH M CONANT	48W090 PRICE RD	BIG ROCK	IL	60511
MARK S & KRISTAN L NELSON	PO BOX 274	BIG ROCK	IL	60511-0274
JUSTYN ARNOLD	413 WILDER STREET	AURORA	IL	60506
KERRY & CYNTHIA KANE	104 RHODES AVE	BIG ROCK	IL	60511
KINGSTON MUTUAL INSURANCE CO, .	420 W MAIN ST	GENOA	IL	60135
SALIJA ZECIRI	954 BLACK WALNUT DR	SUGAR GROVE	IL	60554-9254
MICHAEL BARNES	PO BOX 244	BIG ROCK	IL	60511
DONOHUE FAMILY LIMITED PARTNERSHIP	1775 FODDERSTACK MOUNTAIN LOOP	GREENEVILLE	TN	37745-2637
ELMER J & GERTRUDE BERKE	PO BOX 186	BIG ROCK	IL	60511
JUSTIN E DOUGLAS	102 JEFFERSON AVE	BIG ROCK	IL	60511
MAX D & ROSALEE M WEEKLY	BOX 203	BIG ROCK	IL	60511
CARY L & DENISE K STEVENSON	47W893 GALENA RD	BIG ROCK	IL	60511
PATRICK & JULIE CURRY	211 E GALENA ST	BIG ROCK	IL	60511
MICHAEL D & BERNADETTE A DEAN	104 JEFFERSON	BIG ROCK	IL	60511
LEONARD A & SHIRLEY A WYSOCKI	215 E GALENA ST BOX 127	BIG ROCK	IL	60511
CYNTHIA A & BRYAN HANACEK	106 JEFFERSON AVE	BIG ROCK	IL	60511
SHELLY E JAMES	P O BOX 308, 213 E GALENA ST	BIG ROCK	IL	60511
JOANNE PORCH	PO BOX 85	BIG ROCK	IL	60511-0085
CHRISTOPHER A & MICHELLE I LAPEYRE	206 RHODES ST	BIG ROCK	IL	60511
BRIAN KAHL	203 N CENTER ST	PLANO	IL	60545
DANIEL A & AMY L MOTCHNIK	16475 NEWARK RD	NEWARK	IL	60541-9308
DARRIN A & CHERYL LEE	304 RHODES AVE	BIG ROCK	IL	60511
BARB & DENNIS BAJE	47W341 RTE 30, PO BOX 218	BIG ROCK	IL	60511
JAMES C & LYNDA BRUMMETT	BOX 154	BIG ROCK	IL	60511
RICHARD J & GINA R DUNLAP	310 RHODES	BIG ROCK	IL	60511
PATRICK J & DEBORAH M TESTIN	PO BOX 38 301 JEFFERSON AVE	BIG ROCK	IL	60511
WILLIAM P SCHWICKERT	303 JEFFERSON AVE	BIG ROCK	IL	60511
ANTHONY J & SALLY A KOZUBEK	307 JEFFERSON	BIG ROCK	IL	60511
ROBERT B & TINA M COOK	102 E FOURTH ST	BIG ROCK	IL	60511
EDWARD & MARY BRYANT	47W914 E SECOND ST	BIG ROCK	IL	60511
RAYMOND E TOMLIN	304 JEFFERSON AVE PO BOX 141	BIG ROCK	IL	60511-0141
ADAM OWEN	302 JEFFERSON ST	BIG ROCK	IL	60511
JOSEPH R ROSSLER	PO BOX 11	BIG ROCK	IL	60511
DON & NORMA MARQUARDT	PO BOX 143	BIG ROCK	IL	60511
HARRY R & SHIRLEY SCHOGER	47W662 ROUTE 30	BIG ROCK	IL	60511
RUH RENTALS LLC, RUSSELL S RUH, MANAGER	55230 SWAN RD	BIG ROCK	IL	60511-9772
DELORES SHRADER TRUST, VACATIONLAND INC	PO BOX 246	BIG ROCK	IL	60511-0246
GREGORY J & RONDA F BEELS	400 RHODES ST	BIG ROCK	IL	60511-9412
JOHN E & LAUREEN M DONAHUE-GROSSMAN	PO BOX 6	BIG ROCK	IL	60511-0006
JOHN & MARY ROGERSON	075515 TIMBER VIEW DR	BIG ROCK	IL	60511
JOHN W & RACHEL A WILLEY	407 JEFFERSON AVE	BIG ROCK	IL	60511
JAYNE D DOTY	1700 SHERMAN AVE	BIG ROCK	IL	60511
DOUGLAS DONALDSON	PO BOX 117	BIG ROCK	IL	60511
KEVIN T CRAWFORD	403 JEFFERSON AVE	BIG ROCK	IL	60511-2022
WILLIAM R SHIMKUS	402 JEFFERSON	BIG ROCK	IL	60511
ROBERT D & DAWN R BROWN	404 JEFFERSON AVE	BIG ROCK	IL	60511
DAVID E. CHRISTENSEN	406 JEFFERSON AVE PO BOX 279	BIG ROCK	IL	60511-0279

WILLIAM C & JANE E BRICKERT	408 JEFFERSON PO BOX 52	BIG ROCK	IL	60511
JUSTIN T CLARK	410 JEFFERSON AVE	BIG ROCK	IL	60511
NICHOLAS W GILLIS	101 EAST ST	BIG ROCK	IL	60511
BRADLEY M KOUKOL	103 FIFTH ST	BIG ROCK	IL	60511
WILLIAM H & SHEILA J VAN BUSKIRK	PO BOX 293	BIG ROCK	IL	60511
DENNIS E & PATRICIA J STORZEK	PO BOX 54	BIG ROCK	IL	60511-0054
FREDERICK G & RITA J SETTLES	47W986 SIXTH ST	BIG ROCK	IL	60511
MARTHA J RODDY	47W966 6TH ST	BIG ROCK	IL	60511-9417
DONALD BELCHER	47W954 6TH ST	BIG ROCK	IL	60511-9417
STEVEN E & MARCEY L LARSON	47W928 6TH ST	BIG ROCK	IL	60511
MARGARET SLOSAR & KRISTOPHER SCHRADER	47W908 6TH ST	BIG ROCK	IL	60511-9417
MICHAEL & CHARLES R ENCK	47W898 6TH ST	BIG ROCK	IL	60511
KELLY J KALIN	75384 MADISON AVE	BIG ROCK	IL	60511
JILL RIEDSTRA	75372 MADISON AVE	BIG ROCK	IL	60511
MAXIMILIAN SUCILLA	75362 MADISON AVE	BIG ROCK	IL	60511
ROBERT J & DEBRA RAYMOND	45W468 RAYMOND RD	BIG ROCK	IL	60511
HELEN WILES TRUST	075324 MADISON AVE	BIG ROCK	IL	60511
KELLY D SHAMBLIN	75310 MADISON AVE	BIG ROCK	IL	60511
JUDITH L & THOMAS E BLINCOE	75274 MADISON AVE	BIG ROCK	IL	60511
RICHARD T DAHLEEN	075286 MADISON ST	BIG ROCK	IL	60511
ERIC R & PEGGY SUE EDWARDS	PO BOX 64 400 JEFFERSON ST	BIG ROCK	IL	60511
KEITH MASUR	47W969 SIXTH ST	BIG ROCK	IL	60511
BONNIE J MEYER TRUST	75451 RHODES ST	BIG ROCK	IL	60511
JORGE URDANETA & ASHLEY PIKARSKI	75469 RHODES ST	BIG ROCK	IL	60511
TIMOTHY P WEIGAND	075483 RHODES AVE	BIG ROCK	IL	60511
W & M MCCARTY TRUST	1123 WALKER CT	BIG ROCK	IL	60511
WESTON W & KIMBERLY D WARD	47W910 TIMBERVIEW DR	BIG ROCK	IL	60511
MARK POSS	45W394 JOHN ST	BIG ROCK	IL	60511
PAUL MILLS	47W850 TIMBERVIEW DR	BIG ROCK	IL	60511
BRUCE & MARY THOMPSON	47W820 TIMBERVIEW DR	BIG ROCK	IL	60511
NEAL A MORRIS	47W794 CAREY ST	BIG ROCK	IL	60511
ROBERT J BUCKLEY	47W913 SIXTH ST	BIG ROCK	IL	60511
STEVE ALLEN	47W435 US HIGHWAY 30	BIG ROCK	IL	60511
RONALD R & GEORGE R REINERT	9685 CORNIELS RD	BRISTOL	IL	60512
DENNIS E. & BARBARA A. BAIE	PO BOX 218	BIG ROCK	IL	60511
GREYER TRUST	728 CARRIAGE HILL LN	SUGAR GROVE	IL	60554
ROBERT D. & BARBARA A. RAYMOND	85839 JOHN ST	BIG ROCK	IL	60511
EDWARD R. & SHARON LEMMER	47W481 RTE 30	BIG ROCK	IL	60511
STANLEY RADAVICH	47W379 ROUTE 30	BIG ROCK	IL	60511
SCOTT EDWARD & HEIDI G TREADMAN	75556 RHODES ST	BIG ROCK	IL	60511
TRENT M & ANN M BYRNE	075594 RHODES ST	BIG ROCK	IL	60511
LOWELL & JOANNE TOSCH	75620 RHODES ST	BIG ROCK	IL	60511
LOWELL D & JOANNE BLATNER TOSCH	75620 RHODES ST	BIG ROCK	IL	60511
DANIEL D & SUSAN E ZOHNER	075638 RHODES ST	BIG ROCK	IL	60511
LYNN M & C JOYCE NELSON	075660 RHODES ST	BIG ROCK	IL	60511
VINCENT F JR & BONITA J SMITH	47W972 PRICE RD	BIG ROCK	IL	60511
JIMMY L & JANET K FRANKLIN	75509 RHODES ST	BIG ROCK	IL	60511
CHRISTINE FREELAND	75529 RHODES ST	BIG ROCK	IL	60511
KURT F & DAWN M LINDQUIST	75547 RHODES	BIG ROCK	IL	60511
JAMES PERKIS & KIMBERLY SCHMITZ	075567 RHODES ST	BIG ROCK	IL	60511
STEPHEN T ALLEN	075841 RHODES RD	BIG ROCK	IL	60511
JOSEPH L & CATHERINE G STRATMAN	75536 TIMBERVIEW DR	BIG ROCK	IL	60511
GREGORY K & MARY E GREEN	47W940 TIMBER VIEW	BIG ROCK	IL	60511
DAVID E & ANDREA L BAKER	47W920 TIMBERVIEW DR	BIG ROCK	IL	60511
WILLIAM A & KAREN R THOMPSON	75582 TIMBERVIEW DR	BIG ROCK	IL	60511
LESLIE HEERDT	075773 RHODES ST	BIG ROCK	IL	60511
SCOTT W & GLORIA R BICKFORD	85015 RHODES AVE	BIG ROCK	IL	60511
HEERDT LAND COMPANY	75773 RHODES ST	BIG ROCK	IL	60511-9406
BARBARA S HOLMES	620 E GREEN ST	FARMER CITY	IL	61842
WAYNE & SHIRLEY BICKFORD	75932 RHODES AVE	BIG ROCK	IL	60511
LAWRENCE & MARYL HANKES	47W855 TIMBERVIEW DR	BIG ROCK	IL	60511
TIMOTHY J & KELLY M MAY	47W835 TIMBERVIEW DR	BIG ROCK	IL	60511
ROBERT & PATRICIAVANDYK	75565 TIMBERVIEW DR	BIG ROCK	IL	60511
WILLIAM E & MARY J WHATLEY	47W810 TIMBERVIEW DR	BIG ROCK	IL	60511
ELAINE A & EDWARD A WILLIAMS	75555 TIMBERVIEW DR	BIG ROCK	IL	60511
DEAN C & KIMBERLY A HUMMELL	47W799 TIMBERVIEW DR	BIG ROCK	IL	60511
DUANE CLARK	47W815 TIMBERVIEW DR	BIG ROCK	IL	60511
MARK D FRANSEN	47W845 TIMBERVIEW DR	BIG ROCK	IL	60511
BRAD A & SANDRA M CLAYPOOL	47W875 TIMBER VIEW DR	BIG ROCK	IL	60511
SEIFERT FAMILY TRUST	47W915 TIMBERVIEW DR	BIG ROCK	IL	60511
CHARLES D & PAULA JEAN SABOTTA	47W941 TIMBERVIEW DR	BIG ROCK	IL	60511
KIM M THORSON	75838 RHODES AVE	BIG ROCK	IL	60511-9392
ROBERT GWINN II	1038 CHAMBOARD LN	HOUSTON	TX	77018
VIRGINIA BAUMANN	949 SOUTH BATAVIA AVE	GENEVA	IL	60134
BRUCE C. & SHIRLEY R. THOMPSON	205 OAK ST, PO BOX 113	BIG ROCK	IL	60511
JAMES M. JR & GERMAINE GEMBECK	46W563 RTE 30	SUGAR GROVE	IL	60554
MICHAEL C. DUGAN	46W637 RT30	SUGAR GROVE	IL	60554
DAVID & DEBORAH ROSENWINKEL	PO BOX 219	BIG ROCK	IL	60511
HOWARD A & DOROTHY A DALE	75202 MARY DR	BIG ROCK	IL	60511
PAUL A & ASHLEE A DRAWZ	46W570 DOLLY DR	BIG ROCK	IL	60511
KIMBERLY L AUGUSTINE	46W700 DOLLY DR	BIG ROCK	IL	60511
JASON M & SARAH C ALLEN	46W680 DOLLY DR	BIG ROCK	IL	60511
MICHELE T BLAIR	46W660 DOLLY DR	BIG ROCK	IL	60511

DENNIS M & CATHLEEN P OWEN	46W640 DOLLY DR	BIG ROCK	IL	60511
KEVIN L & CASSANDRA R SCHRAM	46W620 DOLLY DR	BIG ROCK	IL	60511
HENRY & SONJA WASHBURN	46W600 DOLLY DR	BIG ROCK	IL	60511
GREGORY W PEARL	7S203 MARY DR	BIG ROCK	IL	60511
MARK E & MARCIA L LEE	7S217 MARY DR	BIG ROCK	IL	60511
DANIEL S KNIGHT	46W628 GRANART RD	BIG ROCK	IL	60511
JORDAN MROWCZYNSKI	46W618 GRANART RD	BIG ROCK	IL	60511
DANIEL A & KARLA L DAVIS	46W600 GRANART RD	BIG ROCK	IL	60511
MARISA WALSH	46W580 GRANART RD	BIG ROCK	IL	60511
JOHN M & MARY J WALT	46W560 GRANART RD	BIG ROCK	IL	60511
KAREN M FORSELL	46W540 GRANART RD	BIG ROCK	IL	60511
JAY A & BRENDA L TAYLOR	46W528 GRANART RD	BIG ROCK	IL	60511
DAVID & MARCE SUMMER	46W545 KATIE DR	BIG ROCK	IL	60511
JONATHAN D & SARAH J CULVER	7S360 MARY DR	BIG ROCK	IL	60511
JASON GADE & SARAH HUCKS	46W645 KATIE DR	BIG ROCK	IL	60511
JON C & LORI B SIMON	46W625 KATIE LN	BIG ROCK	IL	60511
JEFFREY A & KRISTINE M ODWYER	46W603 KATIE DR	BIG ROCK	IL	60511
LINDA BEAVERS	46W575 KATIE DR	BIG ROCK	IL	60511
VIP HOLDING COMPANY	21791 W HWY 20	GRAYSLAKE		60030
ANTHONY L II & KIMBERLY A DANNA	105 SHANNON RUN RD	VERSAILLES	KY	40383-8698
DALE F & DEBRA A KRAHN	7S270 MARY DR	BIG ROCK	IL	60511
FLORIAN CZINKI	231 PEARL ST	BIG ROCK	IL	60511
REIDY W & MARIA FORTH	46W550 KATIE DR	BIG ROCK	IL	60511
ADAM D & JEANETTE T MARTIN	46W691 DOLLY DR	BIG ROCK	IL	60511
PATTI JEAN & VERNON DALE KAHLE	46W681 DOLLY DR	BIG ROCK	IL	60511
JOHN KESSLER	46W655 DOLLY LANE	BIG ROCK	IL	60511
DAVID W & DONNA S RICHARDSON	46W635 DOLLY LANE	BIG ROCK	IL	60511
LYNN W & DIANA L GOBEN	46W615 DOLLY LANE	BIG ROCK	IL	60511
TIMOTHY E & BRIDGET A BULTHAUP	46W597 DOLLY DR	BIG ROCK	IL	60511
STEVEN T & PATRICIA WEEKS	46W652 KATIE DR	BIG ROCK	IL	60511
LORI MARCO	46W650 KATIE DR	BIG ROCK	IL	60511
RICHARD & SHARON LEE KOKES	46W622 KATIE LANE	BIG ROCK	IL	60511
PAUL C & PHYLLIS L NICKELS	46W598 KATIE DR	BIG ROCK	IL	60511
JACK W & PATRICIA L SWIFT	46W578 KATIE LN	BIG ROCK	IL	60511
PETER A BLOCKER	46W370 GRANART RD	BIG ROCK	IL	60511
RITA J JANDT	46W460 GRANART RD	BIG ROCK	IL	60511
TERRY L & JUDY K MINOR	46W440 GRANART RD	BIG ROCK	IL	60511
MICHAEL SCOTT KRAUS	46W420 GRANART RD	BIG ROCK	IL	60511-9791
ANTHONY L & MARY L SIMMONS	46W330 GRANART RD	BIG ROCK	IL	60511
FRANCISCO & GABRIEL SORIA	46W304 GRANART RD	BIG ROCK	IL	60511-9790
JOHN C BRAUER	7S401 MARY DR	BIG ROCK	IL	60511
HOWARD & REBECCA ROSENWINKEL	16007A FRAZIER RD	PLANO	IL	60545
LARRY L JONES	PO BOX 3522	ST. CHARLES	IL	60174-9091
JAMES F KOVAC & ABIGAIL LIND	7S297 MARY DR	BIG ROCK	IL	60511
VIRGINIA GARZA	07S321 MARY DR	BIG ROCK	IL	60511
JILL M & ROBERTO OLIVARES	7S333 MARY DR	BIG ROCK	IL	60511
JOHN C & CARRIE L LOEHMANN	7S357 MARY DR	BIG ROCK	IL	60511
DAVID T & VICKI L HELLYER	46W390 GRANART RD	BIG ROCK	IL	60511
JOSEPH F & GINA M WEBER	46W360 GRANART RD	BIG ROCK	IL	60511
DAVID J L & JONI K LAMON	PO BOX 158	BIG ROCK	IL	60511
RICHARD H & PENELOPE LEE	07S291 DOLLY DR	BIG ROCK	IL	60511
SHANNON E MISUKANIS	07S241 DOLLY DR	BIG ROCK	IL	60511
BURTON & PATRICIA CALLMER	46W137 WHILDIN RD	BIG ROCK	IL	60511
WAYNE E & SUSANNAH M LEE	46W028 GRANART RD	BIG ROCK	IL	60511
LYDIA ROBERTS TRUSTEE	PO BOX 118	ROCHELLE	IL	61068-0118
SCOTT D PAPENDICK	23W567 PINE DR	CAROL STREAM	IL	60188-2685
SUMMERS FAMILY TRUST	07S295 CAMP DEAN RD	BIG ROCK	IL	60511
CHRISTINE DAVIS	196 MAPLE RIDGE LN	MONTGOMERY	IL	60538
RICHARD W ANTHONY	7S295 CAMP DEAN RD	BIG ROCK	IL	60511
MATTHEW T & MICHELLE M HANSEN	7S323 CAMP DEAN RD	BIG ROCK	IL	60511-9761
MARK & SARAH M HEDBORN	7S349 CAMP DEAN RD	BIG ROCK	IL	60511-9761
JOSE A & CAROLYN J VELIZ	7S359 CAMP DEAN RD	BIG ROCK	IL	60511
LESTER L & MARY L HUME	7S405 CAMP DEAN RD	BIG ROCK	IL	60511
J R INVESTMENTS LLC C/O JEROME A RICH	PO BOX 610	SUGAR GROVE	IL	60554
STEPHEN J MURRAY	PO BOX 280 45W740 GRANART RD	BIG ROCK	IL	60511-0280
JEROME A RICH	07S771 DUGAN RD	SUGAR GROVE	IL	60554
JEROME A & BETTY ANN RICH	PO BOX 610	SUGAR GROVE	IL	60554
JEROME A RICH	PO BOX 369	SUGAR GROVE	IL	60554
RICHARD O'BRIEN TRUST	133 WALNUT CIR	SUGAR GROVE	IL	60554-4221
JANIECO LLC	11 DUFFY LN	SUGAR GROVE	IL	60554
FRANKS CLASSIC CARS LLC	5 DUFFY LN	SUGAR GROVE	IL	60554-9591
ERIC HOLBY	43W510 TALL OAK TRAIL	ELBURN	IL	60119
6 DUFFY LANE LLC, C/O FRANK E KARABETSOS	5 DUFFY LN	SUGAR GROVE	IL	60554-9591
DON RAY LEASING LLC	49W697 HINCKLEY RD	BIG ROCK	IL	60511
RICH HARVEST FARMS CO	PO BOX 610	SUGAR GROVE	IL	60554-0610
OLD SECOND BANK TRUST, OLD SECOND NATL BANK TR #205349	37 S RIVER ST	AURORA	IL	60506-4172
JOSEPH & SHARON RITCHIE	2135 CITY GATE LN STE 420	NAPERVILLE	IL	60563-3062
DAVID T & MARIE G COX	8S076 RHODES ST	BIG ROCK	IL	60511
THOMAS E EVANS	8S097 RHODES ST	BIG ROCK	IL	60511-9482
NICHOLAS & KATIE BRANDONISIO	08S488 GRANART ROAD	BIG ROCK	IL	60511
GERALD & ELIZABETH L INGLES	RR 1	BIG ROCK	IL	60511
GREGORY S & MICHAEL D KLOTZ	48W340 JERICHO RD	BIG ROCK	IL	60511-9209
MICHAEL J & MARLYN K HOLLEMBEAK COTTE	BOX 103 - GRANART RD	BIG ROCK	IL	60511

ALAN T & LISA J ASHER	PO BOX 213	BIG ROCK	IL	60511
JOSEPH & SHARON RITCHIE	2135 CITY GATE LN STE 420	NAPERVILLE	IL	60563
EVERETTE & KATHLEEN WHILDEN	75353 DUGAN ROAD	SUGAR GROVE	IL	60554
ANASTACIA FLYNN TRUSTEE	13 WALNUT CIRCLE	SUGAR GROVE	IL	60554
JAMES & SHARON DODD	75437 DUGAN ROAD	SUGAR GROVE	IL	60554
SCOT INDUSTRIES INC	PO BOX 309	SUGAR GROVE	IL	60554
CHICAGO LAND TITLE TRUST CO.	10 S. LASALLE ST. 2750	CHICAGO	IL	60603
GARY & JUDITH HATTERSLEY TRUST	P.O. BOX 19	SUGAR GROVE	IL	60554
GARY & JUDITH HATTERSLEY TRUST	P.O. BOX 19	SUGAR GROVE	IL	60554
JOHN BASTAIN	1998 BUCKTAIL LANE	SUGAR GROVE	IL	60554
JOHN & WILLIAM BASTAIN C/O HY-TEK MFG	412 BUCKTAIL LANE	SUGAR GROVE	IL	60554
JEFFREY & NANCY ORCZYKOWSKI	1999 FAYS LANE	SUGAR GROVE	IL	60554
SHAWN & KIRSTEN PIESKY	1995 FAYS LANE	SUGAR GROVE	IL	60554
JEFFREY CRISCI	1991 FAYS LANE	SUGAR GROVE	IL	60554
MARTIN JOSEPH VANA	1987 FAYS LANE	SUGAR GROVE	IL	60554
NORMAN & ELAINE ZIELKE	1983 FAYS LANE	SUGAR GROVE	IL	60554
KEVIN & AZUCENA RICK	1979 FAYS LANE	SUGAR GROVE	IL	60554
DONALD & JAMA FORD	1975 FAYS LANE	SUGAR GROVE	IL	60554
JOANN MCBETH	1967 FAYS LANE	SUGAR GROVE	IL	60554
JOHN & MARISUE NAGY	1971 FAYS LANE	SUGAR GROVE	IL	60554
THOMAS ELLIOT & JODIE POWELL	1950 FAYS LANE	SUGAR GROVE	IL	60554
ALAN & VICKI LETTOW	1954 FAYS LANE	SUGAR GROVE	IL	60554
MICHAEL & KELLY HISLOP	1946 FAYS LANE	SUGAR GROVE	IL	60554
RICHARD & GAIL COWHEN	303 HOLIDAY DRIVE	SOMONAUK	IL	60552
ADVANCED INSTALLATION CONCEPTS INC. C/O TOM BROUCH	721 QUEEN GATE CIRCLE	SUGAR GROVE	IL	60554
WARREN PROPERTIES - SUPERIOR METAL PRODUCTS	1993 BUCKTAIL LANE	SUGAR GROVE	IL	60554
BUCKTAIL PARTNERS, LLC	1960 BUCKTAIL LANE	SUGAR GROVE	IL	60554
DARLENE & RONALD VOGEL TRUSTEES	16557 E NICKLAUS DR	FOUNTAIN HILLS	AZ	85268
BRUCE BRUMMEL & NANCY MCCANN	1050 CYPRESS DRIVE	AURORA	IL	60506
BRUCE BRUMMEL & NANCY MCCANN	1050 CYPRESS DRIVE	AURORA	IL	60506
KIGYOS PARTNERSHIP	1981 BUCKTAIL LANE	SUGAR GROVE	IL	60554
TERI LEATHERBURY	1923 ANNETTE'S CIR	SUGAR GROVE	IL	60554
JOSEPH & BARBARA SCHAUBEL	1925 ANNETTE'S CIR	SUGAR GROVE	IL	60554
DONALD & MARY RICE	1927 ANNETTE'S CIR	SUGAR GROVE	IL	60554
RICHARD & TERESA WAGNER	1929 ANNETTE'S CIR	SUGAR GROVE	IL	60554
WALTER & JACQUELINE FLYTE	1931 ANNETTE'S CIR	SUGAR GROVE	IL	60554
MARK WAGNER	1933 FAYS LANE	SUGAR GROVE	IL	60554
MICHAEL DAVIES	1921 ANNETTE'S CIR	SUGAR GROVE	IL	60554
JOHN & TAMULA STRALKA	1919 ANNETTE'S CIR	SUGAR GROVE	IL	60554
DAVID THILL	1917 ANNETTE'S CIR	SUGAR GROVE	IL	60554
JACK HUGGINS	1915 ANNETTE'S CIR	SUGAR GROVE	IL	60554
BRIAN CLISH & TRESSA TIERNEY	1913 ANNETTE'S CIR	SUGAR GROVE	IL	60554
LAWRENCE JACKSON	1945 FAYS LANE	SUGAR GROVE	IL	60554
ANTHONY & HOLLIS ORY	1943 FAYS LANE	SUGAR GROVE	IL	60554
BARBARA SALOGA	1941 FAYS LANE	SUGAR GROVE	IL	60554
MICHAEL & PAULA PARRETTE	1937 FAYS LANE	SUGAR GROVE	IL	60554
KEITH & LISA ANDERSON	1935 FAYS LANE	SUGAR GROVE	IL	60554
STEPHEN PARISH	1909 ANNETTE'S CIR	SUGAR GROVE	IL	60554
RUCH & DONALD PLAUCK	1918 ANNETTE'S CIR	SUGAR GROVE	IL	60554
KEVIN & MARI JOHNSON	1916 ANNETTE'S CIR	SUGAR GROVE	IL	60554
MELINDA MCCORMACK	1914 ANNETTE'S CIR	SUGAR GROVE	IL	60554
NICHOLAS & SAMANTHA KNOBLAUCH	1920 ANNETTE'S CIR	SUGAR GROVE	IL	60554
CURT KARAS	1930 ANNETTE'S CIR	SUGAR GROVE	IL	60554
ANDREW & DEANE MAYERHOFER	1905 FAYS LANE	SUGAR GROVE	IL	60554
JOHN & GENELL THOMAS	1913 FAYS LANE	SUGAR GROVE	IL	60554
RALPH & MARY GABRIELSON	1909 FAYS LANE	SUGAR GROVE	IL	60554
BRIAN & DAWN AUTEN	1891 FAYS LANE	SUGAR GROVE	IL	60554
ELIZABETH RISCHL	1887 FAYS LANE	SUGAR GROVE	IL	60554
MICHAEL & MARY GROEN	1883 FAYS LANE	SUGAR GROVE	IL	60554
MARC & JENNIFER WISKUR	1879 FAYS LANE	SUGAR GROVE	IL	60554
CURT & NELDA KARAS	1875 FAYS LANE	SUGAR GROVE	IL	60554
CLARK JOHNSON	1871 FAYS LANE	SUGAR GROVE	IL	60554
JAMES J FRITZ, TRUSTEE	525 SUNSET ROAD	WINNETKA	IL	60093
STEVEN ANDERSON & LARENE KORTH	1907 ANNETTE'S CIR	SUGAR GROVE	IL	60554
DOUGLAS KOZELUH	1905 ANNETTE'S CIR	SUGAR GROVE	IL	60554
THOMAS HUGHES & NORA LONDON	1896 FAYS LANE	SUGAR GROVE	IL	60554
GARY & SANDRA KNUTH	1892 FAYS LANE	SUGAR GROVE	IL	60554
BARRY & LEAH MORSCH	1888 FAYS LANE	SUGAR GROVE	IL	60554
CHARLES & JUDY SMITH	1884 FAYS LANE	SUGAR GROVE	IL	60554
STEPHEN & JOAN PRESTAGE	1880 FAYS LANE	SUGAR GROVE	IL	60554
THOMAS & TRACY LIMBRUNNER	1876 FAYS LANE	SUGAR GROVE	IL	60554
GERALD & FRANCINE ADAMS	1872 FAYS LANE	SUGAR GROVE	IL	60554
STEPHANIE & MICHAEL KARTHEISER	1868 FAYS LANE	SUGAR GROVE	IL	60554
JOE & RHONDA SITTERLY	1900 FAYS LANE	SUGAR GROVE	IL	60554
HERMANN & INGE BUCHBERGER	1998 FAYS LANE	SUGAR GROVE	IL	60554
RONALD & BARBARA LEISHER	1994 FAYS LANE	SUGAR GROVE	IL	60554
BRADLEY & COREY FREIDERS	1990 FAYS LANE	SUGAR GROVE	IL	60554
WILLIAM & KAREN SHULL	1986 FAYS LANE	SUGAR GROVE	IL	60554
DONALD & CAROL PFEFFER	1982 FAYS LANE	SUGAR GROVE	IL	60554
DAVID & ANN LUKER	1978 FAYS LANE	SUGAR GROVE	IL	60554
CURTIS & DEBORAH MABERRY	1974 FAYS LANE	SUGAR GROVE	IL	60554
TRAVIS THONE	1970 FAYS LANE	SUGAR GROVE	IL	60554
ROBERT & SANDRA BOHLER	1966 FAYS LANE	SUGAR GROVE	IL	60554

TERRENCE & MARGARET GALLES	1962 FAYS LANE	SUGAR GROVE	IL	60554
KARL & DONNA HEINRICH	1958 FAYS LANE	SUGAR GROVE	IL	60554
DAVID & LINDA ARMBRUST	1942 FAYS LANE	SUGAR GROVE	IL	60554
BRENDA JANCO	1938 FAYS LANE	SUGAR GROVE	IL	60554
CHAD & BRIDGETTE OESTER	1934 FAYS LANE	SUGAR GROVE	IL	60554
STEPHEN & RUTH BELL	1930 FAYS LANE	SUGAR GROVE	IL	60554
GARY & NANCY CLUTE	1926 FAYS LANE	SUGAR GROVE	IL	60554
ROBERT HURST & SHARON DINICOLA	1922 FAYS LANE	SUGAR GROVE	IL	60554
RICHARD & MARGIT FOTRE	300 ANNETTE'S LANE	SUGAR GROVE	IL	60554
THOMAS & SANDRA DUFFY	304 ANNETTE'S LANE	SUGAR GROVE	IL	60554
CELESTINO & RENATA ECCHER	308 ANNETTE'S LANE	SUGAR GROVE	IL	60554
JAMES & KARYN PECHINSKI	1918 FAYS LANE	SUGAR GROVE	IL	60554
RICHARD & MARY LAFLEUR	1914 FAYS LANE	SUGAR GROVE	IL	60554
KEVIN WOBROCK	1912 FAYS LANE	SUGAR GROVE	IL	60554
GWEN BOCKMAN	1908 FAYS LANE	SUGAR GROVE	IL	60554
SCOTT & SHARON MARCZUK	1904 FAYS LANE	SUGAR GROVE	IL	60554
JOSEPH LIES	1901 WHISPERING OAKS LN	SUGAR GROVE	IL	60554
MODENE RUCCI	1905 WHISPERING OAKS LN	SUGAR GROVE	IL	60554
BRIDGETT & JASON KEDZIE	1909 WHISPERING OAKS LN	SUGAR GROVE	IL	60554
JOAN & THEODORE MCKEOWN	301 ANNETTE'S LANE	SUGAR GROVE	IL	60554
BEVERLY ELLEBY	305 ANNETTE'S LANE	SUGAR GROVE	IL	60554
KEITH & LAUREL JOHNSON	1917 WHISPERING OAKS LN	SUGAR GROVE	IL	60554
DENISE CROSBY	1922 WHISPERING OAKS LN	SUGAR GROVE	IL	60554
PHILLIP MAULDING	1912 WHISPERING OAKS LN	SUGAR GROVE	IL	60554
KENNETH SCHAUBEL & SHELLY LADD	1908 WHISPERING OAKS LN	SUGAR GROVE	IL	60554
WILLIAM & COLLEEN HOWARD	1904 WHISPERING OAKS LN	SUGAR GROVE	IL	60554

DAUBERMAN ROAD EXTENSION
PHASE I STUDY
ATTACHMENT 4
CERTIFICATIONS OF PUBLICATION

CHICAGO TRIBUNE

media group

Sold To:

TransSystems - CU00042064

1475 E Woodfield Rd

Schaumburg, IL 60173

Bill To:

TransSystems - CU00042064

1475 E Woodfield Rd

Schaumburg, IL 60173

Certificate of Publication:

Order Number: 4735210

Purchase Order: N/A

State of Illinois - Kane

Chicago Tribune Media Group does hereby certify that it is the publisher of the Aurora Beacon News. The Aurora Beacon News is a secular newspaper, has been continuously published Daily for more than fifty (50) weeks prior to the first publication of the attached notice, is published in the County of Kane, State of Illinois, is of general circulation throughout that county and surrounding area, and is a newspaper as defined by 715 IL CS 5/5.

This is to certify that a notice, a true copy of which is attached, was published 2 time(s) in the Aurora Beacon News on Jan 26, 2017, Feb 02, 2017.

This notice was also placed on a statewide public notice website as required by 5 ILCS 5/2.1.

PUBLICATION DATES: Jan 26, 2017, Feb 02, 2017.

Aurora Beacon News

Executed at Chicago, Illinois on this

_____ Day of FEB 02 2017, by
Day Month Year

Chicago Tribune Media Group


Deidra Durham


Notary Public



Chicago Tribune - chicagotribune.com

435 North Michigan Avenue, Chicago, Illinois 60611

(312) 222-2222 - Fax: (312) 222-4014

PUBLIC NOTICE

Public Informational Meeting Dauberman Road Extension - Granart Road to US 30

NOTICE IS HEREBY GIVEN that the Kane County Division of Transportation (KDOT) will hold an Introductory and Scoping Public Meeting concerning the extension of Dauberman Road from Granart Road to US Route 30, with a potential grade-separated crossing at the BNSF Railroad and US Route 30.

The Public Informational Meeting will take place on:

Date: February 9, 2017
Time: 5:00 P.M. to 7:30 P.M.
Place: Hinckley-Big Rock Middle School
47W984 US Route 30
Big Rock, IL 60511

The Public Meeting will be conducted in an informal open house format, with two 20-minute presentations at 5:30 P.M. and 6:30 P.M. Exhibit boards will be available for viewing and discussion with project staff. The purpose of this public meeting is to introduce the project, describe the current planning process, review the existing conditions, and gather input and information from the community and public. Input, comments and questions will be solicited on the project. Written comments and questions are welcome at any time but must be received by the County on or before February 23, 2017 by close of business to be part of the project record. Written comments can be submitted during the meeting, emailed to kdotcomments@co.kane.il.us, or can be mailed after the meeting to:

Kane County Division of Transportation
41W011 Burlington Road
St. Charles, IL 60175
Attn: Ms. Bonnie Flock, Project Manager

In compliance with the American with Disabilities Act and other Federal and State laws, the Public Meeting will be accessible to those with disabilities. If any additional assistance is required, please contact Bonnie Flock, Project Manager, by telephone at (630) 406-7359 or by email at FlockBonnie@co.kane.il.us at least five (5) business days prior to the meeting such that any additional arrangements can be made.
01/26/2017, 02/02/2017
4735210

4735210

The BIG ROCK COMMUNITY CRIER

FEBRUARY 2017



COMMUNITY CRIER CALENDAR

FEBRUARY

2	Lions Club	7:00 @ Town Hall
4	Firemen's Dance	8:00 @ Fire Station
7	Drainage Committee	5:30 @ Town Hall
	Drainage Dist #1	6:30 @ Town Hall
	Park Dist Meeting	7:00 @ Community Park
8	TWP Board Meeting	7:00 @ Town Hall
9	KC Division of Transportation Meeting	5:00 @ H-BR Middle School
	Historical Society Meeting	7:00 @ The Crib
13	Plan Commission Meeting	6:30 @ Fire Station
14	Village Board Meeting	7:00 @ Community Park
28	Village Board Meeting	7:00 @ Community Park

MARCH

2	Lions Club	7:00 @ Town Hall
7	Drainage Committee	5:30 @ Town Hall
	Drainage Dist #1	6:30 @ Town Hall
	Park Dist Meeting	7:00 @ Community Park
8	TWP Board	7:00 @ Town Hall
13	Plan Commission Meeting	6:30 @ Fire Station
14	Village Board Meeting	7:00 @ Community Park
16	Historical Society Meeting	7:00 @ Community Park
17	St Patrick's Day	
28	Village Board Meeting	7:00 @ Community Park

THE KANE COUNTY DIVISION OF TRANSPORTATION

will hold an introductory and scoping public meeting concerning the extension of Dauberman Road from Granart Road to US Route 30, with a potential grade-separation crossing at the BNSF Railroad and US Route 30.

**FEBRUARY 9, 2017
5:00PM TO 7:30PM
HINCKLEY-BIG ROCK
MIDDLE SCHOOL**

SAYING GOODBYE

TO A MAN WHO LOVED BIG ROCK

When Harry Schoger passed away on November 28, 2016, the community of Big Rock lost a very good friend. He was always "there" supporting events in the community, in the schools, in the church, at the Historical Society. He always had a smile on his face and an extended hand to shake.

He helped the Big Rock Fire Department in their efforts to move into their modern facility. He helped Big Rock Township acquire the land on which they could build the garage to house the road district equipment.

He always thought it would be nice to have a restaurant in Big Rock where people could meet and visit and have something to eat. So, he transformed the east building of the old Fire Department into the Big Rock Café.

Thank you, Harry, for all you did for your community.

Hinckley Area Food Pantry—Neighbors Helping Neighbors

Thanks to your generous donations of food items and cash the Hinckley Area Food Pantry has been able to serve local families and provide nutritious food for their tables. Recent food drives were held at the Hinckley Cruise Night, the H-BR Elementary School, the Hinckley Super Car Wash and the Turkey Drop at Plowman's Park in Big Rock. A special thank you to two young Hinckley girls who had a lemonade stand this summer and donated \$27.75 to the HAFP. Boy Scouts from Waterman, Brownie Scouts from Hinckley, H-BR High School organizations, and the local 4-H club are some of the youth organizations who have donated their time to stock the shelves and prepare for holiday distributions.

Volunteer opportunities are available during the week and on Saturdays. To volunteer please contact Nan at 815.286.7275. Financial donations can be mailed to HAFP, PO BOX 463, Hinckley IL 60520. Every \$1.00 donated can provide approximately 20 meals for the hungry.

Food is distributed on the 1st and 3rd Saturday of every month from 8:00 am to 9:00 am at the View Street entrance of St. Paul's UCC at the corner of McKinley & View streets. Volunteers will load your car. HAFP serves the people in Big Rock, Hinckley, Waterman and Maple Park. If you or someone you know can benefit from receiving food, please bring 2 forms of identification including one with a photo ID on the distribution day or contact Nan at 815.286.7275. *Thank You for your continued support.*

PUBLIC NOTICE

Public Informational Meeting

Dauberman Road Extension - Granart Road to US 30
NOTICE IS HEREBY GIVEN that the Kane County Division of Transportation (KDOT) will hold an Introductory and Scoping Public Meeting concerning the extension of Dauberman Road from Granart Road to US Route 30, with a potential grade-separated crossing at the BNSF Railroad and US Route 30.

The Public Informational Meeting will take place on:

Date: February 9, 2017

Time: 5:00 P.M. to 7:30 P.M.

Place: Hinckley-Big Rock Middle School

47W984 US Route 30, Big Rock, IL 60511

The Public Meeting will be conducted in an informal open house format, with two 20-minute presentations at 5:30 P.M. and 6:30 P.M. Exhibit boards will be available for viewing and discussion with project staff. The purpose of this public meeting is to introduce the project, describe the current planning process, review the existing conditions, and gather input and information from the community and public. Input, comments and questions will be solicited on the project. Written comments and questions are welcome at any time but must be received by the County on or before February 23, 2017 by close of business to be part of the project record. Written comments can be submitted during the meeting, emailed to kdotcomments@co.kane.il.us, or can be mailed after the meeting to:

Kane County Division of Transportation

41W011 Burlington Road, St. Charles, IL 60175

Attn: Ms. Bonnie Flock, Project Manager

In compliance with the American with Disabilities Act and other Federal and State laws, the Public Meeting will be accessible to those with disabilities. If any additional assistance is required, please contact Bonnie Flock, Project Manager, by telephone at (630) 406-7359 or by email at FlockBonnie@co.kane.il.us at least five (5) business days prior to the meeting such that any additional arrangements can be made.

Published in Daily Herald Jan 26 & Feb 2, 2017(4462356)

CERTIFICATE OF PUBLICATION

Paddock Publications, Inc.

Daily Herald

Corporation organized and existing under and by virtue of the laws of the State of Illinois, DOES HEREBY CERTIFY that it is the publisher of the **DAILY HERALD**. That said **DAILY HERALD** is a secular newspaper and has been circulated daily in the Village(s) of Addison, Algonquin, Antioch, Arlington Heights, Aurora, Barrington, Barrington Hills, Bartlett, Batavia, Bensenville, Bloomingdale, Buffalo Grove, Burlington, Campton Hills, Carol Stream, Carpentersville, Cary, Deer Park, Des Plaines, East Dundee, Elburn, Elgin, Elk Grove Village, Elmhurst, Fox Lake, Fox River Grove, Geneva, Gilberts, Glen Ellyn, Glendale Heights, Glenview, Grayslake, Green Oaks, Gurnee, Hainesville, Hampshire, Hanover Park, Hawthorn Woods, Hoffman Estates, Huntley, Inverness, Island Lake, Itasca, Keeneyville, Kildeer, Lake Barrington, Lake Villa, Lake in the Hills, Lake Zurich, Libertyville, Lincolnshire, Lindenhurst, Lisle, Lombard, Long Grove, Medinah, Mt. Prospect, Mundelein, Naperville, North Aurora, North Barrington, Oakbrook, Oakbrook Terrace, Palatine, Prospect Heights, Rolling Meadows, Roselle, Schaumburg, Sleepy Hollow, South Barrington, South Elgin, St. Charles, Woodridge, Streamwood, Tower Lakes, Vernon Hills, Villa Park, Volo, Warrenville, Wauconda, Wayne, West Chicago, West Dundee, Wheaton, Wheeling, Wildwood, Winfield, Wood Dale, Round Lake Park, Pingree Grove County(ies) of Cook, DuPage, Kane, Lake, McHenry and State of Illinois, continuously for more than one year prior to the date of the first publication of the notice hereinafter referred to and is of general circulation throughout said Village(s), County(ies) and State.

I further certify that the **DAILY HERALD** is a newspaper as defined in "an Act to revise the law in relation to notices" as amended in 1992 Illinois Compiled Statutes, Chapter 715, Act 5, Section 1 and 5. That a notice of which the annexed printed slip is a true copy, was published January 26, February 2, 2017 in said **DAILY HERALD**.

IN WITNESS WHEREOF, the undersigned, the said **PADDOCK PUBLICATIONS, Inc.**, has caused this certificate to be signed by, this authorized agent, at Arlington Heights, Illinois.

PADDOCK PUBLICATIONS, INC.
DAILY HERALD NEWSPAPERS

BY



Authorized Agent

Control # 4462356

PUBLIC NOTICE
Public Informational Meeting
Dauberman Road Extension - Granart Road to US 30
NOTICE IS HEREBY GIVEN that the Kane County Division of Transportation (KDOT) will hold an Introductory and Scoping Public Meeting concerning the extension of Dauberman Road from Granart Road to US Route 30, with a potential grade-separated crossing at the BNSF Railroad and US Route 30.
The Public Informational Meeting will take place on:
Date: February 9, 2017
Time: 5:00 P.M. to 7:30 P.M.
Place: Hinckley-Big Rock Middle School
47W984 US Route 30, Big Rock, IL 60511
The Public Meeting will be conducted in an informal open house format, with two 20-minute presentations at 5:30 P.M. and 6:30 P.M. Exhibit boards will be available for viewing and discussion with project staff. The purpose of this public meeting is to introduce the project, describe the current planning process, review the existing conditions, and gather input and information from the community and public. Input, comments and questions will be solicited on the project. Written comments and questions are welcome at any time but must be received by the County on or before February 23, 2017 by close of business to be part of the project record. Written comments can be submitted during the meeting, emailed to kdotcomments@co.kane.il.us, or can be mailed after the meeting to:
Kane County Division of Transportation
41W011 Burlington Road, St. Charles, IL 60175
Attn: Ms. Bonnie Flock, Project Manager
In compliance with the American with Disabilities Act and other Federal and State laws, the Public Meeting will be accessible to those with disabilities. If any additional assistance is required, please contact Bonnie Flock, Project Manager, by telephone at (630) 406-7359 or by email at FlockBonnie@co.kane.il.us at least five (5) business days prior to the meeting such that any additional arrangements can be made.
Published in Daily Herald Jan 26 & Feb 2, 2017 (4462356)

CERTIFICATE OF PUBLICATION

Paddock Publications, Inc.

Daily Herald

Corporation organized and existing under and by virtue of the laws of the State of Illinois, DOES HEREBY CERTIFY that it is the publisher of the **DAILY HERALD**. That said **DAILY HERALD** is a secular newspaper and has been circulated daily in the Village(s) of Addison, Algonquin, Antioch, Arlington Heights, Aurora, Barrington, Barrington Hills, Bartlett, Batavia, Bensenville, Bloomingdale, Buffalo Grove, Burlington, Campton Hills, Carol Stream, Carpentersville, Cary, Deer Park, Des Plaines, East Dundee, Elburn, Elgin, Elk Grove Village, Elmhurst, Fox Lake, Fox River Grove, Geneva, Gilberts, Glen Ellyn, Glendale Heights, Glenview, Grayslake, Green Oaks, Gurnee, Hainesville, Hampshire, Hanover Park, Hawthorn Woods, Hoffman Estates, Huntley, Inverness, Island Lake, Itasca, Keeneyville, Kildeer, Lake Barrington, Lake Villa, Lake in the Hills, Lake Zurich, Libertyville, Lincolnshire, Lindenhurst, Lisle, Lombard, Long Grove, Medinah, Mt. Prospect, Mundelein, Naperville, North Aurora, North Barrington, Oakbrook, Oakbrook Terrace, Palatine, Prospect Heights, Rolling Meadows, Roselle, Schaumburg, Sleepy Hollow, South Barrington, South Elgin, St. Charles, Woodridge, Streamwood, Tower Lakes, Vernon Hills, Villa Park, Volo, Warrenville, Wauconda, Wayne, West Chicago, West Dundee, Wheaton, Wheeling, Wildwood, Winfield, Wood Dale, Round Lake Park, Pingree Grove County(ies) of Cook, DuPage, Kane, Lake, McHenry and State of Illinois, continuously for more than one year prior to the date of the first publication of the notice hereinafter referred to and is of general circulation throughout said Village(s), County(ies) and State.

I further certify that the **DAILY HERALD** is a newspaper as defined in "an Act to revise the law in relation to notices" as amended in 1992 Illinois Compiled Statutes, Chapter 715, Act 5, Section 1 and 5. That a notice of which the annexed printed slip is a true copy, was published January 26, February 2, 2017 in said **DAILY HERALD**.

IN WITNESS WHEREOF, the undersigned, the said **PADDOCK PUBLICATIONS, Inc.**, has caused this certificate to be signed by, this authorized agent, at Arlington Heights, Illinois.

PADDOCK PUBLICATIONS, INC.
DAILY HERALD NEWSPAPERS

BY 
Authorized Agent

Control # 4462356

Certificate of the Publisher

Elburn Herald

Description: NOTICE MGT DAUBERMAN RD
1260660

TRANS SYSTEMS
ATTN: LYNN OTTE
SUITE 610
222. S. RIVERSIDE PLAZA
CHICAGO IL 60606

Shaw Media certifies that it is the publisher of the Elburn Herald. The Elburn Herald is a secular newspaper, has been continuously published weekly for more than fifty (50) weeks prior to the date of this certificate, is published in the City of Elburn, in the County of Kane, State of Illinois, is of general circulation throughout that county and surrounding area, and is a newspaper as defined by 715 ILCS 5/5.

A notice, a true copy of which is attached, was published 2 time(s) in the Elburn Herald namely one time per week for two successive week(s). Publication of the notice was made in the newspaper, dated and published on
01/26/2017 02/02/2017

This notice was also placed on a statewide public notice website as required by 5 ILCS 5/2.1.

In witness, Shaw Media has signed this certificate by Karen Pletsch, its publisher, at Elburn, Illinois, on the
2nd day of February, A.D. 2017

Shaw Media By:



Karen Pletsch, Publisher

Account Number 242206

Amount \$207.72

PUBLIC NOTICE

**Public Informational Meeting
Dauberman Road Extension -
Granari Road to US 30**

NOTICE IS HEREBY GIVEN that the Kane County Division of Transportation (KDOT) will hold an Introductory and Scoping Public Meeting concerning the extension of Dauberman Road from Granari Road to US Route 30, with a potential grade-separated crossing at the BNSF Railroad and US Route 30.

The Public Informational Meeting will take place on:

Date: February 9, 2017
Time: 5:00 P.M. to 7:30 P.M.
Place: Hinckley-Big Rock
Middle School
47W984 US Route 30
Big Rock, IL 60511

The Public Meeting will be conducted in an informal open house format, with two 20-minute presentations at 5:30 P.M. and 6:30 P.M. Exhibit boards will be available for viewing and discussion with project staff. The purpose of this public meeting is to introduce the project, describe the current planning process, review the existing conditions, and gather input and information from the community and public. Input, comments and questions will be solicited on the project. Written comments and questions are welcome at any time but must be received by the County on or before February 23, 2017 by close of business to be part of the project record. Written comments can be submitted during the meeting, emailed to kdotcomments@co.kane.il.us, or can be mailed after the meeting to: Kane County Division of Transportation
41W011 Burlington Road
St. Charles, IL 60175
Attn: Ms. Bonnie Flock, Project Manager

In compliance with the American with Disabilities Act and other Federal and State laws, the Public Meeting will be accessible to those with disabilities. If any additional assistance is required, please contact Bonnie Flock, Project Manager, by telephone at (630) 406-7359 or by email at FlockBonnie@co.kane.il.us at least five (5) business days prior to the meeting such that any additional arrangements can be made.

January 26, February 2, 2017
Kane County Chronicle
Elburn Herald
1260660

Certificate of the Publisher

Kane County Chronicle

Description: NOTICE MGT DAUBERMAN RD
1260660

TRANS SYSTEMS
ATTN: LYNN OTTE
SUITE#610
222 S. RIVERSIDE PLAZA
CHICAGO IL 60606

Shaw Media certifies that it is the publisher of the Kane County Chronicle. The Kane County Chronicle is a secular newspaper, has been continuously published daily for more than fifty (50) weeks prior to the first publication of the attached notice, is published in the City of St. Charles, County of Kane, State of Illinois, is of general circulation throughout that county and surrounding area, and is a newspaper as defined by 715 ILCS 5/5.

A notice, a true copy of which is attached, was published 2 time(s) in the Kane County Chronicle, namely one time per week for two successive week(s). Publication of the notice was made in the newspaper, dated and published on
01/26/2017 02/02/2017

This notice was also placed on a statewide public notice website as required by 5 ILCS 5/2.1.

In witness, Shaw Media has signed this certificate by Karen Pletsch, its publisher, at St. Charles, Illinois, on
2nd day of February, A.D. 2017

Shaw Media By:



Karen Pletsch, Publisher

Account Number 242206

Amount \$207.72

PUBLIC NOTICE

**Public Informational Meeting
Dauberman Road Extension -
Granati Road to US 30**

NOTICE IS HEREBY GIVEN that the Kane County Division of Transportation (KDOT) will hold an Introductory and Scoping Public Meeting concerning the extension of Dauberman Road from Granati Road to US Route 30, with a potential grade-separated crossing at the BNSF Railroad and US Route 30.

The Public Informational Meeting will take place on:

Date: February 9, 2017
Time: 5:00 P.M. to 7:30 P.M.
Place: Hinckley-Big Rock
Middle School
47W984 US Route 30
Big Rock, IL 60511

The Public Meeting will be conducted in an informal open house format, with two 20-minute presentations at 5:30 P.M. and 6:30 P.M. Exhibit boards will be available for viewing and discussion with project staff. The purpose of this public meeting is to introduce the project, describe the current planning process, review the existing conditions, and gather input and information from the community and public. Input, comments and questions will be solicited on the project. Written comments and questions are welcome at any time but must be received by the County on or before February 23, 2017 by close of business to be part of the project record. Written comments can be submitted during the meeting, emailed to

kdotcomments@co.kane.il.us, or can be mailed after the meeting to: Kane County Division of Transportation
41W011 Burlington Road
St. Charles, IL 60175
Attn: Ms. Bonnie Flock, Project Manager

In compliance with the American with Disabilities Act and other Federal and State laws, the Public

Meeting will be accessible to those with disabilities. If any additional assistance is required, please contact Bonnie Flock, Project Manager, by telephone at (630) 406-7359 or by email at FlockBonnie@co.kane.il.us at least five (5) business days prior to the meeting such that any additional arrangements can be made.

January 26, February 2, 2017
Kane County Chronicle
Elburn Herald
1260660



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KANE COUNTY OUTREACH COORDINATOR RICK NAGEL



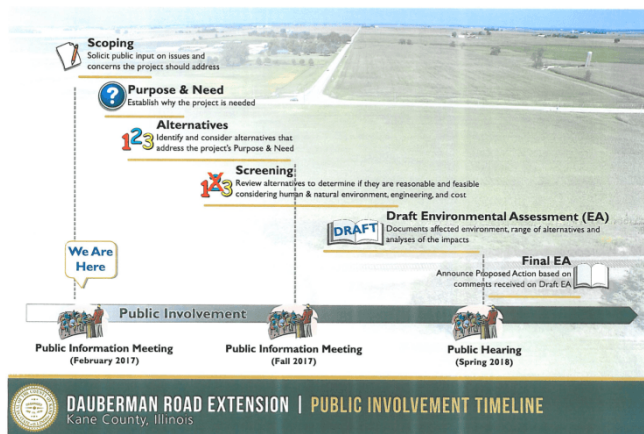
Kane County Government Center, 719 S. Batavia Ave., Geneva, IL 60134
1-630-208-5133
Any time. Email
kanecountyconnects@gmail.com

Browse: Home / 2017 / February / KDOT Holds Feb. 9 Public Meeting to Introduce Dauberman Road Extension Project



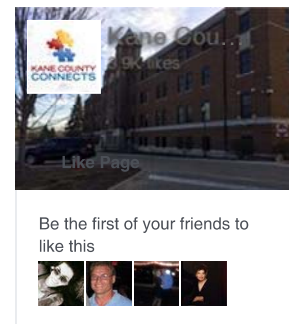
KDOT HOLDS FEB. 9 PUBLIC MEETING TO INTRODUCE DAUBERMAN ROAD EXTENSION PROJECT

February 1, 2017 · by kanecountyconnects · in Communities, Development, Events, Government, Transportation, WK-Features



The Kane County Division of Transportation will host a public meeting to introduce the Dauberman Road Extension project, describe the current planning process, review the existing conditions and gather input and information from the public.

KANE COUNTY CONNECTS FACEBOOK



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Subscribe to KCC's E-Newsletter and receive all the most recent updates and news!

KANE COUNTY COUNTS!



WHY KANE?



The meeting will be held from 5 p.m. to 7:30 p.m. Thursday, Feb. 9, at the Hinckley-Big Rock Middle School.

To learn more about the project, [visit this page of the KDOT website](#) or click on the following links:

- [Exhibits for Public Meeting 1](#)
- [Public Meeting 1 – Brochure](#)

KANE COUNTY OPENGOV



Project Overview



Dauberman Road in Big Rock is undergoing Phase I engineering for the proposed extension from U.S. Route 30 south to Granart Road.

The purpose of the roadway extension is to provide a grade-separated crossing that aims to improve safety, access and system continuity.

The proposed Dauberman Road Extension will cross the Burlington Northern Santa Fe Railroad. One of the key components is to determine if the roadway should cross the BNSF Railroad above-grade or below-grade, as well as the potential impacts at nearby intersections.

Grade Separation

The engineering study will determine if there are existing deficiencies and what impacts the roadway extension could have on the movement of vehicles.

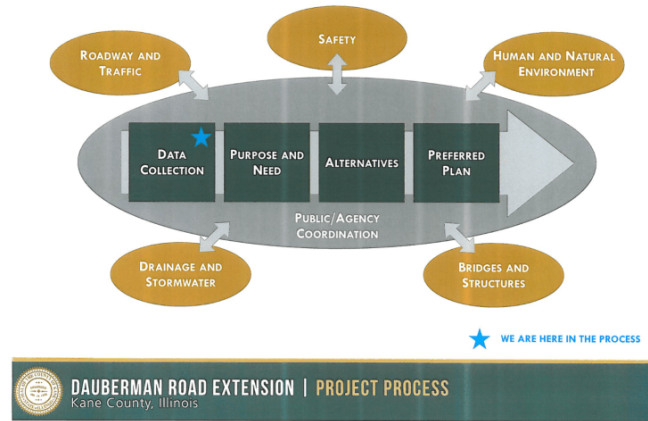
The nearest grade-separated crossing of the BNSF Railroad is IL Route 47 to the east. Crossings just east and west of Dauberman Road at Dugan Road and Rhodes Street, respectively, are at-grade.

Between 25 and 32 trains per day pass along the BNSF line based on actual train counts and information provided by the Illinois Commerce Commission Based on the train counts conducted over a 24-hour period, more than 50 percent of the trains were stopped for two or more minutes at the Dugan Road tracks. The data also indicated that more than one-third of the trains occurred during the critical morning and evening commuter peak periods when traffic volumes are at their highest.



Recent improvements were completed in August 2015 to the Dugan Road / Granart Road intersection by Sugar Grove, which included a round-about. Capacity improvements were also completed by IDOT at the U.S. Route 30 / Dugan Road intersection. Determining the study area deficiencies will be a vital component in identifying the Project Purpose and Need.

Planning Process



The engineering study is the first stage of the project development process.

Data collection is nearly complete including traffic counts, crash data, field surveys, and some environmental field studies.

The planning process, which is outlined in the graphic above, is based on guidelines recommended by the Federal Highway Administration, which follows the National Environmental Policy Act. NEPA is a United States environmental law that promotes the enhancement of the environment and established the President's Council on Environmental Quality.

The planning process includes detailed studies and project coordination to develop a preferred plan that meets the project Purpose and Need. An Environmental Assessment will eventually be prepared.

There are four components in the NEPA process which include Data Collection, Purpose and Need, Alternatives Development, and Preferred Alternative. The Data Collection phase is almost complete and then the Project Purpose and Need will be determined.

How to Stay Involved

KDOT is soliciting input from the public on any local concerns that the project should address.

Comments and questions are welcome at any time, but must be received by the county on or before the close of business on Feb. 23, 2017, to be part of the project record. Written comments and questions can be mailed to the Kane County Division of Transportation with attention to Bonnie Flock, the Kane County Project Manager, or emailed to kdotcomments@co.kane.il.us.

SOURCE: Kane County Division of Transportation website



Share this:



Tags: Big Rock, Dauberman Road, Extension, Grade Separation, Rail Crossing, Sugar Gove

Comments are closed, but [trackbacks](#) and pingbacks are open.

[← Frank Lloyd Wright and More:
History Museum Showcases Geneva's
Astonishing Architecture](#)

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⌂

DAUBERMAN ROAD EXTENSION

PHASE I STUDY

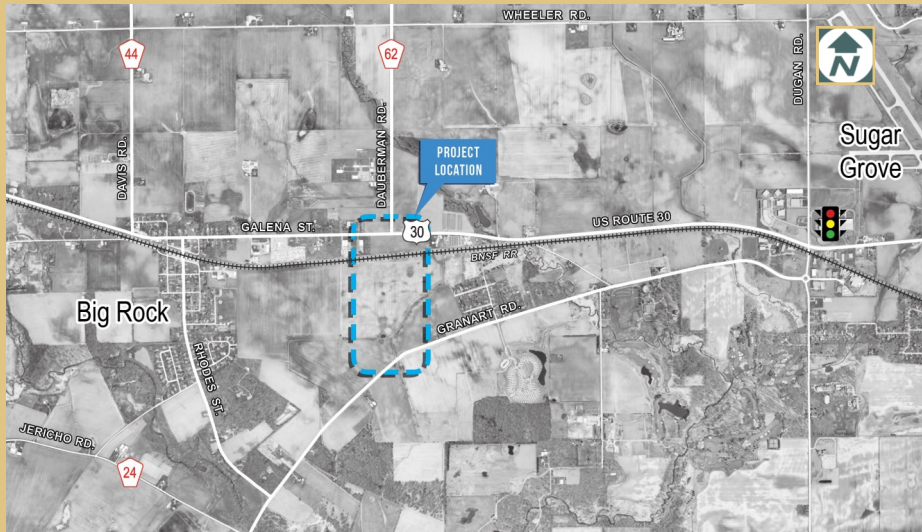
ATTACHMENT 5

INFORMATIONAL BROCHURE

PROJECT CONTACTS

BONNIE FLOCK
KANE COUNTY PROJECT MANAGER
(630) 406-7359
FLOCKBONNIE@CO.KANE.IL.US

ERIC NEUBAUER
TRANSYSTEMS PROJECT MANAGER
(312) 669-5834
ESNEUBAUER@TRANSYSTEMS.COM



FOR MORE INFORMATION, GO TO:

[HTTP://WWW.CO.KANE.IL.US/DOT/CONSTPROJECTS.ASPX](http://www.co.kane.il.us/dot/constprojects.aspx)



KANE COUNTY DIVISION OF TRANSPORTATION
41W011 BURLINGTON ROAD - ST. CHARLES, IL 60175

KANE COUNTY DIVISION OF TRANSPORTATION



DAUBERMAN ROAD EXTENSION

US ROUTE 30 TO GRANART ROAD



PROJECT INTRODUCTION AND SCOPING MEETING

FEBRUARY 9, 2017

5:00PM - 7:30PM

HINCKLEY-BIG ROCK MIDDLE SCHOOL
47W984 US ROUTE 30 - BIG ROCK, IL 60511

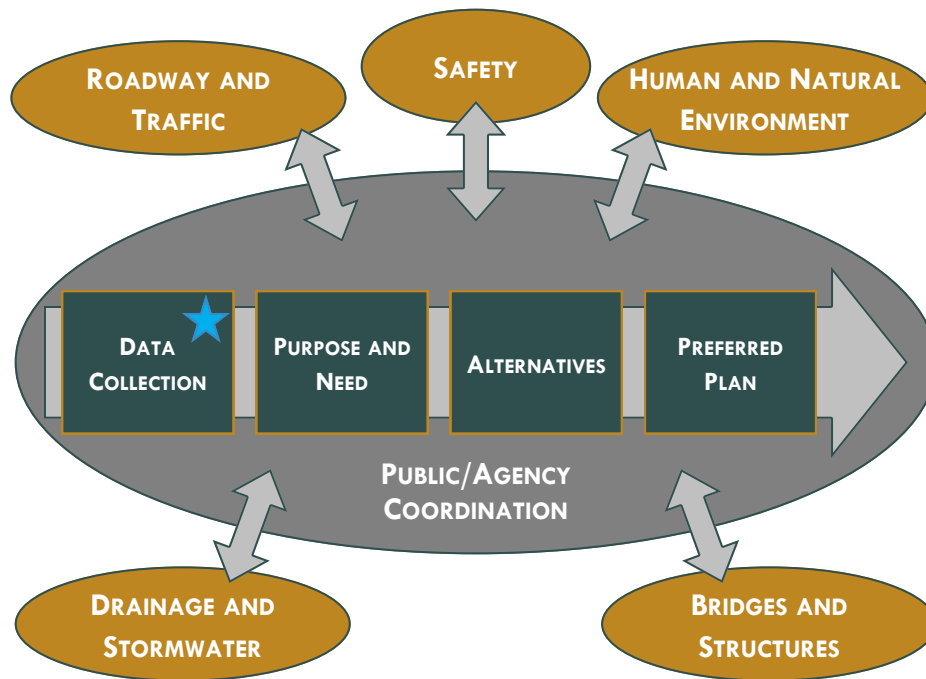
DAUBERMAN ROAD EXTENSION

PURPOSE OF THE MEETING

The purpose of this public meeting is to introduce the project, describe the current planning process, review the existing conditions and gather input and information from the community and public. Your comments are important to the project process. We are seeking your input on the data gathered and developed to date.

PROJECT PLANNING PROCESS

- The planning process is based on guidelines prescribed by the Federal Highway Administration (FHWA) which follows the National Environmental Policy Act (NEPA).
- It includes detailed studies and project coordination to develop a preferred plan that meets the project Purpose and Need.
- An Environmental Assessment (EA) document will be prepared.



 **WE ARE HERE IN THE PROCESS**

TIMELINE



PUBLIC INPUT

KDOT is looking to gather input and information from the community and public. Comments and questions are welcome at any time, but must be received by the County on or before February 23, 2017 by close of business to be part of the project record.

Written comments and questions can be submitted during the meeting, mailed to Kane County at the address on the backside of this brochure to the attention of Ms. Bonnie Flock, the Kane County Project Manager, or emailed to kdotcomments@co.kane.il.us.

DAUBERMAN ROAD EXTENSION

PHASE I STUDY

ATTACHMENT 6

POWERPOINT PRESENTATION/DISPLAY EXHIBITS



DAUBERMAN ROAD EXTENSION | PUBLIC MEETING #1 — FEBRUARY 9, 2017

Kane County, Illinois

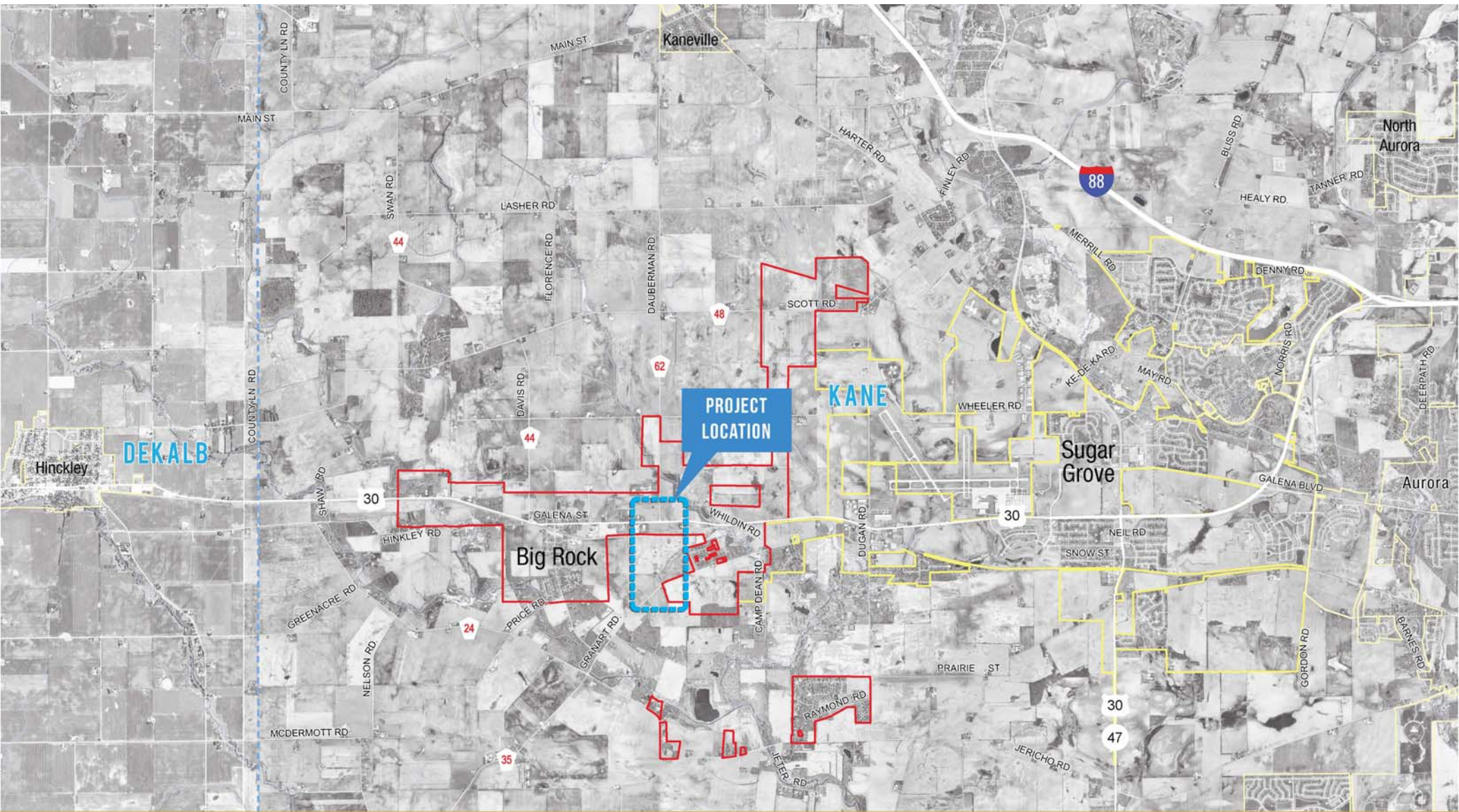
An aerial photograph of a rural landscape. A paved road runs vertically through the center of the image. To the left of the road, there are several large, white, rectangular structures, possibly storage tanks or silos, and some smaller buildings. The surrounding area is mostly green fields with some patches of brown, possibly from harvested crops. In the distance, there are more fields and some small clusters of trees.

Presentation Outline

- Project Overview
- Data Collection to Date
- Project Planning Process and Environmental Study
- Coordination and Community Input
- Next Steps



DAUBERMAN ROAD EXTENSION | PUBLIC MEETING #1 — FEBRUARY 9, 2017
Kane County, Illinois

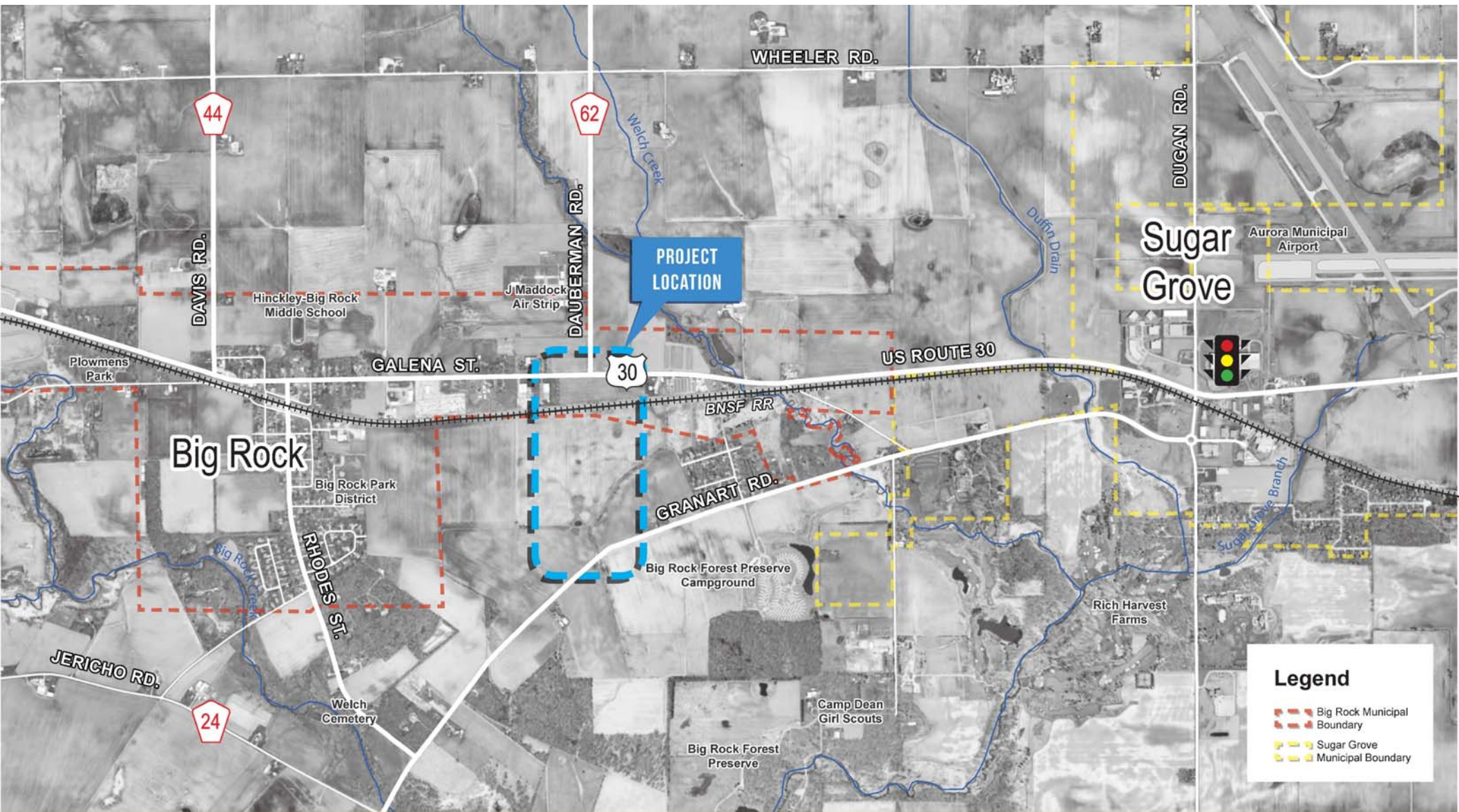


DAUBERMAN ROAD EXTENSION | REGIONAL MAP

Kane County, Illinois



Not to Scale



Legend

- Big Rock Municipal Boundary
- Sugar Grove Municipal Boundary

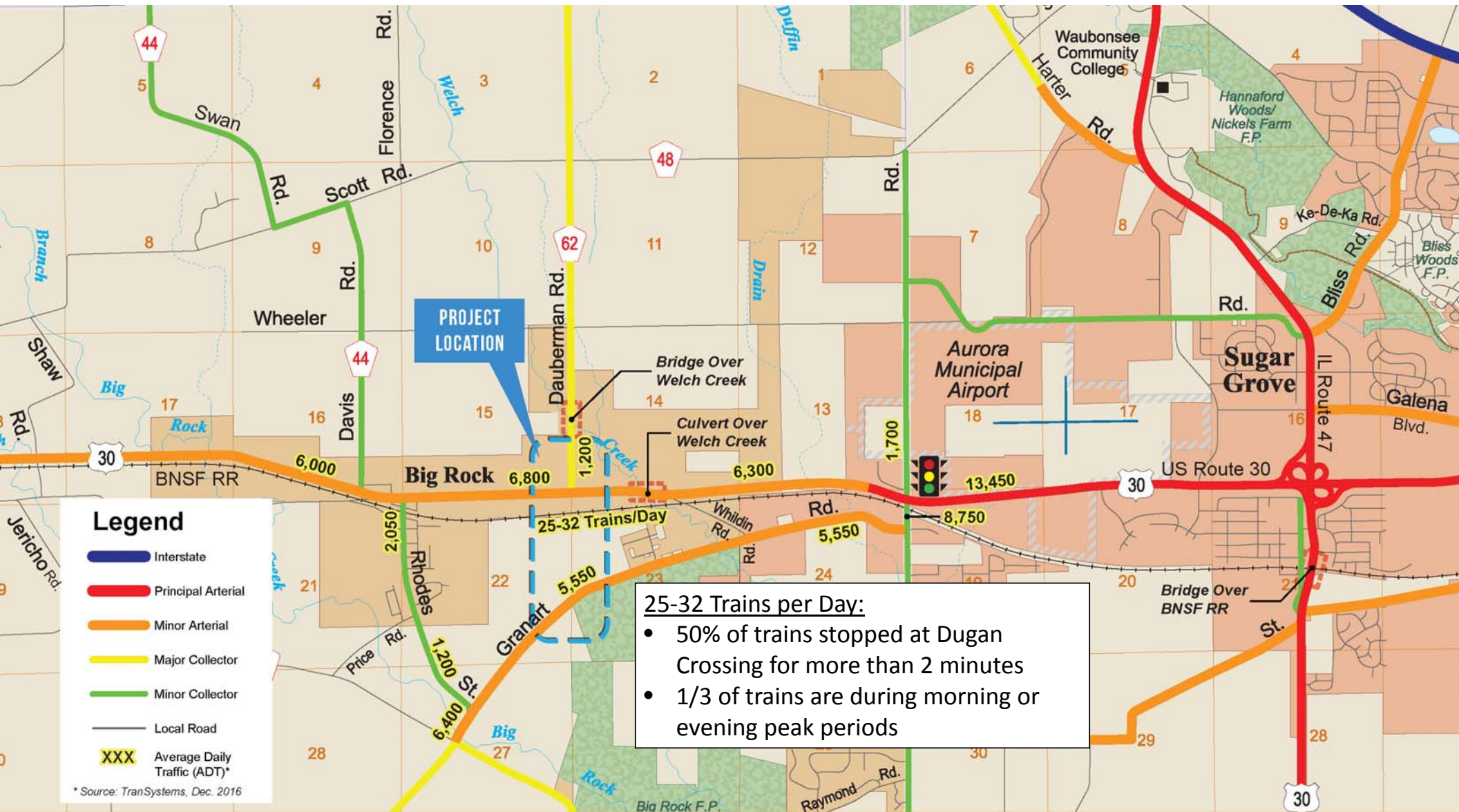


DAUBERMAN ROAD EXTENSION | PROJECT LOCATION MAP

Kane County, Illinois



Not to Scale

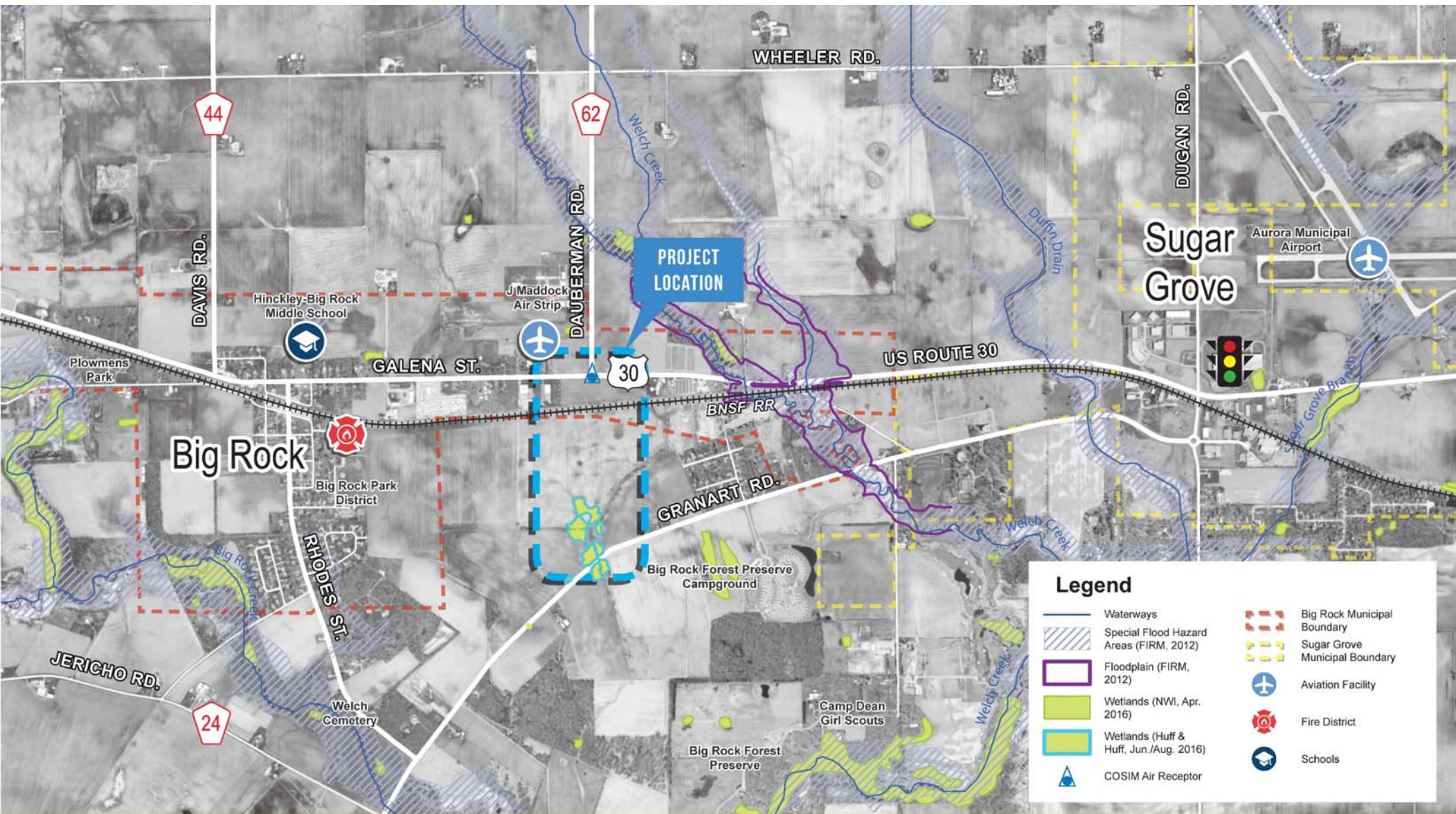


DAUBERMAN ROAD EXTENSION | ROAD CLASSIFICATION WITH EXISTING ADT

Kane County, Illinois



Not to Scale

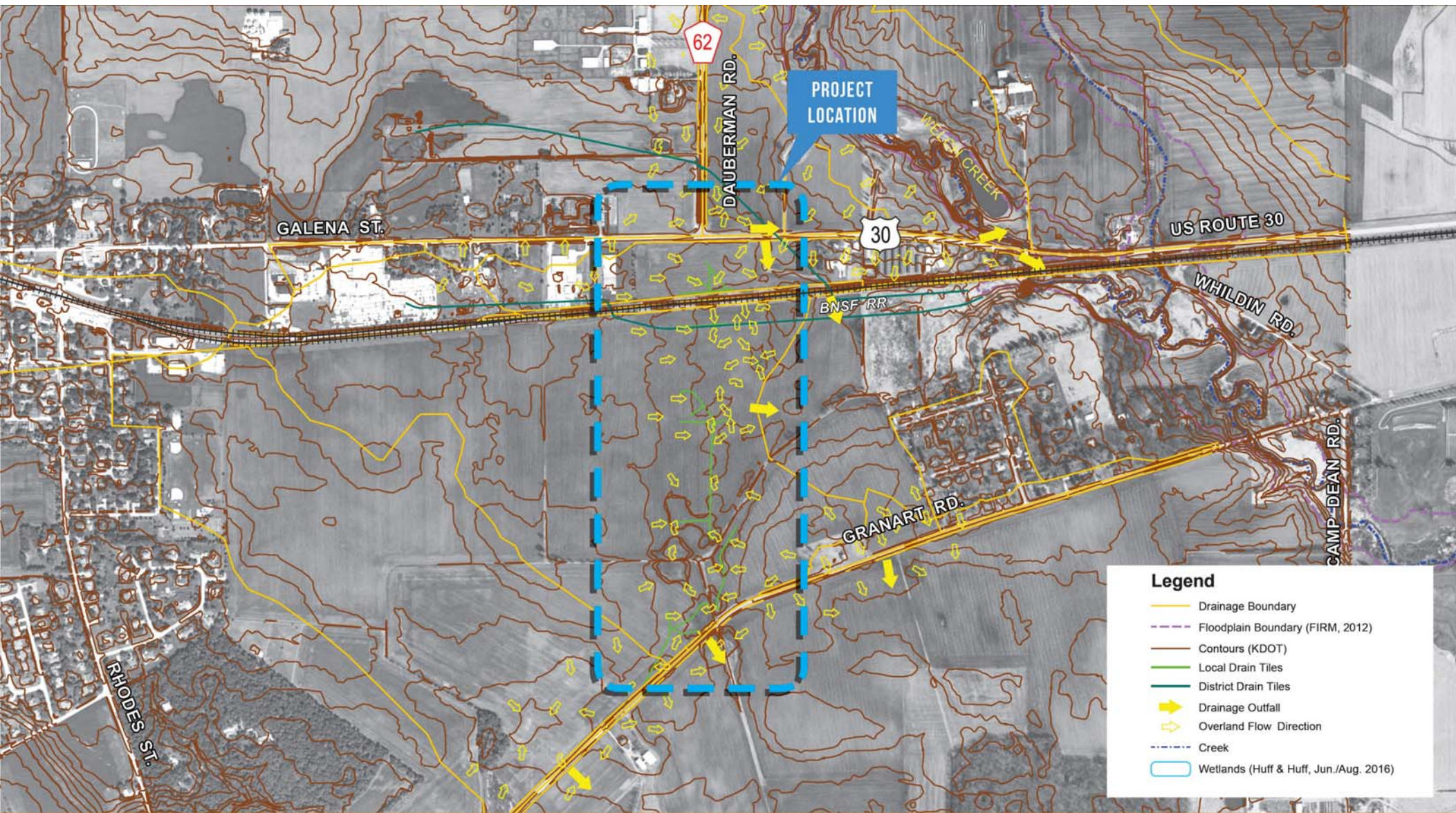


DAUBERMAN ROAD EXTENSION | ENVIRONMENTAL RESOURCES

Kane County, Illinois



Not to Scale

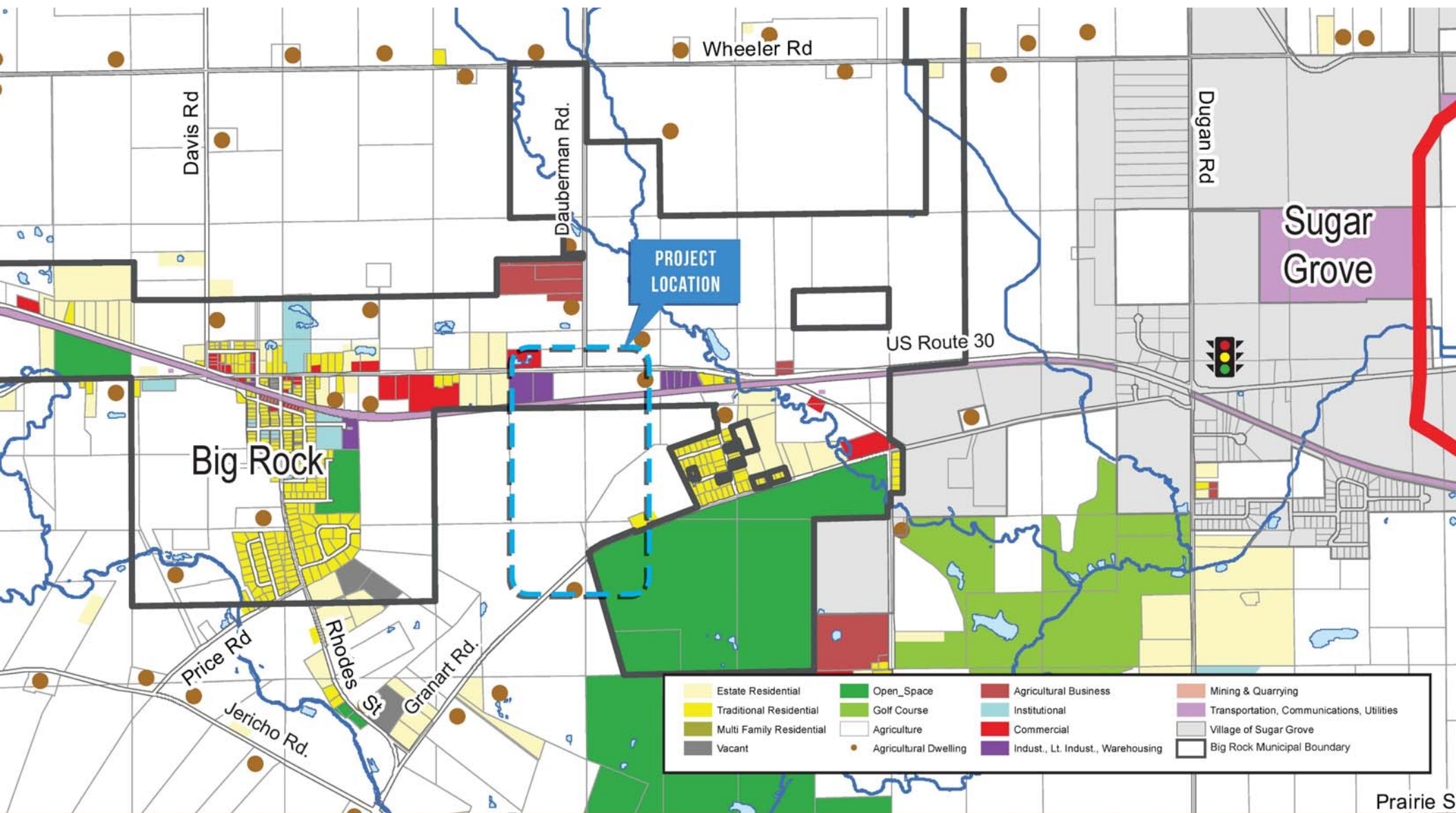


DAUBERMAN ROAD EXTENSION | EXISTING DRAINAGE

Kane County, Illinois



Not to Scale



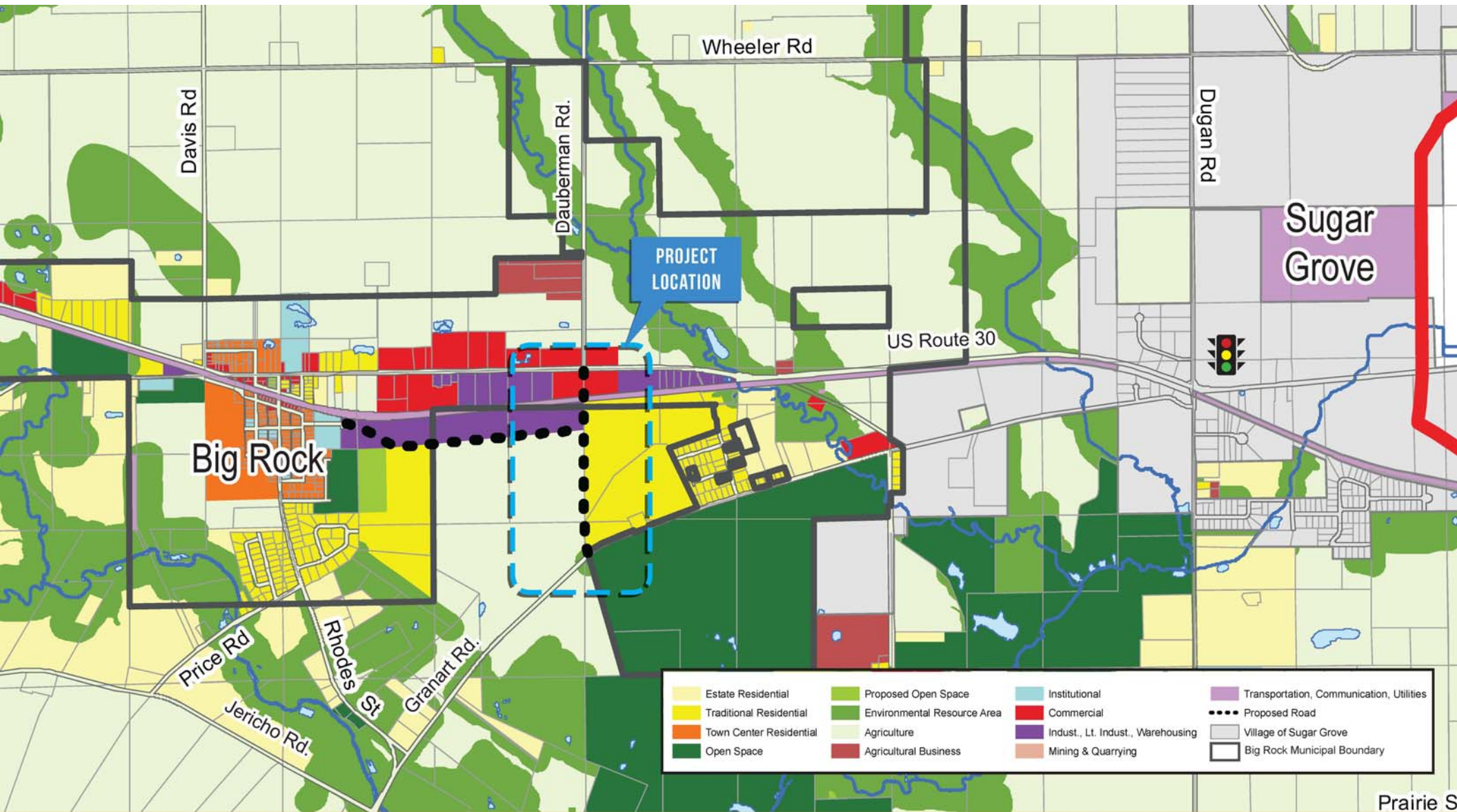
DAUBERMAN ROAD EXTENSION |

Kane County, Illinois

EXISTING LAND USE
 (Village of Big Rock Comprehensive Plan - 2014)



Not to Scale



DAUBERMAN ROAD EXTENSION |

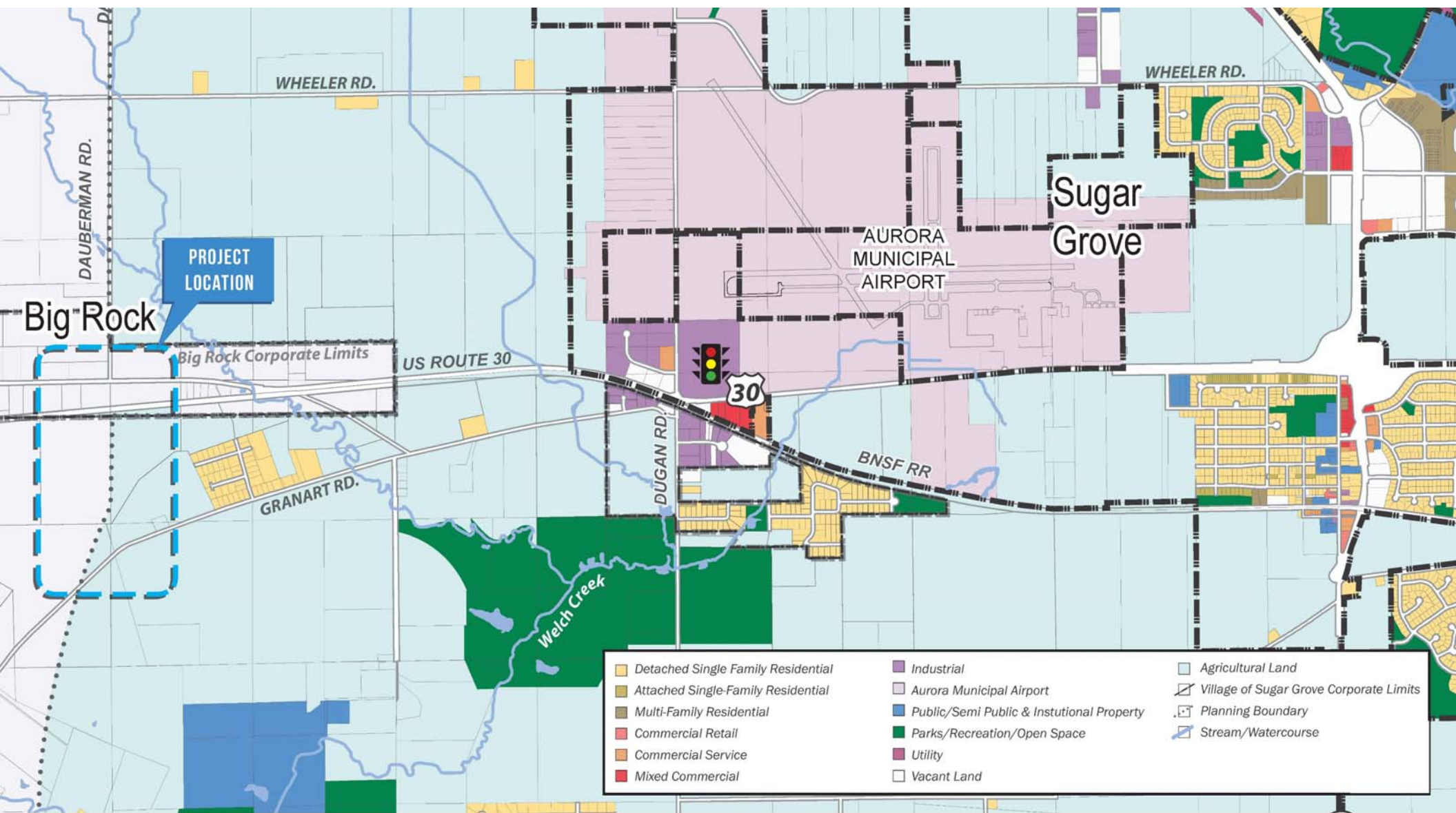
Kane County, Illinois

FUTURE LAND USE

(Village of Big Rock Comprehensive Plan - 2014)



Not to Scale



DAUBERMAN ROAD EXTENSION |

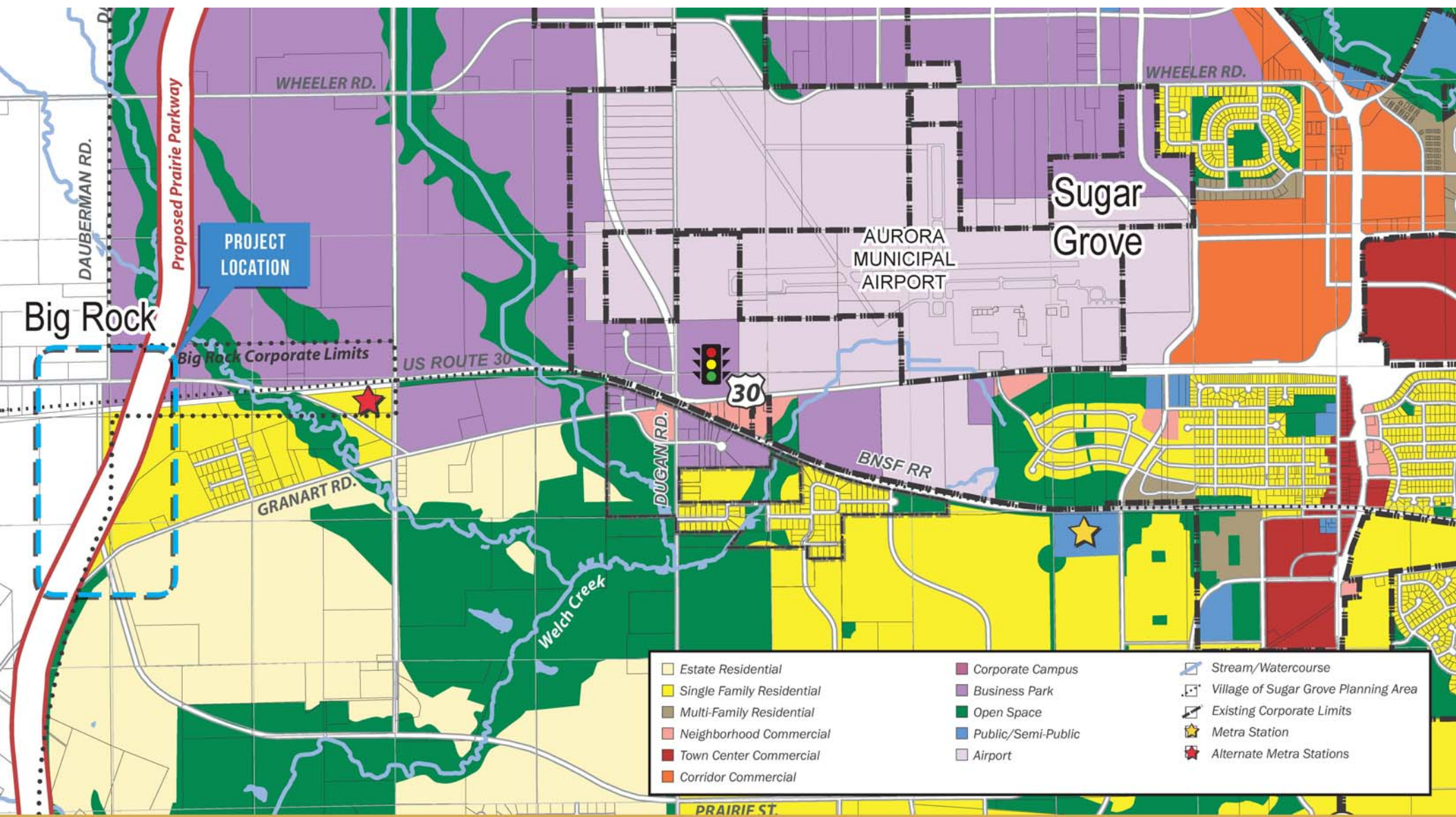
Kane County, Illinois

EXISTING LAND USE

(Village of Sugar Grove Comprehensive Plan - 2016)



Not to Scale



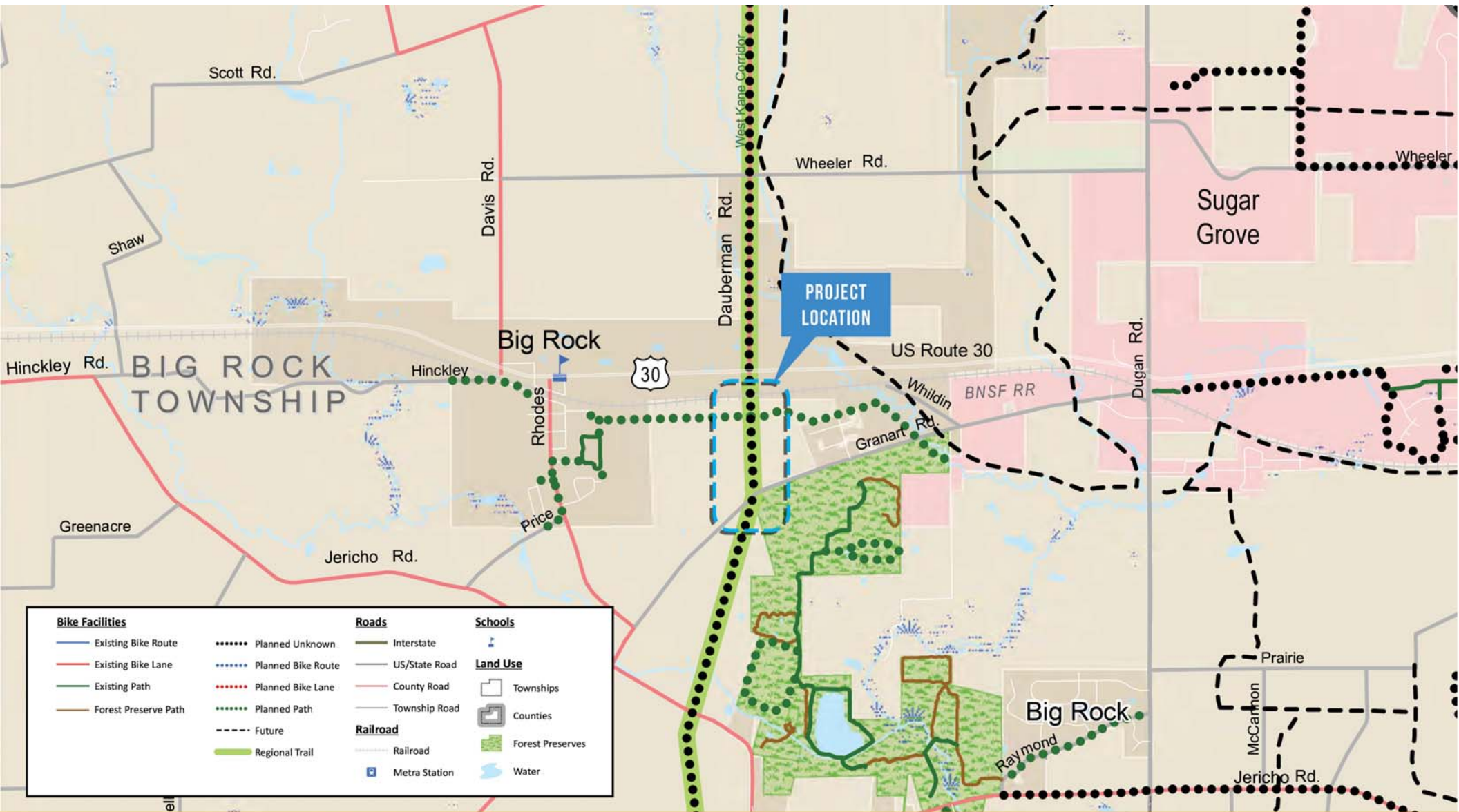
DAUBERMAN ROAD EXTENSION | FUTURE LAND USE PLAN

Kane County, Illinois

(Village of Sugar Grove Comprehensive Plan - 2016)



Not to Scale



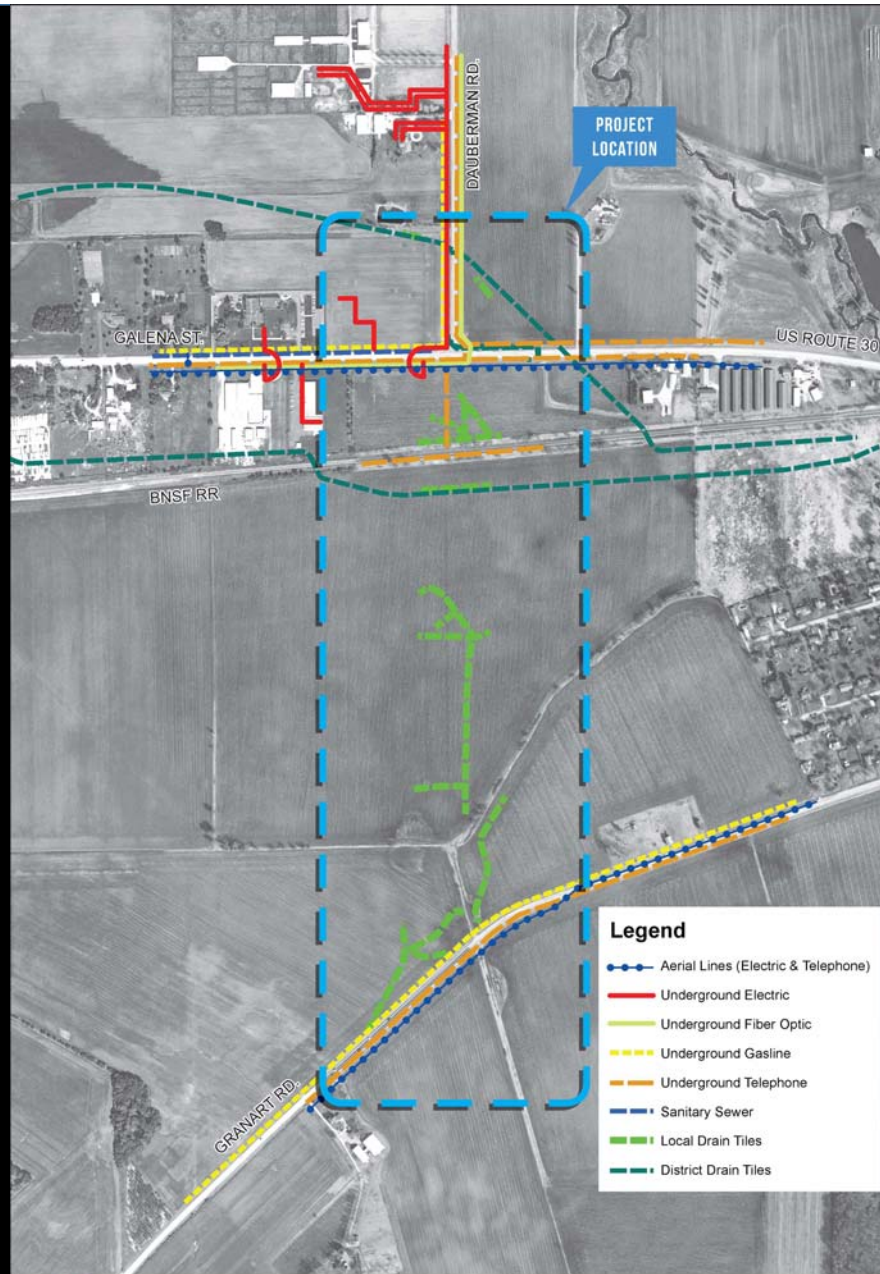
DAUBERMAN ROAD EXTENSION | BIKE LANES & PATHS

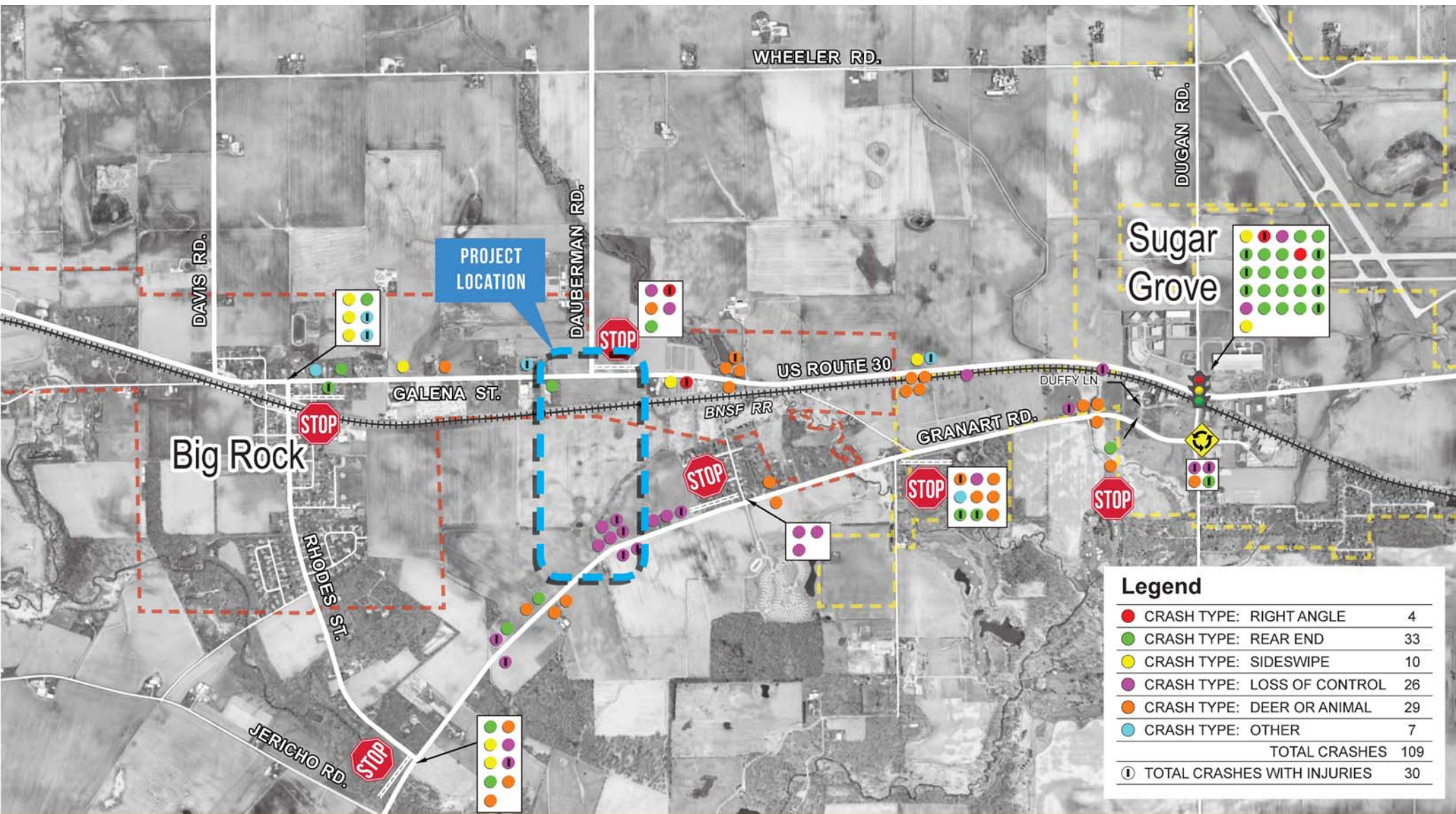
Kane County, Illinois

(2015 Kane/Kendall County Planning Map)



Not to Scale





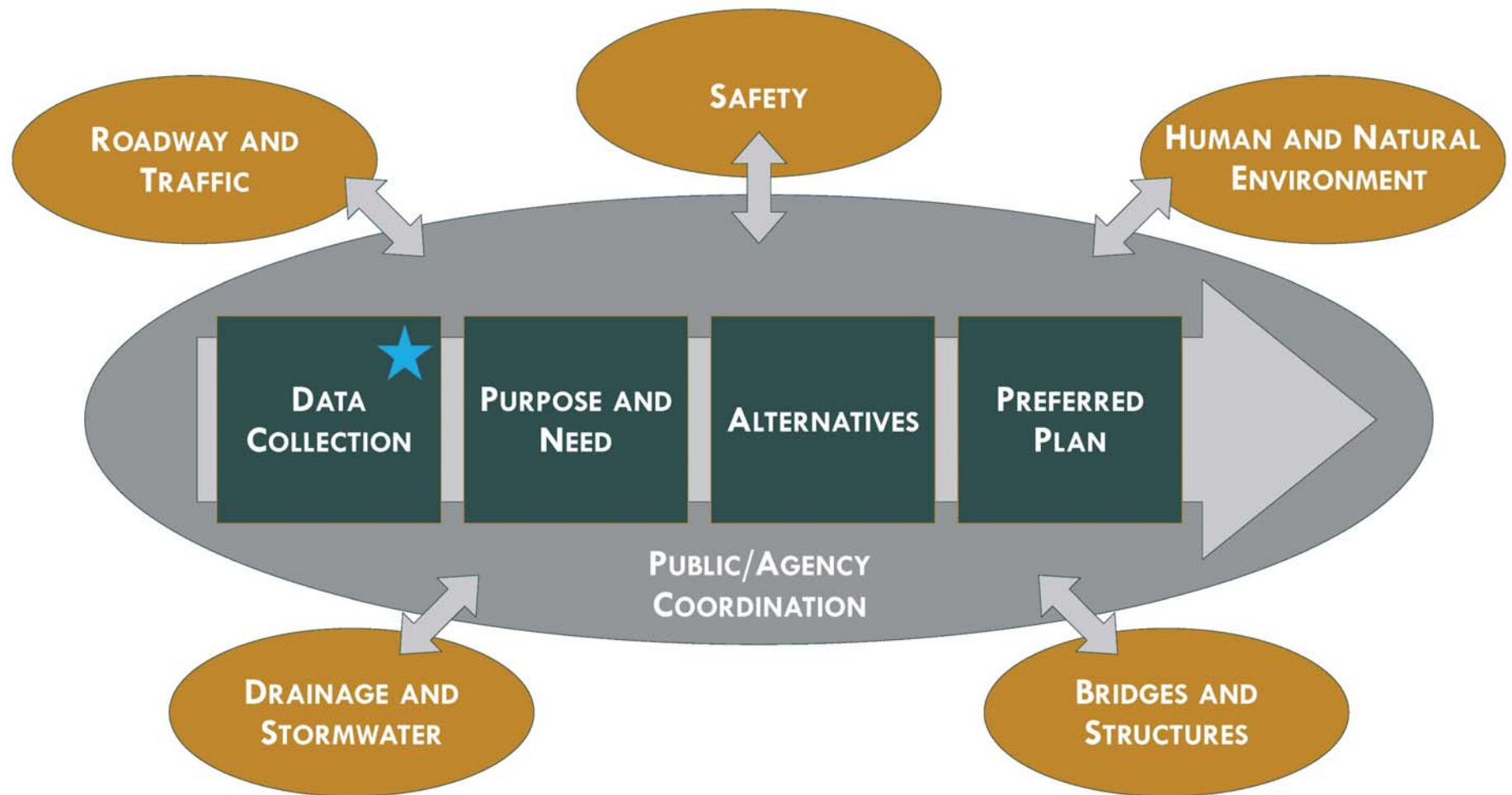
DAUBERMAN ROAD EXTENSION | VEHICLE CRASH DIAGRAM

Kane County, Illinois

(Crash Data from 2010 - 2014, Source: IDOT & KDOT)



Not to Scale



 WE ARE HERE IN THE PROCESS



DAUBERMAN ROAD EXTENSION | PROJECT PROCESS

Kane County, Illinois



Scoping

Solicit public input on issues and concerns the project should address



Purpose & Need

Establish why the project is needed

123

Alternatives

Identify and consider alternatives that address the project's Purpose & Need

1X3

Screening

Review alternatives to determine if they are reasonable and feasible considering human & natural environment, engineering, and cost



Draft Environmental Assessment (EA)

Documents affected environment, range of alternatives and analyses of the impacts

Final EA

Announce Proposed Action based on comments received on Draft EA



We Are Here

Public Involvement



Public Information Meeting
(February 2017)



Public Information Meeting
(Fall 2017)



Public Hearing
(Spring 2018)



DAUBERMAN ROAD EXTENSION | PUBLIC MEETING #1 – FEBRUARY 9, 2017
Kane County, Illinois



THANK YOU

Contact Information

Bonnie Flock
Kane County Project Manager
(630) 406-7359
FlockBonnie@co.kane.il.us

Eric Neubauer
TranSystems Project Manager
(312) 669-5834
esneubauer@transystems.com

kdotcomments@co.kane.il.us
<http://www.co.kane.il.us/dot/constProjects.aspx>



DAUBERMAN ROAD EXTENSION | PUBLIC MEETING #1 — FEBRUARY 9, 2017
Kane County, Illinois

DAUBERMAN ROAD EXTENSION

PHASE I STUDY

ATTACHMENT 7

PUBLIC MEETING PHOTOS



DAUBERMAN ROAD EXTENSION
PHASE I STUDY



PUBLIC INFORMATION MEETING #1 SUMMARY

HINCKLEY-BIG ROCK MIDDLE SCHOOL

FEBRUARY 9, 2017

5:00-7:30 P.M.



MESSAGE BOARD NOTIFICATION AT MEETING LOCATION



DAUBERMAN ROAD EXTENSION
PHASE I STUDY



PUBLIC INFORMATION MEETING #1 SUMMARY

HINCKLEY-BIG ROCK MIDDLE SCHOOL

FEBRUARY 9, 2017

5:00-7:30 P.M.



EXHIBITS IN GYMNASIUM



DAUBERMAN ROAD EXTENSION
PHASE I STUDY



PUBLIC INFORMATION MEETING #1 SUMMARY

HINCKLEY-BIG ROCK MIDDLE SCHOOL

FEBRUARY 9, 2017

5:00-7:30 P.M.



ATTENDEES VIEWING PROJECT EXHIBITS



DAUBERMAN ROAD EXTENSION
PHASE I STUDY



PUBLIC INFORMATION MEETING #1 SUMMARY

HINCKLEY-BIG ROCK MIDDLE SCHOOL

FEBRUARY 9, 2017

5:00-7:30 P.M.



ATTENDEES VIEWING PROJECT EXHIBIT BOARDS



DAUBERMAN ROAD EXTENSION
PHASE I STUDY



PUBLIC INFORMATION MEETING #1 SUMMARY

HINCKLEY-BIG ROCK MIDDLE SCHOOL

FEBRUARY 9, 2017

5:00-7:30 P.M.



PRESENTATION TO ATTENDEES

DAUBERMAN ROAD EXTENSION

PHASE I STUDY



PUBLIC HEARING SUMMARY

Hinckley-Big Rock Middle School
February 15, 2018
5:00 P.M. to 7:30 P.M.

Synopsis

The Public Hearing for the Dauberman Road Extension project was held on Thursday, February 15, 2018 at the Hinckley-Big Rock Middle School, 47W984 US Route 30, Big Rock, Illinois from 5:00 P.M. to 7:30 P.M. The meeting was an open house format with large-scale exhibits of the preferred alternative and other project information boards on display. The exhibits included information on the Dauberman Road Detour Route, and the Temporary Occupancy Section 4(f) impact associated with construction of the multi-use path within the Big Rock Forest Preserve along the south side of Granart Road. A separate comment area was provided for attendees to submit written comments, questions, and concerns. In addition, a court reporter was present if attendees desired to dictate their comment or question. Two, 20-minute live presentations were made to the public at 6:00 P.M. and at 6:45 P.M. by Brian Fairwood of TranSystems.

Representatives from the Kane County Division of Transportation, TranSystems, and BLA, Inc. were available to discuss the project and answer questions.

Outreach

Letters were mailed to approximately **440** stakeholders, **40** representatives from Local Agencies, and **23** Officials on January 25, 2018. A sample invitation letter and mailing list are included as **Attachment 1**.

Advertisements for the public hearing were published in local newspapers and online social media including:

- **Aurora Beacon News:** February 1, 2018 and February 8, 2018
- **Daily Herald:** February 1, 2018 and February 8, 2018
- **Kane County Connects:** (Facebook) February 1, 2018

The certificates of publication are included as **Attachment 2**.

Attendance

The meeting was attended by 61 people. The Sign-In Sheets are included as **Attachment 3**.

The following Agencies were represented:

- Big Rock Drainage District #1
- Big Rock Fire Department
- Illinois House of Representatives
- Kane/Kendall Council of Mayors
- Kane County Board
- Kane County Division of Transportation
- Sugar Grove Fire Protection District

The following Municipalities were represented:

- Big Rock Township
- Sugar Grove Township
- Village of Big Rock
- Village of Sugar Grove

Other Organizations represented included:

- J. Maddock Airport
- Strand Engineering – Village Engineer for the Village of Big Rock

Comments and Input Received

Twelve comments via comment form, email, or mail were received by the County prior to the end of the two-week public comment period on March 1, 2018. The submitted comments covered a variety of topics including:

- | | |
|---------------------|------------------------|
| • Project Benefits | • Neighborhood Impacts |
| • Drainage Concerns | • Project Location |
| • Farm Access | • Safety |
| • Economic Impact | • Sight Distance |
| • Need for Project | • Support for Project |

Table 1 summarizes who comments were received from, the form of the comment, and how it was submitted to Kane County. Five attendees opted to dictate oral statements and questions to the court reporter. The transcripts provided by the court report are included as **Attachment 4**. In one instance, an attendee provided both an oral statement and mailed a comment form to the County. The individual comments and responses are provided in **Attachment 5**.

Table 1: Summary of Comments Received

Comment Received From	Form of Comment	How Submitted	Response Provided
Big Rock Agencies Village President Dean Hummell Township Supervisor Ken Rojek Drainage District #1 President Wade Thompson	Letter	Mail	x x x
Tim Bulthaup	Oral	Court Reporter	x
Marvel Davis (comment submitted by Mr. & Mrs. Hauge)	Oral	Court Reporter	x
David Hall (Big Rock Drainage District)	Oral	Court Reporter	x
Mary Hauge	Email	Email	x
Joe Ludwig	Comment Form	Email	x
Jay Maddock (Maddock Airport)	Comment Form	Mail	x
Robert Moga	Oral	Court Reporter	x
Anthony Spinelli (Strand Assoc. Inc.)	Comment Form	Email	x
Bruce C. Thompson (Summerdale Farm)	Comment Form Oral	Mail Court Reporter	x

Of the nine comments received making a statement of support or concern, the breakdown was as follows:

Support Project	Opposed to Project	Expressed Concerns	Seeking Information
1	0	7	1

No comments were received in opposition to the project. Of the seven comments received expressing concerns, five were concerned about the drainage resulting from the proposed project, one regarding safe sight distance, and one comment regarding farm field access. The drainage concerns were largely focused on regional drainage problems and the desired inclusion of additional improvements to solve significant flooding to the west of the project.

Several additional topics were discussed by attendees with members of the project team including:

- Extension of municipal roadways to the Dauberman Road Extension
- Farm equipment maneuverability
- Pavement condition and jurisdiction of Granart Road
- Project timeline
- Concerns regarding impacts resulting from potentially widening US Route 30
- Localized flooding issues
- Traffic control devices

Presented Materials

The project brochure, presentation slides, exhibit boards, and photographs taken during the event have been included as attachments.

The exhibits boards displayed at the meeting are available on the Kane County Division of Transportation website at <http://www.co.kane.il.us/dot/constProjects/Dauberman.aspx>.

Attachments:

- **Attachment 1:** Mailed Invitation Letter, Recipients, and Notification Area
- **Attachment 2:** Certifications of Publication
- **Attachment 3:** Sign-In Sheets
- **Attachment 4:** Court Reporter Transcript
- **Attachment 5:** Comments Received & Official Responses
- **Attachment 6:** Informational Brochure
- **Attachment 7:** Presentation Slides
- **Attachment 8:** Exhibits Boards
- **Attachment 9:** Public Hearing Photographs

Attachment 1

Mailed Invitation Letter, Recipients, and Notification Area

KANE COUNTY

DIVISION of TRANSPORTATION

Carl Schoedel, P.E.
Director of Transportation
County Engineer



41W011 Burlington Road
St. Charles, IL 60175
Phone: (630) 584-1170
Fax: (630) 584-5265

January 25, 2018

Re: **Dauberman Road Extension – Public Hearing Invitation Letter**

To Whom It May Concern:

The Kane County Division of Transportation (KDOT) wishes to invite you to attend a public hearing to be held concerning the proposed extension of Dauberman Road from US Route 30 to Granart Road, including a grade-separated crossing over the BNSF Railroad and US Route 30.

Date: Thursday, February 15, 2018
Time: 5:00 P.M. to 7:30 P.M.
Location: Hinckley-Big Rock Middle School
47W984 US Route 30
Big Rock, IL 60511

The meeting will present the recommended alignment, completed environmental and field studies, tentative construction schedule, and other information relative to the proposed project. Input, comments, and questions will be solicited on the project. The public hearing will be conducted with an informal open house format with two 20-minute presentations at 6 P.M. and 6:45 P.M. Exhibits will also be available for viewing and discussion with project staff. A detour route will be required during construction of a section of Dauberman Road north of US Route 30. The Kane County Forest Preserve District is a partnering agency on the project and exhibits will be on display which show the Temporary Occupancy Section 4(f) impact associated with the project for the construction of the multi-use path along the south side of Granart Road.

Verbal and written comments from the public regarding this project are requested and may be presented for a period of 14 calendar days following the hearing. Written comments may be submitted either in person or by mail to **Candance Thomas, Senior Project Manager, Kane County Division of Transportation, 41W011 Burlington Road, St. Charles, IL 60175**. Comments must be received on or before March 1, 2018 in order to become part of the official hearing record.

In compliance with the American with Disabilities Act and other Federal and State Laws, the Public Hearing will be accessible to those with disabilities. If any additional assistance is required, please contact Candance Thomas, Senior Project Manager, by telephone at (630) 406-7355 or by email at ThomasCandance@co.kane.il.us at least five (5) business days prior to the hearing such that any additional arrangements can be made.

Sincerely,

Carl Schoedel, P.E.
Director of Transportation/County Engineer



**KANE COUNTY
DIVISION OF TRANSPORTATION**

**DAUBERMAN ROAD EXTENSION
PHASE 1 STUDY**

PUBLIC MEETING NOTIFICATION AREA

BY: DBB DATE: 01/24/17 SCALE: N.T.S.

SENATOR RICHARD DURBIN
UNITED STATES SENATE
230 S. DEARBORN ST. SUITE 3892
CHICAGO, IL 60604

SENATOR TAMMY DUCKWORTH
UNITED STATES SENATE
P.O. BOX 10793
CHICAGO, IL 60610

CONGRESSMAN RANDY HULTGREN
UNITED STATES HOUSE OF
REPRESENTATIVES
40W310 LAFOX ROAD #F2
CAMPTON HILLS, IL 60175

REPRESENTATIVE ROBERT PRITCHARD
ILLINOIS HOUSE OF REPRESENTATIVES
2600 DEKALB AVE.
SYCAMORE, IL 60178

SENATOR DAVE SYVERSON
ILLINOIS STATE SENATE
200 S. WYMAN SUITE #302
ROCKFORD, IL 61101

SUPERVISOR SANDRA CARR
BIG ROCK TOWNSHIP
PO BOX 252
BIG ROCK, IL 60511

EXECUTIVE DIRECTOR MONICA MEYERS
FOREST PRESERVE DISTRICT OF KANE
COUNTY
1996 SOUTH KIRK ROAD, SUITE 320
GENEVA, IL 60134

CHAIRMAN CHRIS LAUZEN
KANE COUNTY BOARD
719 S. BATAVIA AVE.
GENEVA, IL 60134

KANE COUNTY BOARD BILL LENERT
KANE COUNTY BOARD
719 S. BATAVIA AVE.
GENEVA, IL 60134

KANE COUNTY BOARD MICHAEL KENYON
KANE COUNTY BOARD
719 S. BATAVIA AVE.
GENEVA, IL 60134

KANE COUNTY BOARD DREW FRASZ
KANE COUNTY BOARD
719 S. BATAVIA AVE.
GENEVA, IL 60134

SUPERVISOR TOM ROWE
SUGAR GROVE TOWNSHIP
54 SNOW STREET
SUGAR GROVE, IL 60554

PRESIDENT DEAN HUMMELL
VILLAGE OF BIG ROCK
PO BOX 128
BIG ROCK, IL 60511

PRESIDENT P. SEAN MICHELS
VILLAGE OF SUGAR GROVE
10 S MUNICIPAL DRIVE
SUGAR GROVE, IL 60554

CHAIRMAN SCOTT GRYDER
KENDALL COUNTY BOARD
4630 MCLAREN DRIVE
OSWEGO, IL 60543

HIGHWAY CHAIRMAN MATT KELLONG
KENDALL COUNTY HIGHWAY CHAIRMAN
9750 SCHLAPP ROAD
YORKVILLE, IL 60560

VICE CHAIRMAN LYNN CULICK
KENDALL COUNTY BOARD
232 ANGELA CIRCLE
OSWEGO, IL 60543

KENDALL COUNTY BOARD ELIZABETH
FLOWERS
KENDALL COUNTY BOARD
1995 GRANDVIEW
MONTGOMERY, IL 60538

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KENDALL COUNTY BOARD
244 JETER ROAD
PLANO, IL 60545

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KENDALL COUNTY
111 W. FOX STREET
YORKVILLE, IL 60560

COUNTY ENGINEER FRANCIS KLAAS
KENDALL COUNTY
6780 ROUTE 47
YORKVILLE, IL 60560

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200 N. MAIN STREET
SYCAMORE, IL 60178

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DEKALB COUNTY
1826 BARBER GREENE ROAD
DEKALB, IL 60115

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BIG ROCK FIRE DEPARMENT
47W863 E 2ND ST PO BOX 25
BIG ROCK, IL 60511

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BIG ROCK HISTORICAL SOCIETY
48W455 HINCKLEY ROAD
BIG ROCK, IL 60511

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HIGHWAY COMMISSIONER
BIG ROCK TOWNSHIP HIGHWAY DEPT
47W860 E 2ND ST - BIG ROCK, IL 60511

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43W636 US ROUTE 30
SUGAR GROVE, IL 60554

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PROJECT ENGINEER
BNSF RAILWAY - CHICAGO DIVISION
3611 WEST 38TH ST - CHICAGO, IL 60632

GIRL SCOUTS OF NORTHERN ILLINOIS
46W253 CAMP DEAN ROAD
BIG ROCK, IL 60511

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HINCKLEY BIG ROCK CUSD #429
700 E LINCOLN HWY
HINCKLEY, IL 60520

MS. JULIE MELNYK, PRINCIPAL
HINCKLEY BIG ROCK ELEMENTARY SCHOOL
600 W LINCOLN HWY
HINCKLEY, IL 60520

MR. JAY BRICKMAN, PRINCIPAL
HINCKLEY BIG ROCK HIGH SCHOOL
700 E LINCOLN HWY
HINCKLEY, IL 60520

MR. JEFF STROUSS, PRINCIPAL
HINCKLEY BIG ROCK MIDDLE SCHOOL
47W984 US ROUTE 30
BIG ROCK, IL 60511

IDNR/OFFICE OF WATER RESOURCES
2050 WEST STEARNS ROAD
BARTLETT, IL 60103

MR. BRIAN VERCRUYSE
SENIOR RAIL SAFETY SPECIALIST
ILLINOIS COMMERCE COMMISSION
527 EAST CAPITOL AVENUE
SPRINGFIELD, IL 62701

IEPA
1021 NORTH GRAND AVE. EAST
P.O. BOX 19276
SPRINGFIELD, IL 62794

MR. JOSE RIOS, ACTING REGION 1
ENGINEER, IDOT
201 WEST CENTER COURT
SCHAUMBURG, IL 60196

MS. KIMBERLY MURPHY
CONSULTANT STUDIES UNIT HEAD, IDOT
201 WEST CENTER COURT
SCHAUMBURG, IL 60196

MS. MARILYN SOLOMON, FIELD ENGINEER
IDOT
201 WEST CENTER COURT
SCHAUMBURG, IL 60196

ILLINOIS HISTORIC PRESERVATION
AGENCY
1 N OLD STATE CAPITOL PLAZA
SPRINGFIELD, IL 62701

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J MADDOCK AIRPORT
PO BOX 232
SUGAR GROVE, IL 60554

MS. JODIE WOLLNIK, ASST DIRECTOR
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WATER RESOURCES DIVISION
719 S. BATAVIA AVE, BUILDING A
GENEVA, IL 60134

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KANE COUNTY FARM BUREAU
2N710 RANDALL ROAD
ST CHARLES, IL 60174

MR. DONALD KRAMER, SHERIFF
KANE COUNTY SHERRIFF'S OFFICE
37W755 IL RTE 38
ST CHARLES, IL 60175

MR. TOM BUMGARNER, CHIEF DEPUTY
KANE COUNTY SHERRIFF'S OFFICE
37W755 IL RTE 38
ST CHARLES, IL 60175

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KANE-DUPAGE SOIL & WATER
CONSERVATION DISTRICT
2315 DEAN STREET, SUITE 100
ST CHARLES, IL 60175

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REGIONAL PLANNING LIAISON
KANE-KENDALL COUNCIL OF MAYORS
41W011 BURLINGTON ROAD
ST CHARLES, IL 60175

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KANELAND HIGH SCHOOL
47W326 KESLINGER ROAD
MAPLE PARK, IL 60151

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OFFICER
RIDE ILLINOIS
2550 CHESHIRE DR - AURORA, IL 60504

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SUGAR GROVE, IL 60554

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SUGAR GROVE DRAINAGE DISTRICT #1
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SUGAR GROVE FIRE DEPARTMENT
25 S MUNICIPAL DRIVE
SUGAR GROVE, IL 60554

MR. PAT ROLLINGS, POLICE CHIEF
SUGAR GROVE POLICE DEPARTMENT
10 S MUNICIPAL DRIVE
SUGAR GROVE, IL 60554

MR. GREG HUGGINS
HIGHWAY COMMISSIONER
SUGAR GROVE TOWNSHIP HWY DEPT
54 SNOW ST - SUGAR GROVE, IL 60554

US ARMY CORPS OF ENGINEERS
231 SOUTH LASALLE STREET, SUITE 1500
CHICAGO, IL 60604

US FISH & WILDLIFE SERVICES
230 S DEARBORN ST STE 2938
CHICAGO, IL 0

MR. TIM MAY, CLERK
VILLAGE OF BIG ROCK
PO BOX 128
BIG ROCK, IL 60511

MR. BRENT EICHELBERGER
VILLAGE ADMINISTRATOR
VILLAGE OF SUGAR GROVE
10 S MUNICIPAL DRIVE
SUGAR GROVE, IL 60554

MS. MICHELE PIOTROWSKI, VILLAGE ENGINEER
VILLAGE OF SUGAR GROVE
(EEI ENGINEERING)
10 S MUNICIPAL DR - SUGAR GROVE, IL 60554

MR. TONY SPECIALE
DIRECTOR OF PUBLIC WORKS
VILLAGE OF SUGAR GROVE
601 HEARTLAND DRIVE
SUGAR GROVE, IL 60554

MR. WALTER MAGDZIARZ
COMMUNITY DEVELOPMENT DIRECTOR
VILLAGE OF SUGAR GROVE
10 S MUNICIPAL DRIVE
SUGAR GROVE, IL 60554

WELSH CEMETERY ASSOCIATION
PO BOX 145
BIG ROCK, IL 60511

MR. CHRIS BYARS
TRANSPORTATION ENGINEER
FEDERAL HIGHWAY ADMINISTRATION
3250 EXECUTIVE PARK DRIVE
SPRINGFIELD, IL 62703

LESTER & VALERIE LANDMEIER
47W466 WHEELER ROAD
SUGAR GROVE, IL 60554

SANDRA BRACKETT
45W336 WHEELER ROAD
SUGAR GROVE, IL 60554

MARION & JAMES SCOTT
45W833 WHEELER RD
SUGAR GROVE, IL 60554-9534

WEST WIND PROPERTIES LLC
49W697 HINCKLEY RD
BIG ROCK, IL 60511-9324

F&H LEASING, INC.
1020 AIRPARK DRIVE
SUGAR GROVE, IL 60554

OLD SECOND BANK TRUST #205349
37 S RIVER ST
AURORA, IL 60506

SCHEFF GROUP, LLC
1020 AIRPARK DRIVE
SUGAR GROVE, IL 60554

OLD SECOND NATIONAL BANK AURORA,
GUM ROBERT
R F D Box 51
BIG ROCK, IL 60511

EDWARD A COX JR.
119 INDIAN TRAIL RD
OAKBROOK, IL 60523-2794

PAULA MESCH TRUST
P.O. BOX 127 - 222 E. MARKET ST.
PANORA, IA 50216

FRAZER & JUDITH ANN ECKLES
PO BOX 57
BIG ROCK, IL 60511

PAM BLACK
100 GALENA ST EAST RT 30
BIG ROCK, IL 60511

SUSAN L. DUKES
PO BOX 204 200 E GALENA ST
BIG ROCK, IL 60511

RYAN J. ZIMMERMAN
206 E GALENA
BIG ROCK, IL 60511

BANK OF NAPERVILLE, ALBIERO
ERNEST F
PO BOX 75
BIG ROCK, IL 60511

BRIAN & KIM WARD
1775 FODDERSTACK MOUNTAIN LOOP
GREENEVILLE, TN 37745-2637

SCOTT E & TRUDI SCHOGER
47W780 ROUTE 30, PO BOX13
BIG ROCK, IL 60511

ROBERT & SHIRLY SCHOGER
47W662 RTE 30
BIG ROCK, IL 60511

JAY & MARGARET MADDOCK
06S772 DAUBERMAN RD PO BOX 232
SUGAR GROVE, IL 60554

EDMUND & STELLA BRONGIEL
9120 RTE 34
YORKVILLE, IL 60560

MARVIN L. & EUNICE K. GAPINSKE
47W366 RTE 30
BIG ROCK, IL 60511

JAMES R & MARY A PEPICH
47W302 RTE 30
BIG ROCK, IL 60511

OLD KENT BANK, EXECUTIVE CENTER
47W210 RTE 30
BIG ROCK, IL 60511

CORY SCHOLLMMEYER
6S578 DAUBERMAN RD
BIG ROCK, IL 60511

GARY LEE COLEMAN
212 N SCOVILLE AVE
OAK PARK, IL 60302

KAREN & CLAYTON HANNINEN
06S920 DAVIS RD
BIG ROCK, IL 60511

WILLIAM M WACKERLIN
795 AUDREY AVE
AURORA, IL 60505-1003

ARLENE J STRAUS
6S903 DAVIS RD
BIG ROCK, IL 60511-9475

RALPH & SUSAN HARKISON
PO BOX 3
BIG ROCK, IL 60511-0003

THOMAS IMBURGIA
209 OAK STREET
BIG ROCK, IL 60511

CATHERINE MILBURG
PO BOX 594
ELBURN, IL 60119

GEORGE & ASANEE ENGEL
PO BOX 315
BIG ROCK, IL 60511

DAVID K & LORRAINE K HALL
201 OAK ST
BIG ROCK, IL 60511

DREW OLSON
206 OAK ST
BIG ROCK, IL 60511

AARON W. LAHO
207 W GALENA
BIG ROCK, IL 60511

RODNEY D & CHERYL J BARTLETT
205 W GALENA RD
BIG ROCK, IL 60511

DONALD & JEAN WHITE
203 W RTE 30 - P O BOX 245
BIG ROCK, IL 60511

JORDAN S. BICKFORD
209 W GALENA BLVD
BIG ROCK, IL 60511

ANNA MAE MOODY
208 OAK ST
BIG ROCK, IL 60511-2028

THOMAS J & DAWN M LOSINIECKI
214 OAK ST
BIG ROCK, IL 60511

TERRY D & MARTHA L HUNT
1030 FOREST TRL
SUGAR GROVE, IL 60554-9253

ESTHERS PLACE LLC.
201 W GALENA WEST PO BOX 185
BIG ROCK, IL 60511

48 INC.
8930 WAUKEGAN RD STE 230
MORTON GROVE, IL 60053-2116

PATRICK H & JANIS E FINLEY
PO BOX 126 204 OAK ST
BIG ROCK, IL 60511-0126

BYRON G & DANA K INMAN
103 W GALENA ST
BIG ROCK, IL 60511

KATHY A. METZGER
PO BOX 29 101 W RTE 30
BIG ROCK, IL 60511

SAM A & VIRGINIA L WATERSTREET
105 W GALENA BOX 61
BIG ROCK, IL 60511

LISA WEISSINGER
PO BOX 44
BIG ROCK, IL 60511-0044

KENNETH E. CARLSON
400 WEST SECOND ST
BIG ROCK, IL 60511

MARVEL DAVIS, TRUSTEE
47W066 JERICHO RD
BIG ROCK, IL 60511

GEORGE & ASANEE ENGEL
PO BOX 315
BIG ROCK, IL 60511

RON CHRISTOFFEL
13800 BUNNY LN
SOMONAUK, IL 60552

MID-CITY NATIONAL OF CHICAGO,
ALBIERO ERNEST F
RTE 30 & LINCOLN HIGHWAY
BIG ROCK, IL 60511

PATRICIA A JOVANOVIC
PO BOX 234 208 W GALENA RD
BIG ROCK, IL 60511-0234

THOMAS CANZONERI
102 W 1ST ST
BIG ROCK, IL 60511

SHRADER PROPERTY LLC, .
PO BOX 88
BIG ROCK, IL 60511

FREDERICK E. WILLIS
103 LINCOLN PO BOX 183
BIG ROCK, IL 60511

JOE LUDWIG LLC.
05S500 DAVIS RD
BIG ROCK, IL 60511

DOUG & GINA K ALLEN
PO BOX 162
BIG ROCK, IL 60511

ROBERT & ELIZABETH NOKES
205 LINCOLN ST PO BOX 222
BIG ROCK, IL 60511

FIRST BAPTIST CHURCH OF BIG ROCK
201 LINCOLN AVE
BIG ROCK, IL 60511

JOSEPH & THERESA MORAR
200 S LINCOLN AVE BOX 273
BIG ROCK, IL 60511

GLENN A & ELIZABETH A FRANKLIN
204 S LINCOLN
BIG ROCK, IL 60511

JASON & DEBORAH E HALSTEAD
206 LINCOLN ST
BIG ROCK, IL 60511

THERESA M WASSON
14869 BUDD RD
YORKVILLE, IL 60560-9544

ROBERT & JAMIE BRASFIELD
209 RHODES AVE
BIG ROCK, IL 60511

GENE C & CYNTHIA L NEHRING
PO BOX 119
BIG ROCK, IL 60511-0119

ENGLISH CONGREGATIONAL CHURCH
OF CHRIST
PO BOX 156
BIG ROCK, IL 60511

ALBERT E & EVELYN L LAWRENCE
BOX 215
BIG ROCK, IL 60511

WILLIAM WERNER & LAWRENCE
CONANT
101 W FOURTH ST
BIG ROCK, IL 60511

DENNIS & NANCY FLANAGAN
305 RHODES RD
BIG ROCK, IL 60511

DAN K & BRENDA M ROMPF
307 RHODES AVE
BIG ROCK, IL 60511

JOHN H & DEBORAH A RUH
PO BOX 59
BIG ROCK, IL 60511

JOHN E. HAKE
104W FOURTH ST PO BOX 233
BIG ROCK, IL 60511

JACK & DOROTHY HAKE
PO BOX 104 403 RHODES ST
BIG ROCK, IL 60511-0104

KYLE D. DONKA
405 RHODES ST
BIG ROCK, IL 60511

DAVID A & JENNIFER S BAYLER
407 RHODES AVE
BIG ROCK, IL 60511

MICHAEL D. LONG
409 RHODES PO BOX 122
BIG ROCK, IL 60511

JESSICA M. CLARK
100 W 4TH ST
BIG ROCK, IL 60511

VINCENT R STRAZZABOSCO
7S401 RHODES ST
BIG ROCK, IL 60511-9414

JAMES T & PATSY L RAYMOND
48W008 PRICE RD
BIG ROCK, IL 60511

ZACHARY L FRANZEN
7S673 WELTON DR
BIG ROCK, IL 60511

TERRY BUTTS & BARBARA COLE
PO BOX 310
BIG ROCK, IL 60511-0310

ERIC ANDERSON & TRACI UNDERWOOD
7S641 WELTON DR
BIG ROCK, IL 60511-9409

KEVIN D & DONNA L HOGLE
7S623 WELTON DR
BIG ROCK, IL 60511

GARY E & MARY ELIZABETH HAUGE
7S626 WELTON DR
BIG ROCK, IL 60511

DOUGLAS & JENNIFER SCHLACHTER
48W060 MARY ST
BIG ROCK, IL 60511

ZACHERY J RUH
07S672 WELTON DR
BIG ROCK, IL 60511

ROBERT E & ANGELA M CAREY
07S702 WELTON DR
BIG ROCK, IL 60511

CHARLES & SHARON FISHER
7S530 RHODES RD
BIG ROCK, IL 60511

GARY M & PHYLLIS V INMAN
07S541 WELTON DR
BIG ROCK, IL 60511

TIMOTHY B & MARY S KENNER
7S557 WELTON DR
BIG ROCK, IL 60511

RICKY L & CARA RAUSCH
7S571 WELTON DR
BIG ROCK, IL 60511

CESAER M & NICOLE M ENVERGA
7S587 WELTON DR
BIG ROCK, IL 60511

MICHAEL J & SANDRA M UTE
7S601 WELTON DR
BIG ROCK, IL 60511

JAMES A & RUTHANN DUDEK
48W124 MARY ST
BIG ROCK, IL 60511

GARY R & DEBORAH J PETERSEN
7S518 WELTON DR
BIG ROCK, IL 60511

COLLEEN S CHRISTENSEN
7S546 MARY ST
BIG ROCK, IL 60511

DENNIS W & DENISE A SNYDER
7S560 MARY ST
BIG ROCK, IL 60511

MARY FRANCIS WELTON
PO BOX 236
BIG ROCK, IL 60511

JAMES A. FEIFHEIT
PO BOX 125
BIG ROCK, IL 60511

ALAN & DANA ARRIGHI
07S634 MARY ST
BIG ROCK, IL 60511

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SINKS
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MICHAEL R & JOAN BLAIR
7S670 MARY ST
BIG ROCK, IL 60511

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48W095 MARY ST
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RONALD D & ELLEN M WEITEN
7S572 WELTON DR
BIG ROCK, IL 60511

WILBERT & RUTH HAGEMAN
7S596 WELTON DR
BIG ROCK, IL 60511-9408

BYRON H JR & THERESA A SCOTT
07S608 WELTON DR
BIG ROCK, IL 60511

CYNTHIA J & DALE L DEAN
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LAWRENCE L & JUDITH M CONANT
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MARK S & KRISTAN L NELSON
PO BOX 274
BIG ROCK, IL 60511-0274

KERRY & CYNTHIA KANE
104 RHODES AVE
BIG ROCK, IL 60511

KINGSTON MUTUAL INSURANCE CO, .
420 W MAIN ST
GENOA, IL 60135

SALIJA ZECIRI
954 BLACK WALNUT DR
SUGAR GROVE, IL 60554-9254

MICHAEL BARNES
PO BOX 244
BIG ROCK, IL 60511

ELMER J & GERTRUDE BERKE
PO BOX 186
BIG ROCK, IL 60511

JUSTIN E DOUGLAS
102 JEFFERSON AVE
BIG ROCK, IL 60511

CARY L & DENISE K STEVENSON
47W893 GALENA RD
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PATRICK & JULIE CURRY
211 E GALENA ST
BIG ROCK, IL 60511

MICHAEL D & BERNADETTE A DEAN
104 JEFFERSON
BIG ROCK, IL 60511

LEONARD A & SHIRLEY A WYSOCKI
215 E GALENA ST BOX 127
BIG ROCK, IL 60511

CYNTHIA A & BRYAN HANACEK
106 JEFFERSON AVE
BIG ROCK, IL 60511

SHELLY E JAMES
P O BOX 308, 213 E GALENA ST
BIG ROCK, IL 60511

JOANNE PORCH
PO BOX 85
BIG ROCK, IL 60511-0085

CHRISTOPHER A & MICHELLE I
LAPEYRE
206 RHODES ST
BIG ROCK, IL 60511

DANIEL A & AMY L MOTCHNIK
16475 NEWARK RD
NEWARK, IL 60541-9308

DARRIN A & CHERYL LEE
304 RHODES AVE
BIG ROCK, IL 60511

BARB & DENNIS BAIE
47W341 RTE 30, PO BOX 218
BIG ROCK, IL 60511

JAMES C & LYNDA BRUMMETT
BOX 154
BIG ROCK, IL 60511

RICHARD J & GINA R DUNLAP
310 RHODES
BIG ROCK, IL 60511

PATRICK J & DEBORAH M TESTIN
PO BOX 38 301 JEFFERSON AVE
BIG ROCK, IL 60511

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303 JEFFERSON AVE
BIG ROCK, IL 60511

ANTHONY J & SALLY A KOZUBEK
307 JEFFERSON
BIG ROCK, IL 60511

ROBERT B & TINA M COOK
102 E FOURTH ST
BIG ROCK, IL 60511

EDWARD & MARY BRYANT
47W914 E SECOND ST
BIG ROCK, IL 60511

RAYMOND E TOMLIN
304 JEFFERSON AVE PO BOX 141
BIG ROCK, IL 60511-0141

JOSEPH R ROSSLER
PO BOX 11
BIG ROCK, IL 60511

DON & NORMA MARQUARDT
PO BOX 143
BIG ROCK, IL 60511

HARRY R & SHIRLEY SCHOGER
47W662 ROUTE 30
BIG ROCK, IL 60511

RUH RENTALS LLC, RUSSELL S RUH,
MANAGER
5S230 SWAN RD
BIG ROCK, IL 60511-9772

DELORES SHRADER TRUST,
VACATIONLAND INC
PO BOX 246
BIG ROCK, IL 60511-0246

GREGORY J & RONDA F BEELS
400 RHODES ST
BIG ROCK, IL 60511-9412

JOHN E & LAUREEN M DONAHUE-
GROSSMAN
PO BOX 6
BIG ROCK, IL 60511-0006

JOHN & MARY ROGERSON
07S515 TIMBER VIEW DR
BIG ROCK, IL 60511

JOHN W & RACHEL A WILLEY
407 JEFFERSON AVE
BIG ROCK, IL 60511

DOUGLAS DONALDSON
PO BOX 117
BIG ROCK, IL 60511

KEVIN T CRAWFORD
403 JEFFERSON AVE
BIG ROCK, IL 60511-2022

WILLIAM R SHIMKUS
402 JEFFERSON
BIG ROCK, IL 60511

ROBERT D & DAWN R BROWN
404 JEFFERSON AVE
BIG ROCK, IL 60511

DAVID E. CHRISTENSEN
406 JEFFERSON AVE PO BOX 279
BIG ROCK, IL 60511-0279

WILLIAM C & JANE E BRICKERT
408 JEFFERSON PO BOX 52
BIG ROCK, IL 60511

NICHOLAS W GILLIS
101 EAST ST
BIG ROCK, IL 60511

BRADLEY M KOUKOL
103 FIFTH ST
BIG ROCK, IL 60511

WILLIAM H & SHEILA J VAN BUSKIRK
PO BOX 293
BIG ROCK, IL 60511

DENNIS E & PATRICIA J STORZEK
PO BOX 54
BIG ROCK, IL 60511-0054

FREDERICK G & RITA J SETTLES
47W986 SIXTH ST
BIG ROCK, IL 60511

MARTHA J RODDY
47W966 6TH ST
BIG ROCK, IL 60511-9417

DONALD BELCHER
47W954 6TH ST
BIG ROCK, IL 60511-9417

STEVEN E & MARCEY L LARSON
47W928 6TH ST
BIG ROCK, IL 60511

MARGARET SLOSAR & KRISTOPHER
SCHRADER
47W908 6TH ST
BIG ROCK, IL 60511-9417

MICHAEL & CHARLES R ENCK
47W898 6TH ST
BIG ROCK, IL 60511

KELLY J KALIN
7S384 MADISON AVE
BIG ROCK, IL 60511

JILL RIEDSTRA
7S372 MADISON AVE
BIG ROCK, IL 60511

MAXIMILIAN SUCILLA
7S362 MADISON AVE
BIG ROCK, IL 60511

ROBERT J & DEBRA RAYMOND
45W468 RAYMOND RD
BIG ROCK, IL 60511

HELEN WILES TRUST
07S324 MADISON AVE
BIG ROCK, IL 60511

KELLY D SHAMBLIN
7S310 MADISON AVE
BIG ROCK, IL 60511

JUDITH L & THOMAS E BLINCOE
7S274 MADISON AVE
BIG ROCK, IL 60511

RICHARD T DAHLEEN
07S286 MADISON ST
BIG ROCK, IL 60511

ERIC R & PEGGY SUE EDWARDS
PO BOX 64 400 JEFFERSON ST
BIG ROCK, IL 60511

KEITH MASUR
47W969 SIXTH ST
BIG ROCK, IL 60511

BONNIE J MEYER TRUST
7S451 RHODES ST
BIG ROCK, IL 60511

JORGE URDANETA & ASHLEY PIKARSKI
7S469 RHODES ST
BIG ROCK, IL 60511

TIMOTHY P WEIGAND
07S483 RHODES AVE
BIG ROCK, IL 60511

WESTON W & KIMBERLY D WARD
47W910 TIMBERVIEW DR
BIG ROCK, IL 60511

MARK POSS
45W394 JOHN ST
BIG ROCK, IL 60511

PAUL MILLS
47W850 TIMBERVIEW DR
BIG ROCK, IL 60511

BRUCE & MARY THOMPSON
47W820 TIMBERVIEW DR
BIG ROCK, IL 60511

NEAL A MORRIS
47W794 CAREY ST
BIG ROCK, IL 60511

ROBERT J BUCKLEY
47W913 SIXTH ST
BIG ROCK, IL 60511

STEVE ALLEN
47W435 US HIGHWAY 30
BIG ROCK, IL 60511

RONALD R & GEORGE R REINERT
9685 CORNIELS RD
BRISTOL, IL 60512

DENNIS E. & BARBARA A. BAIE
PO BOX 218
BIG ROCK, IL 60511

GREYER TRUST
728 CARRIAGE HILL LN
SUGAR GROVE, IL 60554

ROBERT D. & BARBARA A. RAYMOND
8S839 JOHN ST
BIG ROCK, IL 60511

EDWARD R. & SHARON LEMMER
47W481 RTE 30
BIG ROCK, IL 60511

STANLEY RADAVIDH
47W379 ROUTE 30
BIG ROCK, IL 60511

SCOTT EDWARD & HEIDI G TREADMAN
7S556 RHODES ST
BIG ROCK, IL 60511

TRENT M & ANN M BYRNE
07S594 RHODES ST
BIG ROCK, IL 60511

LOWELL & JOANNE TOSCH
7S620 RHODES ST
BIG ROCK, IL 60511

LOWELL D & JOANNE BLATNER TOSCH
7S620 RHODES ST
BIG ROCK, IL 60511

DANIEL D & SUSAN E ZOHNER
07S638 RHODES ST
BIG ROCK, IL 60511

LYNN M & C JOYCE NELSON
07S660 RHODES ST
BIG ROCK, IL 60511

VINCENT F JR & BONITA J SMITH
47W972 PRICE RD
BIG ROCK, IL 60511

JIMMY L & JANET K FRANKLIN
7S509 RHODES ST
BIG ROCK, IL 60511

CHRISTINE FREELAND
7S529 RHODES ST
BIG ROCK, IL 60511

KURT F & DAWN M LINDQUIST
7S547 RHODES
BIG ROCK, IL 60511

JAMES PERKIS & KIMBERLY SCHMITZ
07S567 RHODES ST
BIG ROCK, IL 60511

STEPHEN T ALLEN
07S841 RHODES RD
BIG ROCK, IL 60511

JOSEPH L & CATHERINE G STRATMAN
7S536 TIMBERVIEW DR
BIG ROCK, IL 60511

GREGORY K & MARY E GREEN
47W940 TIMBER VIEW
BIG ROCK, IL 60511

DAVID E & ANDREA L BAKER
47W920 TIMBERVIEW DR
BIG ROCK, IL 60511

WILLIAM A & KAREN R THOMPSON
7S582 TIMBERVIEW DR
BIG ROCK, IL 60511

LESLIE HEERDT
07S773 RHODES ST
BIG ROCK, IL 60511

SCOTT W & GLORIA R BICKFORD
8S015 RHODES AVE
BIG ROCK, IL 60511

HEERDT LAND COMPANY
7S773 RHODES ST
BIG ROCK, IL 60511-9406

BARBARA S HOLMES
620 E GREEN ST
FARMER CITY, IL 61842

WAYNE & SHIRLEY BICKFORD
7S932 RHODES AVE
BIG ROCK, IL 60511

LAWRENCE & MARYL HANKES
47W855 TIMBERVIEW DR
BIG ROCK, IL 60511

TIMOTHY J & KELLY M MAY
47W835 TIMBERVIEW DR
BIG ROCK, IL 60511

ROBERT & PATRICIA VANDYK
7S565 TIMBERVIEW DR
BIG ROCK, IL 60511

WILLIAM E & MARY J WHATLEY
47W810 TIMBERVIEW DR
BIG ROCK, IL 60511

ELAINE A & EDWARD A WILLIAMS
7S555 TIMBERVIEW DR
BIG ROCK, IL 60511

DEAN C & KIMBERLY A HUMMELL
47W799 TIMBERVIEW DR
BIG ROCK, IL 60511

DUANE CLARK
47W815 TIMBERVIEW DR
BIG ROCK, IL 60511

MARK D FRANSEN
47W845 TIMBERVIEW DR
BIG ROCK, IL 60511

BRAD A & SANDRA M CLAYPOOL
47W875 TIMBERVIEW DR
BIG ROCK, IL 60511

SEIFERT FAMILY TRUST
47W915 TIMBERVIEW DR
BIG ROCK, IL 60511

CHARLES D & PAULA JEAN SABOTTA
47W941 TIMBERVIEW DR
BIG ROCK, IL 60511

KIM M THORSON
7S838 RHODES AVE
BIG ROCK, IL 60511-9392

ROBERT GWINN II
1038 CHAMBOARD LN
HOUSTON, TX 77018

VIRGINIA BAUMANN
949 SOUTH BATAVIA AVE
GENEVA, IL 60134

BRUCE C. & SHIRLEY R. THOMPSON
205 OAK ST, PO BOX 113
BIG ROCK, IL 60511

JAMES M. JR & GERMAINE GEMBECK
46W563 RTE 30
SUGAR GROVE, IL 60554

MICHAEL C. DUGAN
46W637 RT30
SUGAR GROVE, IL 60554

DAVID & DEBORAH ROSENWINKEL
PO BOX 219
BIG ROCK, IL 60511

HOWARD A & DOROTHY A DALE
7S202 MARY DR
BIG ROCK, IL 60511

PAUL A & ASHLEE A DRAWZ
46W570 DOLLY DR
BIG ROCK, IL 60511

KIMBERLY L AUGUSTINE
46W700 DOLLY DR
BIG ROCK, IL 60511

JASON M & SARAH C ALLEN
46W680 DOLLY DR
BIG ROCK, IL 60511

MICHELE T BLAIR
46W660 DOLLY DR
BIG ROCK, IL 60511

DENNIS M & CATHLEEN P OWEN
46W640 DOLLY DR
BIG ROCK, IL 60511

KEVIN L & CASSANDRA R SCHRAM
46W620 DOLLY DR
BIG ROCK, IL 60511

HENRY & SONJA WASHBURN
46W600 DOLLY DR
BIG ROCK, IL 60511

GREGORY W PEARL
7S203 MARY DR
BIG ROCK, IL 60511

MARK E & MARCIA L LEE
7S217 MARY DR
BIG ROCK, IL 60511

DANIEL S KNIGHT
46W628 GRANART RD
BIG ROCK, IL 60511

JORDAN MROWCZYNSKI
46W618 GRANART RD
BIG ROCK, IL 60511

DANIEL A & KARLA L DAVIS
46W600 GRANART RD
BIG ROCK, IL 60511

MARISA WALSH
46W580 GRANART RD
BIG ROCK, IL 60511

JOHN M & MARY J WALT
46W560 GRANART RD
BIG ROCK, IL 60511

KAREN M FORSELL
46W540 GRANART RD
BIG ROCK, IL 60511

JAY A & BRENDA L TAYLOR
46W528 GRANART RD
BIG ROCK, IL 60511

DAVID & MARCE SUMMER
46W545 KATIE DR
BIG ROCK, IL 60511

JONATHAN D & SARAH J CULVER
7S360 MARY DR
BIG ROCK, IL 60511

JASON GADE & SARAH HUCKS
46W645 KATIE DR
BIG ROCK, IL 60511

JON C & LORI B SIMON
46W625 KATIE LN
BIG ROCK, IL 60511

JEFFREY A & KRISTINE M ODWYER
46W603 KATIE DR
BIG ROCK, IL 60511

LINDA BEAVERS
46W575 KATIE DR
BIG ROCK, IL 60511

DALE F & DEBRA A KRAHN
7S270 MARY DR
BIG ROCK, IL 60511

REIDY W & MARIA FORTH
46W550 KATIE DR
BIG ROCK, IL 60511

ADAM D & JEANETTE T MARTIN
46W691 DOLLY DR
BIG ROCK, IL 60511

PATTI JEAN & VERNON DALE KAHLE
46W681 DOLLY DR
BIG ROCK, IL 60511

JOHN KESSLER
46W655 DOLLY LANE
BIG ROCK, IL 60511

DAVID W & DONNA S RICHARDSON
46W635 DOLLY LANE
BIG ROCK, IL 60511

LYNN W & DIANA L GOBEN
46W615 DOLLY LANE
BIG ROCK, IL 60511

TIMOTHY E & BRIDGET A BULTHAUP
46W597 DOLLY DR
BIG ROCK, IL 60511

STEVEN T & PATRICIA WEEKS
46W652 KATIE DR
BIG ROCK, IL 60511

LORI MARCO
46W650 KATIE DR
BIG ROCK, IL 60511

RICHARD & SHARON LEE KOKES
46W622 KATIE LANE
BIG ROCK, IL 60511

PAUL C & PHYLLIS L NICKELS
46W598 KATIE DR
BIG ROCK, IL 60511

JACK W & PATRICIA L SWIFT
46W578 KATIE LN
BIG ROCK, IL 60511

PETER A BLOCKER
46W370 GRANART RD
BIG ROCK, IL 60511

RITA J JANDT
46W460 GRANART RD
BIG ROCK, IL 60511

TERRY L & JUDY K MINOR
46W440 GRANART RD
BIG ROCK, IL 60511

MICHAEL SCOTT KRAUS
46W420 GRANART RD
BIG ROCK, IL 60511-9791

ANTHONY L & MARY L SIMMONS
46W330 GRANART RD
BIG ROCK, IL 60511

FRANCISCO & GABRIEL SORIA
46W304 GRANART RD
BIG ROCK, IL 60511-9790

JOHN C BRAUER
7S401 MARY DR
BIG ROCK, IL 60511

HOWARD & REBECCA ROSENWINKEL
16007A FRAZIER RD
PLANO, IL 60545

LARRY L JONES
PO BOX 3522
ST. CHARLES, IL 60174-9091

JAMES F KOVAC & ABIGAIL LIND
7S297 MARY DR
BIG ROCK, IL 60511

VIRGINIA GARZA
07S321 MARY DR
BIG ROCK, IL 60511

JILL M & ROBERTO OLIVARES
7S333 MARY DR
BIG ROCK, IL 60511

JOHN C & CARRIE L LOEHMANN
7S357 MARY DR
BIG ROCK, IL 60511

DAVID T & VICKI L HELLYER
46W390 GRANART RD
BIG ROCK, IL 60511

JOSEPH F & GINA M WEBER
46W360 GRANART RD
BIG ROCK, IL 60511

DAVID J L & JONI K LAMON
PO BOX 158
BIG ROCK, IL 60511

RICHARD H & PENELOPE LEE
07S291 DOLLY DR
BIG ROCK, IL 60511

SHANNON E MISUKANIS
07S241 DOLLY DR
BIG ROCK, IL 60511

BURTON & PATRICIA CALLMER
46W137 WHILDIN RD
BIG ROCK, IL 60511

WAYNE E & SUSANNAH M LEE
46W028 GRANART RD
BIG ROCK, IL 60511

LYDIA ROBERTS TRUSTEE
PO BOX 118
ROCHELLE, IL 61068-0118

SCOTT D PAPENDICK
23W567 PINE DR
CAROL STREAM, IL 60188-2685

CHRISTINE DAVIS
196 MAPLE RIDGE LN
MONTGOMERY, IL 60538

RICHARD W ANTHONY
7S295 CAMP DEAN RD
BIG ROCK, IL 60511

MATTHEW T & MICHELLE M HANSEN
7S323 CAMP DEAN RD
BIG ROCK, IL 60511-9761

MARK & SARAH M HEDBORN
7S349 CAMP DEAN RD
BIG ROCK, IL 60511-9761

JOSE A & CAROLYN J VELIZ
7S359 CAMP DEAN RD
BIG ROCK, IL 60511

LESTER L & MARY L HUME
7S405 CAMP DEAN RD
BIG ROCK, IL 60511

J R INVESTMENTS LLC C/O JEROME A
RICH
PO BOX 610
SUGAR GROVE, IL 60554

STEPHEN J MURRAY
PO BOX 280 45W740 GRANART RD
BIG ROCK, IL 60511-0280

JEROME A RICH
07S771 DUGAN RD
SUGAR GROVE, IL 60554

JEROME A & BETTY ANN RICH
PO BOX 610
SUGAR GROVE, IL 60554

JEROME A RICH
PO BOX 369
SUGAR GROVE, IL 60554

RICHARD O'BRIEN TRUST
133 WALNUT CIR
SUGAR GROVE, IL 60554-4221

FRANKS CLASSIC CARS LLC
5 DUFFY LN
SUGAR GROVE, IL 60554-9591

ERIC HOLBY
43W510 TALL OAK TRAIL
ELBURN, IL 60119

6 DUFFY LANE LLC, C/O FRANK E
KARABETSOS
5 DUFFY LN
SUGAR GROVE, IL 60554-9591

DON RAY LEASING LLC
49W697 HINCKLEY RD
BIG ROCK, IL 60511

RICH HARVEST FARMS CO
PO BOX 610
SUGAR GROVE, IL 60554-0610

OLD SECOND BANK TRUST, OLD
SECOND NATL BANK TR #205349
37 S RIVER ST
AURORA, IL 60506-4172

JOSEPH & SHARON RITCHIE
2135 CITY GATE LN STE 420
NAPERVILLE, IL 60563-3062

DAVID T & MARIE G COX
8S076 RHODES ST
BIG ROCK, IL 60511

THOMAS E EVANS
8S097 RHODES ST
BIG ROCK, IL 60511-9482

NICHOLAS & KATIE BRANDONISIO
08S488 GRANART ROAD
BIG ROCK, IL 60511

GERALD & ELIZABETH L INGLES
RR 1
BIG ROCK, IL 60511

GREGORY S & MICHAEL D KLOTZ
48W340 JERICO RD
BIG ROCK, IL 60511-9209

MICHAEL J & MARLYN K HOLLEMBEAK
COTTE
BOX 103 - GRANART RD
BIG ROCK, IL 60511

ALAN T & LISA J ASHER
PO BOX 213
BIG ROCK, IL 60511

JOSEPH & SHARON RITCHIE
2135 CITY GATE LN STE 420
NAPERVILLE, IL 60563

EVERETTE & KATHLEEN WHILDEN
7S353 DUGAN ROAD
SUGAR GROVE, IL 60554

ANASTACIA FLYNN TRUSTEE
13 WALNUT CIRCLE
SUGAR GROVE, IL 60554

JAMES & SHARON DODD
7S437 DUGAN ROAD
SUGAR GROVE, IL 60554

SCOT INDUSTRIES INC
PO BOX 309
SUGAR GROVE, IL 60554

GARY & JUDITH HATTERSLEY TRUST
P.O. BOX 19
SUGAR GROVE, IL 60554

JOHN BASTAIN
1998 BUCKTAIL LANE
SUGAR GROVE, IL 60554

JOHN & WILLIAM BASTAIN C/O HY-TEK
MFG
412 BUCKTAIL LANE
SUGAR GROVE, IL 60554

JEFFREY & NANCY ORCZYKOWSKI
1999 FAYS LANE
SUGAR GROVE, IL 60554

SHAWN & KIRSTEN PJESKY
1995 FAYS LANE
SUGAR GROVE, IL 60554

JEFFREY CRISCI
1991 FAYS LANE
SUGAR GROVE, IL 60554

MARTIN JOSEPH VANA
1987 FAYS LANE
SUGAR GROVE, IL 60554

NORMAN & ELAINE ZIELKE
1983 FAYS LANE
SUGAR GROVE, IL 60554

KEVIN & AZUCENA RICK
1979 FAYS LANE
SUGAR GROVE, IL 60554

DONALD & JAMA FORD
1975 FAYS LANE
SUGAR GROVE, IL 60554

JOANN MCBETH
1967 FAYS LANE
SUGAR GROVE, IL 60554

JOHN & MARISUE NAGY
1971 FAYS LANE
SUGAR GROVE, IL 60554

THOMAS ELLIOT & JODIE POWELL
1950 FAYS LANE
SUGAR GROVE, IL 60554

ALAN & VICKI LETTOW
1954 FAYS LANE
SUGAR GROVE, IL 60554

MICHAEL & KELLY HISLOP
1946 FAYS LANE
SUGAR GROVE, IL 60554

WARREN PROPERTIES - SUPERIOR
METAL PRODUCTS
1993 BUCKTAIL LANE
SUGAR GROVE, IL 60554

BUCKTAIL PARTNERS, LLC
1960 BUCKTAIL LANE
SUGAR GROVE, IL 60554

DARLENE & RONALD VOGEL TRUSTEES
16557 E NICKLAUS DR
FOUNTAIN HILLS, AZ 85268

KIGYOS PARTNERSHIP
1981 BUCKTAIL LANE
SUGAR GROVE, IL 60554

TERI LEATHERBURY
1923 ANNETTE'S CIR
SUGAR GROVE, IL 60554

JOSEPH & BARBARA SCHAUBEL
1925 ANNETTE'S CIR
SUGAR GROVE, IL 60554

DONALD & MARY RICE
1927 ANNETTE'S CIR
SUGAR GROVE, IL 60554

RICHARD & TERESA WAGNER
1929 ANNETTE'S CIR
SUGAR GROVE, IL 60554

WALTER & JACQUELINE FLYTE
1931 ANNETTE'S CIR
SUGAR GROVE, IL 60554

MARK WAGNER
1933 FAYS LANE
SUGAR GROVE, IL 60554

MICHAEL DAVIES
1921 ANNETTE'S CIR
SUGAR GROVE, IL 60554

JOHN & TAMULA STRALKA
1919 ANNETTE'S CIR
SUGAR GROVE, IL 60554

DAVID THILL
1917 ANNETTE'S CIR
SUGAR GROVE, IL 60554

JACK HUGGINS
1915 ANNETTE'S CIR
SUGAR GROVE, IL 60554

BRIAN CLISH & TRESSA TIERNEY
1913 ANNETTE'S CIR
SUGAR GROVE, IL 60554

LAWRENCE JACKSON
1945 FAYS LANE
SUGAR GROVE, IL 60554

ANTHONY & HOLLIS ORY
1943 FAYS LANE
SUGAR GROVE, IL 60554

BARBARA SALOGA
1941 FAYS LANE
SUGAR GROVE, IL 60554

MICHAEL & PAULA PARRETTE
1937 FAYS LANE
SUGAR GROVE, IL 60554

KEITH & LISA ANDERSON
1935 FAYS LANE
SUGAR GROVE, IL 60554

STEPHEN PARISH
1909 ANNETTE'S CIR
SUGAR GROVE, IL 60554

RUCH & DONALD PLAUCK
1918 ANNETTE'S CIR
SUGAR GROVE, IL 60554

KEVIN & MARI JOHNSON
1916 ANNETTE'S CIR
SUGAR GROVE, IL 60554

MELINDA MCCORMACK
1914 ANNETTE'S CIR
SUGAR GROVE, IL 60554

NICHOLAS & SAMANTHA KNOBLAUCH
1920 ANNETTE'S CIR
SUGAR GROVE, IL 60554

CURT KARAS
1930 ANNETTE'S CIR
SUGAR GROVE, IL 60554

ANDREW & DEANE MAYERHOFER
1905 FAYS LANE
SUGAR GROVE, IL 60554

JOHN & GENELL THOMAS
1913 FAYS LANE
SUGAR GROVE, IL 60554

RALPH & MARY GABRIELSON
1909 FAYS LANE
SUGAR GROVE, IL 60554

BRIAN & DAWN AUTEN
1891 FAYS LANE
SUGAR GROVE, IL 60554

ELIZABETH RISCHL
1887 FAYS LANE
SUGAR GROVE, IL 60554

MICHAEL & MARY GROEN
1883 FAYS LANE
SUGAR GROVE, IL 60554

MARC & JENNIFER WISKUR
1879 FAYS LANE
SUGAR GROVE, IL 60554

CURT & NELDA KARAS
1875 FAYS LANE
SUGAR GROVE, IL 60554

CLARK JOHNSON
1871 FAYS LANE
SUGAR GROVE, IL 60554

JAMES J FRITZ, TRUSTEE
525 SUNSET ROAD
WINNETKA, IL 60093

STEVEN ANDERSON & LARENE KORTH
1907 ANNETTE'S CIR
SUGAR GROVE, IL 60554

DOUGLAS KOZELUH
1905 ANNETTE'S CIR
SUGAR GROVE, IL 60554

THOMAS HUGHES & NORA LONDON
1896 FAYS LANE
SUGAR GROVE, IL 60554

GARY & SANDRA KNUTH
1892 FAYS LANE
SUGAR GROVE, IL 60554

BARRY & LEAH MORSCH
1888 FAYS LANE
SUGAR GROVE, IL 60554

CHARLES & JUDY SMITH
1884 FAYS LANE
SUGAR GROVE, IL 60554

STEPHEN & JOAN PRESTAGE
1880 FAYS LANE
SUGAR GROVE, IL 60554

THOMAS & TRACY LIMBRUNNER
1876 FAYS LANE
SUGAR GROVE, IL 60554

GERALD & FRANCINE ADAMS
1872 FAYS LANE
SUGAR GROVE, IL 60554

STEPHANIE & MICHAEL KARTHEISER
1868 FAYS LANE
SUGAR GROVE, IL 60554

JOE & RHONDA SITTERLY
1900 FAYS LANE
SUGAR GROVE, IL 60554

HERMANN & INGE BUCHBERGER
1998 FAYS LANE
SUGAR GROVE, IL 60554

RONALD & BARBARA LEISHER
1994 FAYS LANE
SUGAR GROVE, IL 60554

BRADLEY & COREY FREIDERS
1990 FAYS LANE
SUGAR GROVE, IL 60554

WILLIAM & KAREN SHULL
1986 FAYS LANE
SUGAR GROVE, IL 60554

DONALD & CAROL PFEFFER
1982 FAYS LANE
SUGAR GROVE, IL 60554

DAVID & ANN LUKER
1978 FAYS LANE
SUGAR GROVE, IL 60554

CURTIS & DEBORAH MABERRY
1974 FAYS LANE
SUGAR GROVE, IL 60554

TRAVIS THONE
1970 FAYS LANE
SUGAR GROVE, IL 60554

ROBERT & SANDRA BOHLER
1966 FAYS LANE
SUGAR GROVE, IL 60554

TERRENCE & MARGARET GALLES
1962 FAYS LANE
SUGAR GROVE, IL 60554

KARL & DONNA HEINRICH
1958 FAYS LANE
SUGAR GROVE, IL 60554

DAVID & LINDA ARMBRUST
1942 FAYS LANE
SUGAR GROVE, IL 60554

BRENDA JANCO
1938 FAYS LANE
SUGAR GROVE, IL 60554

CHAD & BRIDGETTE OESTER
1934 FAYS LANE
SUGAR GROVE, IL 60554

STEPHEN & RUTH BELL
1930 FAYS LANE
SUGAR GROVE, IL 60554

GARY & NANCY CLUTE
1926 FAYS LANE
SUGAR GROVE, IL 60554

ROBERT HURST & SHARON DINICOLA
1922 FAYS LANE
SUGAR GROVE, IL 60554

RICHARD & MARGIT FOTRE
300 ANNETTE'S LANE
SUGAR GROVE, IL 60554

THOMAS & SANDRA DUFFY
304 ANNETTE'S LANE
SUGAR GROVE, IL 60554

CELESTINO & RENATA ECCHER
308 ANNETTE'S LANE
SUGAR GROVE, IL 60554

JAMES & KARYN PECHINSKI
1918 FAYS LANE
SUGAR GROVE, IL 60554

RICHARD & MARY LAFLEUR
1914 FAYS LANE
SUGAR GROVE, IL 60554

KEVIN WOBROCK
1912 FAYS LANE
SUGAR GROVE, IL 60554

GWEN BOCKMAN
1908 FAYS LANE
SUGAR GROVE, IL 60554

SCOTT & SHARON MARCZUK
1904 FAYS LANE
SUGAR GROVE, IL 60554

JOSEPH LIES
1901 WHISPERING OAKS LN
SUGAR GROVE, IL 60554

MODENE RUCCI
1905 WHISPERING OAKS LN
SUGAR GROVE, IL 60554

BRIDGETT & JASON KEDZIE
1909 WHISPERING OAKS LN
SUGAR GROVE, IL 60554

JOAN & THEODORE MCKEOWN
301 ANNETTE'S LANE
SUGAR GROVE, IL 60554

BEVERLY ELLEBY
305 ANNETTE'S LANE
SUGAR GROVE, IL 60554

KEITH & LAUREL JOHNSON
1917 WHISPERING OAKS LN
SUGAR GROVE, IL 60554

PHILLIP MAULDING
1912 WHISPERING OAKS LN
SUGAR GROVE, IL 60554

KENNETH SCHAUBEL & SHELLY LADD
1908 WHISPERING OAKS LN
SUGAR GROVE, IL 60554

WILLIAM & COLLEEN HOWARD
1904 WHISPERING OAKS LN
SUGAR GROVE, IL 60554

CURRENT RESIDENT
1012 AIRPARK DRIVE
SUGAR GROVE, IL 60554

CURRENT RESIDENT
1016 AIRPARK DRIVE
SUGAR GROVE, IL 60554

CURRENT RESIDENT
84 N DUGAN ROAD
SUGAR GROVE, IL 60554

CURRENT RESIDENT
1015 AIRPARK DRIVE
SUGAR GROVE, IL 60554

CURRENT RESIDENT
213 OAK STREET
BIG ROCK, IL 60511

CURRENT RESIDENT
48W230 ROUTE 30
BIG ROCK, IL 60511

CURRENT RESIDENT
300 RHODES AVE
BIG ROCK, IL 60511

CURRENT RESIDENT
47W415 ROUTE 30
BIG ROCK, IL 60511

CURRENT RESIDENT
07S723 GRANART ROAD
BIG ROCK, IL 60511

CURRENT RESIDENT
7S261 MARY DRIVE
BIG ROCK, IL 60511

CURRENT RESIDENT
07S273 MARY DRIVE
BIG ROCK, IL 60511

CURRENT RESIDENT
46W192 GRANART ROAD
BIG ROCK, IL 60511

CURRENT RESIDENT
08S046 RHODES AVE
BIG ROCK, IL 60511

CURRENT RESIDENT
8S499 GRANART ROAD
BIG ROCK, IL 60511

CURRENT RESIDENT
7S407 DUGAN ROAD
SUGAR GROVE, IL 60554

CURRENT RESIDENT
20 S DUGAN ROAD
SUGAR GROVE, IL 60554

CURRENT RESIDENT
1974 BUCKTAIL LANE
SUGAR GROVE, IL 60554

CURRENT RESIDENT
1867 FAYS LANE
SUGAR GROVE, IL 60554

Attachment 2

Certifications of Publication

SHAW MEDIA
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CRYSTAL LAKE IL 60039-0250
(815)459-4040

ORDER CONFIRMATION

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Ad #: 1500089

Status: New

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333 PIERCE ROAD SUITE 200
ITASCA IL 60143

Start: 02/01/2018 Stop: 02/08/2018
Times Ord: 2 Times Run: ***
CLEG 1.00 X 90.00 Words: 389
Total CLEG 90.00
Class: C8100 PUBLIC NOTICES
Rate: LEGAL Cost: 260.30
Affidavits: 1

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Phone: (630)438-6400

Fax#:

Email:

Agency:

Ad Descrpt: PUBLIC NOTICE THE KANE CO
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CRYSTAL LAKE IL 60039-0250
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ORDER CONFIRMATION (CONTINUED)

Salesperson: DEANNA HARMAN

Printed at 01/22/18 14:16 by dharm-sm

Acct #: 10067087

Ad #: 1500089

Status: New

PUBLIC NOTICE

THE KANE COUNTY DIVISION OF
TRANSPORTATION
INVITES YOU TO ATTEND A
PUBLIC HEARING

FOR THE DAUBERMAN ROAD EX-
TENSION CATEGORICAL EXCLU-
SION AND TEMPORARY OCCUPAN-
CY SECTION 4(F)

DATE: THURSDAY, FEBRUARY
15TH
TIME: 5:00 P.M. TO 7:30 P.M.

LOCATION:
HINCKLEY-BIG ROCK MIDDLE
SCHOOL
47W984 US ROUTE 30
BIG ROCK, IL 60511

All persons interested in this project are invited to attend a Public Hearing to be held by the Kane County Division of Transportation concerning the proposed extension of Dauberman Road from US Route 30 to Granart Road. The public hearing will be held at the Hinckley-Big Rock Middle School, 47W984 Route 30, Big Rock, IL 60511.

The meeting will present the recommended alignment, completed environmental and field studies, tentative construction schedule, and other information relative to the proposed project. Input, comments, and questions will be solicited on the project. The public hearing will be conducted with an informal open house format with two 20-minute presentations at 6 P.M. and 6:45 P.M. Exhibits will also be available for viewing and discussion with project staff. A detour route will be required during construction of a section of Dauberman Road north of US Route 30. The Kane County Forest Preserve District is a partnering agency on the project and exhibits will be on display which show the Temporary Occupancy Section 4(f) impact associated with the project for the construction of the multi-use path along the south side of Granart Road.

Verbal and written comments from the public regarding this project are requested and may be presented for a period of 14 calendar days following the hearing. Written comments may be submitted either in person or by mail to Candance Thomas, Senior Project Manager, Kane County Division of Transportation, 41W011 Burlington Road, St. Charles, IL 60175. Comments must be received on or before March 1, 2018 in order to become part of the official hearing record.

In compliance with the American with Disabilities Act and other Federal and State Laws, the Public Meeting will be accessible to those with disabilities. If any additional assistance is required, please contact Candance Thomas, Senior Project Manager, by telephone at (630) 406-7355 or by email at ThomasCandance@co.kane.il.us at least five (5) business days prior to the meeting such that any additional arrangements can be made. (Published in the Kane County Chronicle on February 1, 8, 2018) 1500089

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This notice was also placed on a statewide public notice website as required by 5 ILCS 5/2.1.

PUBLICATION DATES: **Feb 01, 2018, Feb 08, 2018.**

The Beacon-News

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**THE KANE COUNTY DIVISION
OF TRANSPORTATION
INVITES YOU TO ATTEND A
PUBLIC HEARING**

FOR THE DAUBERMAN ROAD
EXTENSION
CATEGORICAL EXCLUSION AND
TEMPORARY OCCUPANCY SEC-
TION 4(F)

DATE: THURSDAY, FEBRUARY
15TH
TIME: 5:00 P.M. TO 7:30 P.M.

LOCATION:
HINCKLEY-BIG ROCK MIDDLE
SCHOOL
47W984 US ROUTE 30
BIG ROCK, IL 60511

All persons interested in this project are invited to attend a Public Hearing to be held by the Kane County Division of Transportation concerning the proposed extension of Dauberman Road from US Route 30 to Granhart Road. The public hearing will be held at the Hinckley-Big Rock Middle School, 47W984 Route 30, Big Rock, IL 60511.

The meeting will present the recommended alignment, completed environmental and field studies, tentative construction schedule, and other information relative to the proposed project. Input, comments, and questions will be solicited on the project. The public hearing will be conducted with an informal open house format with two 20-minute presentations at 6 P.M. and 6:45 P.M. Exhibits will also be available for viewing and discussion with project staff. A detour route will be required during construction of a section of Dauberman Road north of US Route 30. The Kane County Forest Preserve District is a partnering agency on the project and exhibits will be on display which show the Temporary Occupancy Section 4(f) impact associated with the project for the construction of the multi-use path along the south side of Granhart Road.

Verbal and written comments from the public regarding this project are requested and may be presented for a period of 14 calendar days following the hearing. Written comments may be submitted either in person or by mail to Candance Thomas, Senior Project Manager, Kane County Division of Transportation, 41W011 Burlington Road, St. Charles, IL 60175. Comments must be received on or before March 1, 2018 in order to become part of the official hearing record.

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2/1, 2/8/2018 5438467

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KANE COUNTY
OUTREACH
COORDINATOR RICK
NAGEL

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API key.

Kane County Government
Center, 719 S. Batavia, Ave.,
Geneva, IL 60134
1-630-208-5133

Browse: [Home](#) / [2018](#) / [February](#) / [KDOT Will Hold Feb. 15 Public Hearing on
Extension of Dauberman Road](#)



KDOT WILL HOLD FEB. 15 PUBLIC HEARING ON EXTENSION OF DAUBERMAN ROAD

February 1, 2018 · by [kanecountyconnects](#) · in [Environment](#), [Events](#), [Government](#),
[Taxpayers](#), [Transportation](#)



The Kane County Division of Transportation will hold a public hearing Thursday, Feb. 15, on the [proposed extension of Dauberman Road](#) from U.S. Route 30 to Granart Road. The public hearing will be held from 5 p.m. to 7:30 p.m. at the Hinckley-Big Rock Middle School, 47W984 Route 30.

All persons interested in this project are invited to attend.

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WHY KANE?

Any time. Email

kanecountyconnects@gmail.com

The meeting will present the recommended alignment, completed environmental and field studies, tentative construction schedule, and other information relative to the proposed project. Input, comments, and questions will be solicited on the project.

The public hearing will be conducted with an informal open house format with two 20-minute presentations at 6 p.m. and 6:45 p.m. Exhibits will also be available for viewing and discussion with project staff.



KANE COUNTY OPENGOV



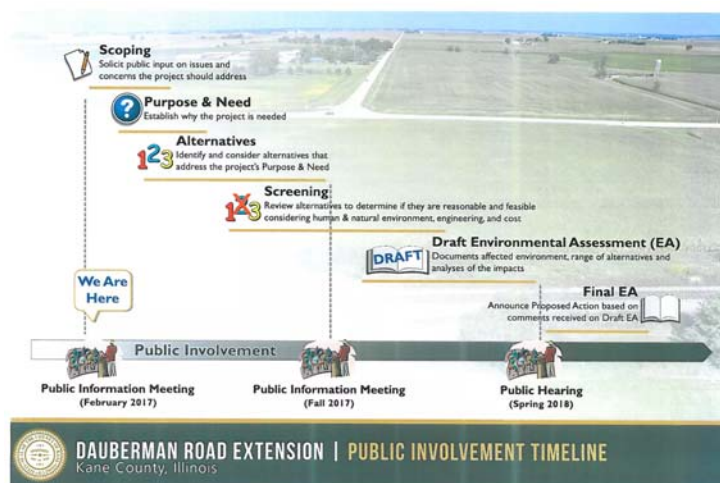
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SOURCE: KDOT website



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Local GOP Congressmen Hits
Garbage Truck In West Virginia](#)

[First Fridays Is Back! Downtown
Aurora Hosts 10 Gallery Openings
Feb. 2 →](#)

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* Agency Commission not included

GROSS PRICE * : **\$457.92**

PACKAGE NAME: IL Govt Legal Aurora Beacon

Order ID: 5438467

* Agency Commission not included

GROSS PRICE * : \$457.92**PACKAGE NAME:** IL Govt Legal Aurora Beacon**Product(s):** SubTrib_Aurora Beacon News, Publicnotices.com, classified.chicagotribune.com**AdSize(s):** 1 Column**Run Date(s):** Thursday, February 01, 2018, Thursday, February 08, 2018**Color Spec.** B/W

Preview

**THE KANE COUNTY DIVISION
OF TRANSPORTATION
INVITES YOU TO ATTEND A
PUBLIC HEARING****FOR THE DAUBERMAN ROAD
EXTENSION
CATEGORICAL EXCLUSION AND
TEMPORARY OCCUPANCY SEC-
TION 4(F)****DATE:** THURSDAY, FEBRUARY
15TH
TIME: 5:00 P.M. TO 7:30 P.M.**LOCATION:**
HINCKLEY-BIG ROCK MIDDLE
SCHOOL
47W984 US ROUTE 30
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2/1, 2/8/2018 5438467

Attachment 3

Sign-In Sheets



PUBLIC MEETING REGISTER

DAUBERMAN ROAD EXTENSION

PUBLIC HEARING

February 15, 2018 - Hinckley-Big Rock Middle School - 5:00 to 7:30 P.M.

Please print your name and address below:

	Name	Address	Phone/E-Mail (optional)
1	Rakel Mays	49W 330 Scott Rd	630 556-3918
2	Ed Mays	474 635 SE Cedar Big Rock	630 649 3520
3	Bob Seiderman	49 511 Forencord Big Rock	630 853-7430
4	Wayne Parson	25 S. Municipal Dr Sugar Grove	630-466-4513
5	Dale Krahn	75270 Mary Dr. Big Rock IL	—
6	Jennifer Becker	Kane Co DOT	beckejennifer@ c.kanecils
7	Bill Lenert	KANE Co. Board	630-319-4947
8	Drew Frazz	Brownlee H. Elm KC Board	AF FRAZZ@AOL.com
9	Robert Pritchard	Hinckley	bobpritchard5@Frontier. com
10	Jim & Marion Scott	Sugar Grove Twp. 49W 365 Scott Rd	
11	Ken & Carol Rojek	Big Rock	KENROJEK@aol.com
12	Joe Ludwig	Big Rock	roosteremc.net
13	Tom Rowe	P.O. Box 465 Sugar Grove	TROWE@SGTOWNSHIP.COM
14	Tom Rickert	41W 011 BURLINGTON RD St. Charles IL 60175	SRUTX@DIEKANE.COM
15	JOHN RUTH	48W 890 Scott Rd Big Rock	



PUBLIC MEETING REGISTER

DAUBERMAN ROAD EXTENSION

PUBLIC HEARING

February 15, 2018 - Hinckley-Big Rock Middle School - 5:00 to 7:30 P.M.

Please print your name and address below:

	Name	Address	Phone/E-Mail (optional)
16	Tom Greyer	47 W 247 RT 3	630 330 1281
17	Ed Wheatley	47 W 810 Timberview	
18	Les Michele	95136 Granart Rd.	630-556-4865
19	MARVIN GAPINSKI	47 W 366 Hwy 30	630-556-4550
20	RALPH HARRISON	Big Rock IL 211 Oak St	630-556-9920 RALPH@HARRISON.COM
21	Don Kramer	1316 Rock sheriff's office	
22	David Shrader	US 30	630 556 3211
23	Bruce Thompson	Box 113, 205 Oak St, Big Rock owas farm across Dauberman	630-608-3286
24	Cheryl Thompson Groetema	719 Sherwood	
25	Linda & James Peterson	La Grange Park, IL 60526 49 W 865 Peterson Rd Maple Park, IL	815-895-8360
26	Joe Whiteside	47 W 210 Ws Hwy 30 Big Rock	630-556-3731 joe@e-a.net
27	Gary & Mary Hauge	75626 Welton Big Rock	630-556-3371
28	Anthony Spinelu	1170 HUBBOLT JOLIET IL	Tony.Spinelu@STRAND.COM
29	Greg Green	47 W 940 TIMBERVIEW DR- BIG ROCK, IL	630 556-3022
30	CARL SCHOGER	47 W 662 Rt-30 Big Rock, IL	(815) 791-1079



PUBLIC MEETING REGISTER

DAUBERMAN ROAD EXTENSION

PUBLIC HEARING

February 15, 2018 - Hinckley-Big Rock Middle School - 5:00 to 7:30 P.M.

Please print your name and address below:

	Name	Address	Phone/E-Mail (optional)
31	WADE Thompson	P O Box 55 65642 DAUS RD	630 649 3960
32	Tim Balthaupt	46 W 597 Dullg Big Rock, IL	630 271-9189
33	Dennis & Barb Bair	47W341 US Rt 30 Big Rock, IL 60511	bbaire2@att.net
34	John Burscher	6280 Route 47 Yorkville, IL 60560	
35	[Signature]		630 586 3851
36	[Signature]	46W618 Granat Rd	630 201-7435
37	[Signature]	46W618 Granat Rd	630-806-0674
38	Joe & Marisa Walsh	46 W 580 Granat Rd	giwalsh@mchsi.com
39	A R. PETERSEN	75518 WELTON DR. Big Rock, IL	GPETERSENE@DYNCO.com
40	John & Lora Long	11461 Co Line Rd Big Rock, IL 60511	longfarms94@gmail.com
41	Rev Bill Riese	70590 Rhodes Big Rock, IL	630-746-1782
42	LARRY & JUDY GRANAT	48W090-PRICE ROAD Big Rock, IL	630-556-4413
43	CHAS BAUMAN	949 S BATAVIA Geneva, IL	6
44	Tony Speciale	601 Heartland Dr Sugar Grove, IL	aspeciale@sugargrove.il.gov
45	SAM & VIRGINIA WATERSTREET	105 W. GALENA Big Rock, IL	



PUBLIC MEETING REGISTER

DAUBERMAN ROAD EXTENSION

PUBLIC HEARING

February 15, 2018 - Hinckley-Big Rock Middle School - 5:00 to 7:30 P.M.

Please print your name and address below:

	Name	Address	Phone/E-Mail (optional)
46	DAVID HALL	201 OAK ST Big Rock, IL	630-450-5439
47	David Chrstensen	P.O. Box 279 Big Rock, IL	(630) 325-2336
48	Steve Michael	55331 Deer Ridge Path Big Rock IL	(630) 556-3902
49	CLAY HAMMEN	63920 DAVIS RD BIG ROCK	630-870-9681
50	GENE NEWRING	305 LINCOLN BIG ROCK PARK DISTRICT	630-556-9599
51	Tom KONEN	424 WHEELER RD. SUGAR GROVE, IL	630-466-8002
52	WALTER MAGDZIARZ	601 HEARTLAND DRIVE SUGAR GROVE, IL 60554	630-391-7225 WMAGDZIARZ@SUGARGROVEIL.GOV
53	Lynn W. Goben	46W615 Dosty Dr Big Rock IL	630-556-3847 lgoben83@aatt.net
54	Beverly Nachtrieb	466192 Granart Big Rock, IL	708-525-2168 bnach2012@gmail.com
55	Mary & John Walt	46W560 Granart Rd. Big Rock	mwalterb2@yahoo.com
56	Don Superst	49W915 Timber View Big Rock IL	630-556-3338
57	Marty Hunt	49W068 Hinckley Rd Big Rock	630-567-1242
58	Jim McCrinnan	43203 Florence Big Rock 60511	630 465 4083
59	Lori Marco	46W650 Katie Driv Big Rock 60511	lmarco@ameritech.net
60	Tom LOSINIECKI	214 OAK ST BIG ROCK IL 60511	630 667 4253 1LTMA21X@GMAIL.COM



PUBLIC MEETING REGISTER

DAUBERMAN ROAD EXTENSION

PUBLIC HEARING

February 15, 2018 - Hinckley-Big Rock Middle School - 5:00 to 7:30 P.M.

Please print your name and address below:

	Name	Address	Phone/E-Mail (optional)
61	KEITH JOHNSON	1917 WHISPERING OARS SUGAR GROVE	KEITHANDLAUREL@YAHOO.COM
62			
63			
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75			

Attachment 4

Court Reporter Transcript

KANE COUNTY DIVISION OF TRANSPORTATION

IN RE:)
)
)
DAUBERMAN ROAD EXTENSION)
US ROUTE 30 TO GRANART ROAD.)

PUBLIC COMMENTS taken at the Public
Hearing of the above-entitled matter, held at
Hinckley-Big Rock Middle School, 47W984 US Route
30, Big Rock, Illinois, reported by Kathleen W.
Bono, CSR, a notary public within and for the
County of DuPage and State of Illinois, on
February 15, 2018, commencing at the hour of
5:00 p.m.

MR. MOGA: Robert Moga. I was the township supervisor out here for 13 years. The road needs to be improved and not only that, but I was glad a year and a half ago, I told a couple of the guys in the drawing room outside the gymnasium, they were talking about putting a subway in under the railroad tracks. I said, you can't do that in Big Rock; we are sitting on top of a lake here. You dig down six or eight feet, you are in water.

05:18:46PM

When I lay in bed, I live up on Scott Road, a mile and three quarters north of here. At night when the train is really moving, like, maybe going 50 miles an hour, a big freight train, I can feel that in bed. I can hear the muffler roar and the little bit of vibrating. That's because we are sitting on water.

05:19:28PM

I introduced Kevin out here to our fire chief and we are doing the emergency route for Kaneville. We have two ambulances here in Big Rock, one of them is for Kaneville people,

and if the train is blocking the crossing, we only have one crossing here in Big Rock. If the train is blocking the crossing, we need a way to get under that and Kevin just told the engineer, I don't care if it's even a gravel road, as long as we are able to get out of the fire station and get up on the bridge and get off and go up Granart Road to Kaneville. John Ruh is our fire chief.

05:28:32PM

We need the project badly. I was the township supervisor for 13 years, and I know what's needed in Big Rock and that project is needed badly.

06:36:10PM

MR. THOMPSON: I'm Bruce Thompson. I own the farm that the road is going through and I'm just wondering about access to the fields, when do I check on that? And I have about five fields that don't show access to the land and I want to know when we decide that or who do I see? My address is 205 Oak Street, P.O. Box 113, Big Rock, 60511, 630-608-3286. I guess that's my biggest concern now is access to the properties.

MR. BULTHAUP: Tim Bulthaup. I live at 46 West 597 Dolly Drive, Big Rock. So that's in Tenerelli subdivision, which is just to the east of this proposed study area, and we already have issues with drainage in that subdivision, have for years. I want to ensure that this project is not going to negatively impact our current drainage and they are taking that into account when they are designing this.

06:42:44PM

MS. HAUGE: Mary Hauge and Gary Hauge. This is for neighbor Marvel Davis. She's concerned whether this is going to end at Granart Road or will it extend to Jericho Road? Her farm is kind of in that pathway, so that was her concern. Her address is 47W066 Jericho Road. She was just kind of concerned that this might travel through her property.

06:43:46PM

MR. HAUGE: Doesn't look like that's going to be the case but we are not sure so we just wanted to put her mind at rest.

MR. HALL: David Hall, 201 Oak Street, Big Rock. I'm here on behalf of myself and the

Big Rock Drainage District. The other two gentlemen over here are Wade Thompson and Carl Schoger. There's three commissioners; I'm one of them.

One of the things that we have noticed in the presentation tonight is the lack of a drainage ditch along the north side of Route 30.

07:03:58PM The drainage district, in cooperation with the village and the township, have long expressed the need for a ditch along the north side of Route 30. Back I believe in the early '70s or the late '60s, the state put curbing in along Route 30 in town.

07:04:54PM The drainage that they put in back then ended and came to the surface in front of what's now DeKane. All of that surface water from Route 30 comes to the surface there in front of DeKane and it's over one of our tiles. If the state tried to do that now, it's illegal; they can't do that. We are not sure how they got away with it back then, but the fact is it

is what it is.

It's currently illegal for surface water from a road to end up draining into back tile and that's the condition that we have.

What we would like to see and what we have been working with or on is to drain the village from that point to the creek. Right now that surface water from in front of DeKane can only flow into our tile. There is no other way for it to drain. The tile wasn't meant for that. This is something that was added.

It can be very simply corrected right now by putting in a drain or a ditch from that point to the creek. By doing so would also help some of the problems that are going to manifest themselves with this Dauberman Road extension.

I don't think that the county, anybody at the county, I don't think that anybody in the village, I don't think that anybody in the township, I don't think that anybody in the drainage district, I don't think

anybody wouldn't agree with the fact that we need this ditch and the current design doesn't include this ditch.

If we don't at least put the provisions for this ditch in place, it's going to be extremely hard to do it later and very costly to do it later. It needs to be done now. We need the state's help. The state needs to become more involved in this project. It's a county project but unfortunately, part of what we are dealing with here is the state. The state needs to address this and correct it and help us. It's not just us, the drainage district, it's the village and the township. I would love to talk to somebody from the state or anybody else.

* * * * *

I, KATHLEEN W. BONO, CSR, do hereby
certify that I am a court reporter doing
business in the State of Illinois, that I
reported in shorthand the testimony given at the
hearing of said cause, and that the foregoing is
a true and correct transcript of my shorthand
notes so taken as aforesaid.

KATHLEEN W. BONO,
C.S.R. No. 84-1423,
Notary Public, DuPage County
237 South Wisconsin Avenue,
Addison, IL 60101-3837

Attachment 5

Comments Received & Official Responses

VILLAGE OF BIG ROCK

KANE COUNTY, ILLINOIS



BIG ROCK TOWNSHIP
408 RHODES STREET
P.O. BOX 252
Big Rock, IL 60511



February 27, 2018

Ms Candace Thomas PE
Senior Project Manager
Kane County Department of Transportation
41W011 Burlington Road
St. Charles, IL 60175

Re: Submittal of Comments for the Dauberman Road Extension project

Dear Ms. Thomas:

On behalf of the residents and boards of the Village of Big Rock, Big Rock Township, and the Big Rock Drainage District #1, we are submitting the following comments for the public record of the February 15 public hearing.

We understand the importance of a future extension of Dauberman Road from it's current southern terminus at US. Route 30 to Granart Road in Big Rock Township. In addition to the planned project benefits already noted, the extension will offer an important alternative to the high traffic volumes currently using Granart Road as a major thoroughfare on a daily basis from I-88 / IL Rt 56 exit to Plano, Sandwich, and other communities in Kendall, DeKalb, and LaSalle counties.

We share a significant concern from the meeting presentation and materials regarding a lack of clear and specific engineering plans related to handling of stormwater runoff and drainage near the Route 30/Dauberman intersection.

We believe that the construction of the overpass and new connector road will significantly impact water drainage flowing from the northwest of Rt 30, under the Rt. 30 and BNSF rights-of-way, and southeast to Welch Creek. The current drainage ditches and/or culverts near and along Rt. 30 are already insufficient to reduce flooding north and south of Rt. 30. The current drainage near the project will require relocation, and there was little discussion of the drainage problems or options for existing enhancements. The placement of retention ponds on the project maps does not adequately address the water outflow, and appears to use existing ditches and culverts which are already inadequate.

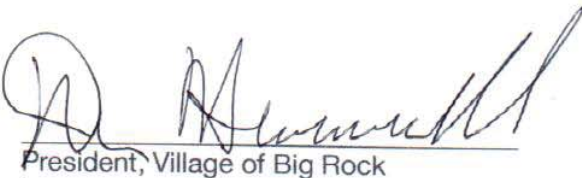
The proposed Dauberman Extension project offers the opportunity to not only address the current drainage problems, but also to allow existing and future commercial development near Rt. 30 to be in compliance with state and county requirements for storm water drainage. We are requesting a copy of the storm water engineering reports associated with this project to date to see how these concerns are adequately addressed.

We remain committed to working with appropriate county or state engineers in the coming months to help provide water drainage solutions that will meet the needs of Big Rock residents and businesses in the future. We look forward to meeting with you and others in the coming weeks.

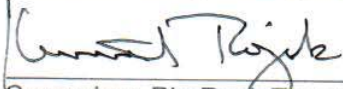
Thank you for the opportunity to provide these comments for inclusion in the public record.

Signed:

Signed:

A handwritten signature in dark ink, appearing to read "D. Hummel", written over a horizontal line.

President, Village of Big Rock

A handwritten signature in dark ink, appearing to read "Curtis Rye", written over a horizontal line.

Supervisor, Big Rock Township

A handwritten signature in dark ink, appearing to read "Wade Thompson", written over a horizontal line.

President, Big Rock Drainage District #1

KANE COUNTY

DIVISION of TRANSPORTATION

Carl Schoedel, P.E.
Director of Transportation
County Engineer



41W011 Burlington Road
St. Charles, IL 60175
Phone: (630) 584-1170
Fax: (630) 584-5265

April 3, 2018

Mr. Dean Hummell, President
Village of Big Rock
408 Rhodes Avenue
PO Box 252
Big Rock, IL 60511

Re: Dauberman Road Extension – *Public Comment*
US Route 30 to Granart Road
Kane County Hwy. No. 62

Dear Mr. Hummell,

Thank you for your interest in the Dauberman Road extension project. As a result of the Public Hearing Open House held on February 15th at the Hinckley-Big Rock Middle School, we received a letter addressed by yourself and two others. This letter is in response to those comments which were provided as a result of the Public Hearing Open House.

We appreciate your involvement in assisting us with addressing the current drainage concerns that exist along US Route 30, in the Village of Big Rock. We understand these existing drainage concerns are significant and resolutions are important to the Big Rock Township. The Kane County Division of Transportation (KDOT) has presented these drainage concerns with the multiple agencies of jurisdiction and there will be additional discussions as the project progresses into the next phase of the project design.

The Dauberman Road Extension improvements were designed to meet the stormwater runoff storage and conveyance requirements of the Kane County Stormwater Management Ordinance, the Illinois Department of Transportation and Federal Emergency Management Agency regulations. Stormwater runoff storage is provided for all impervious areas proposed within the project limits. Release rates from the proposed improvements have been designed to comply with the Kane County Stormwater Ordinance and do not exceed the existing release rates at the existing outlets. Runoff from upstream tributary areas outside of the project limits is being conveyed through the proposed improvements to discharge at existing established drainage ways. Watershed boundaries were delineated and are being maintained through the proposed improvements. Flood hazards are not or will not be increased upstream or downstream of the proposed improvements. Supporting calculations and documentation are provided in the Location Drainage Study demonstrating that applicable local, state and federal regulations have been met by the proposed improvements. Insofar as the Dauberman Road Extension project as a standalone improvement is, there will be no additional flooding as a result of the project.

Additionally, as exhibited at the Public Hearing Open House, the Dauberman Road Extension is consistent with the Village of Big Rock Comprehensive Plan (adopted April 22, 2014). The Future Land Use of the surrounding area has taken into account the extension of Dauberman Road and the future zoning is reflective of this improvement. The proposed project will not preclude any future land use improvements to the area.

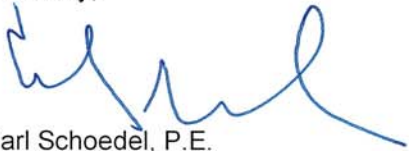
KDOT will be coordinating with the multiple agencies involved, including IDOT, the Village of Big Rock, Big Rock Township, Big Rock Drainage District #1, and the Village of Big Rock's design consultant (Strand Associates). Our goal is to reach an agreement on the appropriate improvements and assigned responsibilities

of the various parties of jurisdiction. We have initiated the outreach and coordination with IDOT and are working to engage them in identifying improvements to the concerns along their route and how, if possible, we can configure the improvements along US 30 to accommodate potential improvements—now or in the future—that can help to alleviate flooding concerns to the west of the Dauberman Road extension project.

As previously requested in your comment letter, an electronic copy of the draft Location Drainage Study was provided for your review on March 5, 2018. If you have any additional questions, please feel free to contact Candi Thomas, Senior Project Manager, by telephone at (630) 406-7355 or by email at thomascandance@co.kane.il.us.

Thank you for your time and interest in the project.

Sincerely,

A handwritten signature in blue ink, appearing to read 'Carl Schoedel', with a stylized flourish at the end.

Carl Schoedel, P.E.
Director of Transportation/County Engineer

KANE COUNTY

DIVISION of TRANSPORTATION

Carl Schoedel, P.E.
Director of Transportation
County Engineer



41W011 Burlington Road
St. Charles, IL 60175
Phone: (630) 584-1170
Fax: (630) 584-5265

April 3, 2018

Mr. Ken Rojek, Township Supervisor
Big Rock Township
408 Rhodes Avenue
PO Box 252
Big Rock, IL 60511

Re: Dauberman Road Extension – *Public Comment*
US Route 30 to Granart Road
Kane County Hwy. No. 62

Dear Mr. Rojek,

Thank you for your interest in the Dauberman Road extension project. As a result of the Public Hearing Open House held on February 15th at the Hinckley-Big Rock Middle School, we received a letter addressed by yourself and two others. This letter is in response to those comments which were provided as a result of the Public Hearing Open House.

We appreciate your involvement in assisting us with addressing the current drainage concerns that exist along US Route 30, in the Village of Big Rock. We understand these existing drainage concerns are significant and resolutions are important to the Big Rock Township. The Kane County Division of Transportation (KDOT) has presented these drainage concerns with the multiple agencies of jurisdiction and there will be additional discussions as the project progresses into the next phase of the project design.

The Dauberman Road Extension improvements were designed to meet the stormwater runoff storage and conveyance requirements of the Kane County Stormwater Management Ordinance, the Illinois Department of Transportation and Federal Emergency Management Agency regulations. Stormwater runoff storage is provided for all impervious areas proposed within the project limits. Release rates from the proposed improvements have been designed to comply with the Kane County Stormwater Ordinance and do not exceed the existing release rates at the existing outlets. Runoff from upstream tributary areas outside of the project limits is being conveyed through the proposed improvements to discharge at existing established drainage ways. Watershed boundaries were delineated and are being maintained through the proposed improvements. Flood hazards are not or will not be increased upstream or downstream of the proposed improvements. Supporting calculations and documentation are provided in the Location Drainage Study demonstrating that applicable local, state and federal regulations have been met by the proposed improvements. Insofar as the Dauberman Road Extension project as a standalone improvement is, there will be no additional flooding as a result of the project.

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KDOT will be coordinating with the multiple agencies involved, including IDOT, the Village of Big Rock, Big Rock Township, Big Rock Drainage District #1, and the Village of Big Rock's design consultant (Strand Associates). Our goal is to reach an agreement on the appropriate improvements and assigned responsibilities

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Thank you for your time and interest in the project.

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Carl Schoedel, P.E.
Director of Transportation/County Engineer

KANE COUNTY

DIVISION of TRANSPORTATION

Carl Schoedel, P.E.
Director of Transportation
County Engineer



41W011 Burlington Road
St. Charles, IL 60175
Phone: (630) 584-1170
Fax: (630) 584-5265

April 3, 2018

Mr. Wade Thompson, President
Big Rock Drainage District #1
408 Rhodes Avenue
PO Box 252
Big Rock, IL 60511

Re: Dauberman Road Extension – *Public Comment*
US Route 30 to Granart Road
Kane County Hwy. No. 62

Dear Mr. Thompson,

Thank you for attending the Public Hearing Open House held on February 15th at the Hinckley-Big Rock Middle School, discussing the proposed Dauberman Road extension. This letter is in response to comments which were provided as a result of the Public Hearing Open House.

We appreciate your involvement in assisting us with addressing the current drainage concerns that exist along US Route 30, in the Village of Big Rock. We understand these existing drainage concerns are significant and resolutions are important to the Big Rock Drainage District #1. The Kane County Division of Transportation (KDOT) has presented these drainage concerns with the multiple agencies of jurisdiction and there will be additional discussions as the project progresses into the next phase of the project design.

The Dauberman Road Extension improvements were designed to meet the stormwater runoff storage and conveyance requirements of the Kane County Stormwater Management Ordinance, the Illinois Department of Transportation and Federal Emergency Management Agency regulations. Stormwater runoff storage is provided for all impervious areas proposed within the project limits. Release rates from the proposed improvements have been designed to comply with the Kane County Stormwater Ordinance and do not exceed the existing release rates at the existing outlets. Runoff from upstream tributary areas outside of the project limits is being conveyed through the proposed improvements to discharge at existing established drainage ways. Watershed boundaries were delineated and are being maintained through the proposed improvements. Flood hazards are not or will not be increased upstream or downstream of the proposed improvements. Supporting calculations and documentation are provided in the Location Drainage Study demonstrating that applicable local, state and federal regulations have been met by the proposed improvements. Insofar as the Dauberman Road Extension project as a standalone improvement is, there will be no additional flooding as a result of the project.

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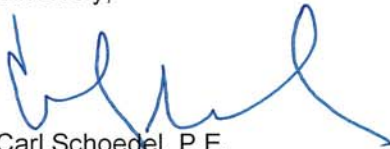
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to engage them in identifying improvements to the concerns along their route and how, if possible, we can configure the improvements along US 30 to accommodate potential improvements—now or in the future—that can help to alleviate flooding concerns to the west of the Dauberman Road extension project.

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Thank you for your interest in the project.

Sincerely,

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Carl Schoedel, P.E.
Director of Transportation/County Engineer

MR. BULTHAUP: Tim Bulthaup. I live at 46 West 597 Dolly Drive, Big Rock. So that's in Tenerelli subdivision, which is just to the east of this proposed study area, and we already have issues with drainage in that subdivision, have for years. I want to ensure that this project is not going to negatively impact our current drainage and they are taking that into account when they are designing this.

06:42:44PM

MS. HAUGE: Mary Hauge and Gary Hauge. This is for neighbor Marvel Davis. She's concerned whether this is going to end at Granart Road or will it extend to Jericho Road? Her farm is kind of in that pathway, so that was her concern. Her address is 47W066 Jericho Road. She was just kind of concerned that this might travel through her property.

06:43:46PM

MR. HAUGE: Doesn't look like that's going to be the case but we are not sure so we just wanted to put her mind at rest.

MR. HALL: David Hall, 201 Oak Street, Big Rock. I'm here on behalf of myself and the

KANE COUNTY

DIVISION of TRANSPORTATION

Carl Schoedel, P.E.
Director of Transportation
County Engineer



41W011 Burlington Road
St. Charles, IL 60175
Phone: (630) 584-1170
Fax: (630) 584-5265

April 3, 2018

Mr. Tim Bulthaup
46W597 Dolly Drive
Big Rock, IL 60511

Re: Dauberman Road Extension – *Public Comment*
US Route 30 to Granart Road
Kane County Hwy. No. 62

Dear Mr. Bulthaup,

Thank you for attending the Public Hearing Open House held on February 15th at the Hinckley-Big Rock Middle School, discussing the proposed Dauberman Road extension. This letter is in response to comments which were provided as a result of the Public Hearing Open House.

We understand you are concerned about additional drainage impacts the project may have in regards to the current drainage concerns within your subdivision. The Dauberman Road Extension improvements were designed to meet the stormwater runoff storage and conveyance requirements of the Kane County Stormwater Management Ordinance, the Illinois Department of Transportation and Federal Emergency Management Agency regulations. Release rates from the proposed improvements have been designed to comply with the Kane County Stormwater Ordinance and do not exceed the existing release rates at the existing outlets. Flood hazards are not or will not be increased upstream or downstream of the proposed improvements. Insofar as the Dauberman Road Extension project as a standalone improvement is, there will be no additional flooding as a result of the project.

The Big Rock Drainage District #1 and Village of Big Rock have made KDOT aware of existing flooding concerns and KDOT is actively working with the multiple agencies of jurisdiction to address the identified drainage concerns.

If you have any additional questions, please feel free to contact Candi Thomas, Senior Project Manager, by telephone at (630) 406-7355 or by email at thomascandance@co.kane.il.us.

Thank you for your time and interest in the project.

Sincerely,

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Carl Schoedel, P.E.
Director of Transportation/County Engineer

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Carl Schoedel, P.E.
Director of Transportation
County Engineer



41W011 Burlington Road
St. Charles, IL 60175
Phone: (630) 584-1170
Fax: (630) 584-5265

April 3, 2018

Mrs. Marvel Davis
47W066 Jericho Road
Big Rock, IL 60511

Re: Dauberman Road Extension – *Public Comment*
US Route 30 to Granart Road
Kane County Hwy. No. 62

Dear Mrs. Davis,

Thank you for your interest in the Dauberman Road extension project. Mr. and Mrs. Hauge had attended the Public Hearing Open House held on February 15th at the Hinckley-Big Rock Middle School which consisted of various information pertaining to the proposed improvements of the Dauberman Road extension. At the Public Hearing Open House, they submitted a comment on your behalf. This letter is in response to the comment Mr. and Mrs. Hauge provided at the Public Hearing Open House, on your behalf.

We understand, from the comment provided, you are interested in knowing about the proposed project limits of the extension of Dauberman Road and if it would end at Granart Road or if it would extend to Jericho Road. The roadway extension of Dauberman Road will end at Granart Road, approximately 0.85 miles north of Jericho Road. Please see the proposed alignment on the enclosed exhibit, for your reference.

If you have any additional questions, please feel free to contact Candi Thomas, Senior Project Manager, by telephone at (630) 406-7355 or by email at thomascandance@co.kane.il.us.

Thank you for your time and interest in the project.

Sincerely,

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Carl Schoedel, P.E.
Director of Transportation/County Engineer

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06:43:46PM

MR. HAUGE: Doesn't look like that's going to be the case but we are not sure so we just wanted to put her mind at rest.

MR. HALL: David Hall, 201 Oak Street, Big Rock. I'm here on behalf of myself and the

Big Rock Drainage District. The other two gentlemen over here are Wade Thompson and Carl Schoger. There's three commissioners; I'm one of them.

One of the things that we have noticed in the presentation tonight is the lack of a drainage ditch along the north side of Route 30.

07:03:58PM The drainage district, in cooperation with the village and the township, have long expressed the need for a ditch along the north side of Route 30. Back I believe in the early '70s or the late '60s, the state put curbing in along Route 30 in town.

07:04:54PM The drainage that they put in back then ended and came to the surface in front of what's now DeKane. All of that surface water from Route 30 comes to the surface there in front of DeKane and it's over one of our tiles. If the state tried to do that now, it's illegal; they can't do that. We are not sure how they got away with it back then, but the fact is it

is what it is.

It's currently illegal for surface water from a road to end up draining into back tile and that's the condition that we have.

What we would like to see and what we have been working with or on is to drain the village from that point to the creek. Right now that surface water from in front of DeKane can only flow into our tile. There is no other way for it to drain. The tile wasn't meant for that. This is something that was added.

It can be very simply corrected right now by putting in a drain or a ditch from that point to the creek. By doing so would also help some of the problems that are going to manifest themselves with this Dauberman Road extension.

I don't think that the county, anybody at the county, I don't think that anybody in the village, I don't think that anybody in the township, I don't think that anybody in the drainage district, I don't think

anybody wouldn't agree with the fact that we need this ditch and the current design doesn't include this ditch.

07:07:00PM If we don't at least put the provisions for this ditch in place, it's going to be extremely hard to do it later and very costly to do it later. It needs to be done now. We need the state's help. The state needs to become more involved in this project. It's a county project but unfortunately, part of what we are dealing with here is the state. The state needs to address this and correct it and help us. It's not just us, the drainage district, it's the village and the township. I would love to talk to somebody from the state or anybody else.

* * * * *

KANE COUNTY

DIVISION of TRANSPORTATION

Carl Schoedel, P.E.
Director of Transportation
County Engineer



41W011 Burlington Road
St. Charles, IL 60175
Phone: (630) 584-1170
Fax: (630) 584-5265

April 3, 2018

Mr. David Hall
201 Oak Street
Big Rock, IL 60511
dkhall@hytechlabs.com

Re: Dauberman Road Extension – *Public Comment*
US Route 30 to Granart Road
Kane County Hwy. No. 62

Dear Mr. Hall,

Thank you for attending the Public Hearing Open House held on February 15th at the Hinckley-Big Rock Middle School, discussing the proposed Dauberman Road extension. This letter is in response to comments which were provided as a result of the Public Hearing Open House.

We appreciate your interest and involvement in assisting us with addressing the current drainage concerns that exist along US Route 30, in the Village of Big Rock. We understand these existing drainage concerns are significant and resolutions are important to the Big Rock Drainage District #1. The Kane County Division of Transportation (KDOT) has presented these drainage concerns with the multiple agencies of jurisdiction and there will be additional discussions as the project progresses into the next phase of the project design.

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KDOT will be coordinating with the multiple agencies involved, including IDOT, the Village of Big Rock, Big Rock Township, Big Rock Drainage District #1, and the Village of Big Rock's design consultant (Strand Associates). Our goal is to reach an agreement on the appropriate improvements and assigned responsibilities of the various parties of jurisdiction. We have initiated the outreach and coordination with IDOT and are working to engage them in identifying improvements to the concerns along their route and how, if possible, we can configure the improvements along US 30 to accommodate potential improvements—now or in the future—that can help to alleviate flooding concerns to the west of the Dauberman Road extension project.

If you have any additional questions, please feel free to contact Candi Thomas, Senior Project Manager, by telephone at (630) 406-7355 or by email at thomascandance@co.kane.il.us.

Thank you for your time and interest in the project.

Sincerely,

A handwritten signature in blue ink, appearing to read 'CSchoedel', with a stylized, cursive script.

Carl Schoedel, P.E.
Director of Transportation/County Engineer

Dan Bruckelmeyer

From: gldysico@transystems.com
Sent: Tuesday, February 20, 2018 1:32 PM
To: Dan Bruckelmeyer; Jacob Thede
Cc: mjsmith@transystems.com; bjholman@transystems.com
Subject: FW: Dauberman Ext - PH comment

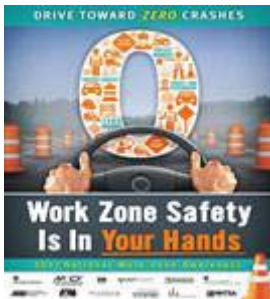
See below...

From: Thomas, Candance [mailto:ThomasCandance@co.kane.il.us]
Sent: Tuesday, February 20, 2018 11:51 AM
To: CH-Brian Fairwood <blfairwood@transystems.com>
Cc: CH-Grace Dysico <gldysico@transystems.com>; CH-Matthew Smith <mjsmith@transystems.com>; CH-Brian Holman <bjholman@transystems.com>
Subject: Dauberman Ext - PH comment

Below is the first email comment I have received...

Candi Thomas, P.E.

Senior Project Manager
Kane County Division of Transportation
Direct Phone: (630) 406-7355
Main Phone: (630) 584-1170
Fax: (630) 584-5265
Email: thomascandance@co.kane.il.us



From: Stack, Lisa
Sent: Tuesday, February 20, 2018 9:02 AM
To: Thomas, Candance <ThomasCandance@co.kane.il.us>
Subject: FW: For Candance Thomas

FYI

From: Mary Hauge [mailto:maryehauge@gmail.com]
Sent: Friday, February 16, 2018 8:38 PM
To: KDOT Comments Email <KDOTComments@co.kane.il.us>
Subject: For Candance Thomas

Thanks for well-organized meeting at Big Rock last night.

After studying the map, I mentally traveled the new route from Rt. 30 onto Dauberman traveling north. When there is a bridge over Rt. 30 and the traffic is moving along at a brisk 55 (or faster) speed, I'm concerned about the sight line of traffic from the south. I would be turning right and accelerating from a complete stop and cars would be somewhat out of sight as they came over the bridge.

Thanks, Mary Hauge



Virus-free. www.avast.com

KANE COUNTY

DIVISION of TRANSPORTATION

Carl Schoedel, P.E.
Director of Transportation
County Engineer



41W011 Burlington Road
St. Charles, IL 60175
Phone: (630) 584-1170
Fax: (630) 584-5265

April 3, 2018

Mrs. Mary Hauge
7S626 Welton Drive
Big Rock, IL 60511
maryehauge@gmail.com

Re: Dauberman Road Extension – *Public Comment*
US Route 30 to Granart Road
Kane County Hwy. No. 62

Dear Mrs. Hauge,

Thank you for attending the Public Hearing Open House held on February 15th at the Hinckley-Big Rock Middle School, discussing the proposed Dauberman Road extension. This letter is in response to comments which were provided as a result of the Public Hearing Open House.

We understand, from the comment form provided, you are concerned about the sight line of traffic when positioned at the new Connector Road and Dauberman Road intersection and looking south along the proposed bridge, which will span US Route 30, when making a right turn to head north along Dauberman Road. Sight distance calculations were completed to ensure that appropriate lines of sight will be provided in the proposed conditions based on design elements of the project. Elevations of the roadway, the slope of the road, speed of the vehicles, and potential obstacles were all evaluated to confirm the safety and design of the intersection.

There will be adequate sight lines from the new Connector Road to safely make a right turn from the stop condition at the new intersection of Dauberman Road.

If you have any additional questions, please feel free to contact Candi Thomas, Senior Project Manager, by telephone at (630) 406-7355 or by email at thomascandance@co.kane.il.us.

Thank you for your time and interest in the project.

Sincerely,



Carl Schoedel, P.E.
Director of Transportation/County Engineer



FEBRUARY 15, 2018
DAUBERMAN ROAD EXTENSION
PUBLIC HEARING
COMMENT FORM

Dauberman Road Extension is a new corridor study that would extend Dauberman Road to the south of US Route 30 and intersect with Granart Road. The project will also include construction of a multi-use path. The proposed project will improve connectivity between the surrounding areas and improve safety by providing a grade-separated crossing over the BNSF Railroad and US Route 30. The proposed multi-use path along the south side of Granart Road will be constructed on Big Rock Forest Preserve property, therefore will require approval of a Section 4(f) Temporary Occupancy document.

This Public Hearing is an opportunity for you to provide input on the project design, recommended alternative, and environmental impacts. KDOT encourages you to submit your input and comments at the public hearing tonight by placing it in the COMMENTS box; provide your verbal comments to a court reporter; or scan and email to kdotcomments@co.kane.il.us or fold it in thirds, tape closed, place a stamp and mail.

All comments submitted through March 1, 2018 will become part of the public hearing record.

Comments/Questions:

We have been very excited as a multi-property business owner in Big Rock for the Dauberman Rd extension to be completed. I have had multiple conversations with the Representative of the State Pritchard and Mayor Hummell as to the positive economic impact the extension would create for the Big Rock community.

Once i reviewed the plan I realized that no consideration was given to the economic growth of Big Rock. Where I believe the proposed will detour traffic and businesses to other corridors.

I strongly suggest the plan be reviewed with the Village of Big Rock to determine if in fact this proposed extension could be changed to create a benefit to all, versus the negative economic impact I believe it will create.

(Optional, Please Print)

Name/Affiliation Joe Ludwig

Address 5S500 Davis Rd

City/State Big Rock, IL

Zip Code 60511

Phone No. (630) 774-5887

E-mail Address rooster@mc.net

I would NOT like to receive information on this project:

☐

KANE COUNTY

DIVISION of TRANSPORTATION

Carl Schoedel, P.E.
Director of Transportation
County Engineer



41W011 Burlington Road
St. Charles, IL 60175
Phone: (630) 584-1170
Fax: (630) 584-5265

April 3, 2018

Mr. Joe Ludwig
5S500 David Road
Big Rock, IL 60511

Re: Dauberman Road Extension – *Public Comment*
US Route 30 to Granart Road
Kane County Hwy. No. 62

Dear Mr. Ludwig,

Thank you for attending the Public Hearing Open House held on February 15th at the Hinckley-Big Rock Middle School, discussing the proposed Dauberman Road extension. This letter is in response to comments which were provided as a result of the Public Hearing Open House.

We appreciate your invested interest in the project and your comment about the economic effect the proposed improvements may have to the Village of Big Rock. The proposed project is a regional improvement and Kane County feels the proposed improvements will improve access across the railroad tracks, enhance safety for all users, improve the regional mobility, and enhance connectivity and linkage of the county's transportation network.

Additionally, as exhibited at the Public Hearing Open House, the Dauberman Road Extension is consistent with the Village of Big Rock Comprehensive Plan (adopted April 22, 2014). The Future Land Use of the surrounding area has taken into account the extension of Dauberman Road and the future zoning is reflective of this improvement. The proposed project will not preclude any future land use improvements to the area.

Kane County has coordinated the project with the Village of Big Rock and will continue to work with the Village as the project progresses through the next phase of the project design. If you have any additional questions, please feel free to contact Candi Thomas, Senior Project Manager, by telephone at (630) 406-7355 or by email at thomascandance@co.kane.il.us.

Thank you for your time and interest in the project.

Sincerely,

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FEBRUARY 15, 2018
DAUBERMAN ROAD EXTENSION
PUBLIC HEARING
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All comments submitted through March 1, 2018 will become part of the public hearing record.

Comments/Questions:

I AM FURNISHING 3 PAGES W/NOTES

RE;

PLEASE INFORM ME

Jay Maddock

60554-0232

(Optional, Please Print)

Name/Affiliation JAY MADDOK

Address 65772 DAUBERMAN

City/State Big Rock, IL (MAIL TO 60554-0232)

Phone No. 630 556 3686

E-mail Address NONE

COMPLETE ZIP IS NECESSARY

I would NOT like to receive information on this project:



RE: 3 DRAWINGS, HERE IN

Kane County DOT
41W011 Burlington Road
St. Charles, IL 60175

20 FEB 2018 PM 4 L



Kane County Division of Transportation
41W011 Burlington Road
St. Charles, IL 60175

60554-0232

60175-649999

Attn: Ms. Candance Thomas, Senior Project Manager
Dauphin Road Extension

(Fold line at line above)

(Fold line at line above)

(Tape this edge)

1 of 3

PROJECT BENEFITS

- New regional connections (new road extension and multi-modal path)
- New grade-separated access over the BNSF Railroad
- New bridge over US Route 30 and Jughandle Intersection
- Stormwater management will be provided via ditches and ponds
- Best Management Practices (BMPs) for water quality benefits will be implemented by using vegetated pond bottoms and grassed bottom ditches

PROJECT REQUIREMENTS

- The Forest Preserve District of Kane County is a partnering agency
 - The path along the south side of Granart Road will be within the Big Rock Forest Preserve
 - A Section 4(f) Temporary Occupancy document will be required for the temporary use of forest preserve property
- Dauberman Road construction, north of US Route 30, will require a road closure and detour (see the Detour Route Map in the exhibit room)
- Land Acquisition
 - 22 properties (20 agricultural, 1 residential, 1 forest preserve)
 - 42.84 acres (41.45 acres of proposed right-of-way, 1.39 acres of temporary easement)
 - Property Acquisitions will comply with the Uniform Relocation Assistance and Real Property acquisition Policies Act of 1970, as amended

NOTE
MAN HOLE
APPROX
18" TILE
6' BELOW GROUND
GIVEN BY B.G.
TOWNSHIP
SIR
OVER

PROJECT SCHEDULE

2016-2018

- Preliminary Engineering
- Environmental Studies and Field Surveys
- Recommended Alternative
- Project Development Report

Phase 1 Planning Process

2018-2020

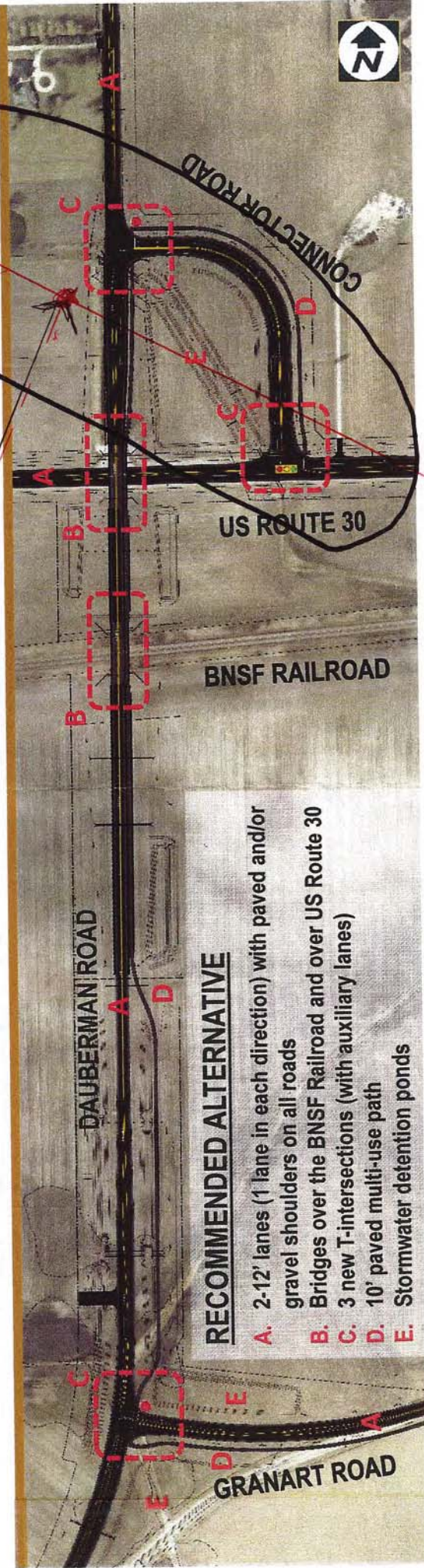
- Plan Preparations
- Contract Documents
- Land Acquisition

Phase 2 Design

- Target Start in 2020/2021
- Construction (2 years)

Phase 3 Construction

PROJECT CONSTRUCTION PENDING AVAILABILITY
OF FUNDING AND RIGHT-OF-WAY ACQUISITION



RECOMMENDED ALTERNATIVE

- A. 2-12' lanes (1 lane in each direction) with paved and/or gravel shoulders on all roads
- B. Bridges over the BNSF Railroad and over US Route 30
- C. 3 new T-intersections (with auxiliary lanes)
- D. 10' paved multi-use path
- E. Stormwater detention ponds

THIS TILE & DRAIN FROM BIG ROCK
TO WELCH CREEK MUST BE PROTECTED
TO DRAIN BIG ROCK * MADDOCK & SCHROGER
FARMS

1043

2 of 3

PROJECT BENEFITS

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How will access this 10 acres?

PROJECT SCHEDULE

2016-2018

- Preliminary Engineering
- Environmental Studies and Field Surveys
- Recommended Alternative
- Project Development Report

2018-2020

- Plan Preparations
- Contract Documents
- Land Acquisition

Target Start in 2020/2021

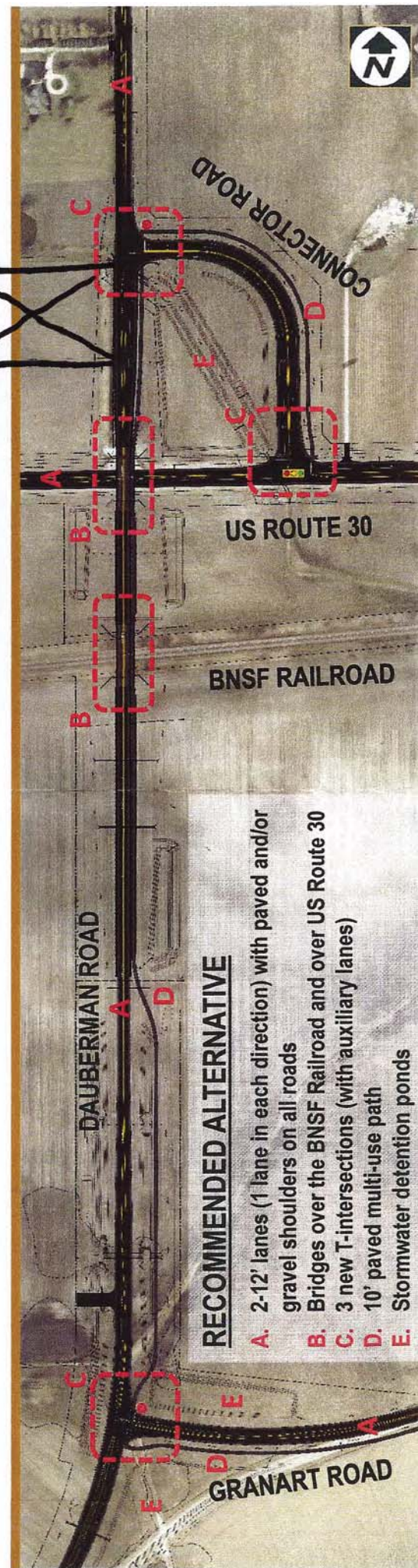
- Construction (2 years)

Phase 1
Planning
Process

Phase 2
Design

Phase 3
Construction

PROJECT CONSTRUCTION PENDING AVAILABILITY
OF FUNDING AND RIGHT-OF-WAY ACQUISITION



3 of 3

PROJECT BENEFITS

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SEE NOTES

WITH OUT THIS DRAINAGE, SURFACE FLOODED

BACK ALMOST TO BIG ROCK TOWN & IN FACT

DID FLOOD THE NORTH SIDE OF BIG ROCK TOWN & IN FACT

AND GAS STATION WITH THE

KANE

FRONT

PROJECT SCHEDULE

2016-2018

- Preliminary Engineering
- Environmental Studies and Field Surveys
- Recommended Alternative
- Project Development Report

2018-2020

- Plan Preparations
- Contract Documents
- Land Acquisition

Target Start in 2020/2021

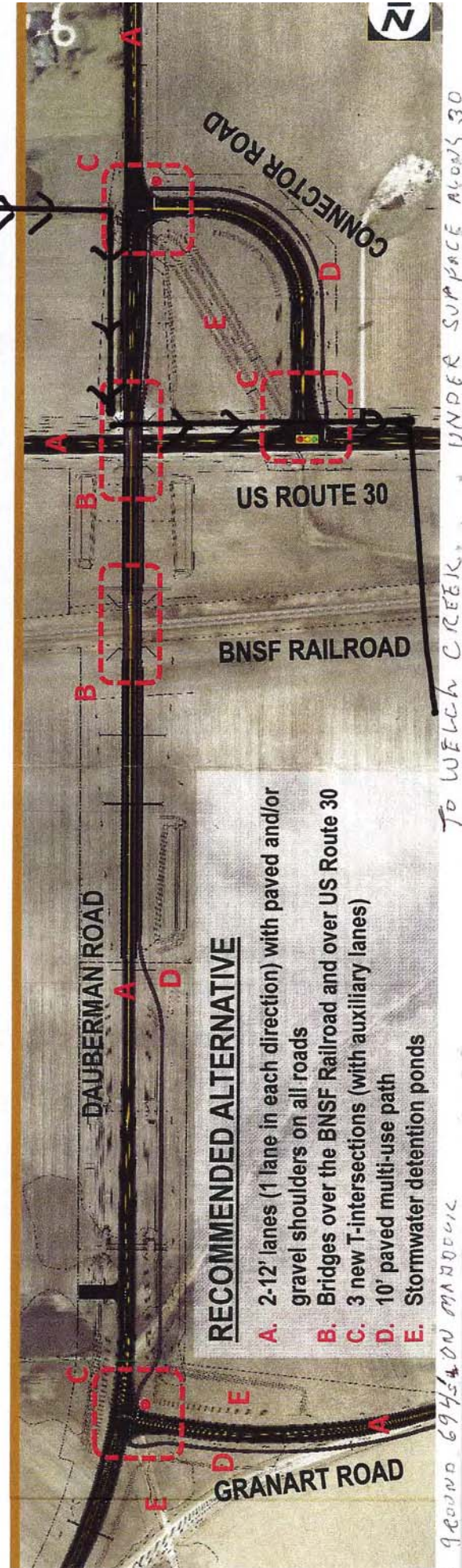
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Phase 1
Planning
Process

Phase 2
Design

Phase 3
Construction

PROJECT CONSTRUCTION PENDING AVAILABILITY OF FUNDING AND RIGHT-OF-WAY ACQUISITION



RECOMMENDED ALTERNATIVE

- A. 2-12' lanes (1 lane in each direction) with paved and/or gravel shoulders on all roads
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- D. 10' paved multi-use path
- E. Stormwater detention ponds

UNDER GROUND EXCEPT ALONG 30 (TWO LINES ALONG RT 30)

UNDER GROUND 69454 ON MANHOLE

TO WELCH CREEK

UNDER SURFACE ALONG 30

KANE COUNTY

DIVISION of TRANSPORTATION

Carl Schoedel, P.E.
Director of Transportation
County Engineer



41W011 Burlington Road
St. Charles, IL 60175
Phone: (630) 584-1170
Fax: (630) 584-5265

April 3, 2018

Mr. Jay Maddock
6S772 Dauberman Road
P.O. Box 232
Big Rock, IL 60554-0232

Re: Dauberman Road Extension – *Public Comment*
US Route 30 to Granart Road
Kane County Hwy. No. 62

Dear Mr. Maddock,

Thank you for attending the Public Hearing Open House held on February 15th at the Hinckley-Big Rock Middle School, discussing the proposed Dauberman Road extension. This letter is in response to comments which were provided as a result of the Public Hearing Open House.

We understand, from the comment form provided, you have a few questions and/or concerns regarding the Dauberman Road extension project. The first comment regarded an 18-inch drain tile near your property. The existing drain tile will be intercepted by a new pipe with equal or greater capacity and re-routed along the proposed improvements. The new pipe will then reconnect to the existing drain tile along the south side of US Route 30.

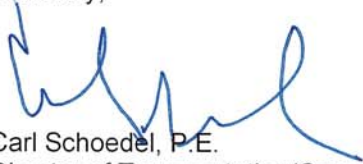
Secondly, a comment was provided regarding the existing drainage channel flowing east from your property, then south along the west side of Dauberman Road to US Route 30 where it continued east, eventually reaching Welch Creek. Runoff from upstream tributary areas outside of the project limits are being conveyed through the proposed improvements to discharge at existing established drainage ways. Watershed boundaries were delineated and are being maintained through the proposed improvements. Flood hazards are not or will not be increased upstream or downstream of the proposed improvements. The Big Rock Drainage District #1 and Village of Big Rock have made the Kane County Division of Transportation (KDOT) aware of existing flooding concerns and KDOT is actively working with the multiple agencies of jurisdiction to address the identified drainage concerns.

Finally, a comment was provided regarding the accessibility to your fields located at the northwest corner of Dauberman Road and US Route 30. As the project progresses into the next phase of the project design, there will be opportunities to coordinate field access locations for each property owner. KDOT will be reaching out to property owners to coordinate the location and conduct field visits, as needed, to ensure that existing field access will be maintained in the proposed condition.

If you have any additional questions, please feel free to contact Candi Thomas, Senior Project Manager, by telephone at (630) 406-7355 or by email at thomascandance@co.kane.il.us.

Thank you for your time and interest in the project.

Sincerely,

A handwritten signature in blue ink, appearing to read 'CSchoedel', with a stylized, flowing script.

Carl Schoedel, P.E.
Director of Transportation/County Engineer

05:18:46PM MR. MOGA: Robert Moga. I was the township supervisor out here for 13 years. The road needs to be improved and not only that, but I was glad a year and a half ago, I told a couple of the guys in the drawing room outside the gymnasium, they were talking about putting a subway in under the railroad tracks. I said, you can't do that in Big Rock; we are sitting on top of a lake here. You dig down six or eight feet, you are in water.

When I lay in bed, I live up on Scott Road, a mile and three quarters north of here. At night when the train is really moving, like, maybe going 50 miles an hour, a big freight train, I can feel that in bed. I can hear the muffler roar and the little bit of vibrating. That's because we are sitting on water.

05:19:28PM I introduced Kevin out here to our fire chief and we are doing the emergency route for Kaneville. We have two ambulances here in Big Rock, one of them is for Kaneville people,

and if the train is blocking the crossing, we only have one crossing here in Big Rock. If the train is blocking the crossing, we need a way to get under that and Kevin just told the engineer, I don't care if it's even a gravel road, as long as we are able to get out of the fire station and get up on the bridge and get off and go up Granart Road to Kaneville. John Ruh is our fire chief.

05:28:32PM We need the project badly. I was the township supervisor for 13 years, and I know what's needed in Big Rock and that project is needed badly.

06:36:10PM MR. THOMPSON: I'm Bruce Thompson. I own the farm that the road is going through and I'm just wondering about access to the fields, when do I check on that? And I have about five fields that don't show access to the land and I want to know when we decide that or who do I see? My address is 205 Oak Street, P.O. Box 113, Big Rock, 60511, 630-608-3286. I guess that's my biggest concern now is access to the properties.

KANE COUNTY

DIVISION of TRANSPORTATION

Carl Schoedel, P.E.
Director of Transportation
County Engineer



41W011 Burlington Road
St. Charles, IL 60175
Phone: (630) 584-1170
Fax: (630) 584-5265

April 3, 2018

Mr. Robert Moga
46W330 Scott Road
Big Rock, IL 60511

Re: Dauberman Road Extension – *Public Comment*
US Route 30 to Granart Road
Kane County Hwy. No. 62

Dear Mr. Moga,

Thank you for attending the Public Hearing Open House held on February 15th at the Hinckley-Big Rock Middle School, discussing the proposed Dauberman Road extension. This letter is in response to comments which were provided as a result of the Public Hearing Open House.

We sincerely appreciate your invested interest in the project and are pleased to hear that you are in support of the proposed improvements. Kane County agrees that the project is greatly needed. The project will improve access across the railroad tracks, enhance the safety for all users, improve the regional mobility, and enhance the connectivity and linkage of the county's transportation network. The Big Rock Drainage District #1 and Village of Big Rock have made KDOT aware of existing flooding concerns and KDOT is actively working with the multiple agencies of jurisdiction to address the identified drainage concerns.

If you have any additional questions, please feel free to contact Candi Thomas, Senior Project Manager, by telephone at (630) 406-7355 or by email at thomascandance@co.kane.il.us.

Thank you for your time and interest in the project.

Sincerely,

A handwritten signature in blue ink, appearing to read "Carl Schoedel", is written over a horizontal line.

Carl Schoedel, P.E.
Director of Transportation/County Engineer



FEBRUARY 15, 2018
DAUBERMAN ROAD EXTENSION
PUBLIC HEARING
COMMENT FORM

Dauberman Road Extension is a new corridor study that would extend Dauberman Road to the south of US Route 30 and intersect with Granart Road. The project will also include construction of a multi-use path. The proposed project will improve connectivity between the surrounding areas and improve safety by providing a grade-separated crossing over the BNSF Railroad and US Route 30. The proposed multi-use path along the south side of Granart Road will be constructed on Big Rock Forest Preserve property, therefore will require approval of a Section 4(f) Temporary Occupancy document.

This Public Hearing is an opportunity for you to provide input on the project design, recommended alternative, and environmental impacts. KDOT encourages you to submit your input and comments at the public hearing tonight by placing it in the COMMENTS box; provide your verbal comments to a court reporter; or scan and email to kdotcomments@co.kane.il.us or fold it in thirds, tape closed, place a stamp and mail.

All comments submitted through March 1, 2018 will become part of the public hearing record.

Comments/Questions:

Strand Associates is representing the Village of Big Rock with input from Big Rock Township and the Big Rock North Side Drainage District.

We offer the following comments,

1. A joint project between the Village, Township and Drainage District is replacing a portion of the existing 20-inch field drainage pipe lying south of Route 30 with a 36-inch diameter pipe. The Village requests that the Dauberman Road improvement plans incorporate a 36-inch field drainage pipe east-west through the right-of-way of the Dauberman Road extension. It is anticipated that the 36-inch pipe will be installed adjacent to the existing 20-inch pipe prior to the construction of Dauberman Road. If the 36-inch field drainage pipe has not been installed prior to the start of the Dauberman Road construction, inclusion of the 36-inch pipe should be coordinated with the Village.
2. It is requested that the Route 30 roadside drainage ditches be designed and graded at a depth such that additional grading modifications can be completed along Route 30 to mitigate existing flooding. The additional ditch modifications would commence approximately 3,500-ft west of Dauberman Road and continue east past Dauberman Road approximately 2,500-ft to the creek crossing at Route 30.
3. Can the proposed Dauberman Road stormwater management facilities include additional storage capacity to assist with drainage issues currently experienced in the Village along this Route 30 corridor?
4. Strand Associates will be conducting a drainage study for the Village in 2018. The Village desires to reserve an opportunity to meet with the Kane County DOT and possibly the Dauberman Road design team to discuss the drainage study results should the study determine additional drainage improvements are necessary. Can the Village submit additional design requests after the public hearing comment period has closed and after the drainage study has been completed?

Thank you.

Name/Affiliation Anthony Spinelli, P.E., CFM / Strand Associates, Inc.

Address 1170 South Houbolt Road

City/State Joliet, Illinois Zip Code 60431

Phone No. 815-744-4200

E-mail Address tony.spinelli@strand.com

I would NOT like to receive information on this project: ☐

KANE COUNTY

DIVISION of TRANSPORTATION

Carl Schoedel, P.E.
Director of Transportation
County Engineer



41W011 Burlington Road
St. Charles, IL 60175
Phone: (630) 584-1170
Fax: (630) 584-5265

April 3, 2018

Mr. Anthony Spinelli
Strand Associates, Inc.
1170 South Houbolt Road
Joliet, IL 60431
Tony.spinelli@strand.com

Re: Dauberman Road Extension – *Public Comment*
US Route 30 to Granart Road
Kane County Hwy. No. 62

Dear Mr. Spinelli,

Thank you for attending the Public Hearing Open House held on February 15th at the Hinckley-Big Rock Middle School, discussing the proposed Dauberman Road extension. We understand that you are representing the Village of Big Rock with input from Big Rock Township and the Big Rock North Side Drainage District. This letter is in response to comments which were provided as a result of the Public Hearing Open House.

We encourage joint participation from all agencies involved in order to address the existing drainage concerns. Thank you for sharing information regarding the replacement of the existing 20-inch diameter field pipe, lying south of US Route 30, with a 36-inch diameter pipe. We will continue to coordinate with the Village of Big Rock, Big Rock Township and the Big Rock Drainage District regarding the proposed size of drainage pipe to be incorporated within the Dauberman Road extension roadway improvements as we continue to progress through the next phase of the project.

The Big Rock Drainage District and Village of Big Rock have made the Kane County Division of Transportation (KDOT) aware of existing flooding concerns along US Route 30 and KDOT is actively working with the multiple agencies of jurisdiction to address the identified drainage concerns. KDOT has presented these drainage concerns and there will be additional discussions, with the multiple agencies of jurisdiction, as the project progresses into the next phase of the project design.

The Dauberman Road Extension improvements were designed to meet the stormwater runoff storage and conveyance requirements of the Kane County Stormwater Management Ordinance, the Illinois Department of Transportation and Federal Emergency Management Agency regulations. Stormwater runoff storage is provided for all impervious areas proposed within the project limits. Release rates from the proposed improvements have been designed to comply with the Kane County Stormwater Ordinance and do not exceed the existing release rates at the existing outlets. Runoff from upstream tributary areas outside of the project limits are being conveyed through the proposed improvements to discharge at existing established drainage ways. Watershed boundaries were delineated and are being maintained through the proposed improvements. Flood hazards are not or will not be increased upstream or downstream of the proposed improvements. Supporting calculations and documentation are provided in the Location Drainage Study demonstrating that applicable local, state and federal regulations have been met by the proposed improvements. Insofar as the Dauberman Road Extension project as a standalone improvement is, there will be no additional flooding as a result of the project.

As the project continues to progress, KDOT along with the Phase II design team will be available to coordinate

Dauberman Road – Public Comment

April 3, 2018

Page 2

with the Village of Big Rock and Strand Associates on its drainage study.

If you have any additional questions, please feel free to contact Candi Thomas, Senior Project Manager, by telephone at (630) 406-7355 or by email at thomascandance@co.kane.il.us.

Thank you for your time and interest in the project.

Sincerely,

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Carl Schoedel, P.E.

Director of Transportation/County Engineer



FEBRUARY 15, 2018
DAUBERMAN ROAD EXTENSION
PUBLIC HEARING
COMMENT FORM

FEB 23 2018

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All comments submitted through March 1, 2018 will become part of the public hearing record.

Comments/Questions:

My biggest concern is field access to the four parcels of land in the four quarters of my divided farm. This access would be important for farm machinery and for commercial development.

** The overpass over Rt. 30 greatly depreciates my road frontage property (Rt. 30) zoned B-2. This would affect my financial position very much.*

(Optional, Please Print)

Name/Affiliation BRUCE C. THOMPSON, OWNER OF SUMMERDALE FARM, SOUTH

Address P.O. Box 113, 205 OAK ST. OF Rt. 30

City/State BIG ROCK, ILLINOIS Zip Code 60511

Phone No. 630-608-3286

E-mail Address cgroetsema@comcast.net (daughter) CHERYL GROETSEMA

I would NOT like to receive information on this project: ☐

and if the train is blocking the crossing, we only have one crossing here in Big Rock. If the train is blocking the crossing, we need a way to get under that and Kevin just told the engineer, I don't care if it's even a gravel road, as long as we are able to get out of the fire station and get up on the bridge and get off and go up Granart Road to Kaneville. John Ruh is our fire chief.

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KANE COUNTY

DIVISION of TRANSPORTATION

Carl Schoedel, P.E.
Director of Transportation
County Engineer



41W011 Burlington Road
St. Charles, IL 60175
Phone: (630) 584-1170
Fax: (630) 584-5265

April 3, 2018

Mr. Bruce Thompson
205 Oak Street
P.O. Box 113
Big Rock, IL 60511

Re: Dauberman Road Extension – *Public Comment*
US Route 30 to Granart Road
Kane County Hwy. No. 62

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We understand that you have concerns regarding access to your farm fields from the new roadway. The current proposed design includes new agricultural field entrances at various locations along the new extended Dauberman Road and also along US Route 30. As the project progresses into the next phase of the project design, there will be opportunities to coordinate field access locations for each property owner. KDOT will be reaching out to property owners to coordinate the locations and conduct field visits, as needed, to ensure that existing field access will be maintained in the proposed condition.

If you have any additional questions, please feel free to contact Candi Thomas, Senior Project Manager, by telephone at (630) 406-7355 or by email at thomascandance@co.kane.il.us.

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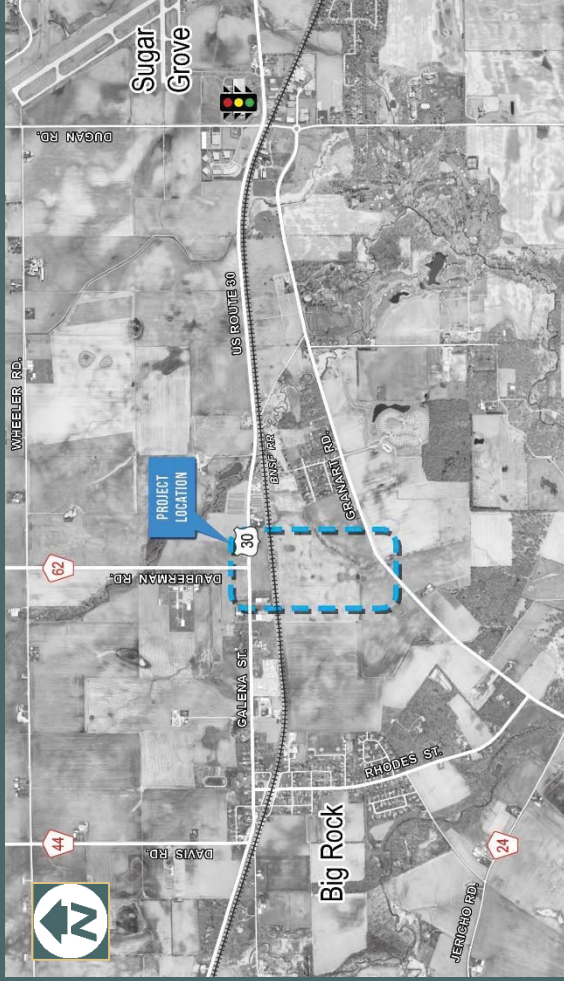
Carl Schoedel, P.E.
Director of Transportation/County Engineer

Attachment 6

Informational Brochure

PROJECT CONTACT

CANDANCE THOMAS, PE
Kane County Senior Project Manager
(630) 406-7355



PROVIDE YOUR INPUT

Comments and questions are welcome at any time, but must be received by the County on or before **March 1, 2018** by close of business to be part of the project record.

Written comments and questions can be submitted during the meeting or mailed to KDOT at the address below to the attention of Ms. Candance Thomas, Senior Project Manager, or emailed to kdotcomments@co.kane.il.us.

FOR MORE INFORMATION, GO TO:

[HTTP://WWW.CO.KANE.IL.US/DOT/CONST/PROJECTS.ASPX](http://www.co.kane.il.us/dot/const/projects.aspx)

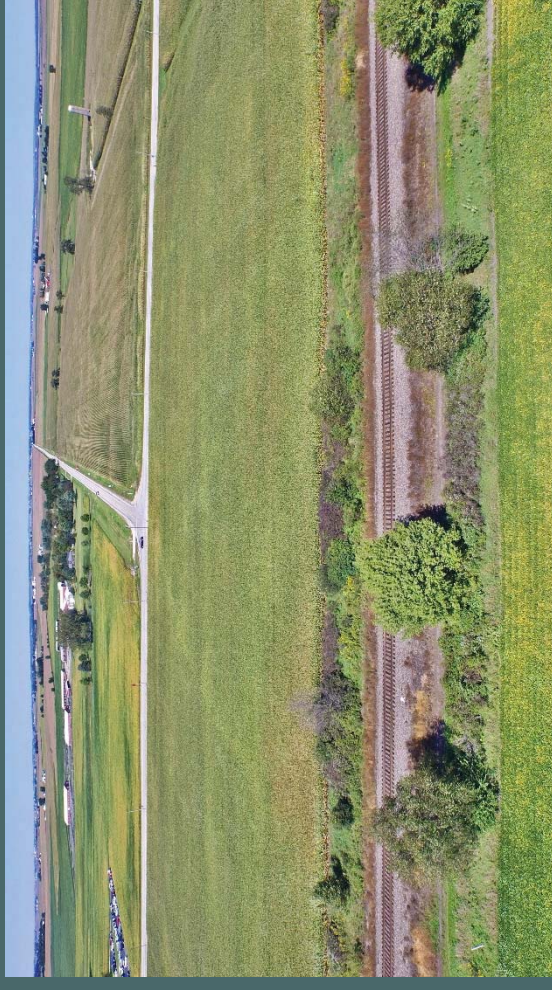


KANE COUNTY DIVISION OF TRANSPORTATION
41W011 BURLINGTON ROAD, ST. CHARLES, IL 60175

KANE COUNTY DIVISION OF TRANSPORTATION



DAUBERMAN ROAD EXTENSION US ROUTE 30 TO GRANART ROAD



PUBLIC HEARING OPEN HOUSE

FEBRUARY 15, 2018
5:00 pm to 7:30 pm

HINCKLEY-BIG ROCK MIDDLE SCHOOL
47W984 US ROUTE 30, BIG ROCK, IL 60511

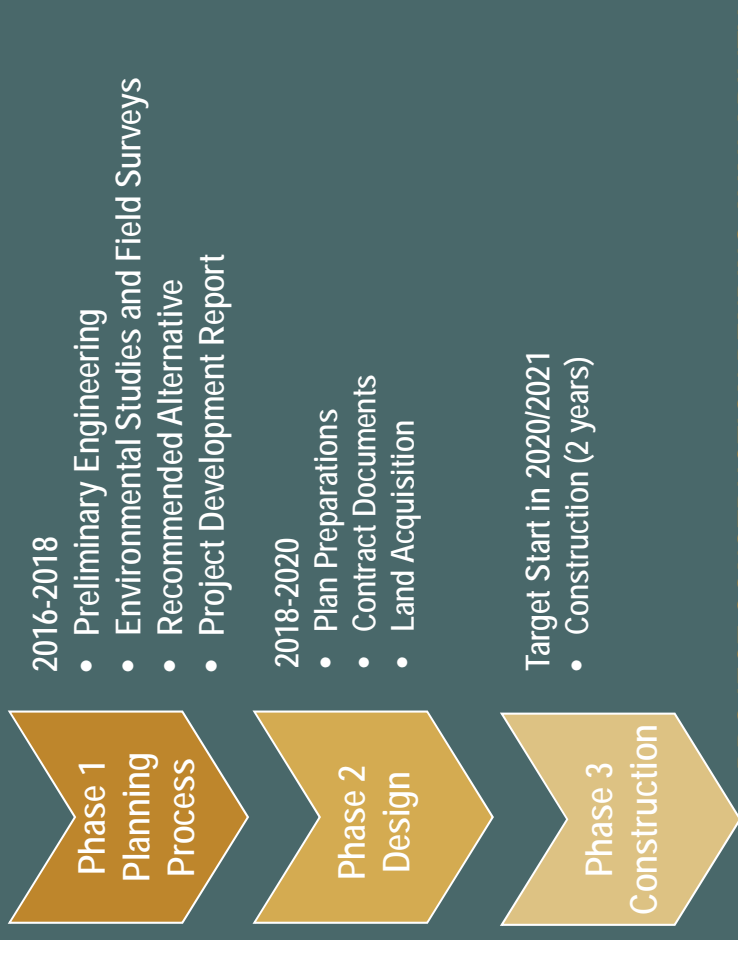
PROJECT BENEFITS

- New regional connections (new road extension and multi-modal path)
- New grade-separated access over the BNSF Railroad
- New bridge over US Route 30 and Jughandle Intersection
- Stormwater management will be provided via ditches and ponds
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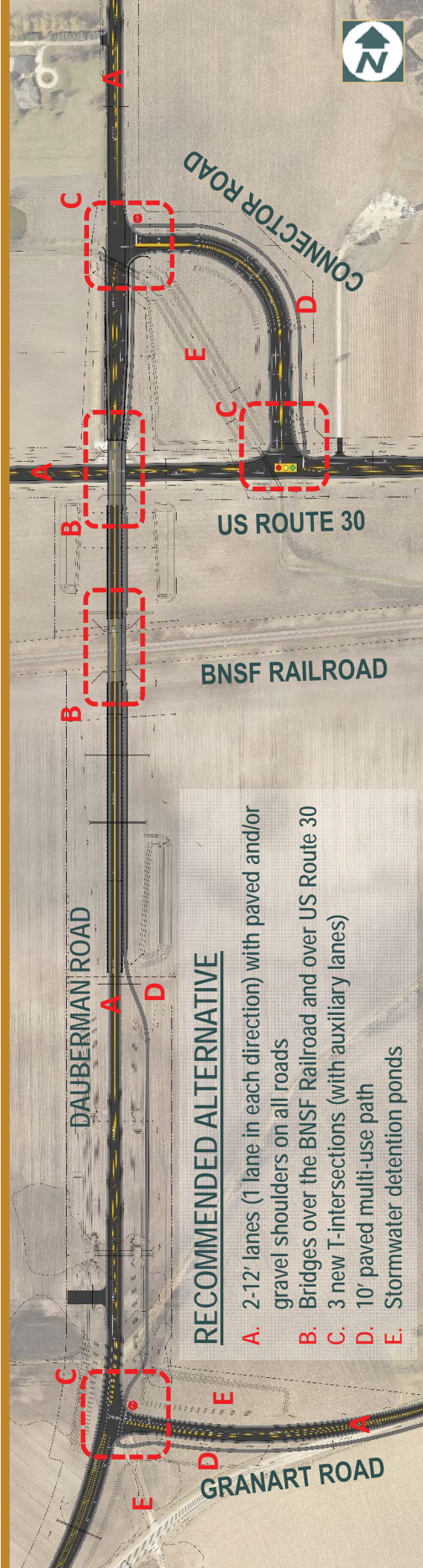
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 - 42.84 acres (41.45 acres of proposed right-of-way, 1.39 acres of temporary easement)
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PROJECT SCHEDULE



PROJECT CONSTRUCTION PENDING AVAILABILITY OF FUNDING AND RIGHT-OF-WAY ACQUISITION

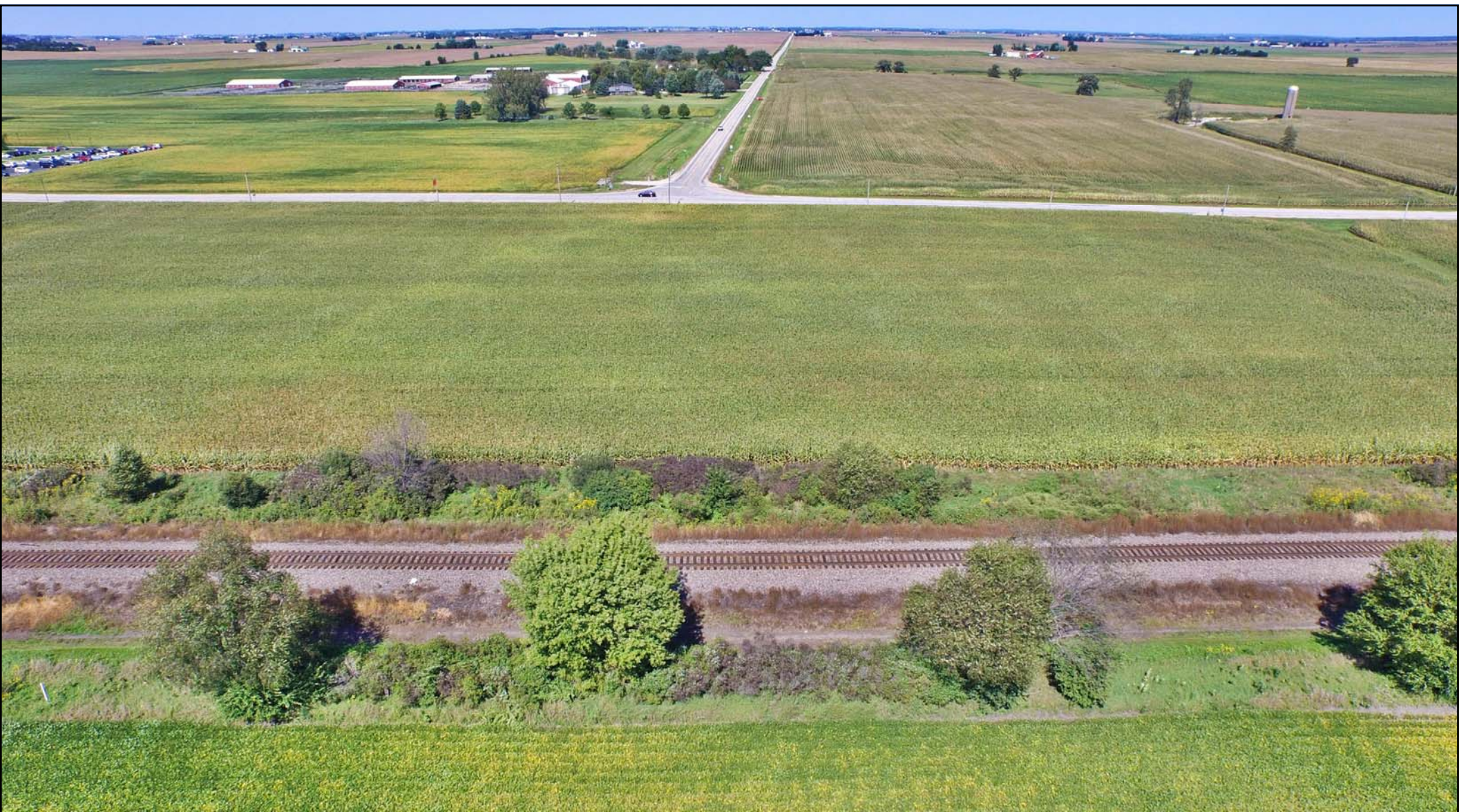


RECOMMENDED ALTERNATIVE

- A. 2-12' lanes (1 lane in each direction) with paved and/or gravel shoulders on all roads
- B. Bridges over the BNSF Railroad and over US Route 30
- C. 3 new T-intersections (with auxiliary lanes)
- D. 10' paved multi-use path
- E. Stormwater detention ponds

Attachment 7

Presentation Slides



DAUBERMAN ROAD EXTENSION | PUBLIC HEARING

Kane County, Illinois

FEBRUARY 15, 2018

An aerial photograph of a rural landscape in Kane County, Illinois. A road extension project is highlighted in red, showing a new road segment running horizontally across the middle of the image. The surrounding area consists of green fields, some trees, and a few buildings in the distance. The sky is clear and blue.

Presentation Outline

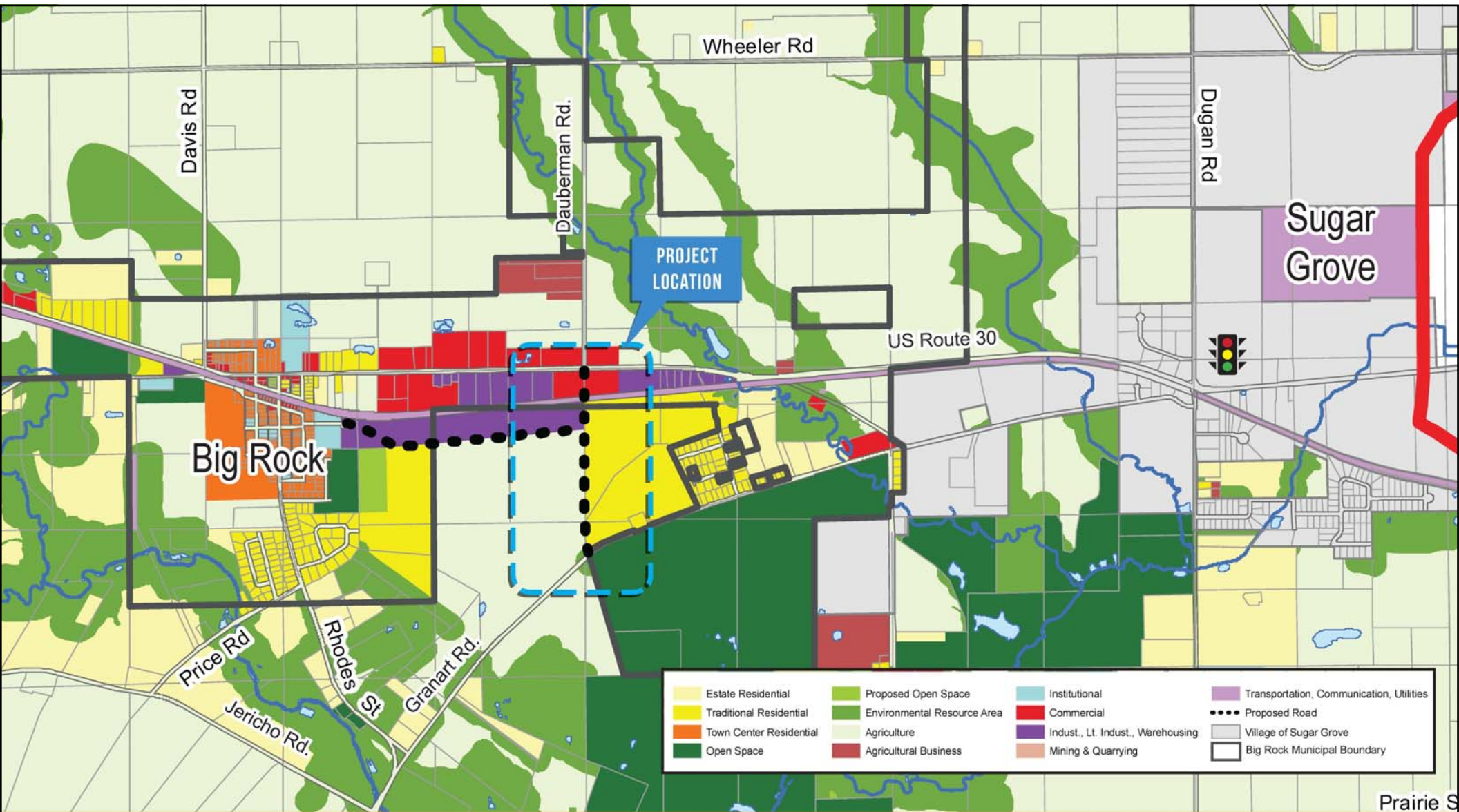
- Project Status Update
- Environmental Studies and Field Surveys
- Alternatives Analysis
- Recommended Alternative
- Section 4(f) Temporary Occupancy Documentation
- Next Steps



DAUBERMAN ROAD EXTENSION | PUBLIC HEARING

Kane County, Illinois

FEBRUARY 15, 2018



DAUBERMAN ROAD EXTENSION |

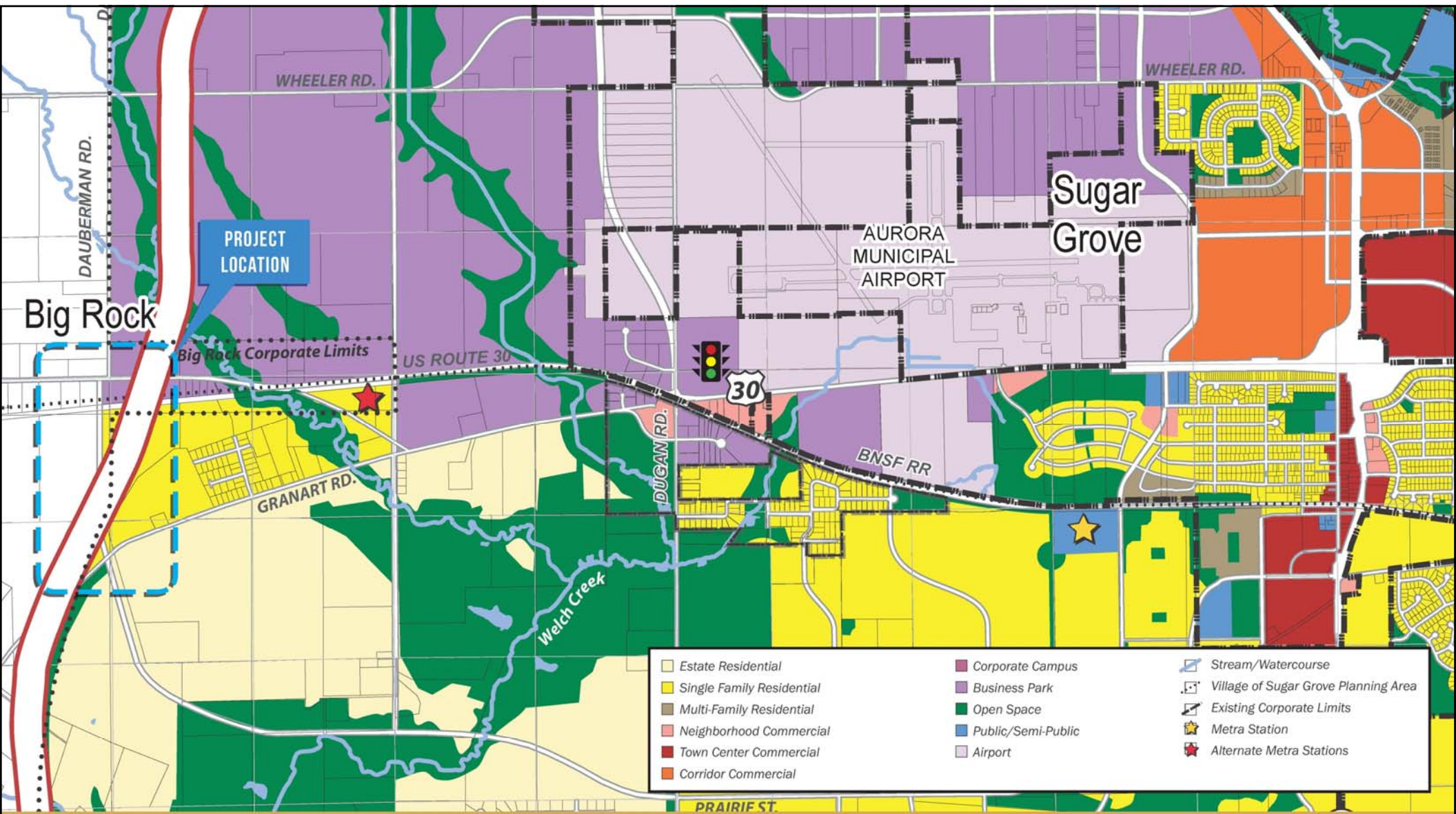
Kane County, Illinois

FUTURE LAND USE

(Village of Big Rock Comprehensive Plan - 2014)



Not to Scale



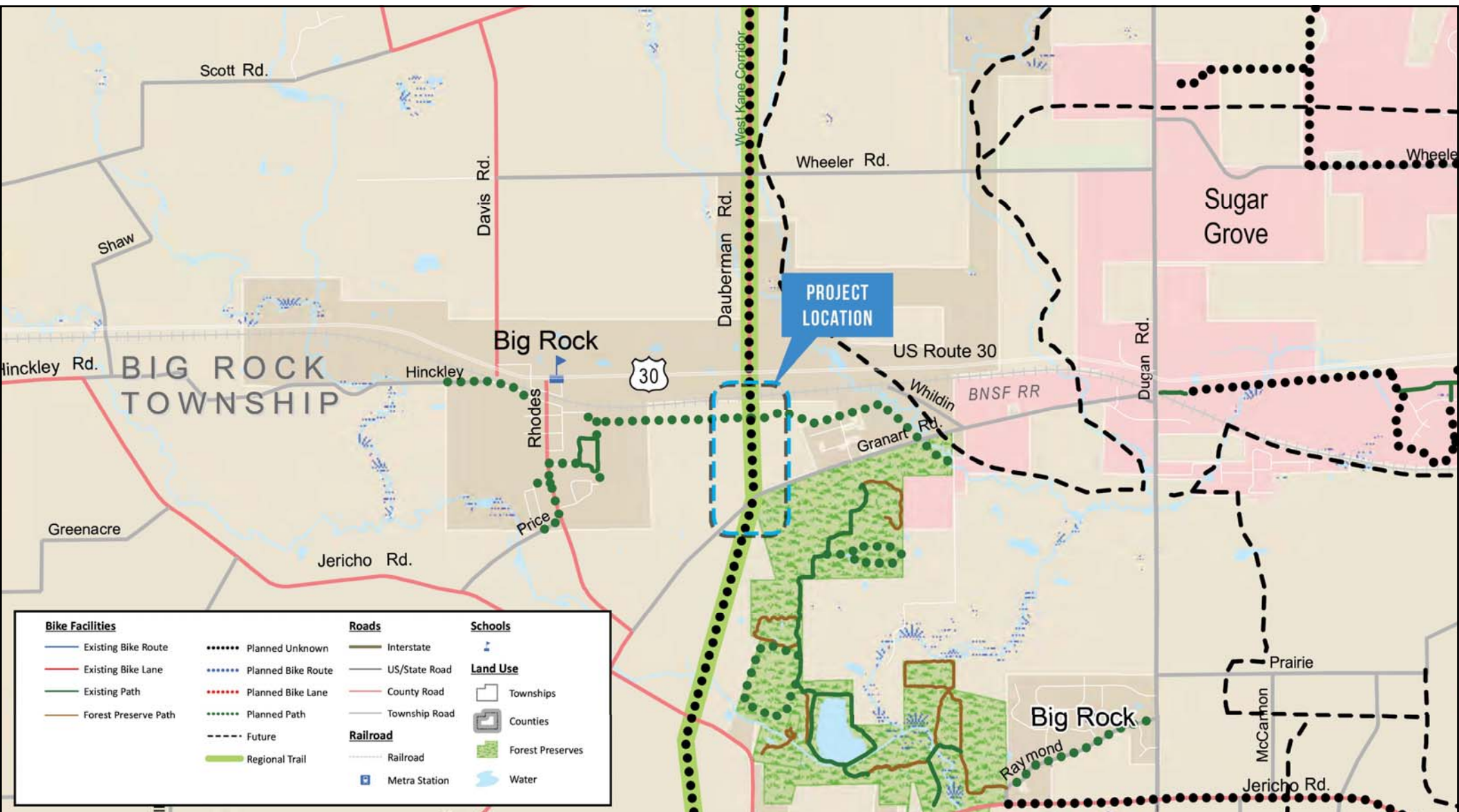
DAUBERMAN ROAD EXTENSION | FUTURE LAND USE PLAN

Kane County, Illinois

(Village of Sugar Grove Comprehensive Plan - 2016)



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DAUBERMAN ROAD EXTENSION | PLANNED BIKE LANES & PATHS

Kane County, Illinois



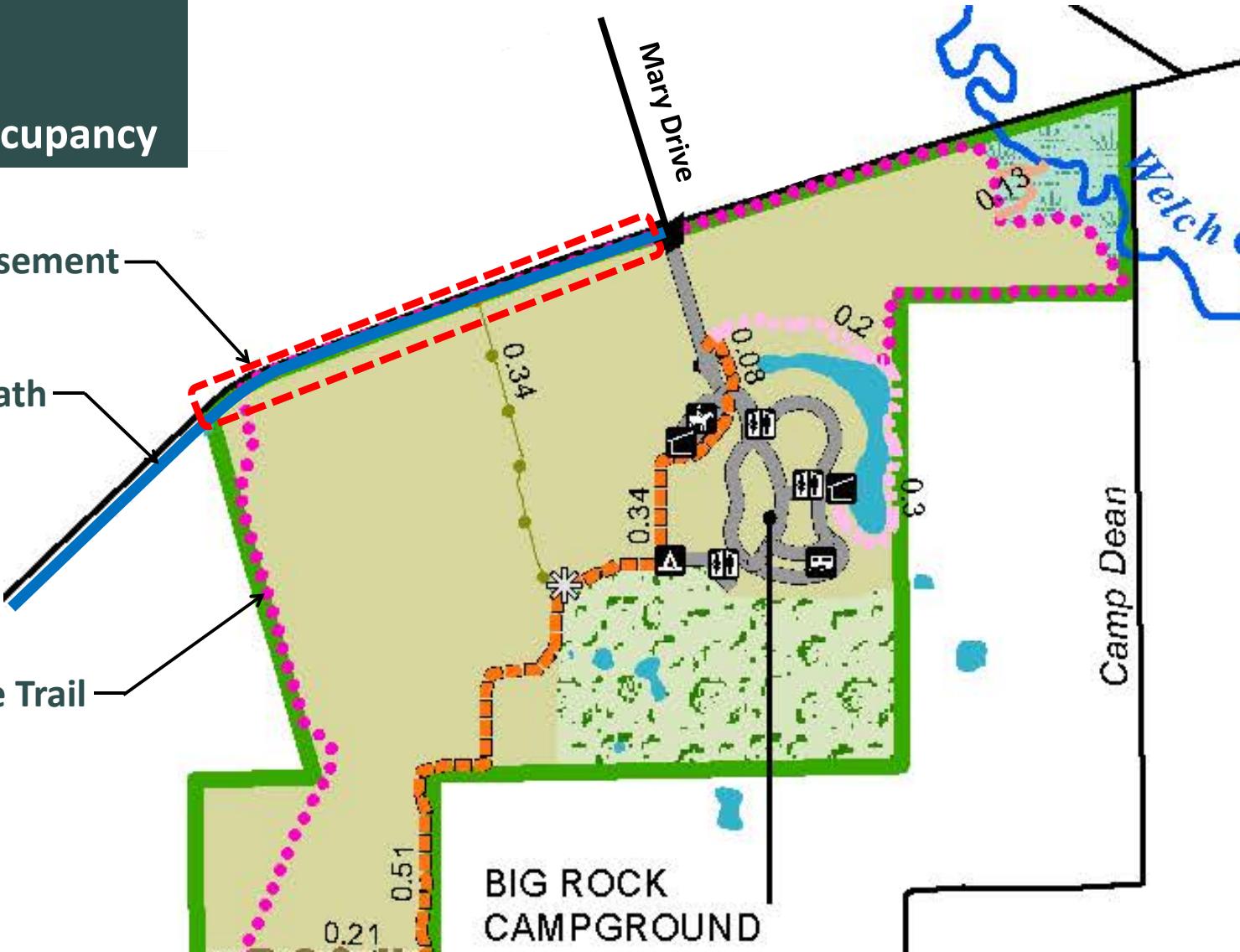
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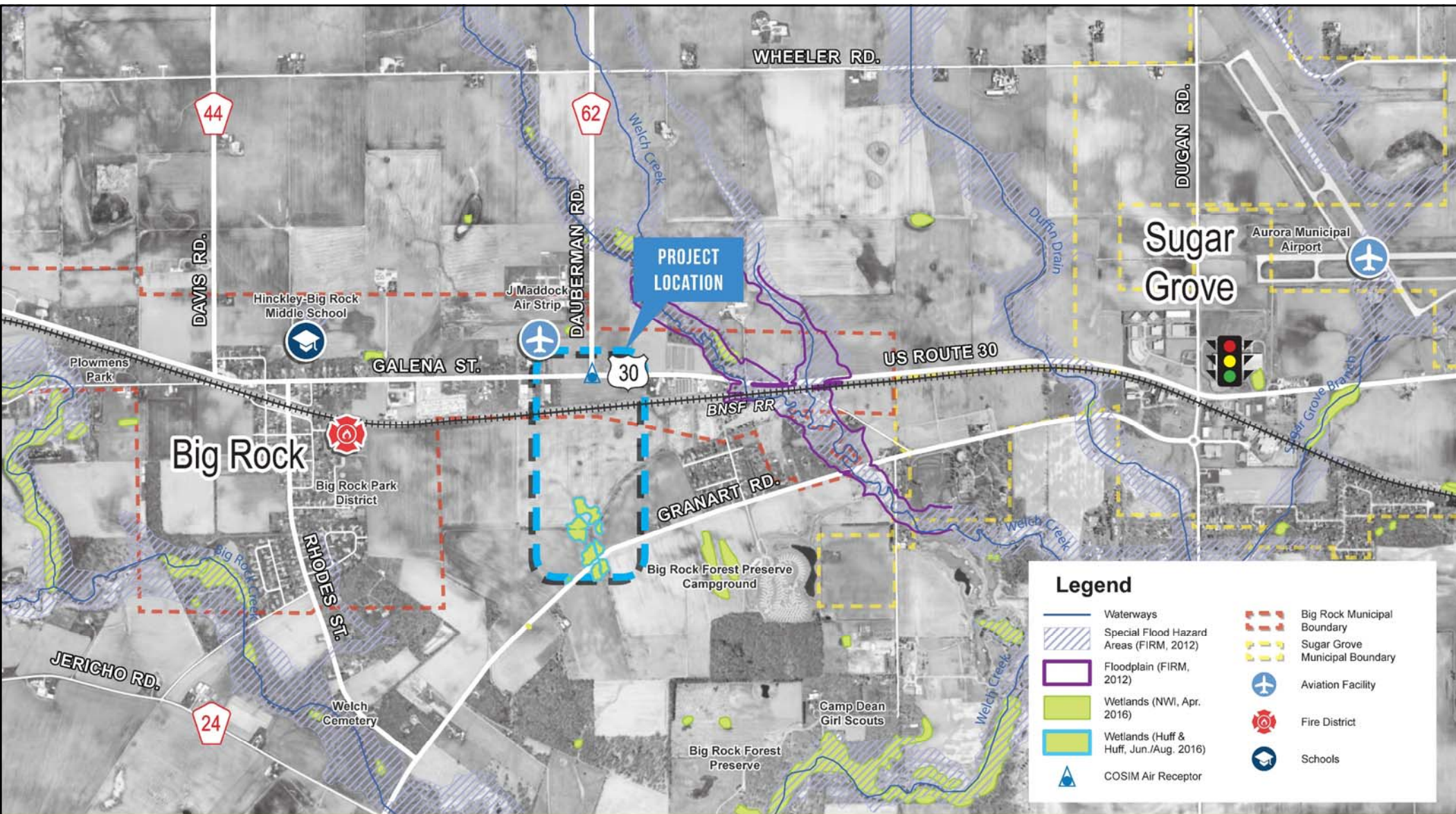
Kane County
Big Rock Forest Preserve
Section 4(f) Temporary Occupancy

Temporary Occupancy Easement

Proposed Paved Multi-use Path

Existing Northwest Lot Line Trail



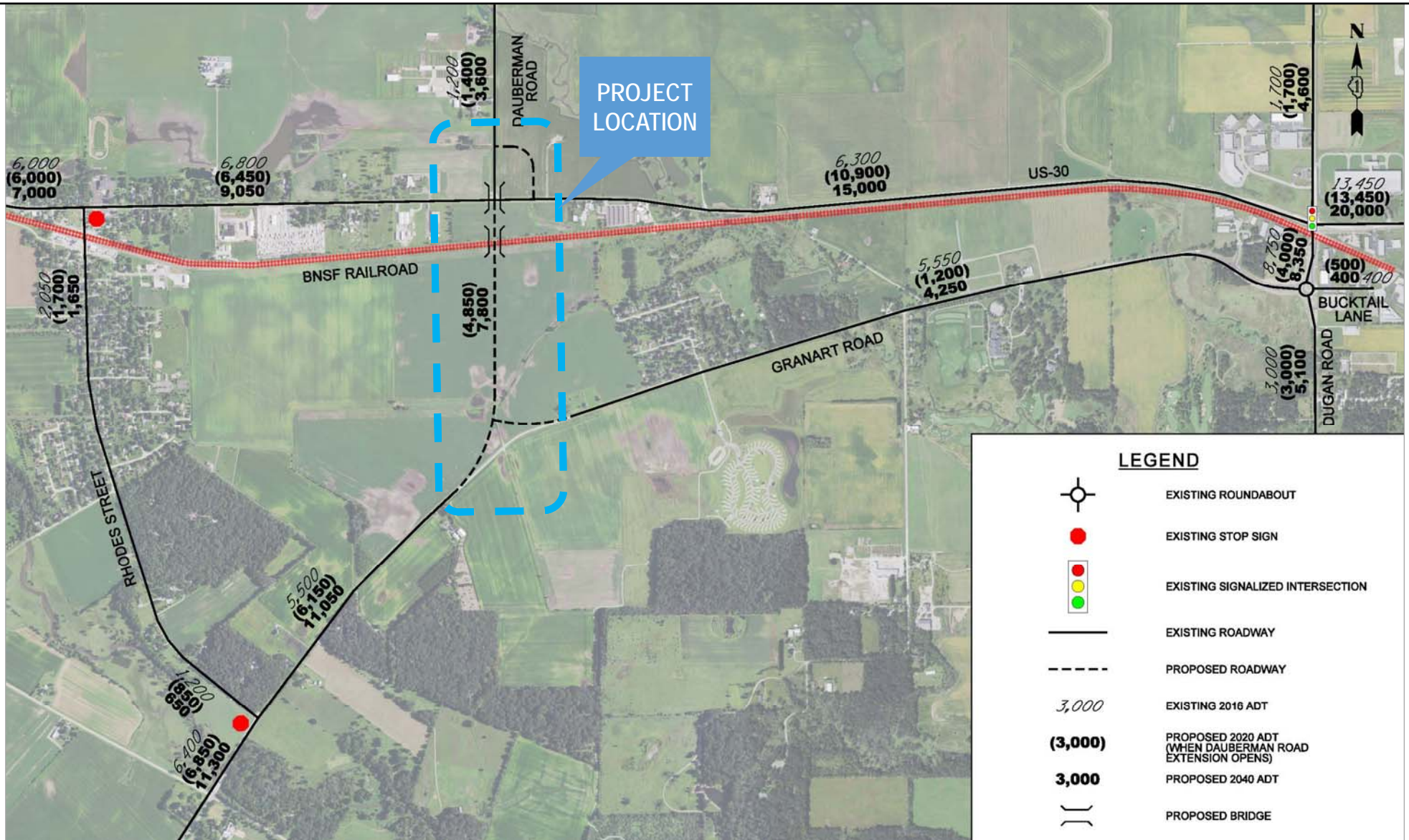


DAUBERMAN ROAD EXTENSION | ENVIRONMENTAL RESOURCES

Kane County, Illinois



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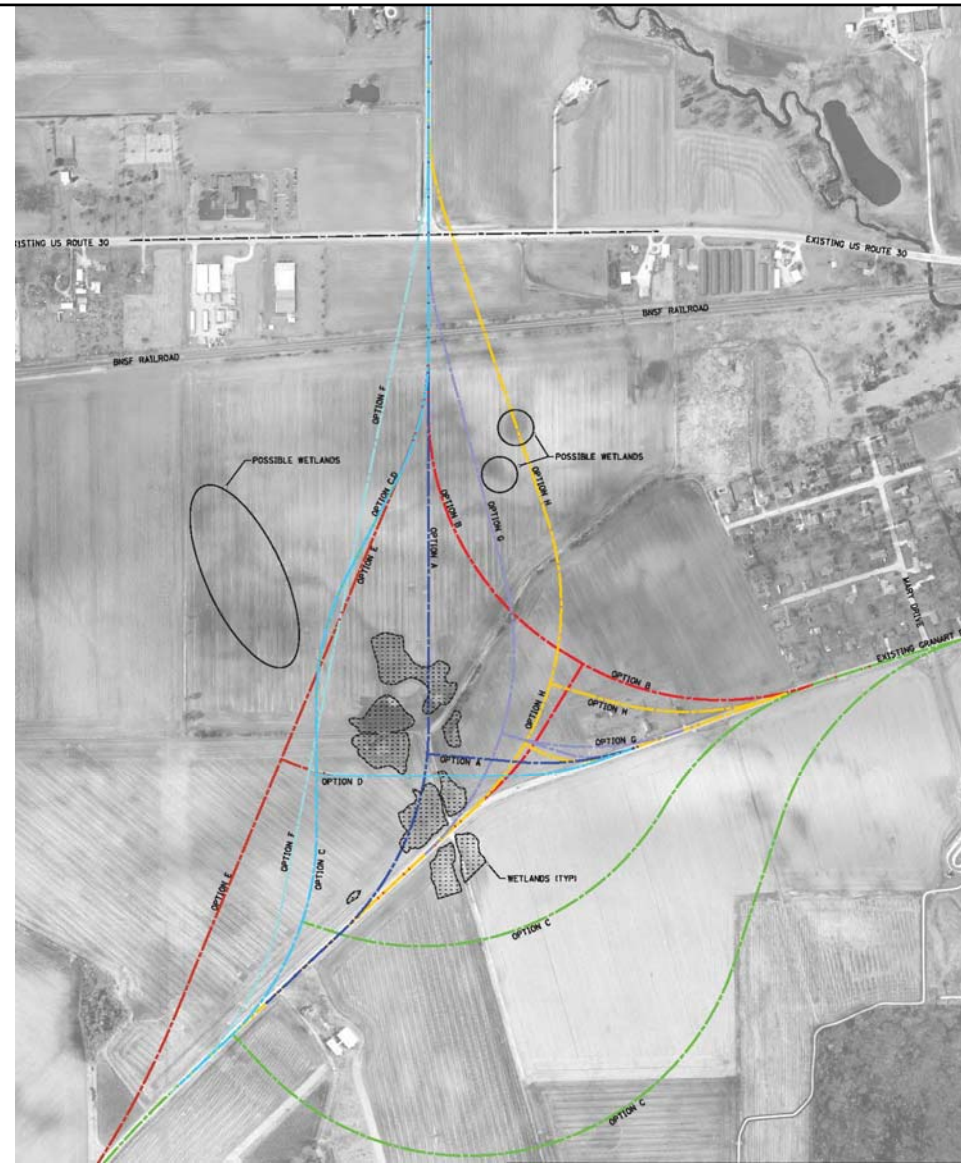
DAUBERMAN ROAD EXTENSION | TRAFFIC VOLUMES

Kane County, Illinois

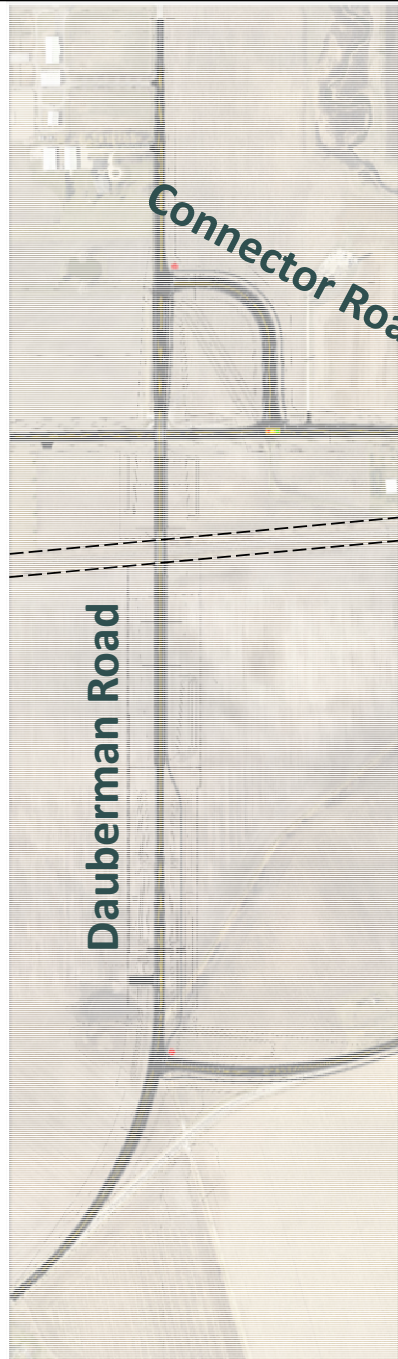
FEBRUARY 15, 2018

Many Alternatives Considered

- Horizontal alignments
- Vertical alignments
 - over/under US Route 30 and BNSF RR
- Dauberman Road/US Route 30 intersection
 - intersection, interchange, roundabout
- Type of road, number of lanes, lane widths
- Multi-modal connections

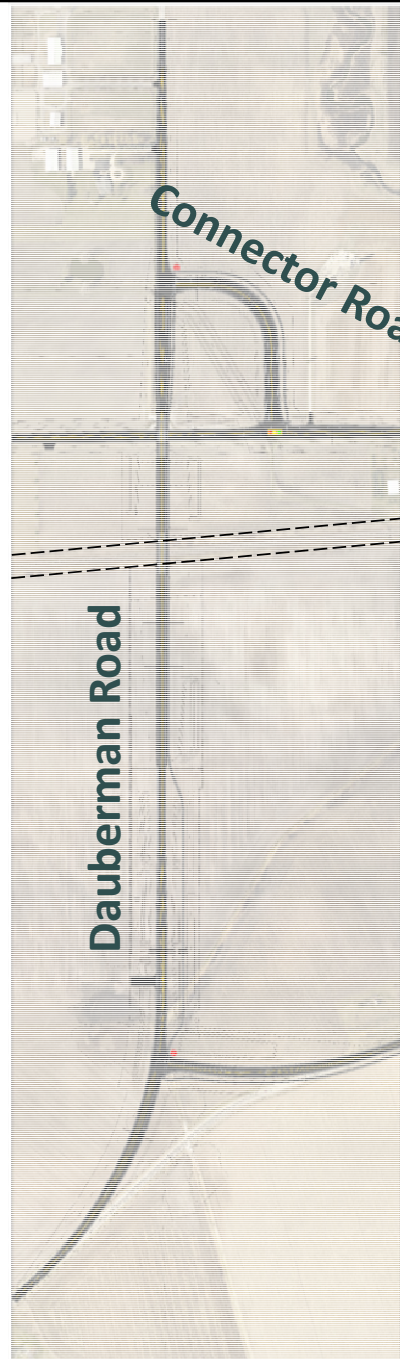


Recommended Alternative Dauberman Road



Recommended Alternative Dauberman Road

Let's rotate it to the right, so
we can zoom in more....



Connector Road

US Route 30

BNSF Railroad

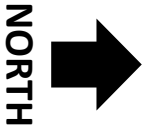
Dauberman Road

Granart Road

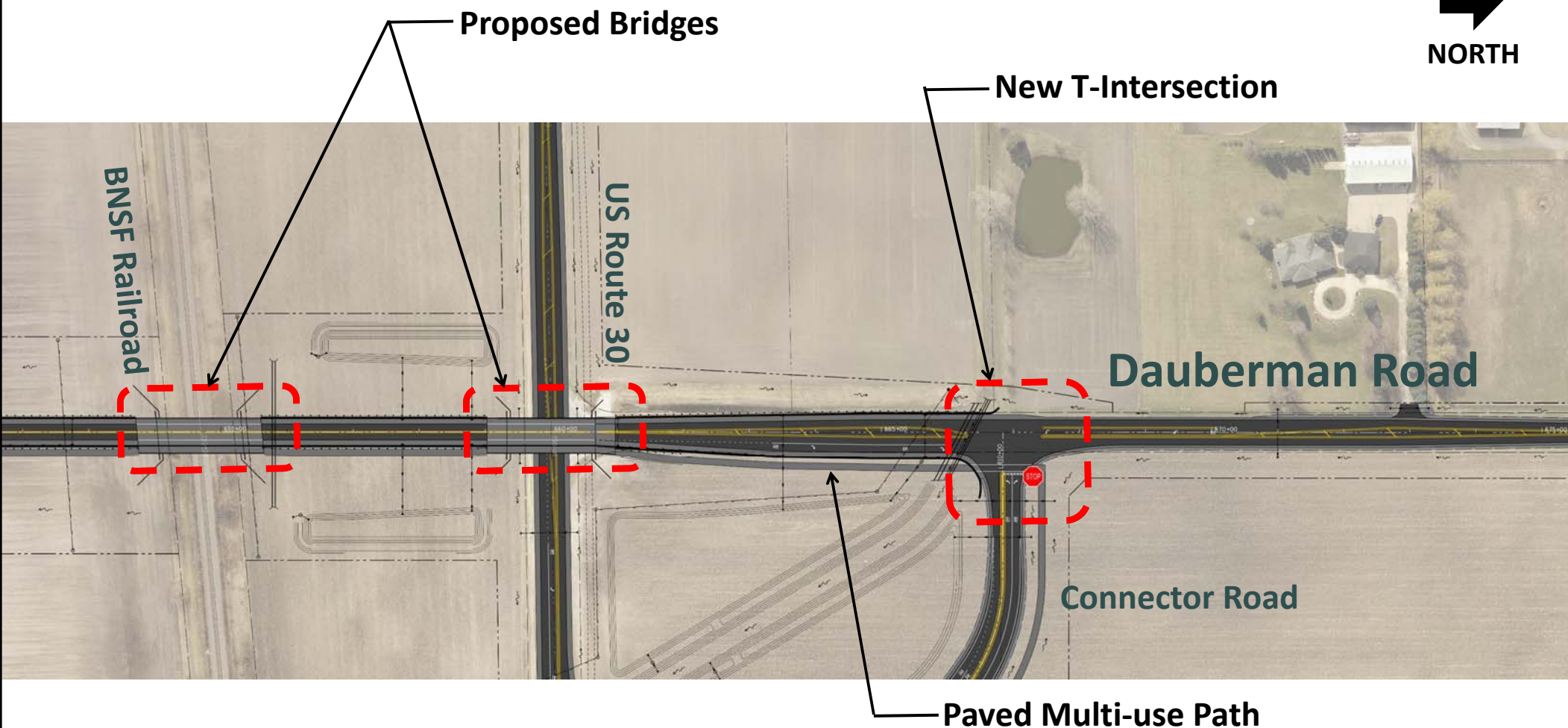
Recommended Alternative
Dauberman Road

BNSF Railroad

US Route 30



Now let's zoom in....

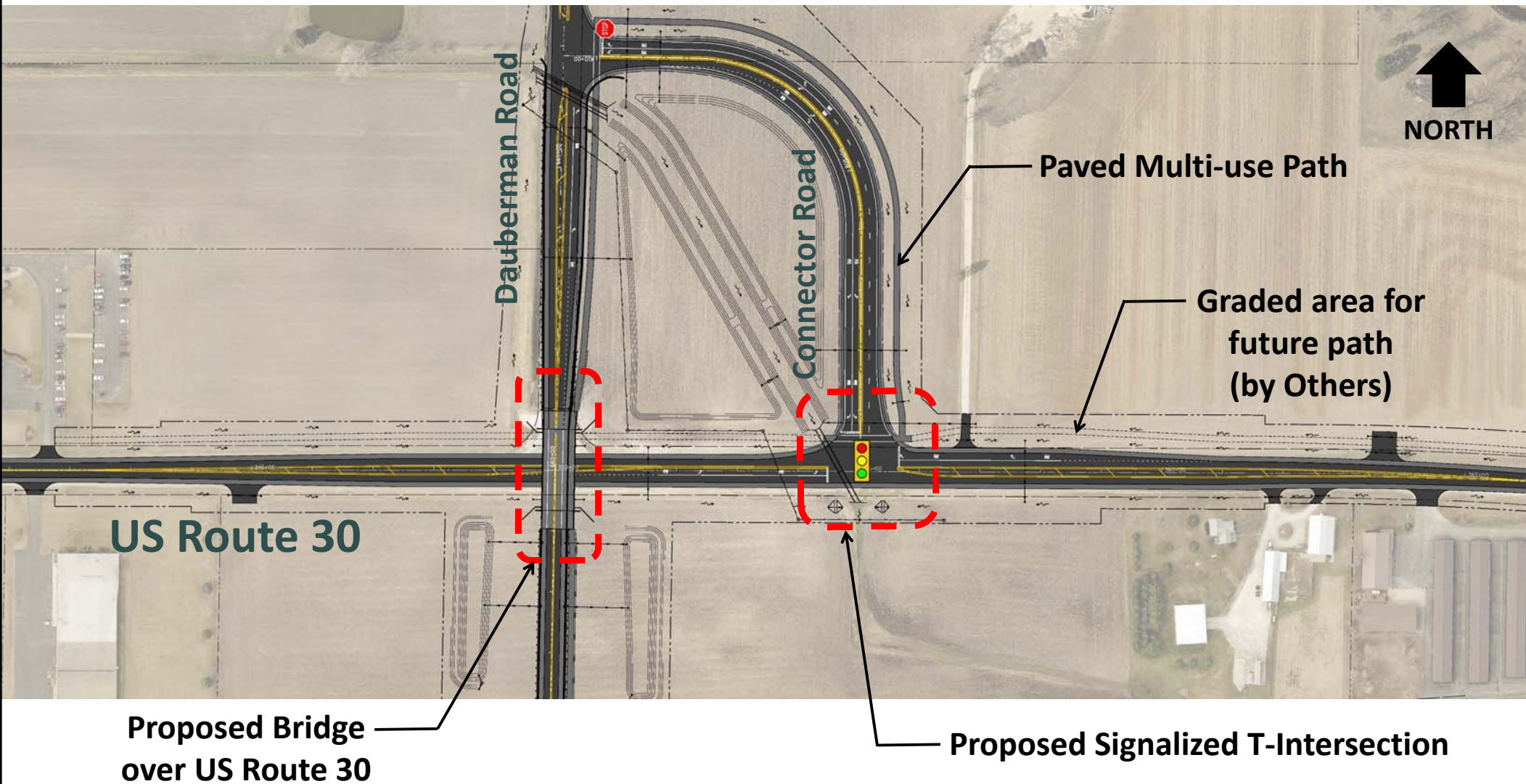


DAUBERMAN ROAD EXTENSION | RECOMMENDED ALTERNATIVE

Kane County, Illinois

DAUBERMAN ROAD

FEBRUARY 15, 2018

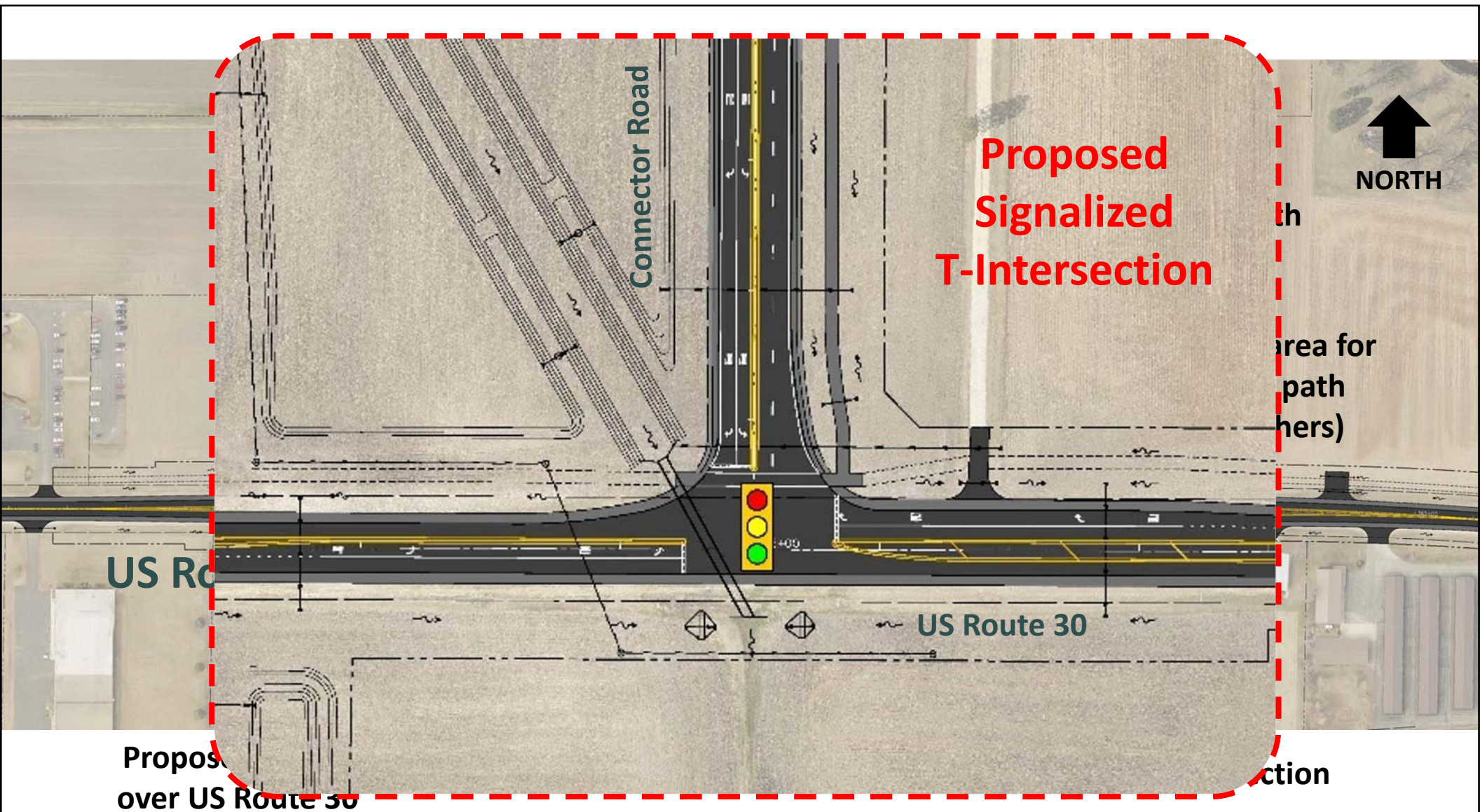


DAUBERMAN ROAD EXTENSION | RECOMMENDED ALTERNATIVE

Kane County, Illinois

US Route 30

FEBRUARY 15, 2018

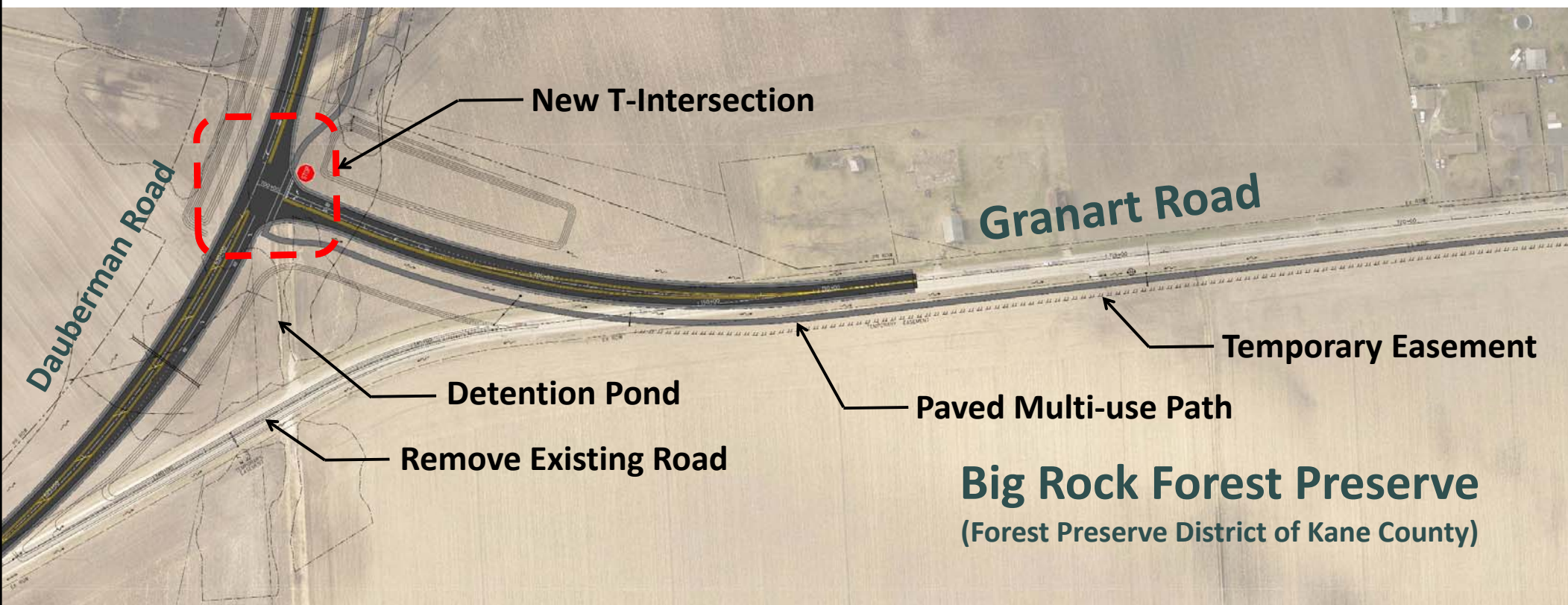


DAUBERMAN ROAD EXTENSION | RECOMMENDED ALTERNATIVE

Kane County, Illinois

US Route 30

FEBRUARY 15, 2018



DAUBERMAN ROAD EXTENSION | RECOMMENDED ALTERNATIVE

Kane County, Illinois

Granart Road

FEBRUARY 15, 2018

Proposed Improvement Benefits

- New regional connections (new road extension and multi-modal path)
- New grade-separated access over BNSF Railroad (locals and emergency responders)
- New bridge over US Route 30 and Jughandle Intersection
- New drainage infrastructure to accommodate off-site flow and the new road
- Best Management Practices for water quality benefits will be implemented
 - Grassed bottom ponds and ditches



DAUBERMAN ROAD EXTENSION | RECOMMENDED ALTERNATIVE

Kane County, Illinois

FEBRUARY 15, 2018

Proposed Improvement Requirements

- Partnership with Forest Preserve
 - The path along the south side of Granart Road will be within the Big Rock Forest Preserve
 - Use of forest preserve property will require a Section 4(f) Temporary Occupancy document and an Interagency Governmental Agreement
- Land Acquisition
 - 22 properties (20 agricultural, 1 residential, 1 forest preserve)
 - 42.84 acres (41.45 acres proposed right-of-way, 1.39 acres temporary easement)
 - Property Acquisitions will comply with the *Uniform Relocation Assistance and Real Property acquisition Policies Act of 1970*, as amended
- Dauberman Road construction north of US Route 30 will require a road closure and detour route



DAUBERMAN ROAD EXTENSION | RECOMMENDED ALTERNATIVE

Kane County, Illinois

FEBRUARY 15, 2018



DAUBERMAN ROAD EXTENSION | DETOUR ROUTE MAP

Kane County, Illinois

FEBRUARY 15, 2018



Phase 1 Engineering (2016-2018)

- Environmental Studies and Field Surveys
- Public Information Meeting – February 2017
- Alternatives Analysis
- Recommended Alternative
- Draft Project Development Report
- Public Hearing – February 2018
- Final Project Development Report
- Design Approval

Phase 2 Engineering & ROW Acquisition (2018-2020)

- Design
- Contract Plans
- Right-of-Way Acquisition

Phase 3 (Target Start in 2020/2021)

- Construction (2 years)
- Pending availability of funding and right-of-way acquisition



DAUBERMAN ROAD EXTENSION | SCHEDULE

Kane County, Illinois

FEBRUARY 15, 2018

THANK YOU

Contact Information

Candance Thomas, PE
Kane County Senior Project Manager
(630) 406-7355

kdotcomments@co.kane.il.us
<http://www.co.kane.il.us/dot/constProjects.aspx>



DAUBERMAN ROAD EXTENSION | PUBLIC HEARING

Kane County, Illinois

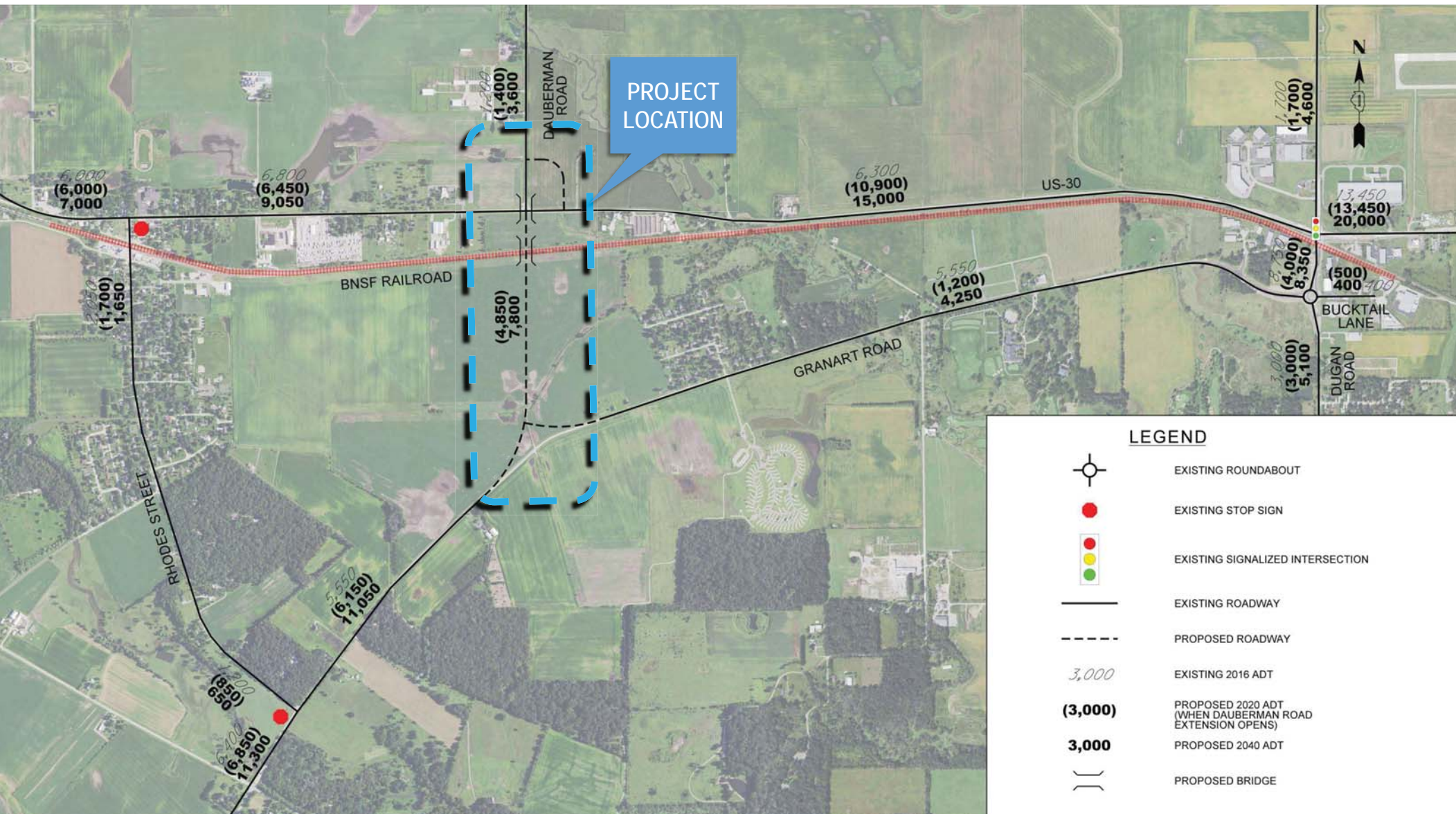
FEBRUARY 15, 2018

Attachment 8

Exhibit Boards



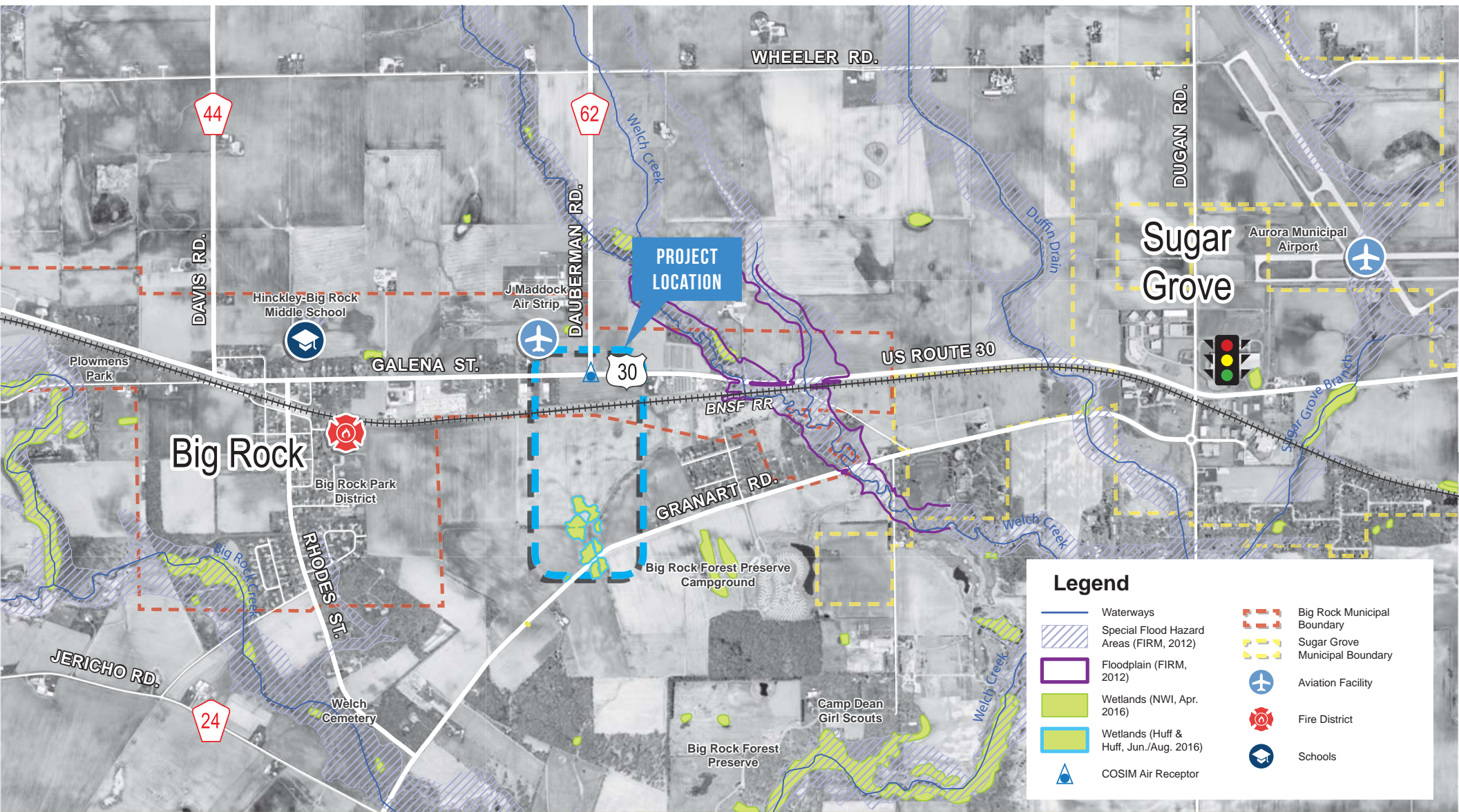
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DAUBERMAN ROAD EXTENSION | TRAFFIC VOLUMES

Kane County, Illinois

FEBRUARY 15, 2018

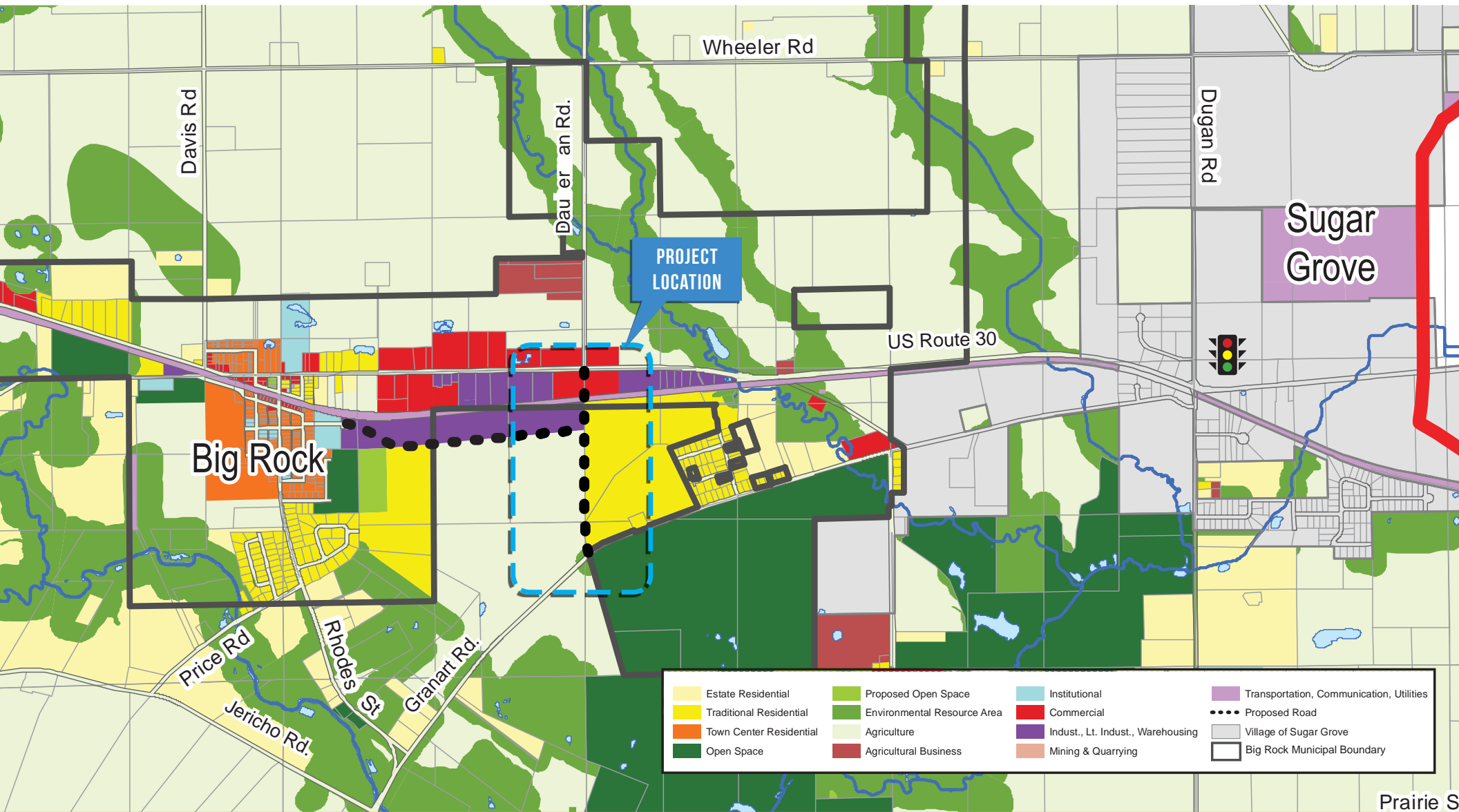


DAUBERMAN ROAD EXTENSION | ENVIRONMENTAL RESOURCES

Kane County, Illinois



Not to Scale



DAUBERMAN ROAD EXTENSION |

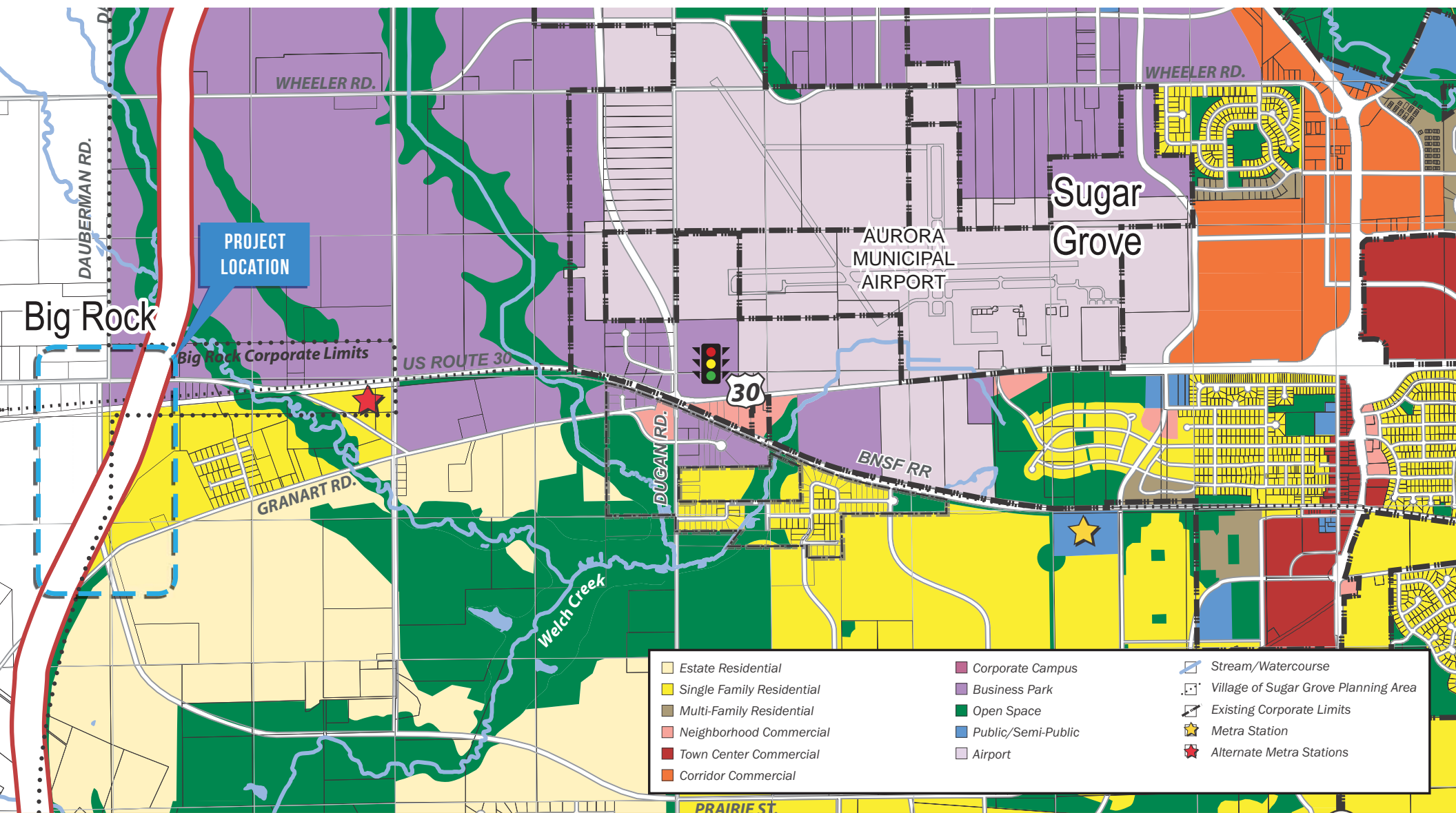
Kane County, Illinois

FUTURE LAND USE

(Village of Big Rock Comprehensive Plan - 2014)



Not to Scale



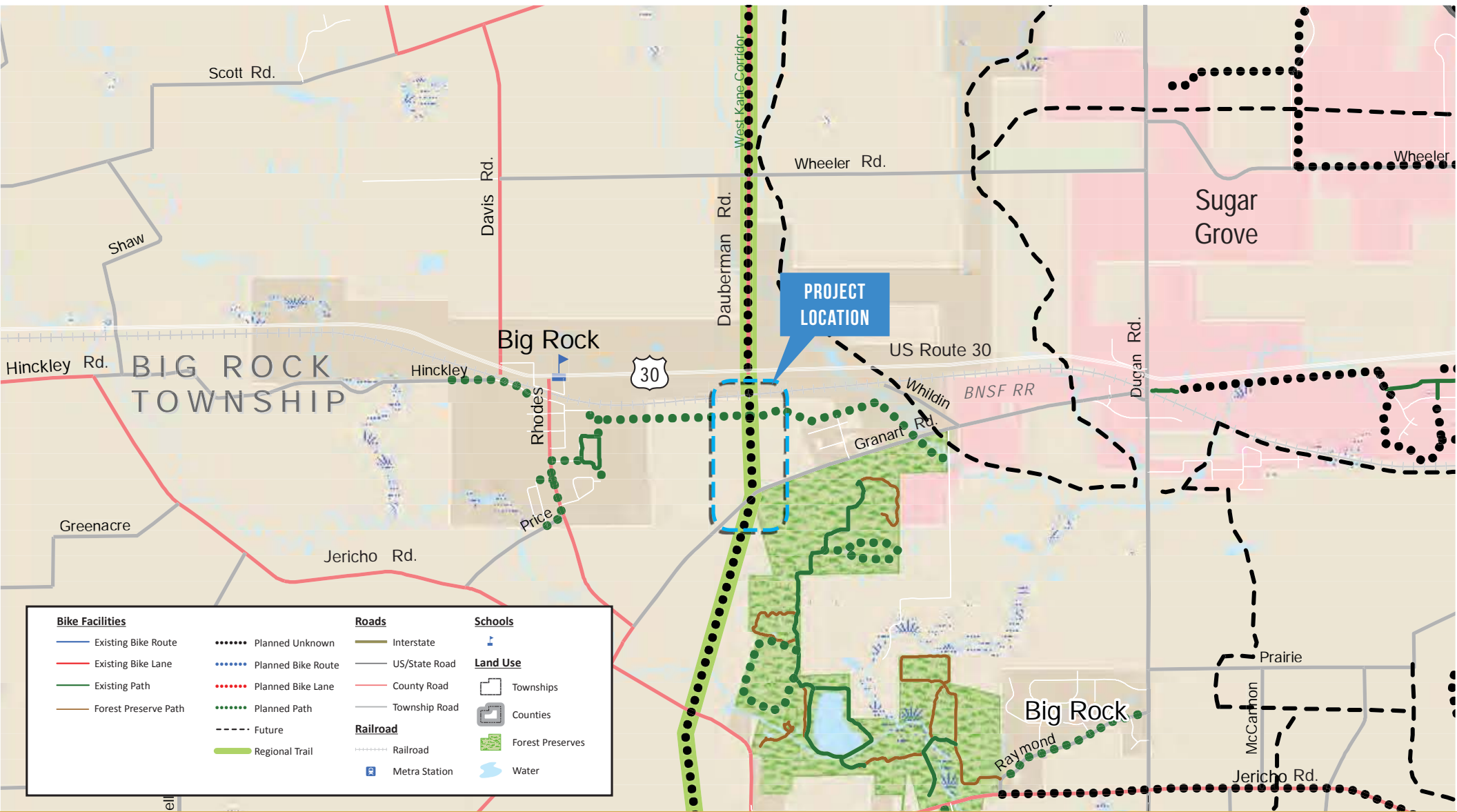
DAUBERMAN ROAD EXTENSION | FUTURE LAND USE PLAN

Kane County, Illinois

(Village of Sugar Grove Comprehensive Plan - 2016)



Not to Scale



DAUBERMAN ROAD EXTENSION |

Kane County, Illinois

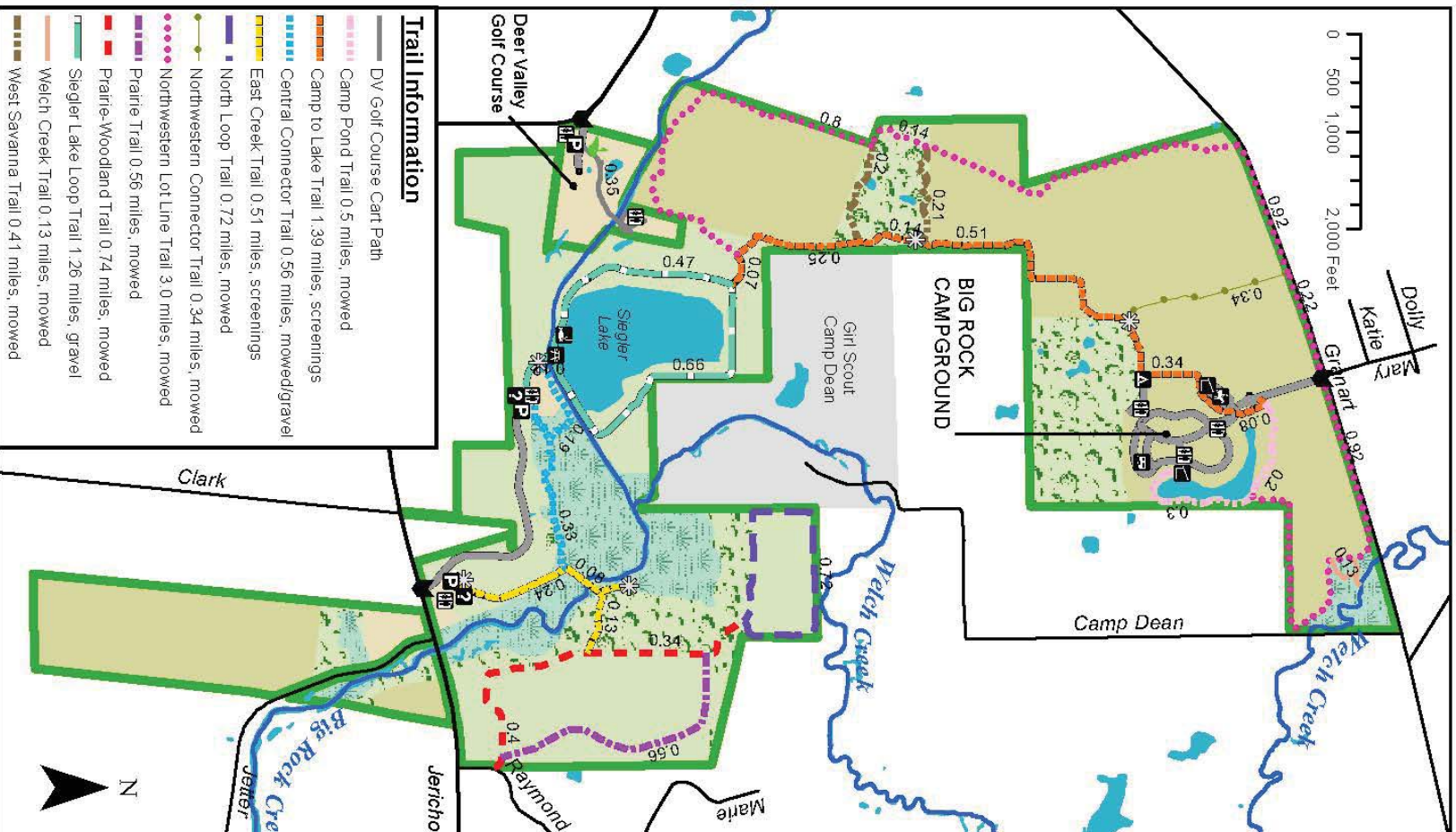
BIKE LANES & PATHS
 (2015 Kane/Kendall County Planning Map - DRAFT)



Not to Scale



Big Rock Forest Preserve



Trail Information

- DV Golf Course Cart Path
- Camp Pond Trail 0.5 miles, mowed
- Camp to Lake Trail 1.39 miles, screenings
- Central Connector Trail 0.56 miles, mowed/gravel
- East Creek Trail 0.51 miles, screenings
- North Loop Trail 0.72 miles, mowed
- Northwestern Connector Trail 0.34 miles, mowed
- Northwestern Lot Line Trail 3.0 miles, mowed
- Prairie Trail 0.56 miles, mowed
- Prairie-Woodland Trail 0.74 miles, mowed
- Siegler Lake Loop Trail 1.26 miles, gravel
- Welch Creek Trail 0.13 miles, mowed
- West Savanna Trail 0.41 miles, mowed

Legend

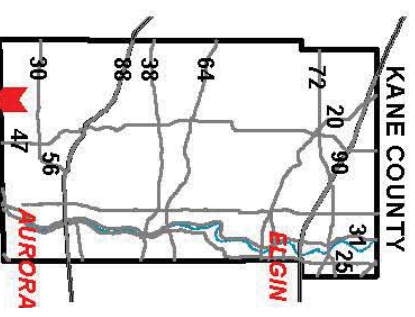
- Main Entrance
- Information
- Parking
- Shelter
- Picnic Area
- Restroom
- RV Campground
- Tent Campground
- Equestrian Campground
- Fishing
- Golf
- Interpretive Sign
- Road
- Forest Preserve Road
- Creek
- Forest Preserve
- Natural Area -
- Cropland
- Grassland
- Maintained
- Open Water
- Wetland
- Woodland

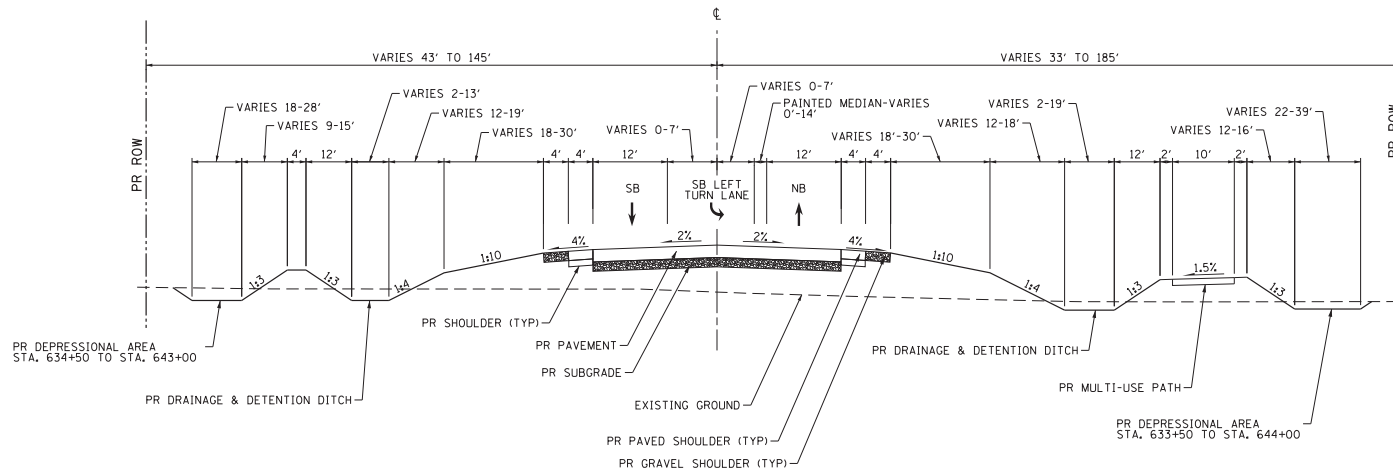
Preserve Main Entrance:
 46W072 Jericho Road,
 Big Rock, IL 60511
 Approx. 3.54 miles west
 of IL Route 47

 Campground:
 46W499 Granart Road,
 Big Rock, IL 60511

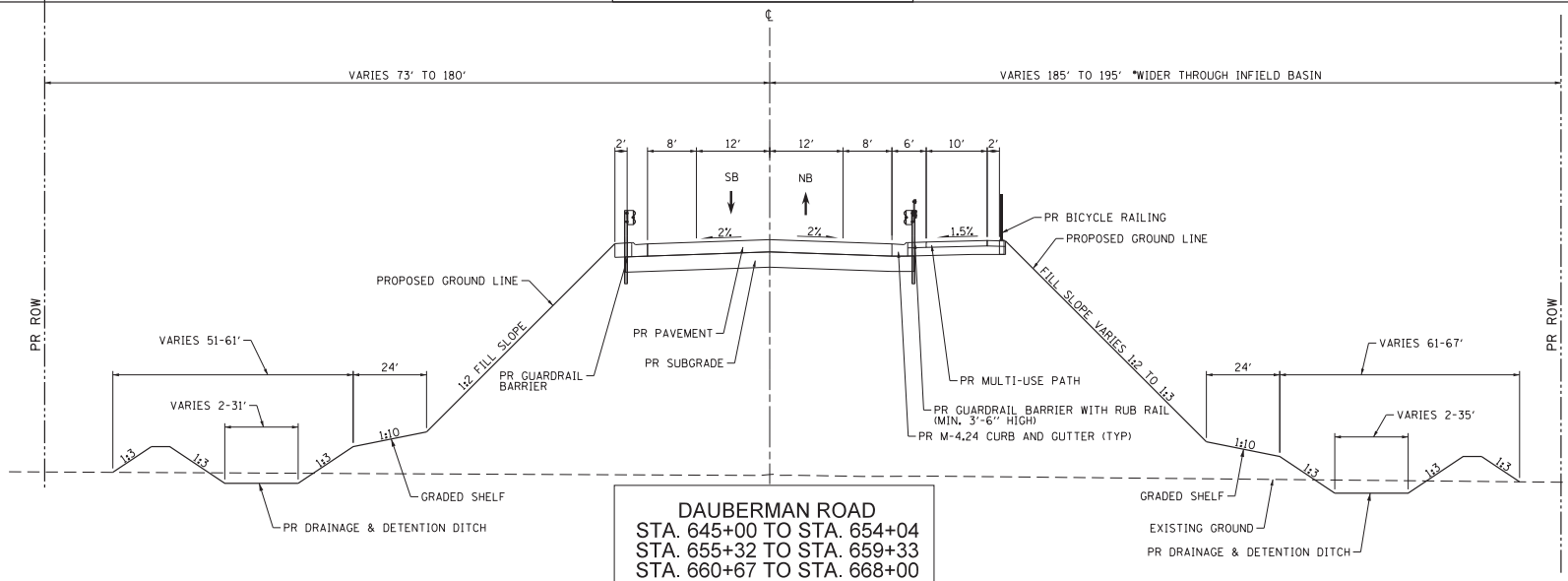
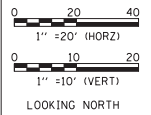
 Acreage = 840
 Total trail miles = 10.12

 Information: (630) 232-5980
www.kaneforest.com
 Hours: Daily, Sunrise-Sunset

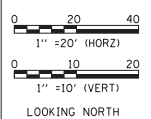


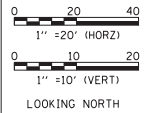
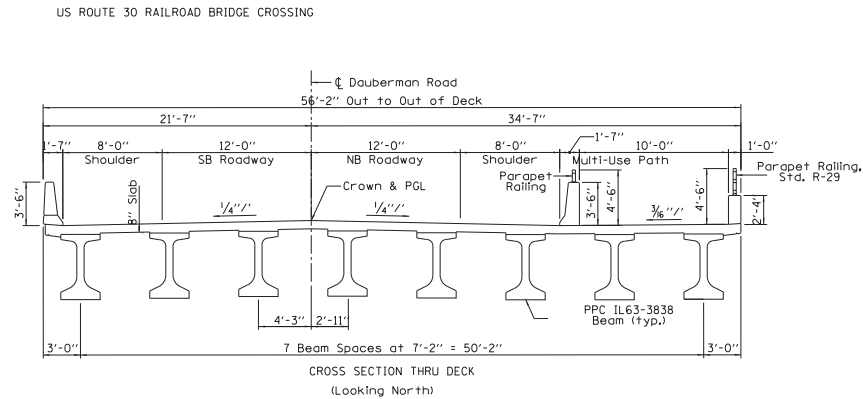
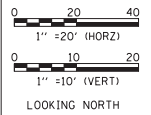
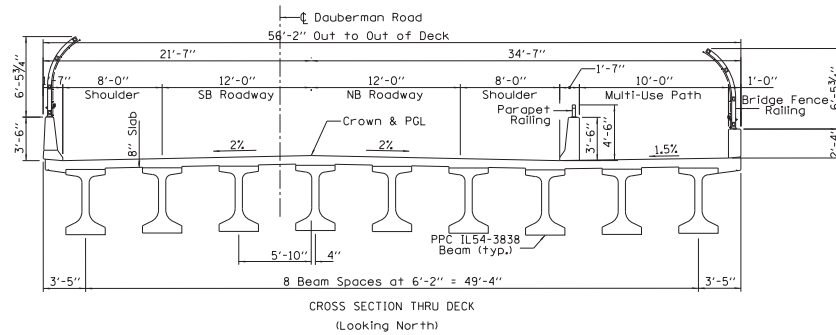


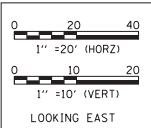
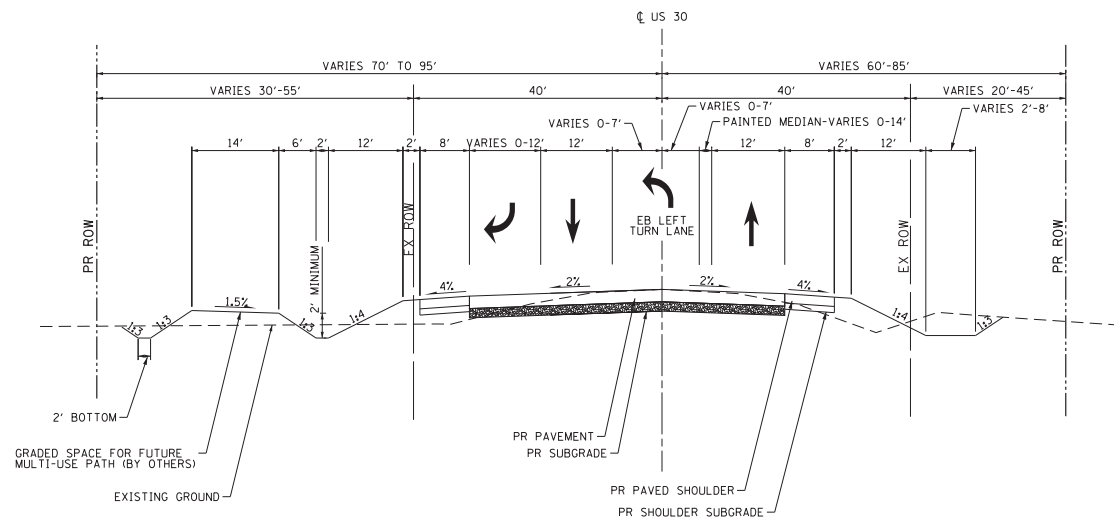
DAUBERMAN ROAD
STA. 618+70 TO STA. 645+00
STA. 668+00 TO STA. 678+84

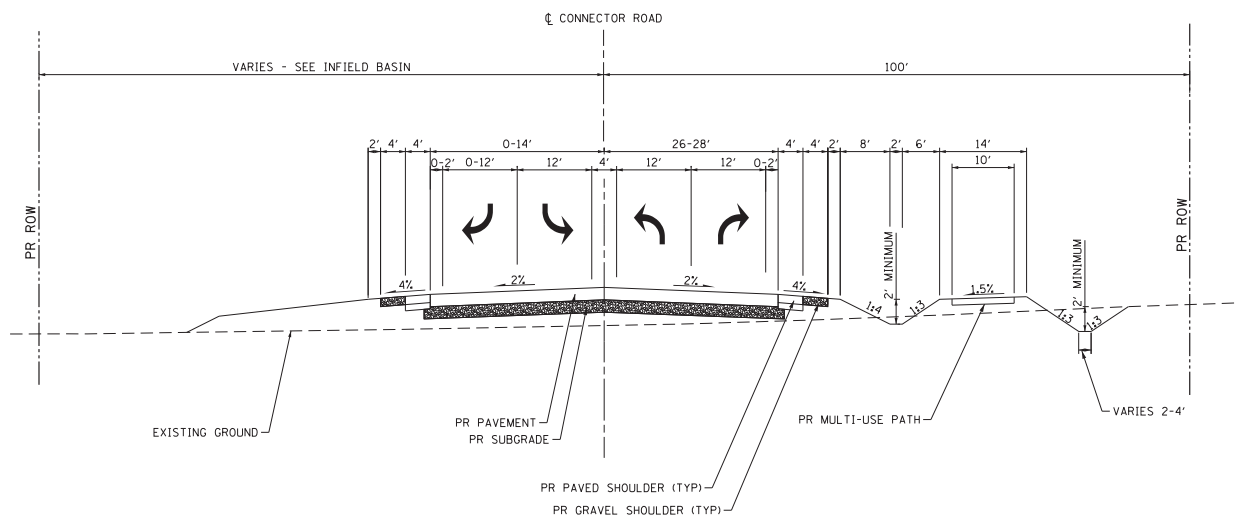


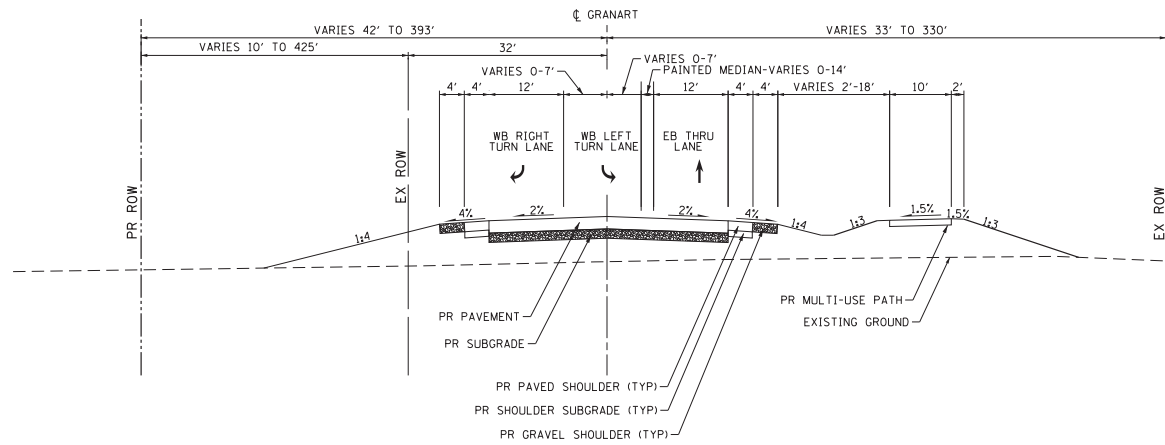
DAUBERMAN ROAD
STA. 645+00 TO STA. 654+04
STA. 655+32 TO STA. 659+33
STA. 660+67 TO STA. 668+00



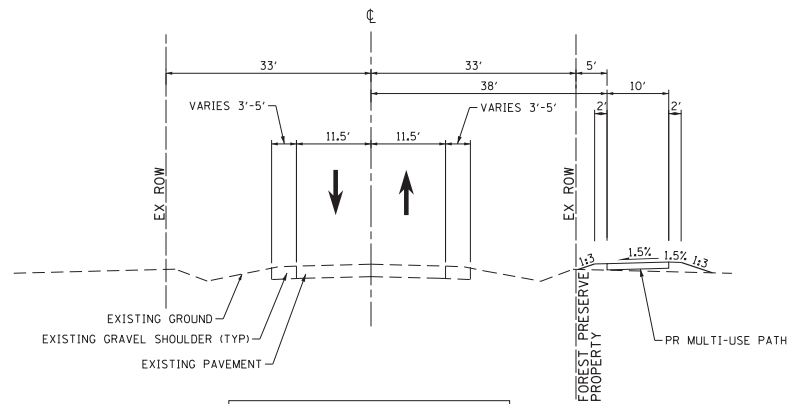
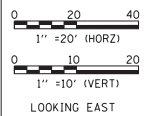




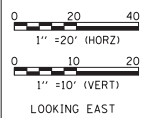




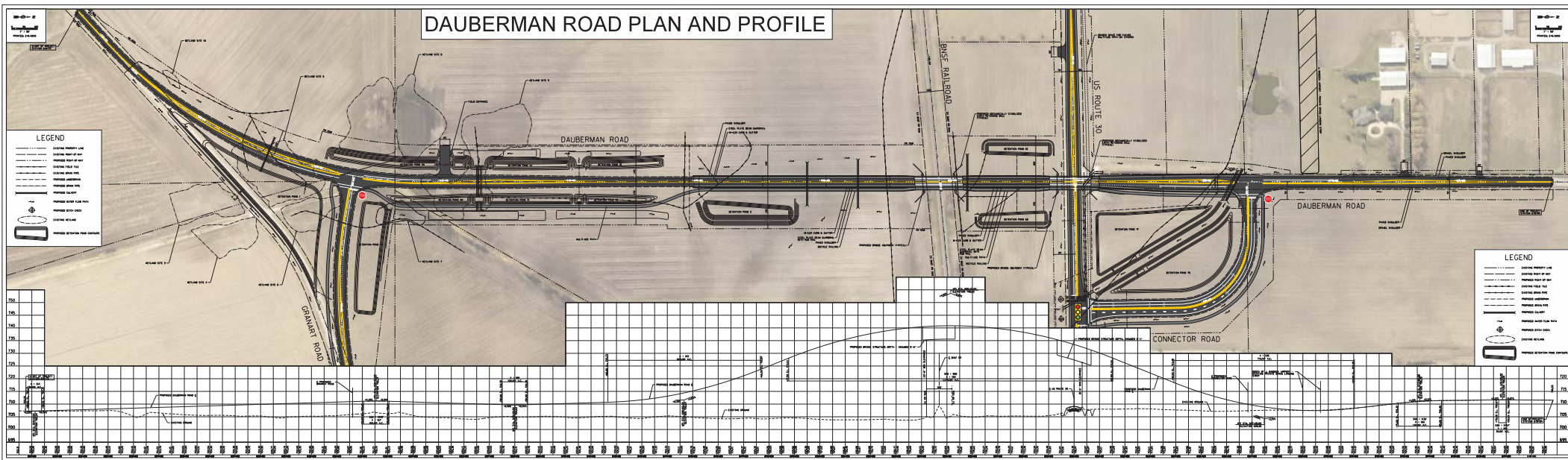
GRANART ROAD
STA. 700+00 TO STA. 711+70



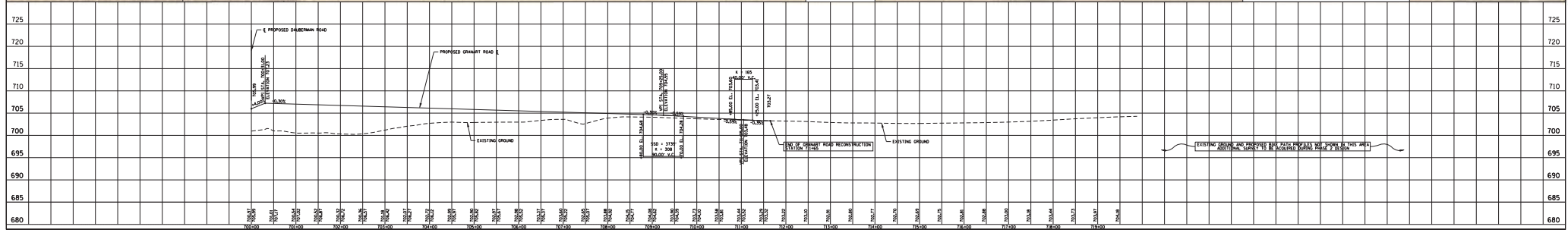
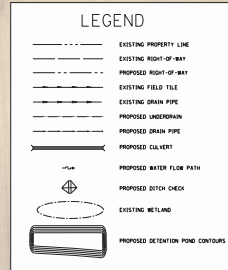
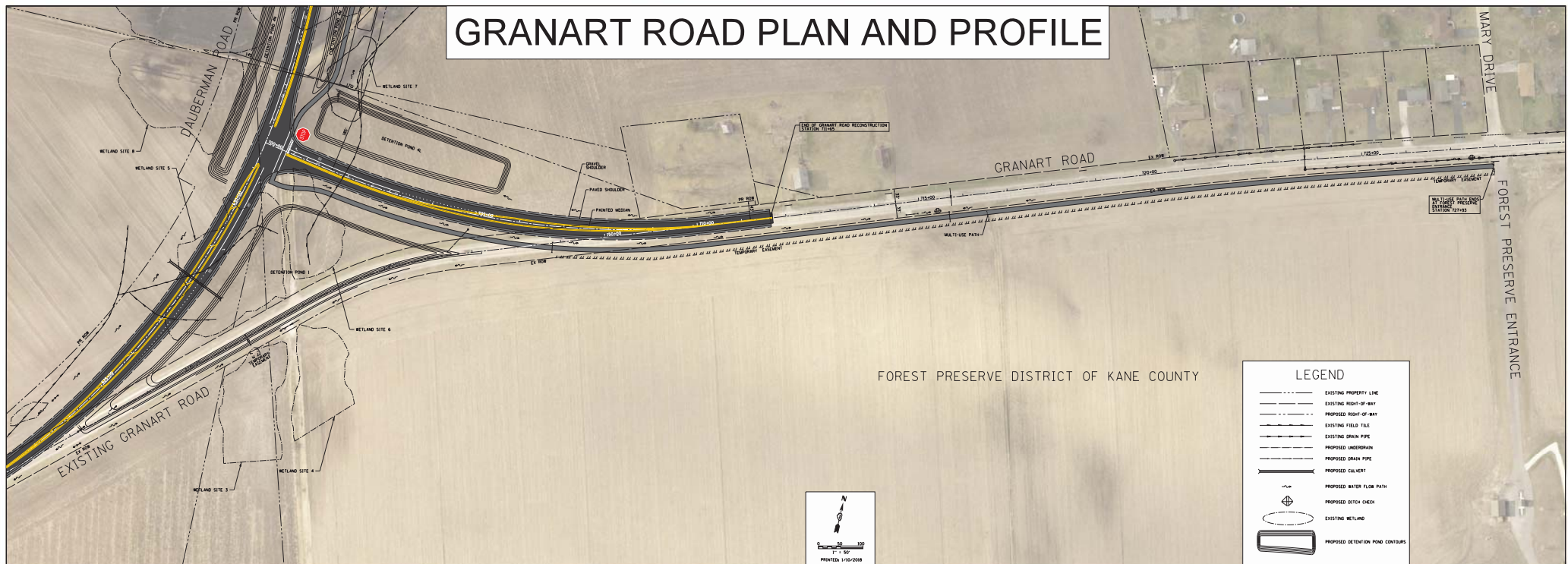
GRANART ROAD
MULTI-USE PATH
STA. 711+70 TO 727+93



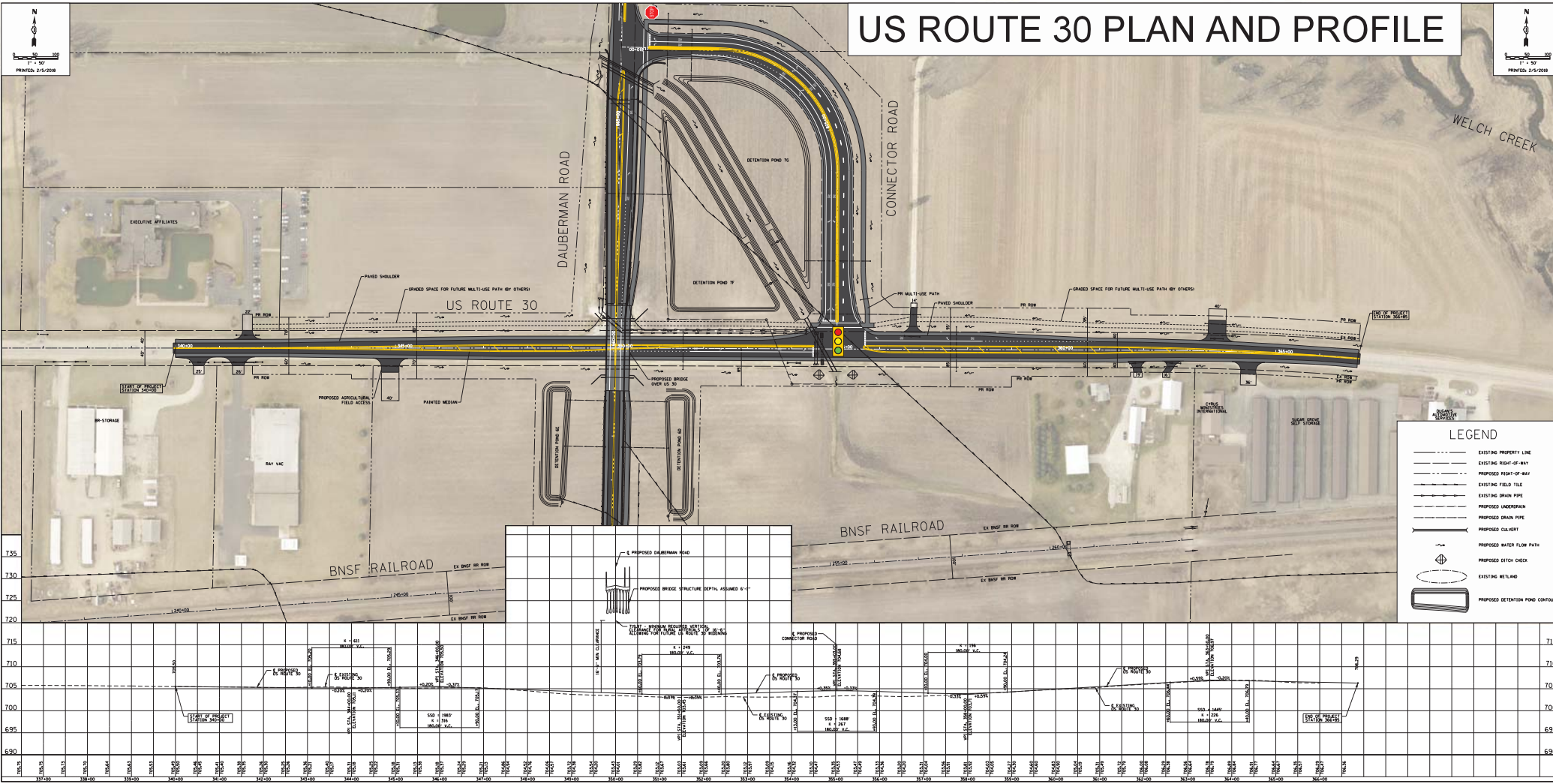
DAUBERMAN ROAD PLAN AND PROFILE



GRANART ROAD PLAN AND PROFILE



US ROUTE 30 PLAN AND PROFILE

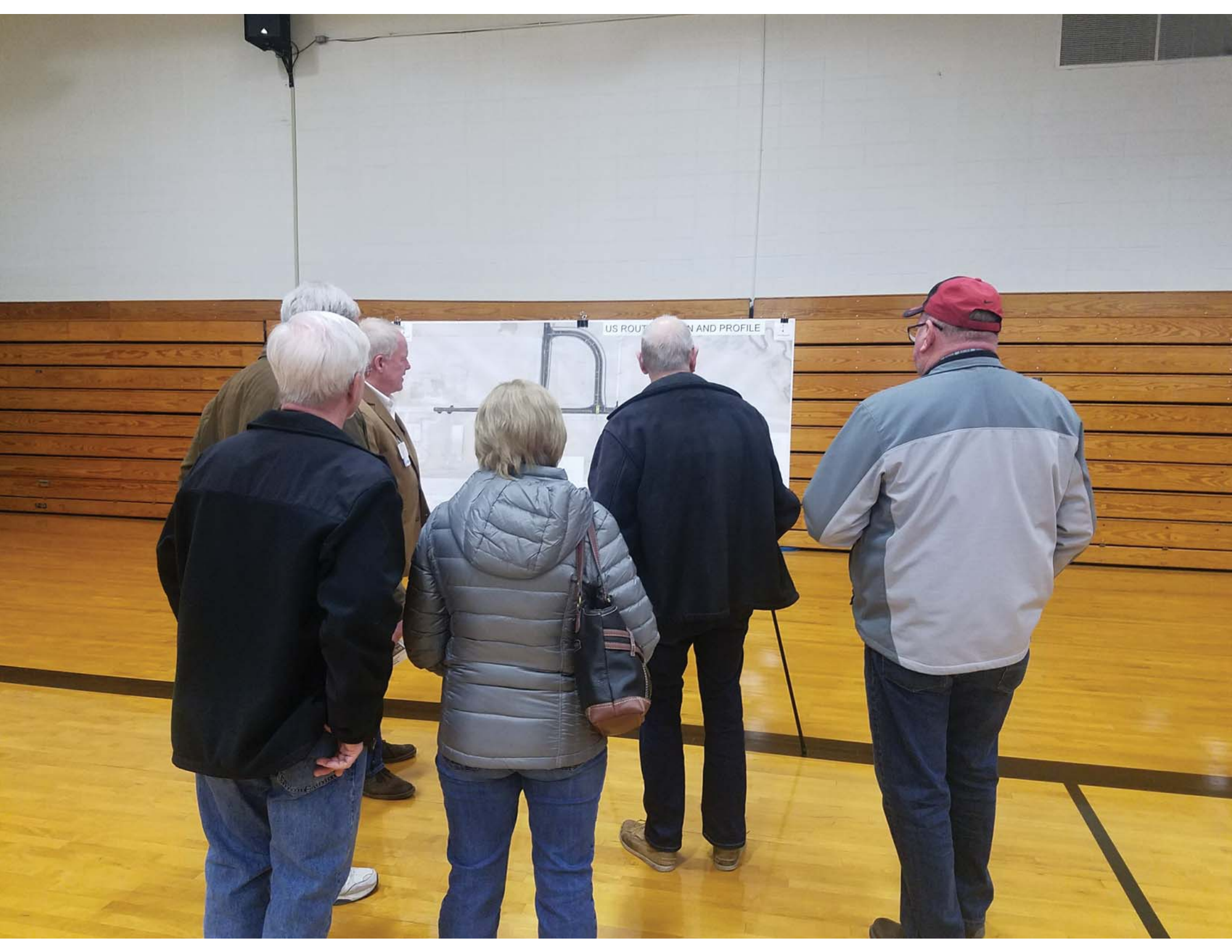


Attachment 9

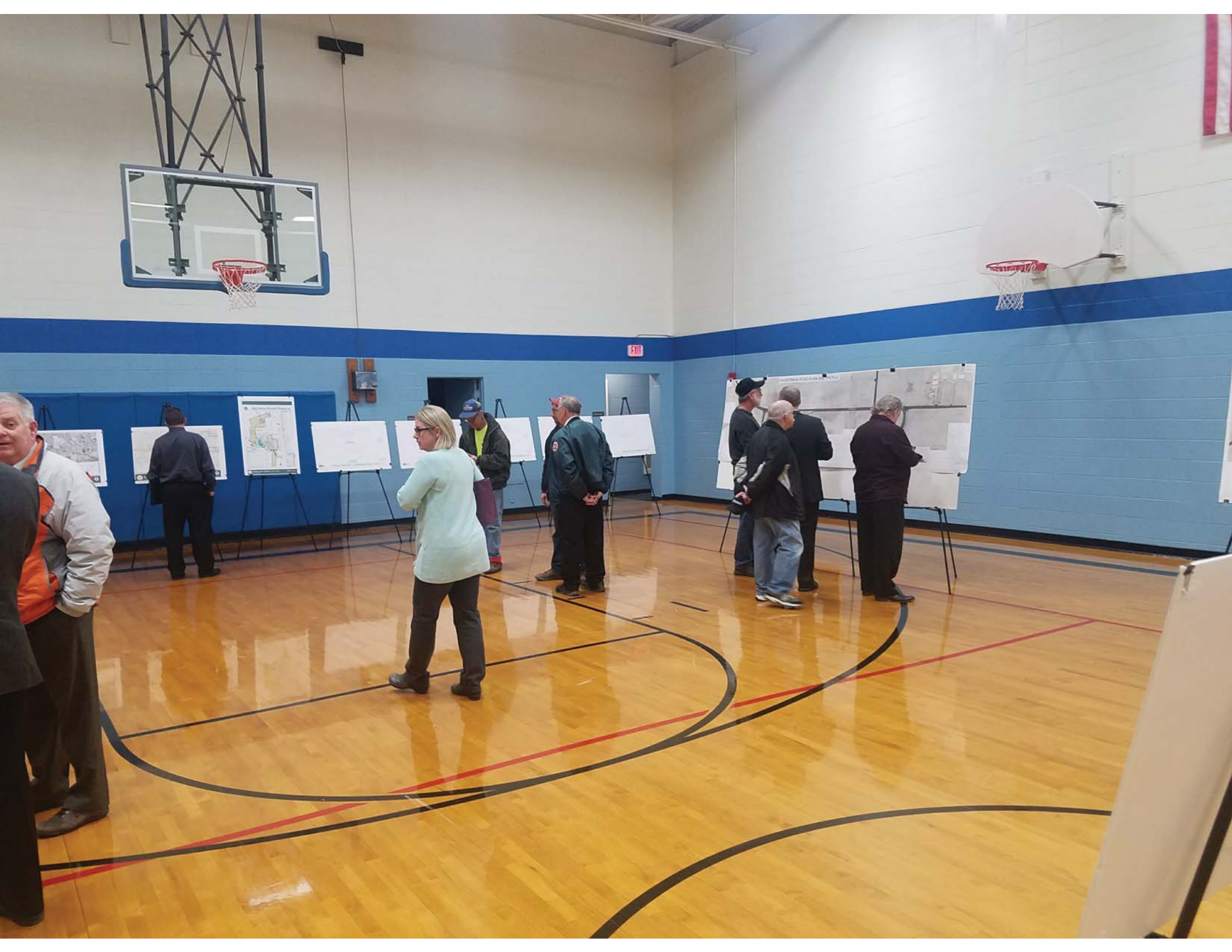
Public Hearing Photographs











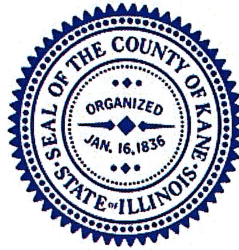




KANE COUNTY

DIVISION of TRANSPORTATION

Carl Schoedel, P.E.
Director of Transportation
County Engineer



41W011 Burlington Road
St. Charles, IL 60175
Phone: (630) 584-1170
Fax: (630) 584-5265

May 8, 2018

Ms. Catherine Batey, Division Administrator
Illinois Division
Federal Highway Administration
U.S. Department of Transportation
3250 Executive Park Drive
Springfield, IL 62703

Re: Dauberman Road Extension – **Public Hearing Certification**
US Route 30 to Granart Road
Kane County Hwy. No. 62

Dear Administrator Batey,

In accordance with 23 CFR 771.111(h)(2)(vi), the Kane County Division of Transportation certifies that a public hearing was held on **February 15, 2018** at the Hinckley-Big Rock Middle School from 5:00 P.M. to 7:30 P.M.

The meeting was an open house format with large-scale exhibits of the preferred alternative and other project information boards on display. The exhibits included information on the Dauberman Road Detour Route, and the Temporary Occupancy Section 4(f) impact associated with construction of the multi-use path within the Big Rock Forest Preserve along the south side of Granart Road. Two, 20-minute live presentations were made to the public at 6:00 P.M. and at 6:45 P.M. by the project consultant, TranSystems.

A separate comment area and comment box were provided for attendees to submit written comments, questions, and concerns. In addition, a court reporter was present if attendees desired to dictate their comment or question. Comments were accepted for two weeks after the hearing, with the comment period concluding on March 1, 2018.

Representatives from the Kane County Division of Transportation, TranSystems, and BLA, Inc. were available during the meeting to discuss the project and answer questions.

Advertisements for the public hearing were published **February 1, 2018** and **February 8, 2018** in local newspapers and online social media including:

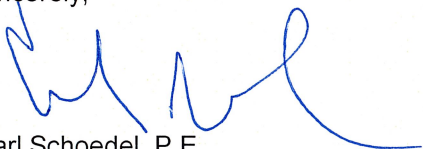
- **Aurora Beacon News**
- **Daily Herald**
- **Kane County Chronicle**
- **Kane County Connects:** (online publication, Facebook & Email) February 1, 2018 only
- **Big Rock Crier:** February 2018 monthly publication

Enclosed for your use and reference is a copy of the **Dauberman Road Public Hearing Summary**. The Summary contains the pertinent documents and information for the public hearing, including:

- **Attachment 1:** Mailed Invitation Letter, Recipients, and Notification Area
- **Attachment 2:** Certifications of Publication
- **Attachment 3:** Sign-In Sheets
- **Attachment 4:** Court Reporter Transcript
- **Attachment 5:** Comments Received & Official Responses
- **Attachment 6:** Informational Brochure
- **Attachment 7:** Presentation Slides and Narrative
- **Attachment 8:** Exhibits Boards
- **Attachment 9:** Public Hearing Photographs

If you have any questions or require additional information, please contact Candi Thomas, Senior Project Manager by telephone at (630) 406-7355 or by email at thomascandance@co.kane.il.us.

Sincerely,

A handwritten signature in blue ink, appearing to read 'CSchoedel', with a long horizontal flourish extending to the right.

Carl Schoedel, P.E.
Director of Transportation/County Engineer

APPENDIX E

Other Coordination and Meeting Minutes

8/25/16, Local Agency Initial Meeting

- Village of Big Rock
- Village of Sugar Grove

2/21/17, Big Rock Drainage District Meeting

4/27/17, Clearance Diagram and FAA Part 77 Notice Criteria Tool

5/17/17, CMAP ADT Concurrence Letter

5/18/17, USACE Jurisdictional Determination Approval Letter

10/25/17, Coordination Meeting with IDOT Programming

2/2/18, Coordination Meeting with IDOT BLRS

2/5/18, Coordination Meeting with IDOT BLRS

2/8/18, IDOT Approval to hold the Public Hearing

2/15/18, IDOT Response regarding the Prairie Parkway

5/10/18, Coordination with the BNSF Railroad - Received Comments

6/7/18, Coordination Meeting with IDOT Hydraulics Unit

Table of Utility Coordination

Signal Warrants Summary

- US Route 30/Connector Road
- Dauberman Road/Connector Road
- Dauberman Road/Granart Road



TranSystems

222 South Riverside Plaza
Suite 610
Chicago, IL 60606
Tel 312 669 9601
Fax 312 276 4805

www.transystems.com

MEETING MINUTES

Dauberman Road Extension
US 30 to Granart Road
Kane County

Date: August 25, 2016
Time: 1:00 PM
Place: Big Rock Village Office
Attendance: See attached sign-in sheet

The purpose of this meeting was to discuss the Dauberman Road Extension project located in Big Rock, Illinois. This was a project information meeting for the local agencies in Big Rock to discuss the progress of data collection by the project team and more importantly to gather any additional stakeholder agency data and input.

Project Overview

The meeting began with introductions and a project overview. TranSystems began by providing information and background about the corridor and the transportation network that exists in the vicinity of the Dauberman Road corridor. The project is located in the Village of Big Rock and Big Rock Township in southwest Kane County. The project team is conducting a preliminary engineering study on extending Dauberman Road from Granart Road (south) to US 30 (north) with a potential for a grade separation at the BNSF Railway and US 30.

Data Collection

The project team has begun data collection including topographic survey, environmental survey, drainage survey, existing and proposed land uses (based on the Big Rock Comprehensive Plan), contacting utility companies, and analyzing crash data in the project area. Items to note from the crash analysis so far are the high number of animal crashes and out of control crashes at the curve in Granart Road.

Discussion

After the brief presentation, the opportunity was presented for open discussion about the project and to receive input from the local agencies and other stakeholders concerning the study area . Below are the discussion points, comments, and questions brought up during the discussion.

1. Are traffic counts going to be taken at US 30 and Granart Road?

Traffic counts will begin in mid-September at US 30/Rhodes Street, US 30/Dugan Road, US 30/Dauberman Road, Granart Road/Rhodes Street, and Granart Road/Dugan Road. Once the traffic counts are completed, the project team will work with Chicago Metropolitan Agency for Planning, the regional planning agency, to determine projected volumes for 2040 and look at redistributing those volumes with an extension of Dauberman Road.

- 2. Chief Ruh (Fire Chief), he has concern about the BNSF Railroad.** They met with the BNSF Railroad approximately 2-3 years ago to discuss getting an overpass because they often need to be north of the tracks, especially with the Kaneland High School. They have a commitment to be able to get to the high school in 9 minutes or less. They would like to know what other kind of access might be available to get to the Dauberman Road extension instead of needing to take Rhodes Street to Granart Road and around. Currently, the Big Rock Fire Protection District has an agreement with the BNSF Railroad to allow for an access road along the south side of the tracks to a crossing east of Big Rock. The Fire Protection District would like a similar direct access to the Dauberman Road grade separation. While it would initially be for emergency vehicle use only initially, the Village of Big Rock would like this roadway to be designed such that it could be turned into a public roadway in the future due to potential growth.

At this point, all options are on the table. This study will take all input and use the data collected to determine the best alternatives and ultimately the preferred alternative for the final improvement. An IGA would be required between the Village and the County in order to provide an emergency access road to Granart Road from downtown Big Rock.

3. How will this be coordinated with the US 30 study?

IDOT is currently doing a separate study of US 30. Based on the Dauberman Road extension project, they are expanding their study limits west to tie into the Dauberman Road improvement. Any alternative that is developed as part of the Dauberman Road extension will incorporate potential improvements to US 30. The project team is coordinating with IDOT on both studies.

4. Is this budgeted?

This project is budgeted into the Transportation Improvement Program (TIP) 5-year program for transportation projects maintained by the Chicago Metropolitan Agency for Planning (CMAP). It identifies local and Illinois Commerce Commission (ICC) funds.

5. The map said approx. 31 trains/day. During the study for the Dugan Road roundabout the information that Sugar Grove gathered was closer to 44 trains/day.

Noted. We are coordinating with the BNSF railroad for updated train traffic numbers. It was noted that the railroad is double tracked to the east and west of the proposed corridor. The proposed grade separation will span the existing railroad right-of-way, thus providing for any future railroad improvement.

6. How will the Prairie Parkway Corridor impact this project?

The Prairie Parkway is within the same corridor as this improvement. The project team is currently working with IDOT to see how the planned improvement for Dauberman Road will affect the Prairie Parkway, and what steps need to be taken to move forward with the improvement. The corridor protection placed for the Prairie Parkway lasts for 10 years and was established in 2007. IDOT will revisit the corridor protection in 2017 to determine if they still want to maintain it. Kane County will be working with IDOT to acquire the necessary right-of-way and it is expected that any future Prairie Parkway would be grade separated over Dauberman Road. In this area, only the centerline is recorded, no ROW has been purchased by IDOT at this time.

7. Drainage is a huge problem within this area. When IDOT did improvements to US 30 they tied into the existing tiles. Kane County Division of Water Resources would like to remove that connection to US 30.

All drainage concerns will be looked at during the preliminary study. It is understood that the drainage tiles in the area are undersized and there are some issues with flooding. The project team and IDOT will be coordinating drainage improvements for the US 30 and the Dauberman Road corridor. A CD containing the existing drainage tile and sewer plans was provided to the project team by the Division of Water Resources.

8. What is the schedule?

Currently we are in Phase I Preliminary Design. We will be working on coming up with alternatives and narrowing down a preferred alternative with the Phase I being completed in 2017. Phase 2 will be for detailed bid plans throughout 2017 with a construction letting anticipated to be in late 2018 with the majority of construction in 2019.

9. What will the connection with US 30 look like?

It is anticipated that there will be a loop style ramp to connect to US 30. The location of that ramp will be determined based on traffic analysis, environmental survey, and property acquisition.

10. What is the overall public sentiment about the extension of Dauberman Road?

The residents along Granart Road and the Big Rock community are very supportive of an extension and most importantly the grade-separated crossing with the BNSF Railway. They view it as a life safety and traffic issue/improvement.

Next Steps

The next steps for this project is continued data collection (traffic counts, additional data gathering with local agencies), traffic analysis for existing and proposed conditions, a public meeting to introduce the project to the public and get input on the corridor, and developing alternatives based on public input and the data collected.



Dauberman Road Extension
Big Rock Local Agency Meeting
Meeting Minutes
August 25, 2016
Page 5 of 5

The meeting ended at 2:00 p.m.

By: _____

Eric Neubauer, P.E.



ATTENDANCE ROSTER

Project: Dauberman Road Extension
Limits: US 30 to Granart Road
Topic: Project Initiation Meeting

Date: August 25, 2016
Time: 1:00 PM
Location: Big Rock Village Office

Name	Representing	Telephone Number	E-Mail
Megan McDonald	Tran Systems	312-669-5824	macedonald@transystems.com
Michael Krzywinski	BLA	630-438-6400	mkrzywinski@bollingerlach.com
Sandy Carr	Big Rock Township	630-742-4792	bigrocktownship@gmail.com
Dan Bruckelmeier	BLA	630-438-6400	dbruckelmeier@bollingerlach.com
Eric Wehner	Tran Systems	(312) 669-5334	esneubauer@transystems.com
Brian Fairwood	TRAN SYSTEMS	847 407 5280	b1fairwood@transystems.com
Tim May	Village of Big Rock	630-556-4365	admin@villageofbigrock.us
Ken Rojek	B.R. Township	630-556-3693	KENROJEK@AOL.COM
John H. Ruck	BR Fire Dept	630-918-7829	JRUCK@DEKANE.COM
Carl Schoger	Drainage District #1	(815) 791-1079	Odie0722@aol.com
Bob Schoger	Farmer's Big Rock Trustee	630 363 9827	R SCHOGER 67@GMAIL.COM
Jodie Wollm	Kane County	630-232-3499	wollnikjodie@co.kane.il.us
WADE THOMPSON	TRAINAGE DIST #1	630 649 3960	thompson.pond@yahoo.com
Dean Annunzio	Village of Big Rock	630 207 3547	dean.annunzio@villageofbigrock.us
Joe WALSH	Village of Big Rock	630 774-9280	Joe.walsh@villageofbigrock.us



TranSystems

222 South Riverside Plaza
Suite 610
Chicago, IL 60606
Tel 312 669 9601
Fax 312 276 4805

www.transystems.com

MEETING MINUTES

Dauberman Road Extension
US 30 to Granart Road
Kane County

Date: August 25, 2016
Time: 10:00 AM
Place: Sugar Grove Village Hall
Attendance: See attached sign-in sheet

The purpose of this meeting was to discuss the Dauberman Road Extension project located in Big Rock, Illinois. This was a project information meeting for the local agencies in Sugar Grove to discuss the progress of data collection by the project team and more importantly to gather any additional stakeholder agency data and input.

Project Overview

The meeting began with introductions and a project overview. TranSystems began by providing information and background about the corridor and the transportation network that exists in the vicinity of the Dauberman Road corridor. The project is located in the Village of Big Rock and Big Rock Township in southwest Kane County. The project team is conducting a preliminary engineering study on extending Dauberman Road from Granart Road (south) to US 30 (north) with a potential for a grade separation at the BNSF Railway and US 30.

Data Collection

The project team has begun data collection including topographic survey, environmental survey, drainage survey, existing and proposed land uses (based on the Big Rock Comprehensive Plan), contacting utility companies, and analyzing crash data in the project area. Items of note from the crash analysis so far are the high number of animal crashes and out of control crashes at the curve in Granart Road.

Discussion

After the brief presentation, the opportunity was presented for open discussion about the project and to receive input from the local agencies and other stakeholders concerning the study area . Below are the discussion points, comments, and questions brought up during the discussion.

1. How will this be coordinated with the US 30 study?

IDOT is currently doing a separate study of US 30. Based on the Dauberman Road extension project, they are expanding their study limits west to tie into the Dauberman Road improvement. Any alternative that is developed as part of the Dauberman Road extension will incorporate potential improvements to US 30. The project team is coordinating with IDOT on both studies.

2. Traffic currently uses Granart Road to Dugan Road to US 30 to IL 56 to access the highway. How will this improvement change that?

This project has the potential to relieve some of the traffic on Dugan Road by providing another north-south connection over the BNSF Railway.

3. The map said approx. 31 trains/day. During the study for the Dugan Road roundabout the information that Sugar Grove gathered was closer to 44 trains/day.

Noted. We are coordinating with the BNSF railroad for updated train traffic numbers.

4. What years are shown in the traffic data?

The traffic data shown on the exhibit was from the IDOT online map, ranging from 2010 to 2015 traffic numbers. Traffic counts will be conducted in mid-September. The time frame is to ensure the normalization of traffic patterns post detour for the Dauberman Road bridge over Welch Creek improvement and once school is in session to ensure a more realistic count. Analysis of that traffic data will begin once the traffic data has been received. The project team will work with the Chicago Metropolitan Agency for Planning, the regional planning agency, to get redistributions and projections of future traffic in the area. It is expected that the traffic analysis will be complete before the end of the year as it will be used to develop the different roadway alternatives.

5. What are the implications of Granart Road not being part of the County? How does that impact this improvement?

The project team and Kane County will be coordinating with Big Rock Township to discuss options for Granart Road and how the jurisdiction could be handled.

6. How will the Prairie Parkway Corridor impact this project?

The Prairie Parkway is within the same corridor as this improvement. The project team is currently working with IDOT to see how the planned improvement for Dauberman Road will affect the Prairie Parkway, and what steps need to be taken to move forward with the improvement. The corridor protection placed for the Prairie Parkway lasts for 10 years and was established in 2007. IDOT will revisit the corridor protection in 2017 to determine if they still want to maintain it. Kane County will be working with IDOT to acquire the necessary right-of-way and it is expected that any future Prairie Parkway would be grade separated over Dauberman Road. In this area, only the centerline is recorded, no ROW has been purchased by IDOT at this time.

7. What is the schedule?

Currently we are in Phase I Preliminary Design. We will be working on coming up with alternatives and narrowing down a preferred alternative with the Phase I being completed in 2017. Phase 2 will be for detailed bid plans throughout 2017 with a construction letting anticipated to be in late 2018 with the majority of construction in 2019.

8. Is this actually budgeted?

Yes, this project is funded through the Illinois Commerce Commission, Surface Transportation Program (STP), and local monies. The Transportation Improvement Program (TIP) maintained by the Chicago Metropolitan Agency for Planning states a target letting date of 2018. The Illinois Commerce Commission (ICC) has provided funding (\$7.2 million) as part of their safety improvement program in order to look at grade-separated crossing with the BNSF Railway. The County will be responsible for the remainder of the funds through various sources.

9. Are there any potential grade crossing closings associated with this project?

No, this project is only looking at creating a grade-separated crossing for Dauberman Road. It is not to close any existing crossings.



10. Will this project move forward even if traffic counts are low?

Yes, Kane County is eager to move forward with this project.

11. What is the public opinion in Sugar Grove about this extension?

This project hasn't been part of the Sugar Grove discussion for quite some time. Prior to the Dugan Road improvements, it was seen as more of a necessity. Now with the Dugan Road construction, Sugar Grove hasn't had much discussion. Sugar Grove does know that the Big Rock residents have complaints about the trains stopping on the tracks at Rhodes Avenue and blocking their main road in their downtown area.

12. BLA requested any information on flooding in the area.

Known history of flooding near the corner of Dauberman Road and US 30 historically. One of the attendees was a Township Trustee and a farmer in the area. He has concern over the effect this improvement will have on the existing drain tiles. This will be studied during the design of the alternatives.

13. There were some discussions about any wet ponds around the airport as they can attract wildlife, so the airports generally like to have only dry ponds.

14. The Village Manager of Sugar Grove asked about cheaper alternatives to the improvement, including the addition of a fire station north of the BNSF railroad.

15. A Sugar Grove trustee believes that the project will cost "too much money for too little improvement." She noted that other projects, such as the intersection of Dauberman Road and Keslinger Road, should be higher priority as there is a greater safety issue.

16. The Sugar Grove Fire Chief noted that this solution would be good for emergency response.

17. The representative from the Kane County Sheriff's Office noted that many of the accidents are the result of ice or drunk driving. A T-intersection at Dauberman Road and Granart Road should improve safety. If this is not possible, the addition of wider shoulders should be explored.

18. Sugar Grove noted that they do not have any utilities in the project area.



Next Steps

The next steps for this project is continued data collection (traffic counts, additional data gathering with local agencies), traffic analysis for existing and proposed conditions, a public meeting to introduce the project to the public and get input on the corridor, and developing alternatives based on public input and the data collected.

The meeting ended at 11:00 a.m.

By: _____

Eric Neubauer, P.E.



ATTENDANCE ROSTER

Project: Dauberman Road Extension
Limits: US 30 to Granart Road
Topic: Project Initiation Meeting

Date: August 25, 2016
Time: 10:00 AM
Location: Sugar Grove Village Hall

Name	Representing	Telephone Number	E-Mail
Dan Dauberman	BLA	630-438-6400	dbruckelmeyer@bollingerlach.com
Michael Krzywinski	BLA	630-438-6400	mkrzywinski@bollingerlach.com
Tom Bumgarner	SHEER	630-208-2001	bumgarner.tom@co.kane.il.us
Wayne Parson	S G F D	630-466-4513	wparson@SugarGroveFire.com
Brent Eichelberger	Village of Sugar Grove	630-466-4707	beichelb@sgsugarville.gov
Tony Speciale	Village of Sugar Grove	630-466-4507	aspecial@sgsugarville.gov
Brian Falkner	TRANSYSTEMS	847 407 5280	brian.falkner@transystems.com
Michele Piotrowski	Village of Sugar Grove	630-466-1070	mpiotrowski@eeinids.com
Megan McDonald	Transystems	312-669-58	namcdonald@transystems.com
Pat Rollins	Sugar Grove Police	630 466 4526 x 31	policechief@sgsugarville.gov
Walter Magdziarz	VOSG	630-466-8954	WMA9021AR2@SUGARGROVEIL.GOV
Mari Johnson	VOSG		
Eric Newbauer	Transystems	(312) 661-5834	ernewbauer@transystems.com

**Dauberman Road Extension
Kane County DOT**

**Big Rock Drainage District #1 Field Meeting
February 21, 2017
4:30 pm – 6:30 pm
Various Locations in Big Rock, IL**

DRAFT - Minutes to Meeting

Attendees:

Eric Neubauer – TranSystems (TS)	(312) 669-5834
Kevin Kenniff – BLA	(630) 438-6400
Michael Krzywinski – BLA	(630) 438-6400
David Hall – Big Rock Water Drainage District Commissioner	(630) 450-5439

The purpose of the meeting was to meet in the field with the Big Rock Drainage District #1 representatives and residents to discuss drainage problems with the existing field tiles and surface drainage problems within the Village of Big Rock.

The discussion was as follows.

Kevin and Michael discussed drainage concerns and wetlands within the Thompson Farm with the property owners. Mr. Thompson said he was concerned why wetlands were mapped on his property. He stated that he has always farmed the land and that the wet conditions are due to poor drainage from the existing drain tiles in the area. Eric stated that he will review the wetland report prepared by Huff and Huff but that the determination is typically based on a combination of soils, observed vegetation, and water retention. Mr. Thompson also stated that the existing 20" drain tile that drains his farm land is not draining correctly. He showed Eric, Kevin and Michael a drain tile manhole and stated that the water level was around 2 feet from the rim elevation after the rainstorm the previous week. The next downstream manhole (east toward Welch Creek) did not have standing water, which means that there may be a blockage in the drain tile between the two manholes.

Eric, Kevin and Michael then joined with several other representatives from the Big Rock Drainage District #1 and residents, where they expressed their drainage concerns in the area. Specifically, David Hall explained that there is a large ditch northwest of Dauberman Road and US Route 30, which drains to two culverts located immediately northwest of the intersection to the east side of Dauberman Road. Drainage then flows to three culverts located approximately 500 feet east of the intersection, which carries the flows south underneath US Route 30. From there, drainage flows southeast through a ditch on the Thompson farm property to a box culvert located underneath the BNSF railroad tracks. Drainage flows then split and either flows south through the box culvert or east through an existing ditch on the north side of the tracks, which flows to Welch Creek. It appears that the majority of drainage was originally intended to flow south through the existing box culvert and then turn east and flow overland to Welch Creek.

However, Mr. Hall stated that because the railroad does not clean the existing box culvert most of the drainage flows through the existing ditch on the north side of the tracks.

Michael asked Mr. Hall if he ever saw floodwaters ever overtop Dauberman Road just north of the US Route 30 intersection and Mr. Hall said that it did not. However, there was about 3-4 feet of standing water in the farmlands northwest of Dauberman Road and US Route 30 as well as south of the BNSF Railroad tracks during the flood of 1996. Mr. Hall added that US Route 30 bridge over Welch Creek was overtopped by floodwaters during the same flood event.

Mr. Hall stated that the Big Rock Drainage District #1 owns and maintains the two main drain tiles in the area. He showed Eric, Kevin and Michael the general layout of the drain tiles which matched what is shown on the drain tile survey. Mr. Hall stated that the north drain tile is clay which was built in the 1890's and the south drain tile was concrete built around 1953. Both of the drain tiles originate from farmland located northwest of US Route 30 and Davis Road. The drain tiles head southeast under Davis Road and to a junction just north of DeKane Equipment Corporation and US Route 30, where the drain tiles change routes. The north drain tile travels northeast to a point approximately 500ft north of the intersections of US Route 30 and Dauberman Road, where the tile turns southeast and crosses under US Route 30 and the BNSF Railroad tracks. The south drain tile travels southeast under US Route 30 and through the Vacationland property, where the tile crosses underneath the BNSF Railroad tracks. After crossing the railroad tracks, both the north and south drain tiles run east to Welch Creek approximately 50 feet apart from each other and converge as the drain tiles get closer to the creek.

Mr. Hall stated that even though the south drain tile is newer, they have experienced more problems with this concrete drain tile. However, the north drain tile is currently experiencing a problem because it too is not draining. Mr. Hall stated that they need to fix both drain tiles this upcoming spring (2017) before the rainy season starts for the area. The Big Rock Drainage District #1 has two miles of drain tiles to maintain and the drainage district only has a \$10,300 budget every year, which is not enough for the amount of drain tile they own. Mr. Hall asked if the county can help to fix the drain tiles by this upcoming spring. Eric stated that the project team will discuss the issues with the County but construction of our project is several years in the future and much of the impacted area is outside the project limits. The Dauberman Road Extension project is currently in Phase 1 Engineering, which is expected to last into 2018 with Phase 2 plan preparation occurring from 2018 to 2020 and Phase 3 construction expected to occur after 2020. Construction depends on additional funding. Thus, the Drainage District will likely need to address their immediate concerns using their own funds.

Mr. Hall then walked Eric, Kevin and Michael along the main drain tiles on the south side of the railroad tracks to the outlet point at Welch Creek. Mr. Hall showed the many sinkholes that appear to follow the location of the drain tiles to Welch Creek. The south drain tile appeared to have more sinkholes than the north drain tile. Kevin took georeferenced pictures of the sinkholes to drop into GIS once back in the office.

Mr. Hall stated that the area east of the access drive nearest to Welch Creek was designated as wetlands by the Army Corps of Engineers when they last performed maintenance of the drain tiles. This was confirmed by Tom Huddleston who indicated that he helped in the permitting of the maintenance work.

Mr. Hall also stated that when the new BNSF railroad bridge was built, the contractors installed a temporary rock crossing along Welch Creek for construction. This rock crossing was not completely removed after construction and has raised the creek about 6 inches upstream of the bridge. This condition was field verified and it appeared that the Welch Creek water surface dropped suddenly before and after the bridge.

While walking back to the Thompson farm property, Mr. Hall showed Eric, Kevin and Michael the past staging area and access location when the railroad bridge was replaced, which was located at the rear lot of Dugan's Automotive Service (46W637 US Route 30). Mr. Hall stated that workers cross the ditches on both sides of the railroad tracks with large rock berms and culverts through the berms. When the bridge replacement was complete, the workers only cleaned out the ditch along the north side of the railroad from this access to Welch Creek.. Mr. Hall stated that the entire ditch along the north side of the railroad needs to be re-sloped and cleaned out from the box culvert under the railroad tracks near the Thompson farm property to Welch Creek.

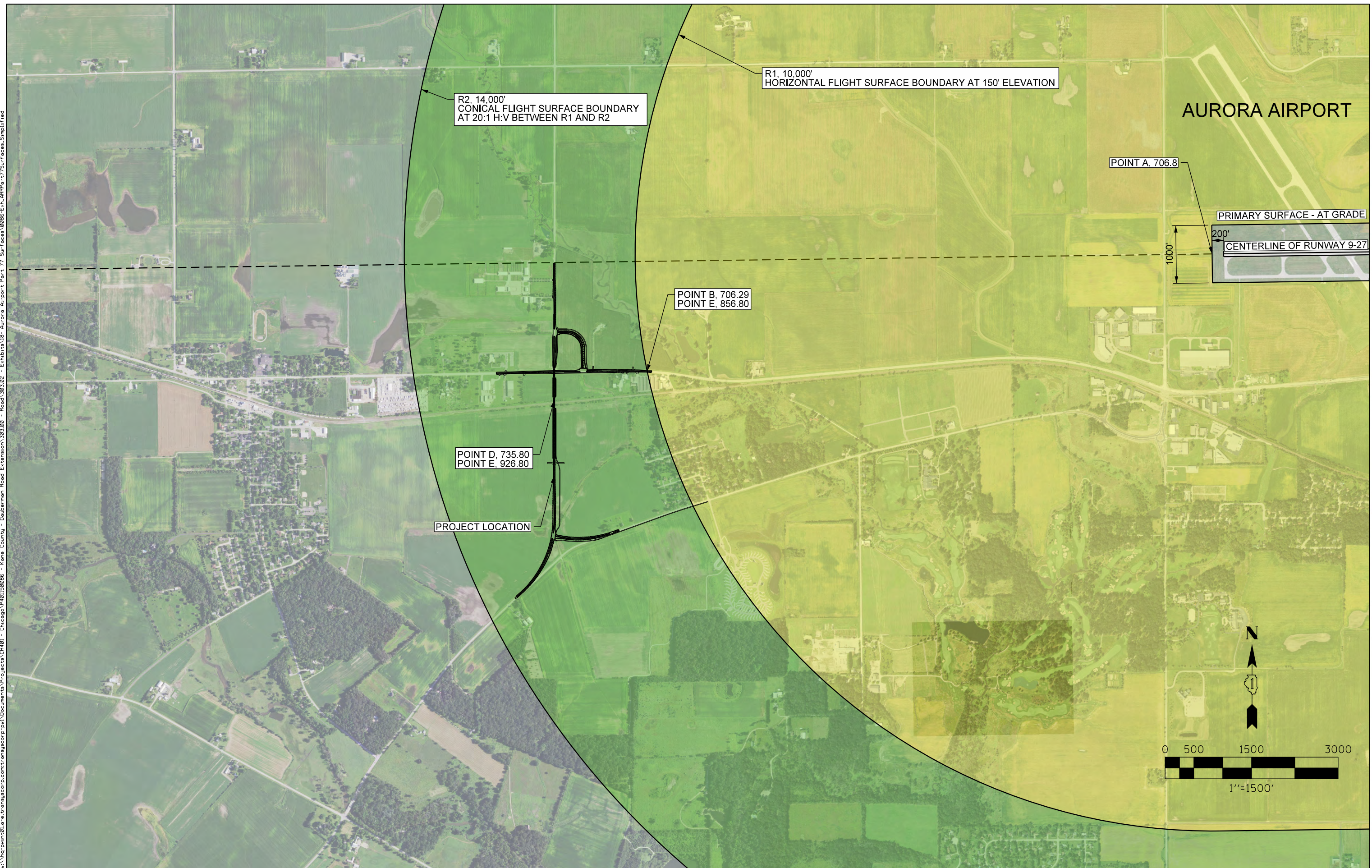
At the Thompson farm and Mr. Hall showed Eric, Kevin and Michael the box culvert under the railroad. Mr. Hall stated that the railroad rarely maintains the ditches and that the Drainage District cannot clean the ditch due to lack of funding and because the ditch is within railroad right-of-way.

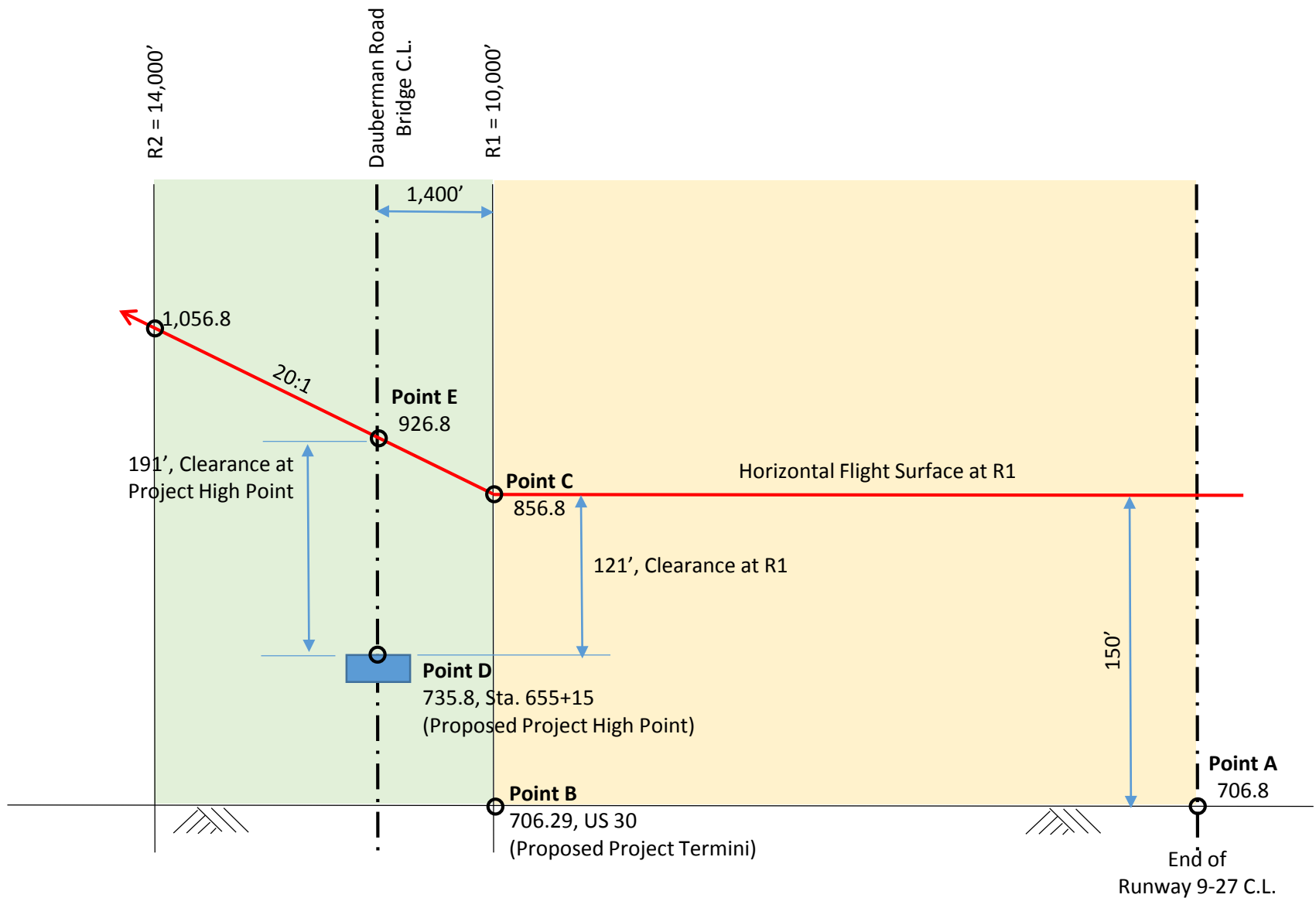
We discussed the storm sewer connection IDOT made to the Big Rock Drainage District #1 drain tile on the north side of US Route 30 approximately 2,000 feet east of Rhodes Street. Mr. Hall stated that IDOT made the connection sometime in the 1990's when US 30 was reconstructed. Instead of draining the IDOT storm sewer to the drain tile, the Drainage District would like the ditch along the north side of the US Route 30 to be extended east to the Welch Creek to carry storm sewer flows to the creek. The existing drain tile that crosses under US Route 30 is in bad condition and a manhole was added over the drain tile on the south side of US Route 30 where there had been previous issues, including damage due to the relocation of a ComEd pole. There are also 2 petroleum pipelines near this drain tile crossing under US Route 30 to the east.

Outstanding Items

Discuss drainage concerns with KDOT

Minutes prepared by BLA (Michael Krzywinski)





Dauberman Road Extension
(US Route 30 to Granart Road)
Kane County Division of Transportation

FAA Clearance Diagram
Vertical Elevations and Clearances
(for illustrative purposes only, not to scale)



Federal Aviation
Administration

The system will be going offline at 7pm ET on Friday, April 28, 2017 for upgrades. We apologize for any inconvenience.

« OE/AAA

Notice Criteria Tool

Notice Criteria Tool - Desk Reference Guide V_2014.2.0

The requirements for filing with the Federal Aviation Administration for proposed structures vary based on a number of factors: height, proximity to an airport, location, and frequencies emitted from the structure, etc. For more details, please reference [CFR Title 14 Part 77.9](#).

You must file with the FAA at least 45 days prior to construction if:

- your structure will exceed 200ft above ground level
- your structure will be in proximity to an airport and will exceed the slope ratio
- your structure involves construction of a traverseway (i.e. highway, railroad, waterway etc...) and once adjusted upward with the appropriate vertical distance would exceed a standard of 77.9(a) or (b)
- your structure will emit frequencies, and does not meet the conditions of the [FAA Co-location Policy](#)
- your structure will be in an instrument approach area and might exceed part 77 Subpart C
- **your proposed structure will be in proximity to a navigation facility and may impact the assurance of navigation signal reception**
- your structure will be on an airport or heliport
- filing has been requested by the FAA

If you require additional information regarding the filing requirements for your structure, please identify and contact the appropriate FAA representative using the [Air Traffic Areas of Responsibility map](#) for Off Airport construction, or contact the [FAA Airports Region / District Office](#) for On Airport construction.

The tool below will assist in applying Part 77 Notice Criteria.

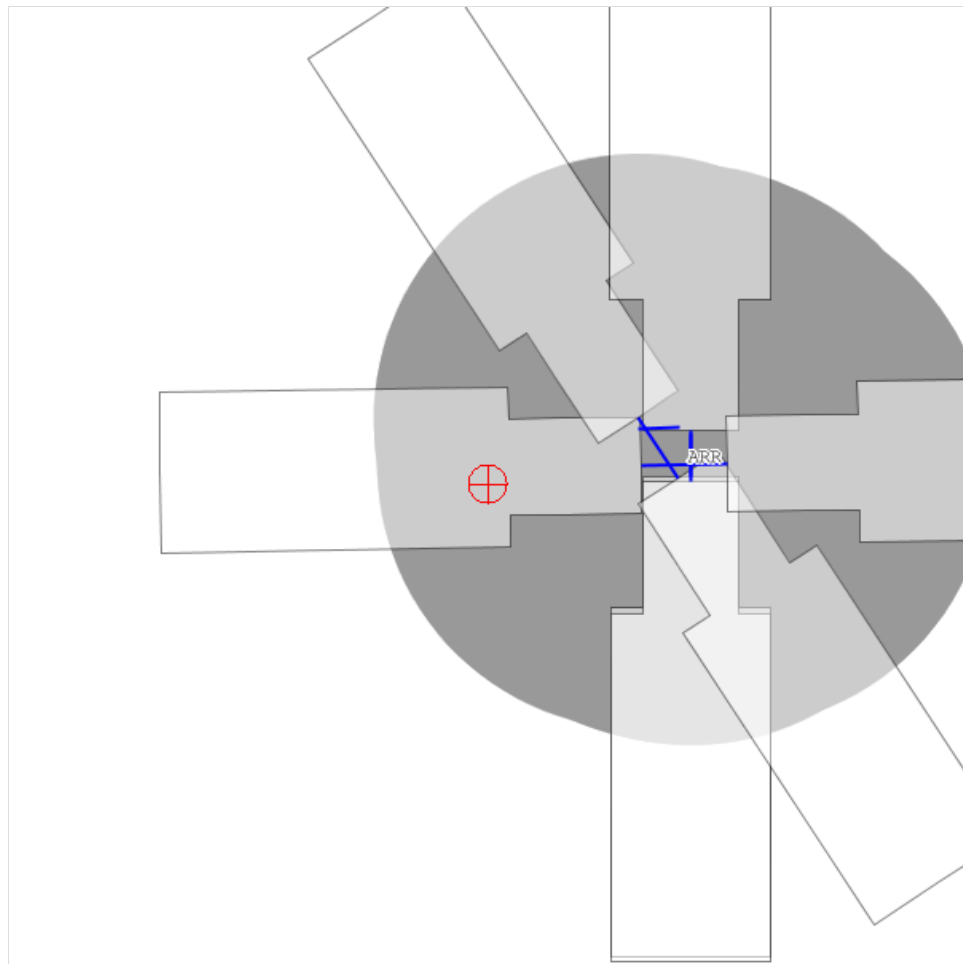
Latitude:	41	Deg	45	M	57.52	S	N ▼
Longitude:	88	Deg	31	M	41.14	S	W ▼
Horizontal Datum:	NAD83 ▼						
Site Elevation (SE):	705	(nearest foot)					
Unadjusted Structure Height :	29	(nearest foot)					
Height Adjustment:	15	(nearest foot)					
Total Structure Height (AGL):	44	(nearest foot)					
Traverseway:	Public Roadway ▼ (Additional height is added to certain structures under 77.9(c)) User can increase the default height adjustment for Traverseway, Private Roadway and Waterway						
Is structure on airport:	<input checked="" type="radio"/> No <input type="radio"/> Yes						

Results

You exceed the following Notice Criteria:

Your proposed structure is in proximity to a navigation facility and may impact the assurance of navigation signal reception. The FAA, in accordance with 77.9, requests that you file.

The FAA requests that you file





Federal Aviation
Administration



FEDERAL AVIATION ADMINISTRATION

OE/AAA®

OBSTRUCTION EVALUATION / AIRPORT AIRSPACE ANALYSIS

DESK REFERENCE GUIDE

SUBJECT: Notice Criteria Tool

**You are not required to have a registered e-filing account*

**Prepared by
CGH Technologies, Inc.
600 Maryland Ave., SW Suite 800W
Washington, DC 20024**

All references to software products remain the protected trademarks of their manufacturers. The instructions in this document may reference Microsoft application(s). This is not meant in any way to express a preference for any particular product since there are many different browsers, programs, and operating systems available to the user. For simplicity only, one brand/product is used in the examples that follow.



Federal Aviation
Administration



Notice Criteria Tool

Obstruction Evaluation Version 2014.2.0.342	Notice Criteria Tool
Home	
FAA OE/AAA Offices	
View Determined Cases	
View Interim Cases	
View Proposed Cases	
View Supplemental Notices (Form 7460-2)	
View Circularized Cases	
Search Archives	
Download Archives	
Circle Search for Cases	
Circle Search for Airports	
General FAQs	
Wind Turbine FAQs	
Discretionary Review FAQs	
Notice Criteria Tool	
DoD Preliminary Tool	Notice Criteria Tool
Wind Turbine Build Out	

To access the OE/AAA Notice Criteria screen, select the **Notice Criteria Tool** link located on the left sidebar of the website, under the gray Obstruction Evaluation header.



Federal Aviation
Administration



As shown below, the Notice Criteria screen summarizes the filing requirements specified in Title 14 of the Code of Federal Regulations Part 77.9 Notice Criteria.

The requirements for filing with the Federal Aviation Administration for proposed structures vary based on a number of factors: height, proximity to an airport, location, and frequencies emitted from the structure, etc. For more details, please reference [CFR Title 14 Part 77.9](#).

You must file with the FAA at least 45 days prior to construction if:

- your structure will exceed 200ft above ground level
- your structure will be in proximity to an airport and will exceed the slope ratio
- your structure involves construction of a traverseway (i.e. highway, railroad, waterway etc...) and once adjusted upward with the appropriate vertical distance would exceed a standard of 77.9(a) or (b)
- your structure will emit frequencies, and does not meet the conditions of the [FAA Co-location Policy](#)
- your structure will be in an instrument approach area and might exceed part 77 Subpart C
- your proposed structure will be in proximity to a navigation facility and may impact the assurance of navigation signal reception
- your structure will be on an airport or heliport
- filing has been requested by the FAA

If you require additional information regarding the filing requirements for your structure, please identify and contact the appropriate FAA representative using the [Air Traffic Areas of Responsibility map](#) for Off Airport construction, or contact the [FAA Airports Region / District Office](#) for On Airport construction.

There is a **CFR Title 14 Part 77.9** link in the first paragraph above. Selecting this link, opens a new browser window where you can view text or PDF sections of the Part 77 regulation. You must ensure that your proposal does not require notice under *any* Notice Criteria prescribed in Part 77.9.

In the bulleted list above, there is a link to the **FAA Co-location Policy**. In the second paragraph above, there are links to the **Air Traffic Areas of Responsibility map** and the **FAA Airports Region / District Office**.

Use the calculation tool to determine if your proposal exceeds the slope ratio criteria.

The slope calculation tool is only provided to assist you in applying the appropriate calculation for Part 77.9.



Federal Aviation
Administration



Notice Criteria Tool:

The tool below will assist in applying Part 77 Notice Criteria.

Latitude:	<input type="text"/> Deg <input type="text"/> M <input type="text"/> S <input type="button" value="N"/>
Longitude:	<input type="text"/> Deg <input type="text"/> M <input type="text"/> S <input type="button" value="W"/>
Horizontal Datum:	<input type="button" value="NAD83"/>
Site Elevation (SE):	<input type="text"/> (nearest foot)
Structure Height (AGL):	<input type="text"/> (nearest foot)
Traverseway:	<input type="button" value="No Traverseway"/> (Additional height is added to certain structures under 77.9(c))
Is structure on airport:	<input checked="" type="radio"/> No <input type="radio"/> Yes
<input type="button" value="Submit"/>	

1. Enter the proposed Latitude, Longitude, Horizontal Datum, Site Elevation, and proposed Structure Height.
2. Traverseway - use the default entry of "No Traverseway" in this field unless your structure is going to cross one of the dropdown entries.

Traverseway:	<input type="button" value="No Traverseway"/>
Is structure on airport:	<input type="button" value="No Traverseway"/> <input type="button" value="Interstate Highway"/> <input type="button" value="Private Road"/> <input type="button" value="Public Roadway"/> <input type="button" value="Railroad"/> <input type="button" value="Waterway"/>

3. Select whether the calculation is being run for a structure on an airport.
4. Then, select the **[Submit]** button.



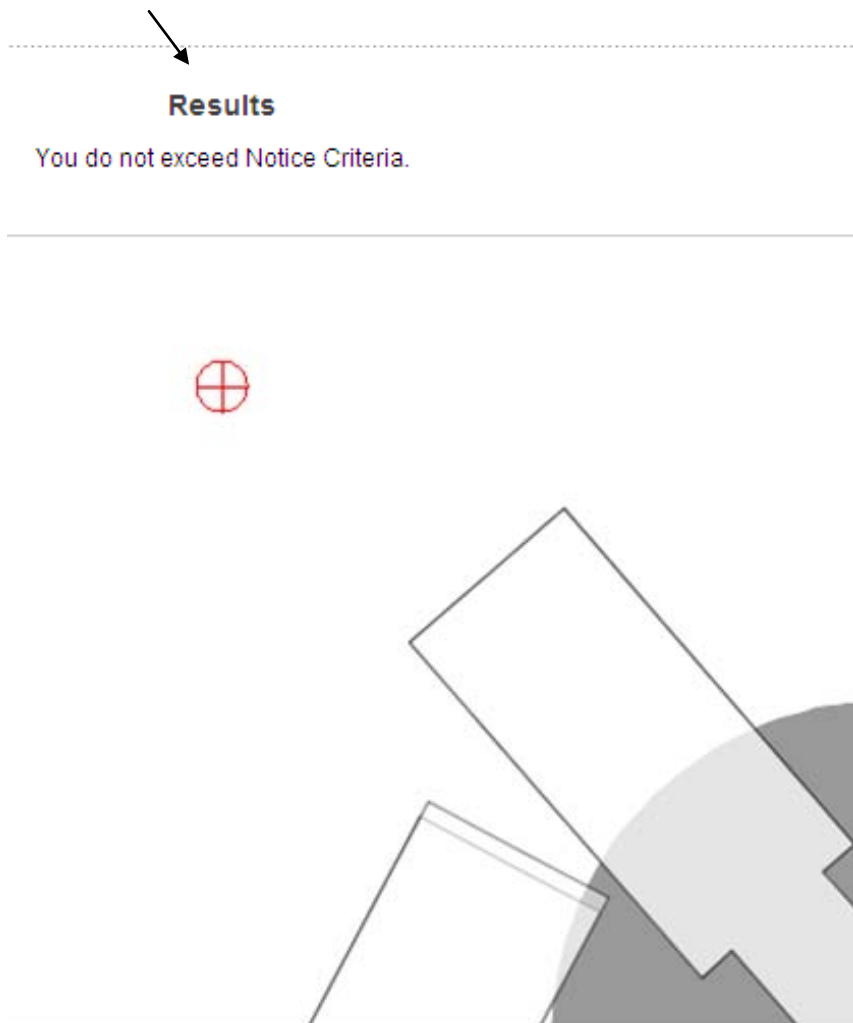
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The results will provide the slope calculation exceed, if any.

The following is an example of a slope calculation that did not exceed FAA Part 77 Notice Criteria:

Example 1: “Does Not Exceed”





Federal Aviation
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The following two examples illustrate On and OFF airport slope calculations that exceed FAA Part 77 Notice Criteria:

In both Exceed result examples below, the slope calculation exceed is provided in feet; the nearest airport and affected runway(s) are also identified.

Example 2: On Airport “Exceed”

Latitude:	<input type="text" value="38"/> Deg	<input type="text" value="51"/> M	<input type="text" value="6.22"/> S	<input type="text" value="N"/>
Longitude:	<input type="text" value="77"/> Deg	<input type="text" value="02"/> M	<input type="text" value="18.00"/> S	<input type="text" value="W"/>
Horizontal Datum:	<input type="text" value="NAD83"/>			
Site Elevation (SE):	<input type="text" value="15"/> (nearest foot)			
Structure Height (AGL):	<input type="text" value="190"/> (nearest foot)			
Traverseway:	<input type="text" value="No Traverseway"/>			
	(Additional height is added to certain structures under 77.9(c))			
Is structure on airport:	<input type="radio"/> No <input checked="" type="radio"/> Yes			
	<input type="button" value="Submit"/>			



Federal Aviation
Administration



Results

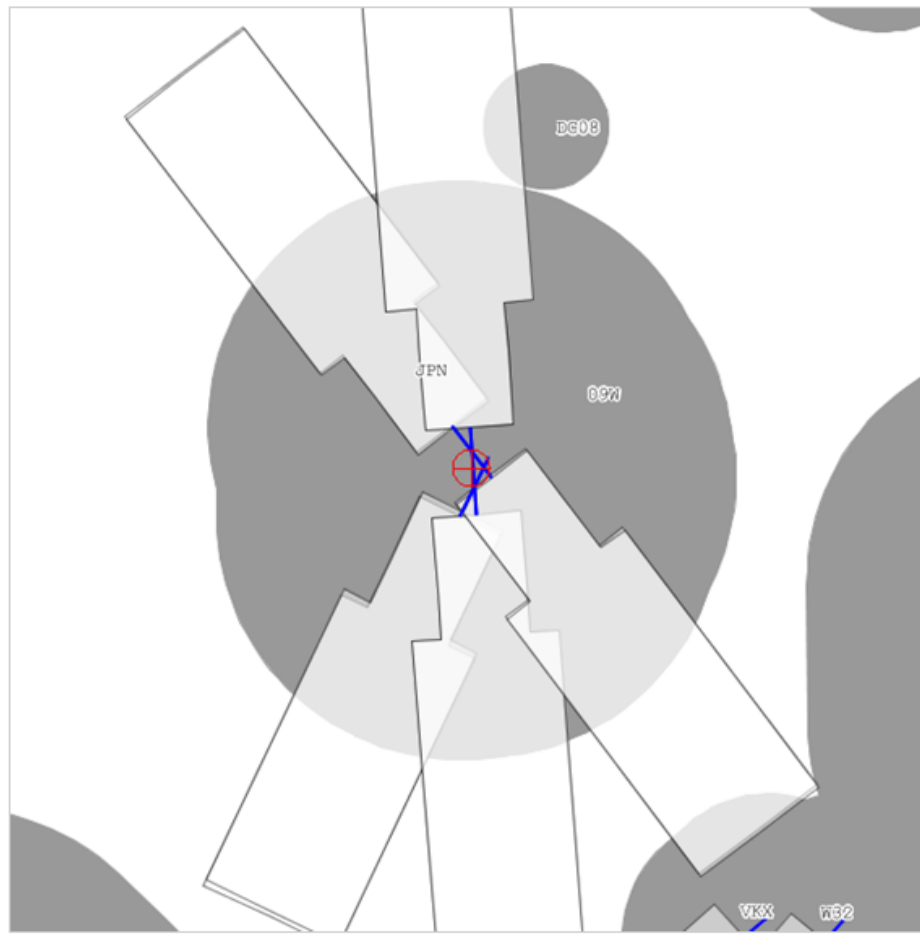
You exceed the following Notice Criteria:

Your proposed structure is in proximity to a navigation facility and may impact the assurance of navigation signal reception. The FAA, in accordance with 77.9, requests that you file.

77.9(b) by 193 ft. The nearest airport is DCA, and the nearest runway is 01/19.

77.9(d) construction is on an airport.

The FAA requests that you file





Federal Aviation
Administration



Example 3: Off Airport “Exceed”

Latitude:	<input type="text" value="38"/> Deg <input type="text" value="51"/> M <input type="text" value="6.90"/> S <input type="text" value="N"/>
Longitude:	<input type="text" value="76"/> Deg <input type="text" value="02"/> M <input type="text" value="16.00"/> S <input type="text" value="W"/>
Horizontal Datum:	<input type="text" value="NAD83"/>
Site Elevation (SE):	<input type="text" value="15"/> (nearest foot)
Structure Height (AGL):	<input type="text" value="345"/> (nearest foot)
Traverseway:	<input type="text" value="No Traverseway"/>
	(Additional height is added to certain structures under 77.9(c))
Is structure on airport:	<input checked="" type="radio"/> No <input type="radio"/> Yes
	<input type="button" value="Submit"/>



Federal Aviation
Administration



Results

You exceed the following Notice Criteria:

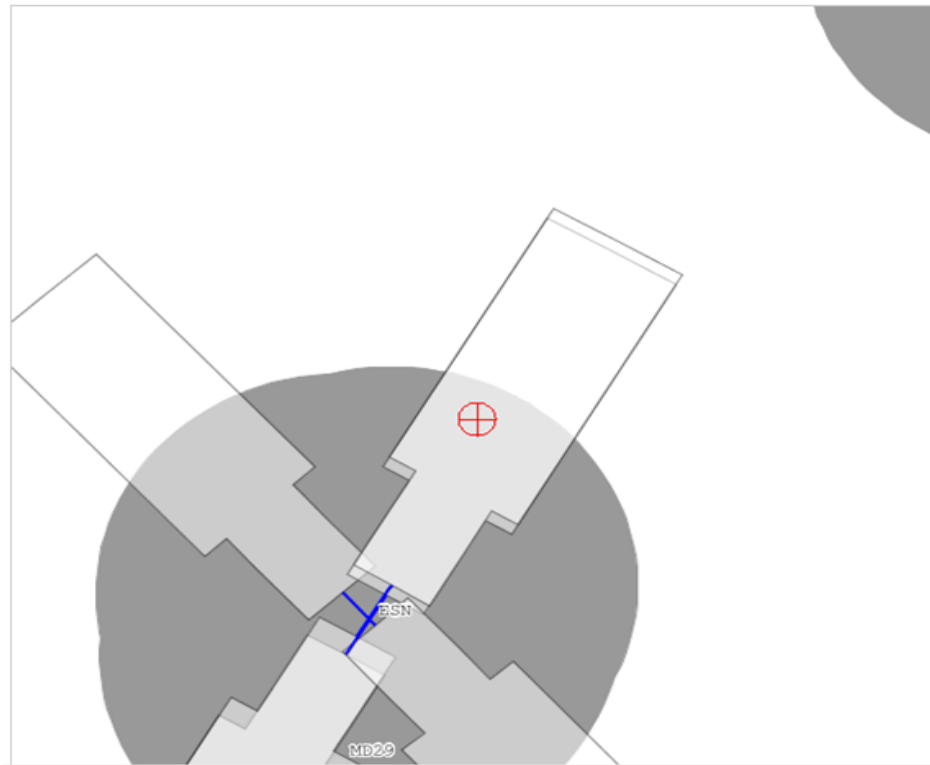
Your proposed structure exceeds an instrument approach area by 193 feet and aeronautical study is needed to determine if it will exceed a standard of subpart C of 14CFR Part 77. The FAA, in accordance with 77.9, requests that you file.

Your proposed structure is in proximity to a navigation facility and may impact the assurance of navigation signal reception. The FAA, in accordance with 77.9, requests that you file.

77.9(a) by 145 ft.

77.9(b) by 136 ft. The nearest airport is ESN, and the nearest runway is 15/33.

The FAA requests that you file



If you require additional information regarding the filing requirements for your structure, please identify and contact the appropriate FAA representative using the Air Traffic Areas of Responsibility map for Off Airport construction, or contact the FAA Airports Region / District Office for On Airport construction.



Chicago Metropolitan
Agency for Planning

233 South Wacker Drive
Suite 800
Chicago, Illinois 60606

312 454 0400
www.cmap.illinois.gov

May 17, 2017

Mr. Carl Schoedel P.E.
County Engineer
Kane County Division of Transportation
41W011 Burlington Road
St. Charles, IL 60175

Subject: *Dauberman Road at US 30 - Build Extension South of US 30*
Kane County DOT

Dear Mr. Schoedel:

In response to a request made on your behalf and dated May 16, 2017, we have reviewed and concur with your consultant's year 2040 average daily traffic (ADT) projections for the subject location under its build condition.

Traffic projections are developed using existing ADT data provided in the request letter and the results from the October 2016 CMAP Travel Demand Analysis. The regional travel model uses CMAP 2040 socioeconomic projections and assumes the implementation of the GO TO 2040 Comprehensive Regional Plan for the Northeastern Illinois area.

If you have any questions, please call me at (312) 386-8806.

Sincerely,

A handwritten signature in black ink, appearing to read 'Jose Rodriguez'.

Jose Rodriguez, PTP, AICP
Senior Planner, Research & Analysis

cc: Neubauer (TranSystems)
S:\AdminGroups\ResearchAnalysis\SmallAreaTrafficForecasts_CY16\KaneCountyDOT\ka-22-16\ka-22-16.docx



DEPARTMENT OF THE ARMY
CHICAGO DISTRICT, CORPS OF ENGINEERS
231 SOUTH LA SALLE STREET
CHICAGO, ILLINOIS 60604-1437

REPLY TO
ATTENTION OF:

May 18, 2017

Technical Services Division
Regulatory Branch
LRC-2017-00252

SUBJECT: Jurisdictional Determination on the Dauberman Road Extension Project in the Village of Big Rock, Kane County, Illinois

Carl Schoedel
Kane County Division of Transportation
41W011 Burlington Road
St. Charles, Illinois 60175

Dear Mr. Schoedel:

This is in response to your request that the U.S. Army Corps of Engineers complete a jurisdictional determination for the above-referenced site submitted on your behalf by Huff & Huff, Inc. The subject project has been assigned number LRC-2017-00252. Please reference this number in all future correspondence concerning this project.

Following a review of the information you submitted, this office has determined that there are no waterways, wetlands or other areas considered "waters of the United States" under Corps of Engineers jurisdiction at the site.

Site Wetlands 3, 4, 5, 6, 7, 8 & 9 have been determined to be isolated and therefore not subject to Federal regulation. Please be informed that this office does not concur with the boundaries of waters not under the jurisdiction of this office.

Roadside Ditch RD 1-1 is a water feature Exempt from Federal regulation. Please be informed that this office does not concur with the boundaries of waters not subject to Federal regulation.

For a detailed description of our determination please refer to the enclosed decision document. This determination covers only your project as depicted in the Wetland & "Waters of the U.S." Investigation Report dated January 2017, prepared by Huff & Huff, Inc.

This determination is valid for a period of five (5) years from the date of the letter, unless new information warrants revision of the determination before the expiration date or a District Commander has identified, after public notice and comment, that specific geographic areas with rapidly changing environmental conditions merit re-verification on a more frequent basis.

This letter is considered an approved jurisdictional determination for your subject site. If you object to this determination, you may appeal, according to 33 CFR Part 331. Enclosed you will find a Notification of Appeal Process (NAP) fact sheet and a Request for Appeal (RFA) form. If you request to appeal the above determination, you must submit a completed RFA form to the Great Lakes/Ohio River Division Office at the following address:

Jacob Siegrist
Appeal Review Officer
Great Lakes and Ohio River Division
CELRD-PD-REG
550 Main Street, Room 10032
Cincinnati, Ohio 45202-3222
Phone: (513) 684-2699 Fax: (513) 684-2460

In order to be accepted, your RFA must be complete, meet the criteria for appeal and be received by the Division Office within sixty (60) days of the date of the NAP. If you concur with the determination in this letter, submittal of the RFA form to the Division office is not necessary.

This determination has been conducted to identify the limits of the Corps Clean Water Act jurisdiction for the particular site identified in this request. This determination may not be valid for the wetland conservation provisions of the Food Security Act of 1985, as amended. If you or your tenant are USDA program participants, or anticipate participation in USDA programs, you should request a certified wetland determination from the local office of the Natural Resources Conservation Service prior to starting work.

It is your responsibility to obtain any required state, county, or local approvals for impacts to wetland areas not under the Department of the Army jurisdiction. In Kane County, please note that isolated non-waters of the United States not under the jurisdiction of the U.S. Army Corps of Engineers are regulated by the Kane County Stormwater Ordinance. For projects in incorporated areas of Kane County, contact the certified community for information related to the ordinance. For projects in unincorporated areas of Kane County, contact the Kane County Department of Environmental Management at (630) 208-3179.

Pursuant to Section 404 of the Clean Water Act, the U.S. Army Corps of Engineers regulates the discharge of dredged or fill material into waters of the United States, including wetlands. A Department of the Army permit is required for any proposed work involving the discharge of dredged or fill material within the jurisdiction of this office. To initiate the permit process, please submit a joint permit application form along with detailed plans of the proposed work. Information concerning our program, including the application form and an application checklist, can be found at and downloaded from our website:

<http://www.lrc.usace.army.mil/Missions/Regulatory.aspx>

If you have any questions, please contact Mr. Mike Machalek of my staff by telephone at (312) 846-5534 or email at Mike.J.Machalek@usace.army.mil.

Sincerely,

Keith L. Wozniak
Chief, West Section
Regulatory Branch

Enclosures

Copy Furnished w/out Enclosures

Kane County Division of Environmental Management (Ken Anderson)
Huff & Huff, Inc. (Kevin Walsh)

NOTIFICATION OF ADMINISTRATIVE APPEAL OPTIONS AND PROCESS AND REQUEST FOR APPEAL

Applicant: Carl Schoedel, Kane County Division of
Transportation

File Number: LRC-2017-00252

Date: May 18, 2017

Attached is:

See Section below

	INITIAL PROFFERED PERMIT (Standard Permit or Letter of Permission)	A
	PROFFERED PERMIT (Standard Permit or Letter of Permission)	B
	PERMIT DENIAL	C
X	APPROVED JURISDICTIONAL DETERMINATION	D
	PRELIMINARY JURISDICTIONAL DETERMINATION	E

SECTION I - The following identifies your rights and options regarding an administrative appeal of the above decision. Additional information may be found at http://www.usace.army.mil/CECW/Pages/reg_materials.aspx or Corps regulations at 33 CFR Part 331.

A. INITIAL PROFFERED PERMIT: You may accept or object to the permit.

- **ACCEPT:** If you received a Standard Permit or a Letter of Permission (LOP), you may sign the permit document and return it to the district commander for final authorization. Your signature on the Standard Permit or acceptance of the LOP means that you accept the permit in its entirety, and waive all rights to appeal the permit, including its terms and conditions, and approved jurisdictional determinations associated with the permit.
- **OBJECT:** If you object to the permit (Standard or LOP) because of certain terms and conditions therein, you may request that the permit be modified accordingly. You must complete Section II of this form and return the form to the district commander. Your objections must be received by the district commander within 60 days of the date of this notice, or you will forfeit your right to appeal the permit in the future. Upon receipt of your letter, the district commander will evaluate your objections and may: (a) modify the permit to address all of your concerns, (b) modify the permit to address some of your objections, or (c) not modify the permit having determined that the permit should be issued as previously written. After evaluating your objections, the district commander will send you a proffered permit for your reconsideration, as indicated in Section B below.

B. PROFFERED PERMIT: You may accept or appeal the permit

- **ACCEPT:** If you received a Standard Permit or a Letter of Permission (LOP), you may sign the permit document and return it to the district commander for final authorization. Your signature on the Standard Permit or acceptance of the LOP means that you accept the permit in its entirety, and waive all rights to appeal the permit, including its terms and conditions, and approved jurisdictional determinations associated with the permit.
- **APPEAL:** If you choose to decline the proffered permit (Standard or LOP) because of certain terms and conditions therein, you may appeal the declined permit under the Corps of Engineers Administrative Appeal Process by completing Section II of this form and sending the form to the division commander. This form must be received by the division commander within 60 days of the date of this notice.

C. PERMIT DENIAL: You may appeal the denial of a permit under the Corps of Engineers Administrative Appeal Process by completing Section II of this form and sending the form to the division commander. This form must be received by the division commander within 60 days of the date of this notice.

D. APPROVED JURISDICTIONAL DETERMINATION: You may accept or appeal the approved JD or provide new information.

- **ACCEPT:** You do not need to notify the Corps to accept an approved JD. Failure to notify the Corps within 60 days of the date of this notice, means that you accept the approved JD in its entirety, and waive all rights to appeal the approved JD.
- **APPEAL:** If you disagree with the approved JD, you may appeal the approved JD under the Corps of Engineers Administrative Appeal Process by completing Section II of this form and sending the form to the division commander. This form must be received by the division commander within 60 days of the date of this notice.

E. PRELIMINARY JURISDICTIONAL DETERMINATION: You do not need to respond to the Corps regarding the preliminary JD. The Preliminary JD is not appealable. If you wish, you may request an approved JD (which may be appealed), by contacting the Corps district for further instruction. Also you may provide new information for further consideration by the Corps to reevaluate the JD.

SECTION II - REQUEST FOR APPEAL or OBJECTIONS TO AN INITIAL PROFFERED PERMIT

REASONS FOR APPEAL OR OBJECTIONS: (Describe your reasons for appealing the decision or your objections to an initial proffered permit in clear concise statements. You may attach additional information to this form to clarify where your reasons or objections are addressed in the administrative record.)

ADDITIONAL INFORMATION: The appeal is limited to a review of the administrative record, the Corps memorandum for the record of the appeal conference or meeting, and any supplemental information that the review officer has determined is needed to clarify the administrative record. Neither the appellant nor the Corps may add new information or analyses to the record. However, you may provide additional information to clarify the location of information that is already in the administrative record.

POINT OF CONTACT FOR QUESTIONS OR INFORMATION:

If you have questions regarding this decision and/or the appeal process you may contact:

Regulatory Branch
Chicago District Corps of Engineers
231 South LaSalle Street, Suite 1500
Chicago, IL 60604-1437
Phone: (312) 846-5530
Fax: (312) 353-4110

If you only have questions regarding the appeal process you may also contact:

Jacob Siegrist
Appeal Review Officer
Great Lakes and Ohio River Division
CELRD-PD-REG
550 Main Street, Room 10032
Cincinnati, Ohio 45202-3222
Phone: (513) 684-2699 Fax: (513) 684-2460

RIGHT OF ENTRY: Your signature below grants the right of entry to Corps of Commanders personnel, and any government consultants, to conduct investigations of the project site during the course of the appeal process. You will be provided a 15-day notice of any site investigation, and will have the opportunity to participate in all site investigations.

Signature of appellant or agent.

Date:

Telephone number:

**IDOT Coordination Meeting
US Route 30 and Dauberman Road**

Dauberman Road Extension
Kane County DOT
Section No. 15-00277-01-BR

Illinois Department of Transportation, District 1
Bureau of Local Roads, Conference Room B
10:00 A.M.

October 25, 2017

The purpose of the meeting was to discuss and receive input from IDOT-GSU on the proposed intersection operation of US Route 30 with the Dauberman Road connector road within the Village of Big Rock. The “Analysis of Intersection Options” memo, with supporting documentation, was submitted to IDOT on September 1, 2017 for review and comment in regards to the recommended signalization of the proposed intersection. Additionally, discussion and comments were provided on the proposed geometry of US Route 30 at the intersection, the planned multi-use path and its connection to future bicycle and pedestrian access along US Route 30, and the proposed drainage concept for the intersection area as presented at the meeting.

The following points were discussed:

- 1) Introduction and Recap
 - a) B. Fairwood introduced the project and explained the intended purpose of the meeting to solicit comments from IDOT about the intersection between the Dauberman Road Connector Road and US Route 30.
- 2) Project Overview and Questions
 - a) J. Thede gave a brief overview of the project history and then explained the geometrics along US Route 30.
 - b) IDOT indicated that designing US Route 30 as a 3-lane rural section was correct, as was the design to accommodate a future four lane divided highway. Currently the preferred alternative for US Route 30 design has not been selected by IDOT, however, the proposed Dauberman Road bridge opening for US Route 30 is adequate as discussed for the four lane divided highway option for both rural and urban with the multi-use path.
 - c) The intention to provide a shelf for a future 10-foot multi-use path along the north side of US Route 30 was presented. In response to IDOT inquiries, it was explained that the path was located on the north side to avoid the need for users to cross over US Route 30 to connect to the path on Dauberman Road, but if local comments preferred to place the path on the south side there were no apparent fatal flaws to that.
 - i) IDOT definitely agreed that the right-of-way for the path should be acquired and the shelf should be provided to meet the Complete Streets requirements, but indicated that construction of the path would require a local partner per IDOT policy.
 - d) IDOT inquired if a roundabout had been considered for the intersection. B. Holman noted that it had, but that SIDRA analysis had shown that with a one-lane roundabout, southbound-to-eastbound traffic in the 2040 AM peak would be so heavy that there would not be enough gaps for eastbound traffic on US Route 30 to enter the roundabout, backing up that movement. A two-lane roundabout would provide acceptable LOS in 2040, but the need for two receiving eastbound lanes would have extended

the project limits to or past the Welch Creek bridge on US Route 30, along with the other environmental and financial costs inherent to widening US Route 30.

3) Drainage

- a) K. Kenniff provided an overview of the proposed storm water system. He noted that there were three stormwater systems in the area – US Route 30, Dauberman Road, and regional farm and field tile drainage. The water from the three systems would be kept separate through the project area, with water from the two roads being detained separately prior to being released.
- b) IDOT noted that drain tiles are often present in rural projects and inquired if a field tile survey had been done. K. Kenniff stated that one had, and that the intention was to reconnect field tile across the right-of-way with manholes at either side of the ROW.
- c) IDOT requested a copy of the Drain Tile Survey.

4) Discussion

- a) IDOT asked if lighting had been considered. The project team noted it had not been included at this time. *(Post meeting follow-up: IDOT-GSU provided direction, 'Underpass lighting at Dauberman & US 30 is not required'.)*
- b) IDOT requested that the planned four-foot paved shoulder with four-foot aggregate shoulder along US Route 30 be replaced with an eight-foot paved shoulder.
- c) IDOT inquired about receiving the Existing and Proposed Drainage Plan, and if the drainage design met local stormwater ordinances. K. Kenniff indicated the EDP had been submitted, the PDP was nearing completion and would be submitted when it was done, and that the local stormwater ordinances were met.

The meeting concluded at 10:30 A.M.

Minutes prepared by TranSystems (B. Holman)

Attachments:

- 1. *Attendance Sheet*
- 2. *Exhibits*



Illinois Department of Transportation

Attendance Roster

Bureau: Local Roads and Streets

Section: Kane County, 15-00277-01-BR

Project/Topic: Dauberman Road Extension at US Route 30, Intersection Operations Memo review

Date: Wednesday, October 25, 2017

Time: 10:00 AM

Location: Local Roads Conference Room B

	Attendees	Representing	Phone Number	Email Address
1.	Marilyn Solomon MS	IDOT - D1 - BLRS	(847) 705-4407	Marilyn.Solomon@illinois.gov
2.	Hannah Knight HK	IDOT - D1 - BLRS (HR Green, Inc.)	(847) 705-4205	Hannah.Knight@illinois.gov
3.	Jacob Thiede JT	Bollinger Leach & Associates	630-438-6400	jthiede@bollingerleach.com
4.	Brian Fairwood	TransSystems	847 407 5200	bfairwood@transystems.com
5.	Brian Holman	TSC	747 407 5232	bjholman@transystems.com
6.	KEN KENNIFF	BLA	630-438-6400	KKENNIFF@BOLLINGERLEACH.COM
7.	JOHN FORTMANN	TRANSYSTEMS	847-407-5225	jafortmann@transystems.com
8.	LORI BRON	IDOT-PROGRAMMING	847-705-4427	Lori.S.Bron@illinois.gov
9.	Theresa Pelletier	IDOT Consultant Mgr	847.705.4479	theresa.pelletier@illinois.gov
10.	JASON SALLEY	IDOT-PROGRAMMING	847/705-4085	JASON.SALLEY@ILLINOIS.GOV
11.	Evelina Perry	IDOT Consultant	847 705-	eperry@illinois.gov
12.	CANDI THOMAS	KDOT	630-584-1170	thomascandance@co.Kane.il.us
13.				
14.				
15.				



**Illinois Department
of Transportation**

Informal Transmittal

EP/BS
Due → 10/19/17

To:	Pete Harmet <i>ISSAM RAYHAN</i>
Bureau:	Programming - Geometrics Section <i>UNIT</i>
Attn:	Jason Salley
Date:	9/7/17 ✓ <i>(2ND SUBMITTAL)</i>

From:	Chris Holt
Bureau:	Local Roads and Streets
	By: Marilyn Solomon/ Hannah Knight
Subject:	Kane County*
	Section No.: 15-00277-01-BR

Please check appropriate box below:

- | | | |
|---|--|--|
| <input type="checkbox"/> Take Necessary Action | <input type="checkbox"/> For Your Information | <input checked="" type="checkbox"/> Reply |
| <input checked="" type="checkbox"/> For Your Comments | <input type="checkbox"/> See Me About the Attached | <input checked="" type="checkbox"/> Return |
| <input type="checkbox"/> Per Your Request | <input type="checkbox"/> Draft (Letter)(Memo) For my signature | <input type="checkbox"/> Route |
| <input checked="" type="checkbox"/> For Your Approval | | <input type="checkbox"/> File |

Message

*Dauberman Road Extension at US Route 30

Attached is the Dauberman Road and US Route 30 Jughandle Analysis of Intersection Options. Please review and provide your comments.

Should you have any questions please contact Marilyn Solomon at 4407 or Hannah Knight at 4205. Thanks.

Marilyn Solomon / HK

Signature

Copies to

file

Response

10/25/17

MARILYN- PLEASE ASK THE COUNTY TO SUBMIT A TRAFFIC SIGNAL WARRANT STUDY FOR REVIEW & APPROVAL AT US 30 & JUGHANDLE BY THE BUREAU OF TRAFFIC. BASED ON TODAY'S MEETING, THE PROPOSED SHOULDER ON US 30 SHOULD BE ENTIRELY HMA.

IN FOLLOW UP TO OUR MEETING - ~~THE~~ UNDERPASS LIGHTING AT DAUBERMAN & US 30 IS NOT REQUIRED.

THANKS, JASON SALLEY 4085
 PROVIDE AN IDS ALONG WITH PLAN & PROFILES FOR US 30 & JUGHANDLE,
 CC: J L & J K (BOT)

Signature

IDOT Coordination Meeting

Dauberman Road Extension
Kane County DOT
Section No. 15-00277-01-BR

Illinois Department of Transportation, District 1
Bureau of Local Roads and Streets Conference Room
2:00 P.M.

February 2, 2018

Attendee	Representing	Phone Number	Email Address
Marilyn Solomon	IDOT – D1 – BLRS	847-705-4407	Marilyn.Solomon@illinois.gov
Moe Kawash	IDOT – D1 – BLRS	847-705-4205	Mohammad.Kawash@illinois.gov
Alex Househ	IDOT – D1 – BLRS	847-705-4410	Alex.Househ@illinois.gov
Grace Dysico	TranSystems	847-407-5247	gldysico@transystems.com
Matt Smith	TranSystems	847-407-5300	mjsmith@transystems.com

The purpose of the meeting was to review the draft Project Development Report comments provided by BLRS District 1 and determine what clearances are remaining to hold the Public Hearing.

The following points were discussed:

1. Noise Report – Previously, noise reports were reviewed and approved internally at District 1 by Sam Mead. However, Sam Mead (D1 Programming/ESU) retired. Therefore, Bill Raffensperger (CBLRS) will now coordinate the review and approval of the report with BDE.
2. Bridge over the BNSF RR – needs ICC approval. Get a record of telephone conversation (ROC) or an email from Brian Vercruysse (ICC) to confirm the proposed bridge is acceptable. Brian Fairwood will provide the ROC.
3. Follow-up with Jason Salley – need his concurrence with the proposed work along U.S. Route 30 (IDS and geometry).
4. Follow-up with Jonathan Lloyd (Traffic) – need his concurrence on the proposed signal at US Route 30/Connector Road intersection.
5. Section 4(f) – need to get Carl Schoedel to sign the Temporary Occupancy form, then get the FHWA to sign. No need for IDOT approval. Need to coordinate with Bill Raffensperger on this. The revised draft Section 4(f) Temporary Occupancy form was resubmitted earlier this morning. It was revised to address the FHWA comment and include the draft IGA between the County and Forest Preserve.
6. Bureau of Agricultural – need AD-1006 form in the final PDR. Conclusion of this item is not required before the public hearing.
7. We shouldn't make edits to the BLR 22210 form; no changes to signature page. Grace explained we changed it based on a request from Bill, at the December 2017 FHWA/IDOT meeting, to have the Forest Preserve District of Kane County sign the report as a project co-sponsor.
8. Crash data must be updated to include 2016 and 2017.
9. Local PESA –include an executive summary from Huff & Huff in the PDR. Include the report in an appendix.
10. Connector Road typical sections are confusing. Remove the pavement marking arrows completely.
11. Add stationing for Dauberman Road existing typical section.

The four most critical items that need to be approved prior to the public hearing are:

1. Coordinate the Section 4(f) with Bill Raffensperger and FHWA
2. Coordinate the Noise Report Bill Raffensperger for approval
3. Coordinate the proposed geometry and IDS on us Route 30 with Jason Sally
4. Coordinate the proposed signal warrants with Jonathan Lloyd for approval

Follow up items:

1. It was agreed that Grace would attend a meeting with Bill Raffensperger and Alex at 10:00 AM on Monday, February 5 to review the status of the PDR and various approvals required (Noise and 4(f)).
2. Need John Fortmann to request from Jason an email saying he approves the IDS and geometrics for U.S. Route 30.
3. Need John Fortmann to request from Jonathan an email saying he approves the recommendation for a signal at the U.S. Route 30/Connector Road intersection

The meeting concluded at 3:50 P.M.

Minutes prepared by TranSystems (G. Dysico)

IDOT Coordination Meeting

Dauberman Road Extension
Kane County DOT
Section No. 15-00277-01-BR

Illinois Department of Transportation, District 1
Bureau of Local Roads and Streets Conference Room
10:00 A.M.

February 5, 2018

Attendee	Representing	Phone Number	Email Address
Bill Raffensperger	IDOT – CBLRS	217-785-1676	william.raffensperger@illinois.gov
Alex Househ	IDOT – D1 – BLRS	847-705-4410	alex.Househ@illinois.gov
Grace Dysico	TranSystems	847-407-5247	gldysico@transystems.com
John Fortmann	TranSystems	847-407-5225	jafortmann@transystems.com

The purpose of the meeting was to review with CBLRS the remaining concurrence items needed for the public hearing.

The following points were discussed:

1. Grace Dysico provided a brief summary of the project status, the project resubmittals that have been made, and environmental clearances and project approvals received to date.
2. Bill Raffensperger indicated that clearances required by CBLRS have been received but there are outstanding clearances that are still needed from BDE, FHWA or IDOT D1; these are items that are related to US Route 30 whose approvals are not under BLRS jurisdiction.
3. The outstanding items are:
 - a. **US Route 30 IDS and Geometrics** concurrence from Jason Salley. Alex Househ said it would be alright for John Fortmann to reach out to Jason directly and get an email with his concurrence to go to the public hearing.
 - b. **Signal Warrant Study** concurrence from Bureau of Traffic. The resubmittal package was delivered on 1/23/18 to BLRS. Alex said it would be alright for John to reach out directly to Jonathan Lloyd to facilitate their review.
 - c. **Variance Approval (BLR 22120)** needs to be signed by the District Engineer. However, Alex offered to take the form directly to the Executive Office and request a signature. He indicated that Steve Travia has typically signed the form for Tony Quigley. A hard copy of the form was provided to Alex.
 - d. **Noise Report** must be coordinate with BDE directly now that D1 cannot approve the report locally. Bill indicated that he would coordinate this with Scott Stitt (BDE) when he is back in the office on Wednesday. A hard copy of the Noise Report was provided to Bill.
 - e. **Section 4(f) Temporary Occupancy** document needs approval from the FHWA. The 4(f) had been reviewed by Robin Helmerichs (FHWA) and her comment was addressed. Bill indicated that he will coordinate this with FHWA when he is back in the office on Wednesday. A hard copy of the Section 4(f) was provided to Bill.

4. In the event that the required concurrences are not received from IDOT by EOB Wednesday, 2/7/18, Bill suggested that the Kane DOT submit a *Request Letter for a Policy Exception for holding the Public Hearing*.
 - a. The letter should be on letterhead signed by the County, addressed to Marilyn Solomon and copied to Bill.
 - b. The letter should be emailed on Thursday morning, 2/8/18 to Bill with a copy to Marilyn.
 - c. The letter should include a list of the items that have been submitted awaiting concurrence and the dates the documents were submitted to IDOT BLRS.
 - d. Approval of the letter would allow the County to continue with the public hearing.
 - e. Bill will coordinate approval of the Request Letter with Tom Winkelman (CBLRS) and Scot Stitt (BDE).

Follow up items:

1. John Fortmann will reach out to Jason Salley and Jonathan Lloyd on the status of their reviews and concurrence to allow the project to proceed with the public hearing.
2. Alex Househ will coordinate the District signature for the BLR 22120 Variance Approval form.
3. Bill Raffensperger will coordinate the Noise Report and Section 4(f) with Central Office and FHWA, respectively.
4. Grace will draft the request letter for KDOT's review and use.

The meeting concluded at 11:00 A.M.

Minutes prepared by TranSystems (G. Dysico)

From: Raffensperger, William
To: [Solomon, Marilyn D](#)
Cc: [CH-Brian Fairwood](#); [CH-Matthew Smith](#); ThomasCandance@co.kane.il.us; zakosekmike@co.kane.il.us; [Kawash, Mohammad](#); [CH-Grace Dysico](#)
Subject: RE: Kane County; Dauberman Road Ext., Sec 15-00277-01-BR
Date: Thursday, February 8, 2018 3:00:13 PM
Attachments: [image001.png](#)
[Xerox Scan_02082018153839.pdf](#)
[Pages from Chapter 21 - Working Copy 2017-04-21 v4.pdf](#)

Ms. Solomon –

Please see the attached design exception approval. When the PDR is submitted, please ensure that a copy of the signed form and approval memo are included in the report.

In consultation with BDE, Kane County may proceed to the public hearing without the approval of the following items:

1. Signal warrant study,
2. Noise analysis, and
3. Section 4(f) temporary occupancy approval.

BDE Chapter 19 has been revised and does not include a requirement for these items to be approved prior to a public hearing. Depending on the complexity or controversy of a project, public hearings are held without completing many studies and reports.

The intent of the public hearing is to gather comments from the public while there is still the ability to revise the project. Having an approved study, such as an IDS, gives the impression that all of the decisions have been finalized and no changes will be made.

The policy exception for holding a public hearing is not required.

As a reminder, a transcript of the public hearing must be sent to the FHWA, via IDOT. Attached is a sample letter that will be in the updated BLRS Chapter 21 when it is finalized and issued. The submittal should consist of a hard copy of the letter and a CD of the attachments.

If you have any questions, please call.

William Raffensperger, PE, PTOE, PTP
Project Development Engineer

Illinois Department of Transportation
Bureau of Local Roads and Streets
2300 S. Dirksen Parkway
Springfield, IL 62764

Work hours: 7:00 am to 3:00 pm.

(O) 217-785-1676

(C) 217-720-2787

(F) 217-782-3971

william.raffensperger@illinois.gov

From: Thomas, Candance
To: [CH-Brian Fairwood](#); [CH-Grace Dysico](#)
Cc: [CH-Matthew Smith](#); [CH-Brian Holman](#)
Subject: FW: Prairie Parkway Public Hearing
Date: Thursday, February 15, 2018 11:58:18 AM
Attachments: [image001.jpg](#)

FYI...just in from IDOT...a response to the proposed Prairie Parkway Corridor.

Candi Thomas, P.E.

Senior Project Manager
Kane County Division of Transportation
Direct Phone: (630) 406-7355
Main Phone: (630) 584-1170
Fax: (630) 584-5265
Email: thomascandance@co.kane.il.us



From: Sullivan, Robert M. [mailto:Robert.Sullivan@illinois.gov]
Sent: Thursday, February 15, 2018 11:53 AM
To: Coffinbargar, Steve <coffinbargarsteve@co.kane.il.us>
Cc: Carlson, Brian M <Brian.Carlson@illinois.gov>; Zakosek, Mike <zakosekmike@co.kane.il.us>; Thomas, Candance <ThomasCandance@co.kane.il.us>; Rickert, Tom <rickerttom@co.kane.il.us>
Subject: RE: Prairie Parkway Public Hearing

Steve,

Here is IDOT's statement on the Prairie Parkway corridor.

With the Federal Highway Administration's Record of Decision on Prairie Parkway officially rescinded in 2012 and the proposal not being included in the Illinois Department of Transportation's multiyear plans for several years, the state is moving forward with allocating funds remaining from the project toward improving Illinois Route 47. The amount of funding and scope of work will be provided in greater detail when IDOT's FY 2019-2024 Proposed Highway Improvement Program is released later this spring.

Please let me know if you have any additional questions.

Mike Sullivan
Area Programmer
Illinois Department of Transportation
District 1/Bureau of Programming/Program Development
847-705-4078

From: Coffinbargar, Steve [<mailto:coffinbargarsteve@co.kane.il.us>]
Sent: Friday, February 09, 2018 11:18 AM
To: Sullivan, Robert M. <Robert.Sullivan@illinois.gov>
Cc: Carlson, Brian M <Brian.Carlson@illinois.gov>; Zakosek, Mike <zakosekmike@co.kane.il.us>;
Thomas, Candance <ThomasCandance@co.kane.il.us>; Rickert, Tom <rickerttom@co.kane.il.us>
Subject: [External] FW: Prairie Parkway Public Hearing

Mike,

KDOT will be hosting a public hearing for our proposed Dauberman Road Extension improvement on Thursday, Feb. 15th from 5:00 to 7:30 in Big Rock (that's in Illinois, BTW, see attached brochure). As you are likely aware, this project is located in the shadow of what use to be the Prairie Parkway corridor (PP). Based upon our experience at the last public meeting for this improvement, there will be some attendees asking about the current status of the PP at our hearing. If an IDOT rep is not going to attend to answer these questions (which is fine), I would ask that IDOT provide us with a short statement that we could share as to the current status of the PP. I'd prefer to do that than instead say something like "You're going to have to ask IDOT that question."

Can you provide us a statement that we can share with attendees? Thanks!!

Steve

CH-Brian Holman

From: CH-Matthew Smith
Sent: Thursday, May 10, 2018 4:10 PM
To: CH-Brian Holman; CH-Grace Dysico; CH-Brian Fairwood; CH-Matthew Santeford
Cc: CH-Dave Block
Subject: FW: Dauberman Road Grade Separation (Kane County, IL)
Attachments: 0003-0049.21 -- Dauberman Road OH BRIDGE DESIGN TS&L Plan Review - Big R....docx

FYI

From: Nutt, Calvin G <Calvin.Nutt@BNSF.com>
Sent: Thursday, May 10, 2018 3:57 PM
To: CH-Matthew Smith <mjsmith@transystems.com>
Cc: CH-David Irving <djirving@transystems.com>; Stilley, John R <John.Stilley@BNSF.com>; Nutt, Calvin G <Calvin.Nutt@BNSF.com>
Subject: RE: Dauberman Road Grade Separation (Kane County, IL)

Matt,

Comment sheet is attached. Please record responses in the Designer's Response column and return with future plan sets. I would also recommend that this grade separation be located in an area that could eliminate an existing grade crossing in order to have the highest impact on public safety. In the absence of an associated grade crossing closure, BNSF will not contribute to this project. If the County is willing to consider other locations, BNSF would be interested in discussing.

Thanks

Calvin Nutt, PE
BNSF Railway
Manager Public Projects
IA, IL, WI
763-782-3495
Calvin.Nutt@BNSF.com

From: mjsmith@transystems.com [<mailto:mjsmith@transystems.com>]
Sent: Friday, March 9, 2018 4:05 PM
To: Nutt, Calvin G <Calvin.Nutt@BNSF.com>
Cc: djirving@transystems.com
Subject: RE: Dauberman Road Grade Separation (Kane County, IL)

EXTERNAL EMAIL

I hit send too soon!

Here's my contact information.

Thanks,
Matt

Matthew J. Smith PE

Senior Professional

Assistant Vice President



TranSystems

1475 East Woodfield Road, Suite 600

Schaumburg, IL 60173-5440

Main: 847-605-9600

Direct: 847-407-5300

Fax: 847-463-0565

Cell: 630-772-0142

www.transystems.com

From: CH-Matthew Smith

Sent: Friday, March 09, 2018 4:04 PM

To: CH-David Irving <djirving@transystems.com>; Nutt, Calvin G <Calvin.Nutt@BNSF.com>

Subject: RE: Dauberman Road Grade Separation (Kane County, IL)

Hi Calvin, I'm following up from our phone messages from last month regarding the Dauberman Road project in Kane County.

Did you have any comments on what we provided? Are there any issues we should be aware of now that we should address while we are still in the planning and design phase? It would be helpful to us if we could share your input on the design with Kane County so that we know we are all on the same page.

Thanks,
Matt Smith

From: CH-David Irving

Sent: Tuesday, February 06, 2018 4:32 PM

To: Nutt, Calvin G <Calvin.Nutt@BNSF.com>

Cc: CH-Matthew Smith <mjsmith@transystems.com>

Subject: Dauberman Road Grade Separation (Kane County, IL)

Calvin,

Thanks for calling back and sorry I missed you. I passed your voice message along to Matt Smith who is leading the design effort for Dauberman Road. One reason for the call was to introduce you to Matt who is one of our senior highway guys in Schaumburg.

One question Matt had for you was they were wondering if you had a timeframe he could expect some comments back from BNSF? We assume at this stage you can't provide any approvals of the concept, but the County was wondering if you would be able to provide an acknowledgement the plan was acceptable pending final design or however you qualify statements at this point in the design. In addition, please identify any issues you would want them to address so they can review them with the County and work to address or answer them as they move forward with the design.

With that information, the County would be able to authorize Matt to progress the design beyond the conceptual stage and they would layout a schedule for further coordination with BNSF. If you have any questions or thoughts on this let us know. We are available this evening or tomorrow if you have any questions for us.

Again, thanks for your return call and sorry I missed you.

David Irving PE
Vice President



TranSystems
1475 East Woodfield Road, Suite 600
Schaumburg, IL 60173-5440
Main: 847-605-9600
Direct: 847-407-5274
Fax: 847-463-0565
Cell: 847-331-4959
www.transystems.com

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No Comment / All Items CLOSED* ☐

Submittal Essentials in RED **

MPP Review Items; [KC Structures Review Items](#)

BNSF Engineering Services-Structures Review Comments

Grade Separation (OH Design) Review Comments Sheet

Check for compliance with current AREMA and [Union Pacific - BNSF Guidelines for Railroad Grade Separation Projects](#)

Project Name:	Dauberman Road Overpass	BNSF Point of Contact:	Calvin Nutt
Submittal Name:	Dauberman Road TS&L	Project CM Contact:	Matt Smith, 847-407-5300
Plan File Name:	“02-Dauberman – BNSF Bridge TSL – 2018-01-22.pdf”	Asset MP Name:	New asset MP 49.21
Plan File Date:	12/01/2017	Plan %:	TS&L

Horizontal Clearance

☒ No Exceptions Taken ☐ Comments Below ☐ N/A

Track Geometry (Curves, run off, grades, track spacing, etc.)

☐ No Exceptions Taken ☒ Comments Below ☐ N/A

Temporary Horizontal Clearance (Includes Falsework)

☐ No Exceptions Taken ☒ Comments Below ☐ N/A

Clearance for Access Road (Input from Division on Need)

☒ No Exceptions Taken ☐ Comments Below ☐ N/A

Drainage – Diverted away from BNSF ROW & Drain Locations

☐ No Exceptions Taken ☒ Comments Below ☐ N/A

Ditches Shown and Drainage Path Indicated

☐ No Exceptions Taken ☒ Comments Below ☐ N/A

Piers on BNSF ROW (Check Pier Protection Requirements)

☒ > 25’ to CL ☐ ≤ 25’ to CL ☐ ≤ 12’ to CL

Clearance for Future Track(s) (Input from Cap Planning; Left / Right?)

☒ No Exceptions Taken ☐ Comments Below ☐ N/A

Vertical Clearance

☒ No Exceptions Taken ☐ Comments Below ☐ N/A

Temporary Vertical Clearance (Includes Falsework)

☐ No Exceptions Taken ☒ Comments Below ☐ N/A

RR Track Profile (1000’ Either direction on all Existing and Future Tracks)

☐ No Exceptions Taken ☒ Comments Below ☐ N/A

2:1 Slopes w/ Type / limits of Paving

☐ No Exceptions Taken ☐ Comments Below ☒ N/A

Impacts to signal, telecom, etc.

☐ No Exceptions Taken ☒ Comments Below ☐ N/A

Splice Locations Shown w/ respect to tracks

☐ No Exceptions Taken ☐ Comments Below ☒ N/A

BNSF MP & Direction of Increasing MP on Plan View

☐ No Exceptions Taken ☒ Comments Below ☐ N/A

Fence / Barrier Rail (Shoulder width, height, limits w/ respect to tracks)

☒ No Exceptions Taken ☐ Comments Below ☐ N/A

TOR Elevations at All Tracks

☒ No Exceptions Taken ☐ Comments Below ☐ N/A

ROW Limits Shown (all piers should be off of BNSF ROW if possible)

☒ No Exceptions Taken ☐ Comments Below ☐ N/A

Depth of Foundations from TOT Dimensioned

☒ No Exceptions Taken ☐ Comments Below ☐ N/A

Width of Superstructure & Depth Dimensioned on Section View

☒ No Exceptions Taken ☐ Comments Below ☐ N/A

MSE Walls

☐ None Proposed ☒ Off BNSF ROW ☐ Crash-Protected

Item No.	Sheet No.	Reviewer Comment Date: May 9, 2018	Initial	Designer's Response Date:	Reviewer's Comment Date:	Initial	Designer's Response Date:	Reviewer's Comment Date:	Initial	Designer's Response Date:	Reviewer's Comment Date:	Initial	Status (Open or Closed)
1.	1	Include direction of increasing mile post	CGN										
2.	1	Include top of rail elevations 1,000’ in each direction	CGN										
3.	1	Include temporary clearance requirements	CGN										
4.	1	Confirm all surface drainage to be contained in roadway storm sewer system, with no drainage directed towards BNSF right of way. No freefall deck drains.	CGN / JMC										
5.	1	Confirm one-call and BNSF locates were completed prior to survey	CGN										
6.	Gen.	Preliminary phasing plan should be provided	JMC										
7.													

*BNSF has reviewed these submittals and no exceptions are taken with regard to BNSF’s ability to use or accommodate the project as intended. BNSF has not reviewed the design details or calculations for structural integrity or engineering accuracy. BNSF accepts no responsibility for errors or omissions in the design or execution of the project.

** Check Submittal Essentials prior to transmittal to Structures Review. If not included, Project Engineer to reject and request resubmittal.



Dauberman Road Extension Kane County DOT

US 30 @ Dauberman Road & Local Drainage Concerns

June 7, 2018 - 11:00 a.m.

IDOT/District 1 – Programming Conference Room

Minutes to Meeting

<u>Attendee:</u>	<u>Representing:</u>	<u>Phone:</u>	<u>Email Address:</u>
E. Perry Masouridis	IDOT/Hyd.	847-705-4474	elftherios.masouridis@illinois.gov
Medhi Geraminegad	IDOT/Hyd.	847-705-4574	medhi.geraminegad@illinois.gov
Fernando Ponce	IDOT/Hyd.	847-705-7389	fernando.ponce@illinois.gov
Candi Thomas	KDOT	630-584-1170	thomascandance@co.kane.il.us
Mike Zakosek	KDOT	630-584-1170	zakosekmike@co.kane.il.us
Jodie Wollnik	Kane Co. Water Res.	630-232-3499	wollnikjodie@co.kane.il.us
Phyllis Bleck	Big Rock Drainage Dist.	630-373-9422	p.bleck2@att.net
David Hall	Big Rock Drainage Dist.	630-450-5439	dkhall@hytechlabs.com
Wade Thompson	Big Rock Drainage Dist.	630-201-7987	brhighwaydept@att.net
Ted McCannon	Village of Big Rock	630-556-3100	e-d-s@mchsi.com
Ken Rojek	Big Rock Township	630-556-4340	bigrocktownship@gmail.com
Jerald Thompson	Big Rock Landowner	630-556-3400	thompsonjerald@att.net
Dan Bruckelmeyer	BLA, Inc.	630-438-6400	dbruckelmeyer@bla-inc.com
Kevin Kenniff	BLA, Inc.	630-438-6400	kkenniff@bla-inc.com
Tom Huddleston	Huddleston-McBride	815-757-6007	huddmac@aol.com

This meeting is being held as a follow-up to a February 21, 2018 email to IDOT-District 1 from the Big Rock Drainage District regarding the Dauberman Road Extension project being led by the Kane County Division of Transportation (KDOT). The email stated that the Drainage District, Village of Big Rock, and Big Rock Township collectively have the same concerns with respect to the increased water load generated by the project into the existing drainage system. The email also discusses the connection of an IDOT storm sewer into a Drainage District main field tile and the desired re-profiling of the existing north ditch along US 30 in order to alleviate the existing drainage issues occurring within the Village and Township.

- 1) Following introductions of the attendees, IDOT began the meeting by discussing the status of the Dauberman Road Extension project to date. The project is currently in Phase I Engineering which is generally considered ~30% development of project plans including environmental impact studies, identification of land necessary to construct the project, and overall drainage methodology for the project. Phase II is the development of contract plans used for construction, and Phase III is the actual project construction. KDOT confirmed that the Dauberman Road Extension project is currently funded and therefore construction is estimated to begin in the next 2-3 years, pending land acquisition.
- 2) IDOT has reviewed the Location Drainage Study (LDS) prepared by BLA, Inc. (BLA) on behalf of KDOT as well as reviewed the Drainage District's concerns. IDOT's position is that the drainage ditch from US 30 is a small part of a 2.0+ square mile watershed. Currently US 30 in Big Rock outlets to a depressed area which is connected to a drain tile. Altering existing drainage patterns to flow through

a ditch parallel to US 30 towards Welch Creek would result in a ditch which is approximately 18' deep at its lowest point. The tiebacks necessary for the ditch embankment would result in a Right-of-Way acquisition nearly 100' wide which is not feasible. A storm sewer pipe is a more realistic option. However, IDOT stated this is a regional problem which requires study beyond that needed for a roadway project.

- 3) IDOT also expressed concern that by directly connecting a ditch or storm pipe to the north side of the bridge carrying US 30 over Welch Creek that it may have impacts to surrounding properties. IDOT is currently in a Phase I study for US 30 which encompasses this bridge over Welch Creek, and while not very far along in this study, discharging additional stormwater in the vicinity of the bridge could result in an increased span length as well as a potentially increased profile which would be a significant cost addition to the State of Illinois.
- 4) Big Rock Drainage District (BRDD) stated that the word "ditch" as stated in the email to IDOT dated February 21st was not intended to mean a literal open roadway ditch immediately adjacent to US 30, rather some form of drainage relief (offsite overland flow channel/drainage pipe, etc.) which would reduce or eliminate roadway discharge into the drain tile system which is already overtaxed and is experiencing significant issues.
- 5) Recognizing that the BRDD, Village of Big Rock, and Big Rock Township are experiencing significant drainage issues, KDOT has agreed to prepare a drainage study as part of the Phase II engineering which will investigate several options for means of improving drainage in the area. Currently the only overland flow path which exists includes crossing both US 30 and the BNSF Railroad which are two significant obstacles for a Drainage District.
- 6) Kane County Environmental & Water Resources (KEWR) noted that the design of an agricultural drainage system differs from a storm sewer system in that agricultural systems allow for a 5-day drawdown time for stormwater; anything beyond 5 days and crop damage starts to occur. Due to this drawdown time, a 36" equivalent pipe is the appropriately-sized relief sewer which would be necessary to alleviate many of the concerns which are currently taxing the existing agricultural tile. KEWR noted that much of the property along US 30 is commercially zoned; the only discharge location to Welch Creek for that area are the two agricultural tiles which run south and east to Welch Creek. Therefore, they envision a relief sewer to accommodate this drainage and reduce the load on the agricultural system.
- 7) KEWR noted that the BNSF performed a drainage study in 2016 which indicated a 0.2' head reduction and calculations depict that any additional flow generated from a relief sewer would not increase the head on the existing bridge, beyond what existed prior to the BNSF bridge replacement.
- 8) BLA stated that the biggest concern about running a storm pipe is finding ways to cross underneath the existing oil/gas pipelines which cross US 30. There are four (4) known pipeline crossings in the area. IDOT provided potholing data in order to determine how deep these pipes are relative to the surface. There would be approximately 0.2' of clearance between the bottom of the pipeline and the top of a 36" relief sewer pipe at the tightest location. KEWR stated that conflict structures may be possible or even splitting the flow between two smaller pipes to increase cover. Dan noted that many times these major pipelines are in privately held easement and carry with them significant restrictions on what can occur without incurring the cost of relocation, which can be extremely expensive as well as a very time consuming legal process to get permission to cross the easement.

- 9) BLA also expressed concern regarding the tailwater from Welch Creek. If the intent is to route a storm sewer pipe, a hydraulic study of Welch Creek will need to be conducted in order to accurately determine the various flood stage elevations and how effective a relief sewer would be given the tailwater concerns. Backflow prevention is a possibility. BLA has this study included in its scope of work with KDOT as part of the Phase II engineering. IDOT also expressed concern with Welch Creek backing up through the pipe.

[Post meeting note: IDOT does not favor back-flow prevention devices.]

- 10) IDOT discussed several items as to jurisdiction and cost. If a relief sewer is constructed, who will own it? Will it be Drainage District sewer, County sewer, or other? KEWR stated that the Village of Big Rock has utilized CDBG funding in the past to finance improvements in the past. If improvements were made as a result of the BLA study, would there be any cost sharing opportunities, proration of cost-share, or other funding mechanisms available. IDOT noted that if the Phase I Study being performed as part of the widening up to Welch Creek indicates a larger pipe is needed, there may be a cost-sharing opportunity, as a storm sewer would be considered east of Dauberman in the area currently tributary to Welch Creek.

- 11) BRDD stated that in regards to the Dauberman Road drainage system, overland flow makes its way through a series of ditches and culverts underneath Dauberman Road which flows to three culverts located approximately 500 feet east of the intersection, which carries the flows south underneath US Route 30. From there, drainage flows southeast through a ditch on the Thompson farm property to a box culvert located underneath the BNSF railroad tracks. Drainage flows then split and either flows south through the box culvert or east through an existing ditch on the north side of the tracks, which flows to Welch Creek. It appears that the majority of drainage was originally intended to flow south through the existing box culvert and then turn east and flow overland to Welch Creek. However, the railroad does not clean the existing box culvert so most of the drainage flows through the existing ditch on the north side of the tracks. It is the desire of the Drainage District to fill in or eliminate the existing ditch between US 30 and the BNSF tracks and divert this system to the east which would also, desirably, include the Dauberman Road system.

[Post meeting note: KEWR checked the property ownership after the meeting. The creek between US 30 and the BNSF along with the floodplain to the east is owned entirely by IDOT under two separate PINs. There is one property owner to the west with floodplain on their property; the floodplain on that property is encompassed within their pond area.]

- 12) IDOT stated that "diversion" is a word which should be used with caution as water generally aims to travel in the same location as it always has in the past. BLA noted that the elimination of the entire ditch would result in an extremely large pipe to carry the 100-year storm event, one which would likely not be feasible. A more realistic scenario would be to construct a system that carried a lower-flow (2-Year, 5-Year storm event, etc.) and larger storms would make its way to Welch Creek the way it does today. IDOT noted that sediment transfer is also something that IDOT will be concerned with. If overland routes are not properly maintained, they fill in with sediment over time and ultimately water reverts to the way it originally travelled.
- 13) BRDD indicated stated that the primary tile constructed in the 1890s which is made of clay pipe has far fewer breaks than the newer, concrete tile built in 1953. Huddleston-McBride (HM) noted that due to soil acidity, clay pipe is a much better choice of tile than concrete but that many tiles installed

during that time are concrete as was the trend. A recent improvement was made which upsized the outfall of the existing tile to 36".

- 14) BRDD asked about the timeline of the study moving forward. KDOT said that the Phase I is nearing approval and that once Phase I was approved, Phase II engineering could begin in a couple of months (estimated). At this time, a kickoff meeting would be held which would mark the start of the Drainage Study. From this point, BLA would need to perform calculations and determine options which could be then advanced, refined, or eliminated based on feasibility and cost. It is estimated that the Drainage Study would take approximately 4-6 months to complete from start to finish. BRDD asked if this supplemental Drainage Study would include the information included in the Dauberman Road drainage study; Dan noted that as BLA is the same consultant that designed the drainage system for Dauberman Road that there would be no issues integrating the two investigations as well as including other studies which have been done in the area.
- 15) BRDD inquired about potential funding sources to start looking into now. IDOT noted it has had to make significant cuts in its overall maintenance program. If a Capital Bill were proposed, it could generate new funding sources but none exists at this time. BRDD said that they would need to know the cost of these projects before asking for funding. BLA identified that conceptual cost estimates would be developed as part of the study.
- 16) The meeting ended at approximately 12:10 P.M.

The above constitutes my understanding of the discussion and decisions reached. Should there be any additions, deletions, or clarifications, please contact the undersigned immediately.

Sincerely,
BLA, Inc.

A handwritten signature in black ink, appearing to read 'D. Bruckelmeyer', with a long horizontal flourish extending to the right.

Dan Bruckelmeyer, P.E.
Vice President/Sr. Project Manager



Illinois Department
of Transportation

ATTENDANCE ROSTER
BUREAU OF PROGRAMMING

PROJECT/TOPIC: US 30 @ Duane & Local Drainage Concerns

DATE: June 7, 2018 TIME: 11:00am

LOCATION: IDOT Dist. 1 ROOM: Prog Conf. Room

Attendees	Representing	Phone Number	Email Address
1. <u>PERCY MASOURIDIS</u>	<u>IDOT/PROG/HND</u>	<u>847-705-4474</u>	<u>eleftherios.masouridis@illinois.gov</u>
2. <u>TOM HUDDLESTON</u>	<u>HUDDLESTON POC/IDE</u>	<u>815-757-6007</u>	<u>HUDDMAC@AOL.COM</u>
3. <u>mehdi garimnegar</u>	<u>IDOT / HND</u>	<u>847 705 4574</u>	<u>mehdi.garimnegar@illinois.gov</u>
4. <u>Fernando Ponce</u>	<u>IDOT / HND</u>	<u>847 - 705 - 4389</u>	<u>Fernando.Ponce@illinois.gov</u>
5. <u>Ann Beckelmeyer</u>	<u>BLA, INC.</u>	<u>630-438-6400</u>	<u>dbeckelmeyer@bla-inc.com</u>
6. <u>KAREN KANNETT</u>	<u>BLA, INC.</u>	<u>630-438-6400</u>	<u>KKANNETT@BLA-INC.COM</u>
7. <u>WADE THOMPSON</u>	<u>BIG ROCK DRAINAGE</u>	<u>630 201 7987</u>	<u>brhighwaydept@att.net</u>
8. <u>DAVID HALL</u>	<u>"</u>	<u>630-438-5439</u>	<u>DKHALL@HYTECHLABS.COM</u>
9. <u>Phyllis BLECK</u>	<u>"</u>	<u>630-373-9422</u>	<u>p.bleck2@att.net</u>
10. <u>Tim McCANNON</u>	<u>Village of Big Rock</u>	<u>630-556-3100</u>	<u>e-r-s@mcchsi.com</u>
11. <u>Chad Thomas</u>	<u>KDOT</u>		<u>thomascannon@kdot.kanl.il.us</u>
12. <u>Jodie Wollard</u>	<u>Kone Co. Water Res.</u>	<u>630-833-3499</u>	<u>wollardjodie@co-kone.il.us</u>
13. <u>Mike ZAKAREK</u>	<u>KDOT</u>	<u>630-584-1170</u>	<u>ZAKAREK@KDOT.KONE.IL.US</u>



Illinois Department
of Transportation

ATTENDANCE ROSTER
BUREAU OF PROGRAMMING

PROJECT/TOPIC: US30 @ Danbarn Rd. & Local Drainage Concerns

DATE: June 7, 2018

TIME: 11:00 am

LOCATION: IDOT Dist. 1

ROOM: Proc Conf. Room

Attendees	Representing	Phone Number	Email Address
1. PERRY MASOVIDIS	IDOT/PROG/HND	847-705-4474	eleftherios.masovidis@illinois.gov
2. KEN ROSEK	BIG ROCK TOWNSHIP	630-556-4340	BIGROCKTOWNSHIP@GMAIL.COM
3. Jerald Thompson	Big Rock Landowner	630-856-3400	thompsonjerald@att.net
4.			
5.			
6.			
7.			
8.			
9.			
10.			
11.			
12.			
13.			

Dauberman Road Extension
Kane County

Utility Summary Form

Engineer:

Updated: 6/1/2016

ID No.	Utility Company/Agency	Letter Sent & Date		Response Received		
				(Yes/No)	Date	Information Received
1	ANR Pipeline Company	Phase 1	27-Apr-16	No		Sent email 7/28 as follow up
	Mr. Dave Huebner	Phase 2				
		Preliminary				
		Prefinal				
2	ATT Distribution	Phase 1	20-Apr-16	Yes	5-May-16	Letter & Atlas
	Ms. Janet C. Ahern	Phase 2				
	630-573-6414	Preliminary				
		Prefinal				
3	ComEd	Phase 1	20-Apr-16	Yes	11-May-16	Letter & Atlas
	Mr. Tim Tamason	Phase 2				
	815-490-2335	Preliminary				
		Prefinal				
4	DeKalb Fiber Optic, LLC	Phase 1	20-Apr-16	Yes	20-Apr-16	Letter & Google Earth
	Mr. Roger Engle	Phase 2				
	815-991-2450	Preliminary				
		Prefinal				
5	Kane County DOT (Client)	Phase 1		Yes	21-Apr-16	Letter
		Phase 2				
		Preliminary				
		Prefinal				
6	Enbridge Energy Partners	Phase 1	20-Apr-16	Yes	21-May-16	Letter (& Ground Markings?)
	Mr. David Schultz	Phase 2				
	608-756-0071	Preliminary				
		Prefinal				
7	Nicor Gas	Phase 1	20-Apr-16	Yes	16-May-16	Letter & Atlas
	Mr. Bruce Koppang	Phase 2				
	630-388-32362	Preliminary				
		Prefinal				
8	MediaCom	Phase 1	28-Jul-16	No		
	Mr. Patrick McGraw	Phase 2				
	815-597-5103	Preliminary				
	815-716-0582 (cell)	Prefinal				
9	USIC Locating Services	Phase 1				
		Phase 2				
		Preliminary				
		Prefinal				

**US Route 30 & Jughandle Connector Road
2020 Warrant Analysis**

Warrants Summary												
Information												
Analyst	BJH					Intersection	US Route 30 - Jughandle					
Agency/Co	Transystems					Jurisdiction						
Date Performed	7/14/2017					Units	U.S. Customary					
Project ID						Time Period Analyzed	AM&PM Peaks					
East/West Street	US Route 30					North/South Street	Jughandle					
File Name	SE_Warrants.xhy					Major Street	East-West					
Project Description												
General							Roadway Network					
Major Street Speed (mph)	55	<input type="checkbox"/>	Population < 10,000				Two Major Routes			<input type="checkbox"/>		
Nearest Signal (ft)	0	<input type="checkbox"/>	Coordinated Signal System				Weekend Count			<input type="checkbox"/>		
Crashes (per year)	0	<input type="checkbox"/>	Adequate Trials of Alternatives				5-yr Growth Factor			0		
Geometry and Traffic	EB			WB			NB			SB		
	LT	TH	RT	LT	TH	RT	LT	TH	RT	LT	TH	RT
Number of lanes, N	1	1	0	0	1	1	0	0	0	1	0	1
Lane usage	L	T			T	R				L		R
Vehicle Volume Averages (vph)	7	42	0	0	36	37	0	0	0	35	0	7
Peds (ped/h) / Gaps (gaps/h)	--	0 / 0	--	--	0 / 0	--	--	0 / 0	--	--	0 / 0	--
Delay (s/veh) / (veh-hr)	--	0 / 0	--	--	0 / 0	--	--	0 / 0	--	--	0 / 0	--
Warrant 1: Eight-Hour Vehicular Volume												<input type="checkbox"/>
1 A. Minimum Vehicular Volumes (Both major approaches --and-- higher minor approach) --or--												<input type="checkbox"/>
1 B. Interruption of Continuous Traffic (Both major approaches --and-- higher minor approach) --or--												<input type="checkbox"/>
1 (56%) Vehicular --and-- Interruption Volumes (Both major approaches --and-- higher minor approach)												<input type="checkbox"/>
Warrant 2: Four-Hour Vehicular Volume												<input type="checkbox"/>
2 A. Four-Hour Vehicular Volumes (Both major approaches --and-- higher minor approach)												<input type="checkbox"/>
Warrant 3: Peak Hour												<input checked="" type="checkbox"/>
3 A. Peak-Hour Conditions (Minor delay --and-- minor volume --and-- total volume) --or--												<input type="checkbox"/>
3 B. Peak- Hour Vehicular Volumes (Both major approaches --and-- higher minor approach)												<input checked="" type="checkbox"/>
Warrant 4: Pedestrian Volume												<input type="checkbox"/>
4 A. Four Hour Volumes --or--												<input type="checkbox"/>
4 B. One-Hour Volumes												<input type="checkbox"/>
Warrant 5: School Crossing												<input type="checkbox"/>
5. Student Volumes --and--												<input type="checkbox"/>
5. Gaps Same Period												<input type="checkbox"/>
Warrant 6: Coordinated Signal System												<input type="checkbox"/>
6. Degree of Platooning (Predominant direction or both directions)												<input type="checkbox"/>
Warrant 7: Crash Experience												<input type="checkbox"/>
7 A. Adequate trials of alternatives, observance and enforcement failed --and--												<input type="checkbox"/>
7 B. Reported crashes susceptible to correction by signal (12-month period) --and--												<input type="checkbox"/>
7 C. (56%) Volumes for Warrants 1A, 1B --or-- 4 are satisfied												<input type="checkbox"/>

**US Route 30 & Jughandle Connector Road
2020 Warrant Analysis**

Warrant 8: Roadway Network	<input type="checkbox"/>
8 A. Weekday Volume (Peak hour total --and-- projected warrants 1, 2 or 3) --or--	<input type="checkbox"/>
8 B. Weekend Volume (Five hours total)	<input type="checkbox"/>
Warrant 9: Grade Crossing	<input type="checkbox"/>
9 A. Grade Crossing within 140 ft --and--	<input type="checkbox"/>
9 B. Peak-Hour Vehicular Volumes	<input type="checkbox"/>

US Route 30 & Jughandle Connector Road 2020 Warrant Analysis

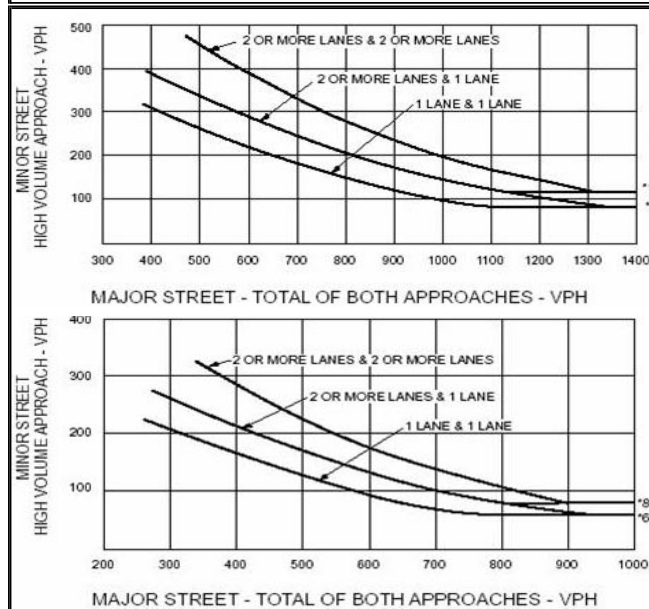
Warrants Volume			
Information			
Analyst	BJH	Intersection	US Route 30&DaubermanJughandle
Agency/Co	Transystems	Jurisdiction	
Date Performed	7/14/2017	Units	U.S. Customary
Project ID		Time Period Analyzed	AM&PM Peaks
East/West Street	US Route 30	North/South Street	Jughandle
File Name	01-US30Jug-2020-00Warrant.xhy	Major Street	East-West
Project Description			

Warrant 1

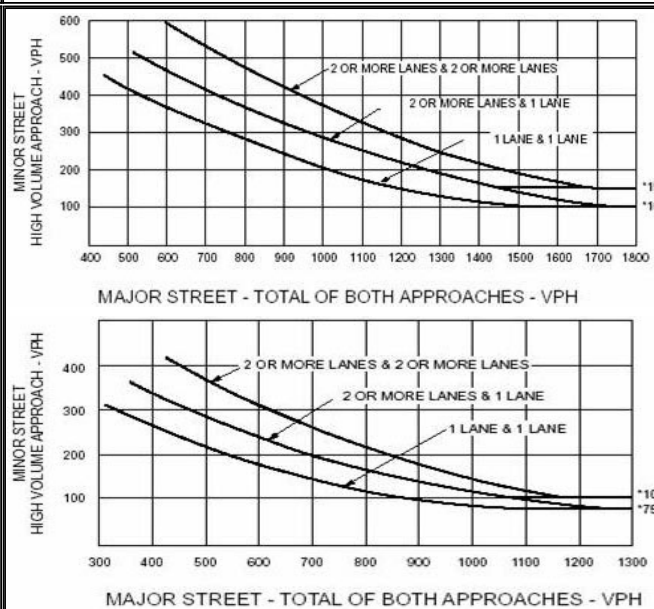
Condition A—Minimum Vehicular Volume									
Number of lanes for moving traffic on each approach		Vehicles per hour on major street (total of both approaches)				Vehicles per hour on higher-volume minor-street approach (one direction only)			
Major Street	Minor Street	100%	80%	70%	56%	100%	80%	70%	56%
1	1	500	400	350	280	150	120	105	84
2 or more	1	600	480	420	336	150	120	105	84
2 or more	2 or more	600	480	420	336	200	160	140	112
1	2 or more	500	400	350	280	200	160	140	112

Condition B—Interruption of Continuous Traffic									
Number of lanes for moving traffic on each approach		Vehicles per hour on major street (total of both approaches)				Vehicles per hour on higher-volume minor-street approach (one direction only)			
Major Street	Minor Street	100%	80%	70%	56%	100%	80%	70%	56%
1	1	750	600	525	420	75	60	53	42
2 or more	1	900	720	630	504	75	60	53	42
2 or more	2 or more	900	720	630	504	100	80	70	56
1	2 or more	750	600	525	420	100	80	70	56

Warrant 2



Warrant 3



Volume Summary

Major Street Lanes 2+		Minor Street Lanes 2+		Speed		Population		10000+		
Hours	Major Volume	Minor Volume	Total Volume	1A (70%)	1A (56%)	1B (70%)	1B (56%)	2 (70%)	3A (70%)	3B (70%)
07-08	595	345	940	Yes	Yes	No	Yes	Yes	No	Yes
08-09	0	0	0	No	No	No	No	No	No	No
09-10	0	0	0	No	No	No	No	No	No	No
10-11	0	0	0	No	No	No	No	No	No	No
11-12	0	0	0	No	No	No	No	No	No	No
12-13	0	0	0	No	No	No	No	No	No	No
13-14	0	0	0	No	No	No	No	No	No	No
14-15	0	0	0	No	No	No	No	No	No	No
15-16	0	0	0	No	No	No	No	No	No	No
16-17	0	0	0	No	No	No	No	No	No	No
17-18	890	165	1055	Yes	Yes	Yes	Yes	Yes	No	No
18-19	0	0	0	No	No	No	No	No	No	No
Totals	1485	510	1995	2	2	1	2	2	0	1

Dauberman Road & Jughandle Connector Road
2020 Warrant Analysis

Warrants Summary													
Information													
Analyst	BJH					Intersection	Dauberman-Jughandle						
Agency/Co						Jurisdiction							
Date Performed	7/14/2017					Units	U.S. Customary						
Project ID						Time Period Analyzed	AM&PM Peaks						
East/West Street	Jughandle					North/South Street	Dauberman Road						
File Name	NW_Warrants.xhy					Major Street	North-South						
Project Description													
General							Roadway Network						
Major Street Speed (mph)	55	<input type="checkbox"/>	Population < 10,000				Two Major Routes				<input type="checkbox"/>		
Nearest Signal (ft)	0	<input type="checkbox"/>	Coordinated Signal System				Weekend Count				<input type="checkbox"/>		
Crashes (per year)	0	<input type="checkbox"/>	Adequate Trials of Alternatives				5-yr Growth Factor				0		
Geometry and Traffic	EB			WB			NB			SB			
	LT	TH	RT	LT	TH	RT	LT	TH	RT	LT	TH	RT	
Number of lanes, N	0	0	0	1	0	1	0	1	1	1	1	0	
Lane usage				L		R		T	R	L	T		
Vehicle Volume Averages (vph)	10	56	0	37	47	7	0	2	35	7	2	33	
Peds (ped/h) / Gaps (gaps/h)	--	0 / 0	--	--	0 / 0	--	--	0 / 0	--	--	0 / 0	--	
Delay (s/veh) / (veh-hr)	--	0 / 0	--	--	0 / 0	--	--	0 / 0	--	--	0 / 0	--	
Warrant 1: Eight-Hour Vehicular Volume												<input type="checkbox"/>	
1 A. Minimum Vehicular Volumes (Both major approaches --and-- higher minor approach) --or--												<input type="checkbox"/>	
1 B. Interruption of Continuous Traffic (Both major approaches --and-- higher minor approach) --or--												<input type="checkbox"/>	
1 (56%) Vehicular --and-- Interruption Volumes (Both major approaches --and-- higher minor approach)												<input type="checkbox"/>	
Warrant 2: Four-Hour Vehicular Volume												<input type="checkbox"/>	
2 A. Four-Hour Vehicular Volumes (Both major approaches --and-- higher minor approach)												<input type="checkbox"/>	
Warrant 3: Peak Hour												<input type="checkbox"/>	
3 A. Peak-Hour Conditions (Minor delay --and-- minor volume --and-- total volume) --or--												<input type="checkbox"/>	
3 B. Peak- Hour Vehicular Volumes (Both major approaches --and-- higher minor approach)												<input type="checkbox"/>	
Warrant 4: Pedestrian Volume												<input type="checkbox"/>	
4 A. Four Hour Volumes --or--												<input type="checkbox"/>	
4 B. One-Hour Volumes												<input type="checkbox"/>	
Warrant 5: School Crossing												<input type="checkbox"/>	
5. Student Volumes --and--												<input type="checkbox"/>	
5. Gaps Same Period												<input type="checkbox"/>	
Warrant 6: Coordinated Signal System												<input type="checkbox"/>	
6. Degree of Platooning (Predominant direction or both directions)												<input type="checkbox"/>	
Warrant 7: Crash Experience												<input type="checkbox"/>	
7 A. Adequate trials of alternatives, observance and enforcement failed --and--												<input type="checkbox"/>	
7 B. Reported crashes susceptible to correction by signal (12-month period) --and--												<input type="checkbox"/>	
7 C. (56%) Volumes for Warrants 1A, 1B --or-- 4 are satisfied												<input type="checkbox"/>	

**Dauberman Road & Jughandle Connector Road
2020 Warrant Analysis**

Warrant 8: Roadway Network	<input type="checkbox"/>
8 A. Weekday Volume (Peak hour total --and-- projected warrants 1, 2 or 3) --or--	<input type="checkbox"/>
8 B. Weekend Volume (Five hours total)	<input type="checkbox"/>
Warrant 9: Grade Crossing	<input type="checkbox"/>
9 A. Grade Crossing within 140 ft --and--	<input type="checkbox"/>
9 B. Peak-Hour Vehicular Volumes	<input type="checkbox"/>

Dauberman Road & Jughandle Connector Road 2020 Warrant Analysis

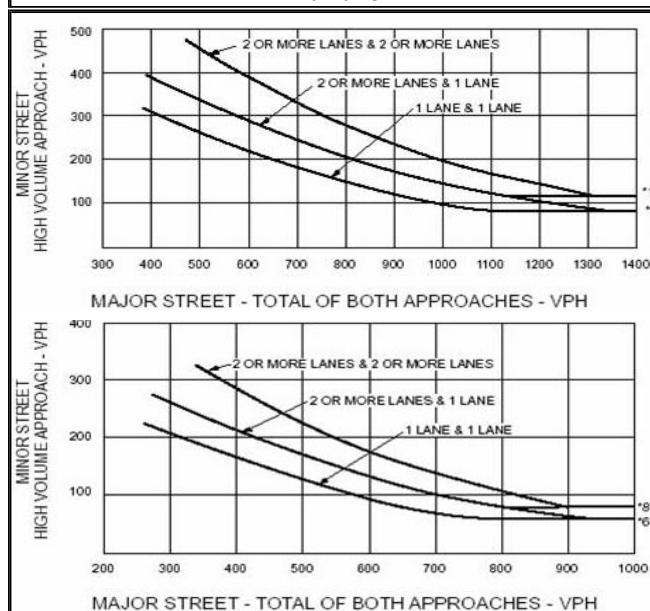
Warrants Volume			
Information			
Analyst	BJH	Intersection	Dauberman-Jughandle
Agency/Co		Jurisdiction	
Date Performed	7/14/2017	Units	U.S. Customary
Project ID		Time Period Analyzed	AM&PM Peaks
East/West Street	Jughandle	North/South Street	Dauberman Road
File Name	02-DaubJug-2020-00Warrant.xhy	Major Street	North-South
Project Description			

Warrant 1

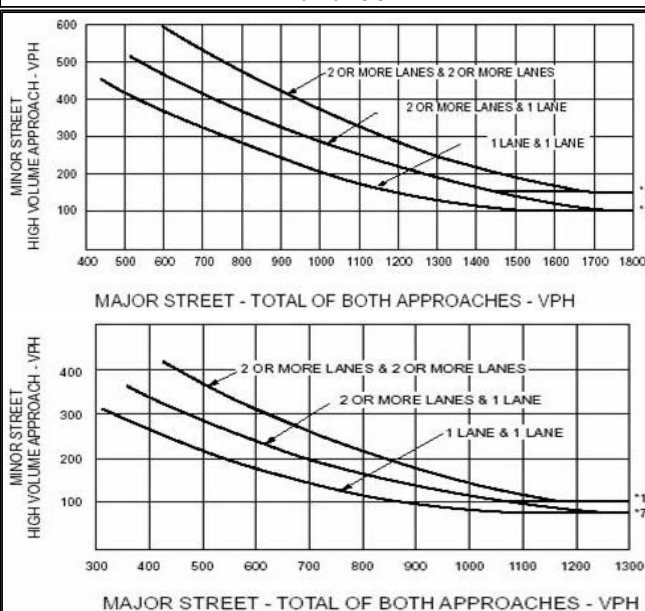
Condition A—Minimum Vehicular Volume									
Number of lanes for moving traffic on each approach		Vehicles per hour on major street (total of both approaches)				Vehicles per hour on higher-volume minor-street approach (one direction only)			
Major Street	Minor Street	100%	80%	70%	56%	100%	80%	70%	56%
1	1	500	400	350	280	150	120	105	84
2 or more	1	600	480	420	336	150	120	105	84
2 or more	2 or more	600	480	420	336	200	160	140	112
1	2 or more	500	400	350	280	200	160	140	112

Condition B—Interruption of Continuous Traffic									
Number of lanes for moving traffic on each approach		Vehicles per hour on major street (total of both approaches)				Vehicles per hour on higher-volume minor-street approach (one direction only)			
Major Street	Minor Street	100%	80%	70%	56%	100%	80%	70%	56%
1	1	750	600	525	420	75	60	53	42
2 or more	1	900	720	630	504	75	60	53	42
2 or more	2 or more	900	720	630	504	100	80	70	56
1	2 or more	750	600	525	420	100	80	70	56

Warrant 2



Warrant 3



Volume Summary

Major Street Lanes 2+		Minor Street Lanes 2+		Speed		55		Population		10000+
Hours	Major Volume	Minor Volume	Total Volume	1A (70%)	1A (56%)	1B (70%)	1B (56%)	2 (70%)	3A (70%)	3B (70%)
07-08	355	160	515	No	Yes	No	No	No	No	No
08-09	0	0	0	No	No	No	No	No	No	No
09-10	0	0	0	No	No	No	No	No	No	No
10-11	0	0	0	No	No	No	No	No	No	No
11-12	0	0	0	No	No	No	No	No	No	No
12-13	0	0	0	No	No	No	No	No	No	No
13-14	0	0	0	No	No	No	No	No	No	No
14-15	0	0	0	No	No	No	No	No	No	No
15-16	0	0	0	No	No	No	No	No	No	No
16-17	0	0	0	No	No	No	No	No	No	No
17-18	200	385	585	No	No	No	No	No	No	No
18-19	0	0	0	No	No	No	No	No	No	No
Totals	555	545	1100	0	1	0	0	0	0	0

Granart Road & Dauberman Road Extension 2020 Warrant Analysis

Warrants Summary												
Information												
Analyst BJH Agency/Co TranSystems Date 7/14/2017 Performed Project ID East/West Granart Road Street Granart Road File Name GranartDauberman_2020_00Warrant.xhy						Intersection Dauberman-Granart Jurisdiction KDOT Units U.S. Customary Time Period Analyzed AM&PM Peak North/South Street Dauberman Road Major Street North-South						
Project Description												
General							Roadway Network					
Major Street Speed (mph)	55	<input type="checkbox"/>	Population < 10,000				Two Major Routes			<input type="checkbox"/>		
Nearest Signal (ft)	0	<input type="checkbox"/>	Coordinated Signal System				Weekend Count			<input type="checkbox"/>		
Crashes (per year)	0	<input type="checkbox"/>	Adequate Trials of Alternatives				5-yr Growth Factor			0		
Geometry and Traffic	EB			WB			NB			SB		
	LT	TH	RT	LT	TH	RT	LT	TH	RT	LT	TH	RT
Number of lanes, N	0	0	0	1	0	1	0	1	1	1	1	0
Lane usage				L		R		T	R	L	T	
Vehicle Volume Averages (vph)	10	56	0	7	47	2	0	35	14	2	36	33
Peds (ped/h) / Gaps (gaps/h)	--	0 / 0	--	--	0 / 0	--	--	0 / 0	--	--	0 / 0	--
Delay (s/veh) / (veh-hr)	--	0 / 0	--	--	0 / 0	--	--	0 / 0	--	--	0 / 0	--
Warrant 1: Eight-Hour Vehicular Volume												<input type="checkbox"/>
1 A. Minimum Vehicular Volumes (Both major approaches --and-- higher minor approach) --or--												<input type="checkbox"/>
1 B. Interruption of Continuous Traffic (Both major approaches --and-- higher minor approach) --or--												<input type="checkbox"/>
1 (56%) Vehicular --and-- Interruption Volumes (Both major approaches --and-- higher minor approach)												<input type="checkbox"/>
Warrant 2: Four-Hour Vehicular Volume												<input type="checkbox"/>
2 A. Four-Hour Vehicular Volumes (Both major approaches --and-- higher minor approach)												<input type="checkbox"/>
Warrant 3: Peak Hour												<input type="checkbox"/>
3 A. Peak-Hour Conditions (Minor delay --and-- minor volume --and-- total volume) --or--												<input type="checkbox"/>
3 B. Peak- Hour Vehicular Volumes (Both major approaches --and-- higher minor approach)												<input type="checkbox"/>
Warrant 4: Pedestrian Volume												<input type="checkbox"/>
4 A. Four Hour Volumes --or--												<input type="checkbox"/>
4 B. One-Hour Volumes												<input type="checkbox"/>
Warrant 5: School Crossing												<input type="checkbox"/>
5. Student Volumes --and--												<input type="checkbox"/>
5. Gaps Same Period												<input type="checkbox"/>
Warrant 6: Coordinated Signal System												<input type="checkbox"/>
6. Degree of Platooning (Predominant direction or both directions)												<input type="checkbox"/>
Warrant 7: Crash Experience												<input type="checkbox"/>
7 A. Adequate trials of alternatives, observance and enforcement failed --and--												<input type="checkbox"/>
7 B. Reported crashes susceptible to correction by signal (12-month period) --and--												<input type="checkbox"/>

**Granart Road & Dauberman Road Extension
2020 Warrant Analysis**

7 C. (56%) Volumes for Warrants 1A, 1B --or-- 4 are satisfied	<input type="checkbox"/>
Warrant 8: Roadway Network	<input type="checkbox"/>
8 A. Weekday Volume (Peak hour total --and-- projected warrants 1, 2 or 3) --or--	<input type="checkbox"/>
8 B. Weekend Volume (Five hours total)	<input type="checkbox"/>
Warrant 9: Grade Crossing	<input type="checkbox"/>
9 A. Grade Crossing within 140 ft --and--	<input type="checkbox"/>
9 B. Peak-Hour Vehicular Volumes	<input type="checkbox"/>

Granart Road & Dauberman Road Extension 2020 Warrant Analysis

Warrants Volume

Information

Analyst	BJH	Intersection	Dauberman-Granart
Agency/Co	TranSystems	Jurisdiction	KDOT
Date Performed	7/14/2017	Units	U.S. Customary
Project ID		Time Period Analyzed	AM&PM Peak
East/West Street	Granart Road	North/South Street	Dauberman Road
File Name	03-GranDaub-2020-00Warrant.xhy	Major Street	North-South

Project Description

Warrant 1

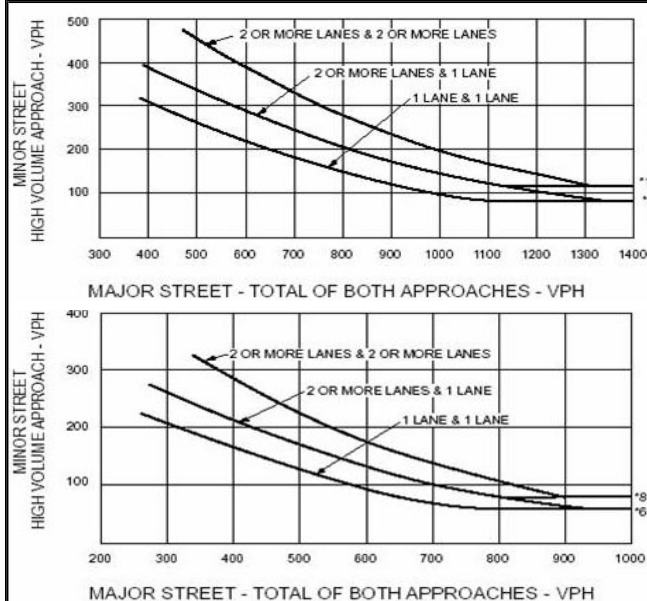
Condition A—Minimum Vehicular Volume

Number of lanes for moving traffic on each approach		Vehicles per hour on major street (total of both approaches)				Vehicles per hour on higher-volume minor-street approach (one direction only)			
Major Street	Minor Street	100%	80%	70%	56%	100%	80%	70%	56%
1	1	500	400	350	280	150	120	105	84
2 or more	1	600	480	420	336	150	120	105	84
2 or more	2 or more	600	480	420	336	200	160	140	112
1	2 or more	500	400	350	280	200	160	140	112

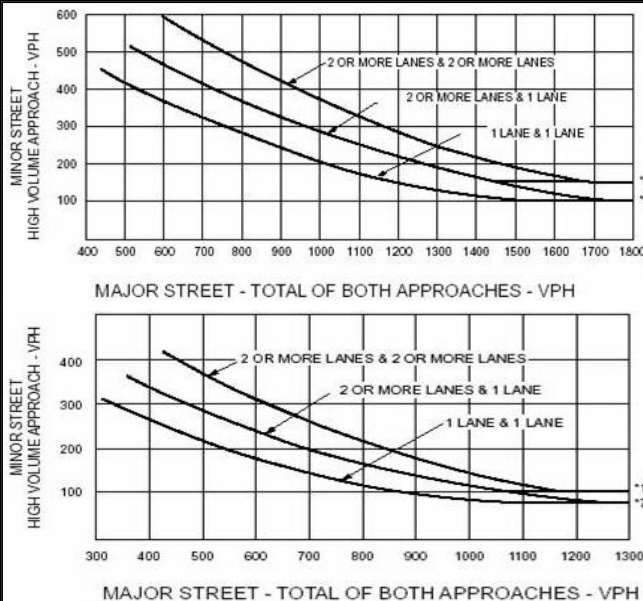
Condition B—Interruption of Continuous Traffic

Number of lanes for moving traffic on each approach		Vehicles per hour on major street (total of both approaches)				Vehicles per hour on higher-volume minor-street approach (one direction only)			
Major Street	Minor Street	100%	80%	70%	56%	100%	80%	70%	56%
1	1	750	600	525	420	75	60	53	42
2 or more	1	900	720	630	504	75	60	53	42
2 or more	2 or more	900	720	630	504	100	80	70	56
1	2 or more	750	600	525	420	100	80	70	56

Warrant 2



Warrant 3



Volume Summary

Major Street Lanes 2+		Minor Street Lanes 2+		Speed		Population		10000+		
Hours	Major Volume	Minor Volume	Total Volume	1A (70%)	1A (56%)	1B (70%)	1B (56%)	2 (70%)	3A (70%)	3B (70%)
07-08	555	35	590	No	No	No	No	No	No	No
08-09	0	0	0	No	No	No	No	No	No	No
09-10	0	0	0	No	No	No	No	No	No	No
10-11	0	0	0	No	No	No	No	No	No	No
11-12	0	0	0	No	No	No	No	No	No	No
12-13	0	0	0	No	No	No	No	No	No	No
13-14	0	0	0	No	No	No	No	No	No	No
14-15	0	0	0	No	No	No	No	No	No	No
15-16	0	0	0	No	No	No	No	No	No	No
16-17	0	0	0	No	No	No	No	No	No	No
17-18	510	90	600	No	No	No	Yes	No	No	No
18-19	0	0	0	No	No	No	No	No	No	No
Totals	1065	125	1190	0	0	0	1	0	0	0